



CPED STAFF REPORT

Prepared for the City Planning Commission

CPC Agenda Item #11
 August 11, 2014
 BZZ-6675

LAND USE APPLICATION SUMMARY

Property Location: 2004, 2008, 2012, 2014, and 2018 Lyndale Avenue South
Project Name: Theatre Garage and Marquee Apartments
Prepared By: [Mei-Ling Anderson](#), City Planner, (612) 673-5342
Applicant: TGMA Developers, LLC
Project Contact: Elizabeth Liebhard, Master Properties MN
Request: To construct a new, six-story mixed-use building with 82 dwelling units.
Required Applications:

Rezoning	Petition to rezone the properties located at 2008, 2012, 2014, and 2018 from the C1 Neighborhood Commercial District to the C2 Neighborhood Corridor Commercial District.
Conditional Use Permit	To increase the maximum allowed height in the C2 Neighborhood Corridor Commercial District from 4 stories/56 feet to 6 stories/73 feet-6 inches.
Variance	To reduce the minimum rear yard setback from 15 feet to 3.5 feet.
Variance	To reduce the south interior side yard setback from 15 feet to 5 feet.
Site Plan Review	For a new, six-story, mixed-use building with 82 dwelling units.

SITE DATA

Existing Zoning	2004 Lyndale Avenue South: C2 Neighborhood Corridor Commercial District 2008, 2012, 2014, 2018 Lyndale Avenue South: C1 Neighborhood Commercial District
Lot Area	35,928 square feet / 0.82 acres
Ward(s)	10
Neighborhood(s)	Lowry Hill East Neighborhood Association; adjacent to Whittier Alliance, Stevens Square Community Organization
Designated Future Land Use	Mixed Use
Land Use Features	Commercial Corridor (Lyndale Avenue South); Community Corridor (West Franklin Avenue)
Small Area Plan(s)	Lyn-Lake Small Area Plan

Date Application Deemed Complete	July 18, 2014	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	September 16, 2014	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject site contains five parcels totaling approximately 34,928 square feet/0.82 acres. The site is located at the southwest corner of the intersection of Lyndale Avenue South and West Franklin Avenue, just south of Interstate 94. The five parcels currently contain two one-story commercial buildings and two surface parking lots with a total of 66 stalls. The northernmost building was constructed in 1923 as an automotive sales and showroom building, and is now occupied by a theater, thrift store, and barber and beauty shop. The building on the southern portion of the site was constructed in 1958 and now contains a dry cleaner and tailors shop and art gallery. The site is generally flat, however, the north side of the site along Franklin has a grade increase between Lyndale Avenue South and the west side of the property. The properties adjacent to the west side of the site are also situated between seven and ten feet above the grade of the subject site.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. There are commercial uses on all four corners of the intersection and along the Lyndale Avenue South corridor. The southeast corner of the intersection contains a bar and restaurant, the northeast corner contains a restaurant, and the northwest corner contains a vision loss resource center. Most of the commercial buildings are between one and two stories. There are multi-family housing residential buildings directly to the west of the site, which are generally between three and four stories tall. The greater surrounding neighborhood contains both low and high-density residential buildings, a grocery co-operative, restaurants, bars, cafés, a gas-station, and a variety of retail uses. The intersection receives a significant amount of traffic from vehicles, pedestrians, cyclists, and busses.

PROJECT DESCRIPTION. The applicant is proposing to construct a new, mixed use building with 82 residential units, a 150-seat theater (approximately 3,600 square feet), ground-level retail along Lyndale (approximately 3,800 square feet), and a restaurant occupying the northeast corner of the parcel (approximately 7,900 square feet). The building frontage along Lyndale would total five stories, while the restaurant and residential uses along Franklin would total six stories. The applicant is also proposing to construct a four-story parking garage to the rear of the site to hold the development's off-street accessory parking. The garage would contain 163 vehicle parking spaces and could be accessed off of both West Franklin Avenue and Lyndale Avenue South. The applicant has also indicated they would be providing a total of 132 bicycle parking spaces for the residents and commercial uses.

The applicant is requesting to rezone the four properties located at 2008-2018 Lyndale Avenue South from the C1 Neighborhood Commercial District to the C2 Neighborhood Corridor Commercial District. The property located at 2004 Lyndale Avenue South is currently in the C2 zoning district. As the zoning code does not allow developments to belong to more than one zoning classification, the applicant is requesting to rezone four of the five parcels to C2 district so that the entire site belongs to the same zoning district.

While the majority of the buildings along Lyndale Avenue South would be five stories, or approximately 61 ½ feet, the development would total 73 feet-6 inches in height at its tallest point near the intersection. A conditional use permit is requested to increase the maximum allowed height in the C2 district from 4 stories/56 feet to 6 stories/73 feet.

Two setback variances are requested as part of the application. The rear and south sides of the property are each subject to a minimum yard requirement of 15 feet. The applicant is requesting to reduce both the rear and south interior side yard setbacks to three and five feet, respectively.

Site plan review is required for any new mixed use building or any building containing five or more new or additional dwelling units.

The applicant has completed a Travel Demand Management Plan, which is included in the attachments.

PUBLIC COMMENTS. As of the printing of this report, staff has received 12 comments, 11 of which express opposition to the proposed project. The Lowry Hill East Neighborhood Association has not formally commented on the project. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

REZONING

The Department of Community Planning and Economic Development has analyzed the application for a petition to rezone the properties located at 2008, 2012, 2014, and 2018 from the C1 Neighborhood Commercial District to the C2 Neighborhood Corridor Commercial District based on the following [findings](#):

- I. *Whether the amendment is consistent with the applicable policies of the comprehensive plan.*

The proposed zoning would be consistent with the applicable policies of *The Minneapolis Plan for Sustainable Growth*. The property is designated as Mixed Use on the future land use map. There is no requirement that every building in a Mixed Use area be mixed use, but the designation allows for mixed use developments, such as residential uses mixed with office or retail.

The site is also located along two land use features: Lyndale Avenue South is a designated Commercial Corridor and West Franklin Avenue is a Community Corridor. Commercial Corridors like Lyndale Avenue South have historically been prominent destinations with a mix of uses, with highest density residential uses (50 to 120 dwelling units per acre) along the corridor and medium density uses in surrounding areas. Community Corridors, such as West Franklin Avenue, are primarily residential with intermittent commercial uses clustered at intersections.

The following principles and policies outlined in the plan apply to this proposal:

Land Use Policy 1.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

- I.4.1 Support a variety of commercial districts and corridors of varying size, intensity of development, mix of uses, and market served.

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

- I.5.1 Support an appropriate mix of uses within a district or corridor with attention to surrounding uses, community needs and preferences, and availability of public facilities.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- I.10.1 Support a mix of uses – such as retail sales, office, institutional, high-density residential and clean low-impact light industrial – where compatible with the existing and desired character.

The remainder of the site is currently zoned C2. The C2 district allows a mix of uses, both commercial and residential. The extension of the commercial zoning as proposed should not significantly affect adjacent residential properties.

2. *Whether the amendment is in the public interest and is not solely for the interest of a single property owner.*

The site currently has split zoning. The rezoning will result in uniform zoning of the site and would allow for a wider range of goods and services along a Community Corridor and Commercial Corridor, as called for in adopted City policies. The amendment is in the public interest and not solely in the interest of the property owner.

3. *Whether the existing uses of property and the zoning classification of property within the general area of the property in question are compatible with the proposed zoning classification, where the amendment is to change the zoning classification of particular property.*

The proposed zoning classification would be compatible with those of other properties in the area. The parcels at the northwest, northeast, and southwest portions of the intersection are all zoned C2, and the C2 classification extends to both the north and south of the intersection along Lyndale Avenue South. The four parcels that the applicant is requesting to rezone from C1 to C2 would be directly adjacent and to the south of 2004 Lyndale Avenue South, which is zoned C2. The existing uses and zoning classification of adjacent and nearby properties are consistent with the C2 zoning district that is proposed for the four parcels in question.

4. *Whether there are reasonable uses of the property in question permitted under the existing zoning classification, where the amendment is to change the zoning classification of particular property.*

Because the site has split zoning, the most restrictive provisions of the base zoning districts apply. The C2 district allows four stories by-right instead of the three stories allowed in the C1 district, as well as larger retail spaces compared to the C1 zoning district. Because the site is located on a Commercial Corridor in a mixed use area, allowing a wider range of available goods and services and more dense residential development is appropriate.

The total development site is 35,928 square feet. The portion of the site that is located in the C2 district is approximately 14,300 square feet. If the rezoning petition were not granted, the northernmost parcel would be limited to a development that would fit on a 14,300 square feet and subject to setbacks for any walls with residential windows facing the interior or rear property lines, while meeting the objectives for a property zoned C2 along a Commercial and Community Corridor. The remaining 21,600 square feet of the subject site – the four parcels located in the C1 district – would have to be developed separately from the parcel at the corner and would also subject to the setbacks and height for the C1 zoning district. Those factors limit the reasonable use of the property under the current zoning. Allowing the four properties to belong to the same zoning classification would make it possible for future developments on the properties to be constructed in an efficient, coherent manner to emphasize the corner of Franklin and Lyndale and establish a strong, pedestrian-oriented façade along the two intersecting corridors.

5. *Whether there has been a change in the character or trend of development in the general area of the property in question, which has taken place since such property was placed in its present zoning classification, where the amendment is to change the zoning classification of particular property.*

The five parcels on the subject site have been zoned C1 and C2 since 1999. Prior to that time, when a new zoning code was adopted, all five parcels belonged to the B3S2 Community Service District for at least 30 years, which is comparable to today's C2 zoning district. The proposed zoning would be consistent with the historical zoning classification for this corridor as well as with the more recent mixed use development that has occurred in the surrounding area.

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to increase the maximum allowed height in the C2 Neighborhood Corridor Commercial District from 4 stories/56 feet to 6 stories/73 feet-6 inches based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The maximum allowed height of a building in the C2 Neighborhood Commercial District is 4 stories or 56 feet, whichever is less. The majority of the building along Lyndale Avenue South would be 5 stories, or 61 ½ feet tall. The proposed building would not be detrimental to or endanger the public health, safety, comfort or general welfare.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The surrounding area contains a range of building heights. The residential buildings directly to the west have three and four stories and are situated on a grade that is between approximately seven and ten feet above that of the subject site. The general surrounding area contains one and two-story commercial buildings and medium-density residential buildings with three or four stories.

The zoning classifications in the surrounding area support as-of-right building heights of four (C2 and OR2) and six stories (R6). The C2 zoning district applies to approximately ten properties at or near the intersection of Lyndale Avenue South and West Franklin Avenue. The OR2 and R6 zoning districts are located just to the east and west of the intersection of Lyndale Avenue South and West Franklin Avenue. As the measurement of building height in the zoning code is based on the adjacent natural grade rather than actual sea elevation, the varied topography of the area plays an important role in the impact that a building's height has in conjunction with the structure's bulk and height as measured by the zoning code. If all of the parcels located in the surrounding area were redeveloped up to their permitted building heights, the proposed development would be at or below the relative height of the properties in the area since the intersection is located in a valley.

For these reasons, CPED does not find that increasing the height of the building would be injurious to the use and enjoyment of other property in the vicinity and would not impede the normal or orderly development and improvement of surrounding property for uses permitted in the district.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Increasing the height of the proposed building will not have an impact on utilities, access roads, or drainage. The applicant has prepared a comprehensive utility and drainage plan for the subject site.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Increasing the height of the proposed building will have no impact on traffic congestion in the public streets. The number of units proposed for the development is permitted and the TDMP for the project found that all four intersections included in the study area would operate acceptably at Level of Service C or better in the weekday a.m. and p.m. peak hours. In addition, the TDMP found that the site is providing adequate on-site parking. The development would also reduce the number of existing curb cuts from four to two, and would reconstruct the curb alignment along the northern part of Lyndale Avenue South to reduce the width of the pedestrian crossing across Lyndale and widening the sidewalk adjacent to the site.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

In addition to the comprehensive plan policies listed elsewhere in this report, the proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

- 1.5.1 Support an appropriate mix of uses within a district or corridor with attention to surrounding uses, community needs and preferences, and availability of public facilities.
- 1.5.2 Facilitate the redevelopment of underutilized commercial areas by evaluating possible land use changes against potential impacts on the surrounding neighborhood.

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

- 1.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.

The site currently contains two one-story commercial buildings and two surface parking areas. The site is designated as Mixed Use in the future land use map and is located on two land use features: Lyndale Avenue South, a Commercial Corridor, and a West Franklin Avenue, a Community Corridor. The comprehensive plan calls for higher-density developments along designated land use features, and the proposed development would be consistent with the comprehensive plan by providing 82 dwelling units with ground-level commercial uses.

In addition, the applicant has designed the proposed building in such a way that the building would be stepped down in height for the majority of the Lyndale Avenue South-facing façade from six stories to five stories in order to offset the taller height of the building near the northeast corner of the site. The parking garage that is proposed between Lyndale Avenue South and the residential properties to the west would be stepped down to provide a transition in building heights between structures on the proposed development and adjacent properties.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the C2 Neighborhood Corridor Commercial District.

Additional Standards to Increase Maximum Height

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following [factors](#) when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

The development addresses light and air of surrounding properties by varying the height and building setbacks of the structures on the site, and incorporating the existing topography on and near the site. The residential buildings to the west have between three and four stories and are elevated between seven and ten feet above the elevation of the subject site. The proposed development would have five and six stories. As a result, the tallest points of the six-story portion of the proposed building would be approximately 19 feet above the nearest neighboring property and 11 feet to the east of the residential building. However, the majority of the proposed mixed-use building would be five stories and would be almost level with the top of the roof of the neighboring, existing properties.

The four-story parking garage would be located in between the proposed mixed-use structure and the residential uses to the west. The parking garage would be approximately seven feet from the west property line and 20 feet from the nearest residential structure. The existing adjacent commercial building to the south does not contain north-facing windows and would not be negatively impacted by the proposed development. Increasing the height of the proposed development should not impede access to the light and air that the surrounding properties receive.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The site in question is between approximately seven and ten feet below the grade of the adjacent residential properties. Approving the conditional use permit for height would not result in substantial shadowing effects on nearby residential properties.

The applicant has submitted a shadowing study that demonstrates the proposed development's impacts on nearby properties during the winter solstice, summer solstice, and spring equinox at 4 p.m., noon, and 9 or 10 a.m. in each scenario. The shadow study indicates that there would be minimal shadowing cast on the residential properties to the west at 4 p.m. and at noon in all scenarios and at 9 a.m. during the summer solstice. The study does indicated some impacts on the residential properties to the west during the winter solstice and spring equinox at 10 a.m. and 9 a.m., respectively, but the shadowing decreases by noon in both cases.

There are no significant public spaces or existing solar energy systems, if any, in the immediate vicinity that would be affected by the development.

3. *The scale and character of surrounding uses.*

The building heights in the surrounding area range between one and four stories. The three- and four-story, medium-density residential buildings to the east and west of the site are situated on a hill relative to the subject site, which is in a valley, so the resulting difference in the heights of the existing and proposed buildings would be 19 feet at most, and the remainder of the proposed structures would be close to level or shorter than surrounding residential uses.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The proposed development will not block views of landmark buildings, significant open spaces, or bodies of water.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the minimum rear yard setback from 15 feet to 3.5 feet based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the rear yard setback from 15 to 3.5 feet. The minimum building setback of 15 feet along the west property line is triggered by the adjacent residential uses.

The site is located along a Community Corridor and Commercial Corridor, and is located in an area that is designated as Mixed Use in the future land use map. The applicant's proposal balances the policies that support increased commercial activity and residential density on the site with the need to provide off-street parking and access for the proposed uses on the site. The applicant states that the soils on the site are not suitable for constructing the parking facility underground. As the entire structure must be built above-grade based on the applicant's studies, the minimum aisle dimensions of the structured parking facility are such that the total depth of the parking stalls, drive aisles, and access ramps is 80 feet in order to comply with the City's code requirements. The total depth of the site (measured from east to west) is 119 feet, leaving approximately 39 feet for the depth of the residential units and hallways facing Lyndale. The narrow depth of the lot makes it challenging for any feasible development to provide adequate off-street parking for the uses the development is serving while establishing an active building wall adjacent to the public street and sidewalk. CPED staff finds that practical difficulties exist in complying with the ordinance because of the circumstances unique to the property.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The request to allow a reduced setback along the west property line is reasonable and is in keeping with the spirit and intent of the ordinance and comprehensive plan. The intent of having yard controls is to provide for the orderly development and use of land, and to minimize conflicts between adjacent land uses by regulating the dimension and use of yards by providing adequate light, air, open space, and separation of uses.

The proposed structure will not diminish light, air, or open space for the adjacent properties. While the applicant is requesting to reduce the rear setback to 3.5 feet, the proposed building is set back 3.5 feet from the property line only for the first 37 feet away from the property line abutting West Franklin Avenue. The reason the applicant is requesting to reduce the setback dimension to 3.5 feet is to maintain the active building wall along West Franklin Avenue rather than leaving the space dark and underutilized. In addition, the proposed building is a total of 11 feet away from the building wall of the neighboring residential property in this location. The actual distance between the proposed building walls and the existing residential structures to the west is greater than 11 feet for the remainder of the west property line (approximately 250 feet).

Staff's primary concern with regards to the variance request is the building setback distances between the proposed parking structure and the residential uses directly to the west. The closest building is located 20 feet from the west wall of the proposed four-story parking structure, not 3.5 feet, which is the dimension of the enumerated variance. The applicant is proposing to use the property in a reasonable manner and staff finds that the applicant's request is in keeping with the spirit of the ordinance and comprehensive plan. The applicant is proposing to provide only 32 stalls above the minimum code requirement in order to minimize the number of above-grade parking levels. They are also proposing to provide an entire row of compact parking spaces on the second, third, and fourth levels of the parking facility in order to provide an additional three feet between the building and property line. The proximity of the parking structure to the residential uses to the west would be further ameliorated by the landscaped buffer and plantings that is proposed between the parking facility and west property line.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The applicant has demonstrated that the development, as proposed, would provide sufficient light and air for adjacent residential uses while maintaining the mix of uses that is characteristic of the site and surrounding properties. Staff has concerns with the proposed design of the parking structure, as it does not sufficiently screen the vehicles in the parking garage adjacent to the residential uses. Staff is recommending that the Planning Commission add a condition to enclose all sides of the parking garage to minimize any negative impacts on the residential uses to the west.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the minimum south interior side yard setback from 15 feet to five feet based on the following [findings](#):

- I. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the south interior side yard setback from 15 feet to five feet. The minimum building setback along the south property line is triggered by presence of residential windows on the south façade of the development.

The site is located along a Community Corridor and Commercial Corridor, and is located in an area that is designated as Mixed Use in the future land use map. The applicant's proposal balances the policies that support increased commercial activity and residential density on the site with the need to provide off-street parking and access for the proposed uses on the site. The applicant states that the soils on the site are not suitable for constructing the parking facility underground. As the entire structure must be built above-grade based on the applicant's studies, the minimum aisle dimensions of the structured parking facility are such that the total depth of the parking stalls, drive aisles, and access ramps is 80 feet in order to comply with the City's code requirements. The total depth of the site (measured from east to west) is 119 feet, leaving approximately 39 feet for the depth of the residential units and hallways facing Lyndale. The narrow depth of the lot makes it challenging for any feasible development to provide adequate off-street parking for the uses the development is

serving while establishing an active building wall adjacent to the public street and sidewalk. CPED staff finds that practical difficulties exist in complying with the ordinance because of the circumstances unique to the property.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The request to allow a reduced setback along the south property line is reasonable and is in keeping with the spirit and intent of the ordinance and comprehensive plan. The intent of having yard controls is to provide for the orderly development and use of land, and to minimize conflicts between adjacent land uses by regulating the dimension and use of yards by providing adequate light, air, open space, and separation of uses. The proposed structure will not diminish light, air, or open space for the adjacent properties. The property to the south is a one-story commercial building with zero north-facing windows. The residential windows on the proposed development would overlook the existing commercial building to the south, and would not have a negative impact on the light and air for those facing the south property line on either side.

In addition, the applicant is proposing to use the property in a reasonable manner and staff finds that the applicant's request is in keeping with the spirit of the ordinance and comprehensive plan. The applicant is proposing to provide only 32 stalls above the minimum code requirement in order to minimize the number of above-grade parking levels.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The applicant has demonstrated that the development, as proposed, would provide sufficient light and air for adjacent residential uses while maintaining the mix of uses that is characteristic of the site and surrounding properties. Staff has concerns with the proposed design of the parking structure, as it does not sufficiently screen the vehicles in the parking garage adjacent to the residential uses. Staff is recommending that the Planning Commission add a condition to enclose all sides of the parking garage to minimize any negative impacts on the residential uses to the west.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – Requires alternative compliance

- The building reinforces the street wall, facilitates pedestrian access, and maximized natural surveillance. The building entrances at street level can be accessed by residents, theater-goers, employees, and customers. There are large windows facing the both Lyndale Avenue South and West Franklin Avenue that maximize the opportunities for people to observe adjacent spaces and the public sidewalks.
- The principal entrance to the building is oriented toward Lyndale Avenue South. There are multiple other entrances to the retail, restaurant, and residential spaces on Lyndale Avenue South.

- The site is located on a corner lot, which requires that both walls abutting the streets be located within eight feet of the property line. The first floor of building is within eight feet of both the front property lines on both Lyndale Avenue South and West Franklin Avenue.
- The area between the building and property lines would be used for additional sidewalk space. The area between the building and public street would contain landscaping, benches, and planters, subject to Public Works' approval.
- All of the proposed parking is located to the rear of the site within a four-story parking facility.
- The building wall along Lyndale Avenue South is approximately 286 feet in length. The applicant is proposing a variety of materials, recesses in the building wall, bay windows, and differently-sized window openings, and varied building heights within the structure to break up the façade into smaller sections.
- There are no areas of the development that are over 25 feet in length and void of windows, entries, recesses or projections, or other architectural elements. If the Planning Commission requires a condition that the parking structure be enclosed on all four sides, staff would recommend also adding a condition to ensure that the applicant would continue to comply with the blank wall restriction.
- The exterior materials of the building will include face brick, metal panel, precast concrete, concrete masonry units, and glass. The sides and rear of the building are similar to and compatible with the front of the building. Plain face concrete block is not proposed as an exterior material.
- The windows are vertical in nature and are evenly distributed along the building walls.
- The minimum window requirement applies to the building facades along both Lyndale Avenue South and West Franklin Avenue. Thirty percent of the walls on the first floor, non-residential uses must be windows, and ten percent of the residential walls above the first floor must be windows. The project is in compliance with the minimum window requirement (see **Table I**).
- Ninety-one percent of the building frontage along Lyndale Avenue South and 79 percent of the building frontage along West Franklin Avenue contain active functions. Both ground floors exceed the minimum requirement for active building frontage facing a public street, sidewalk, or pathway.
- The principal roof line of the building will be flat. The majority of the commercial buildings and multiple-family buildings in the area have flat roofs and the majority of the single-family homes in the area have pitched roofs.
- The proposed parking garage is located to the rear of the building. Chapter 530 of the zoning code requires that sloped floors do not dominate the appearance of the walls and that vehicles be screened from view. The proposed facility contains large openings on the west façade that would not sufficiently screen the vehicles in the ramp adjacent to residential uses, and the sloped floors of the facility would be visible. This item requires alternative compliance.

Table I. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

	Code Requirement		Proposed	
Nonresidential Uses (Lyndale)				
1 st Floor	30% minimum	684 sq. ft.	52%	1,184 sq. ft.
Residential Uses (Lyndale)				
2 nd Floor	10% minimum	358 sq. ft.	30%	1,084 sq. ft.
3 rd Floor	10% minimum	300 sq. ft.	33%	981 sq. ft.
4 th Floor	10% minimum	300 sq. ft.	29%	873 sq. ft.
5 th Floor	10% minimum	271 sq. ft.	35%	948 sq. ft.
6 th Floor	10% minimum	97 sq. ft.	24%	232 sq. ft.
Nonresidential Uses (Franklin)				
1 st Floor	30% minimum	214 sq. ft.	51%	361 sq. ft.
Residential Uses (Franklin)				
2 nd Floor	10% minimum	145 sq. ft.	42%	602 sq. ft.
3 rd Floor	10% minimum	122 sq. ft.	49%	562 sq. ft.
4 th Floor	10% minimum	122 sq. ft.	49%	562 sq. ft.
5 th Floor	10% minimum	122 sq. ft.	49%	602 sq. ft.
6 th Floor	10% minimum	149 sq. ft.	42%	631 sq. ft.

Access and Circulation – Meets requirements

- All of the principal entrances leading into and out of the building are connected to the public sidewalk either directly or via a walkway.
- No transit shelters are proposed as part of this development.
- All proposed parking would be located to the rear of the site within an above-grade parking facility. Vehicular access has been designed to minimize conflicts with pedestrians and surrounding residential uses. A previous version of the proposed project contained two curb cuts along Lyndale Avenue South. Since then, the applicant has removed the north curb cut on Lyndale to further reduce potential vehicle and pedestrian conflicts.
- There are no public alleys adjacent to the site.
- There is no maximum impervious surface requirement in the C2 zoning district. According to the materials submitted by the applicant, 94.2 percent of the site would be impervious. Currently, 99.7 percent of the site is impervious.

Landscaping and Screening – Meets requirements

- The composition and location of landscaped areas complement the scale of the development and its surroundings.
- The zoning code requires that at least 20 percent of the site not occupied by buildings be landscaped. The lot area of the site is 35,928 square feet and the building footprint is 33,457 square feet. The difference is 2,471 square feet, and 20 percent of this number is 494 square feet. The applicant is proposing approximately 2,094 square feet of landscaping on the site, or approximately 85 percent of the site not occupied by the building.
- The zoning code requires that the site contain at least one canopy tree per 500 square feet of required green space and at least 1 shrub for each 100 square feet of required green space (see **Table 2**). The tree requirement for this site is one and the shrub requirement is five. The

applicant is providing a total of nine canopy trees and 14 shrubs on-site, as well nine evergreens, six ornamental trees, and a variety of perennials, grasses, and vines. The applicant is also proposing eight new canopy trees and perennials that would be located in the public the right-of-way along both Lyndale Avenue South and West Franklin Avenue.

- All areas not occupied by buildings contain landscaping.
- There is no surface parking proposed for the site, so the site is not subject to the screening and landscaping requirements for parking areas per Chapter 530.

Table 2. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	35,928 sq. ft.
Building footprint	--	33,457 sq. ft.
Remaining Lot Area	--	2,471 sq. ft.
Landscaping Required	494 sq. ft.	2,094 sq. ft.
Canopy Trees (1: 500 sq. ft.)	1 trees	9 trees
Shrubs (1: 100 sq. ft.)	5 shrubs	14 shrubs

Additional Standards – Meets requirements

- There are no surface parking spaces proposed on the site.
- There are no important elements of the city near the site that will be obstructed by the proposed building.
- This building should have minimal shadowing effects on public spaces and adjacent properties.
- This building should have minimal wind effects on the surrounding area.
- The site plan complies with crime prevention design elements as the building entrances are located up to the front property lines, there are windows where people can see in and out along all levels of the building and there are lights located near all of the entrances and throughout the grounds.
- This site is neither historically designated nor is it located in a historic district.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed residential and commercial uses are permitted in the C2 district. The proposed building height of 73 feet-6 inches requires a conditional use permit in the C2 district.

Off-street Parking and Loading – Meets requirements with Conditions of Approval

- The minimum parking requirement for the development is 131 spaces. There will be a total of 163 accessory parking spaces provided, as shown in **Table 3**. The applicant is proposing 105 standard spaces, 52 compact spaces, and six accessible spaces throughout the parking structure. The parking facility would contain the minimum accessory parking serving the on-site uses, which are the indoor theater (3,600 square feet), retail (3,800 square feet), a restaurant (7,900 square feet), and five levels of residential uses (82 dwelling units).
- Staff recommends requiring that a minimum of 74 parking spaces be designated for the on-site residential units and are signed as such to ensure that the residents have access to adequate parking within the parking facility.
- The minimum bicycle parking requirement for multiple-family dwellings with five or more units is one space per two dwelling units. The development contains 82 dwelling units and the minimum number of required bicycle parking spaces for the residential uses is 41, of which at

least 37 must meet the standards for long-term bicycle parking per section 541.180 of the zoning code. The applicant is proposing to provide 118 long-term bicycle parking spaces for residents in ground-floor storage lockers and within enclosed rooms on the second, third, and fourth floors.

- The minimum bicycle parking requirement for the general retail sales and services and restaurant use is six spaces, of which at least four must meet the standards for short-term parking. The minimum bicycle parking requirement for the theater use is 3 spaces, of which at least 2 must meet the definition of long-term bicycle parking spaces. The applicant is providing 14 short-term bicycle spaces along the south wall of the ground floor of the development. In order to meet the zoning code standards, staff is recommending that the Planning Commission add a condition that at least two long-term bicycle spaces will provided for theater guests in the final plans.
- The development is not subject to a minimum loading requirement. The applicant has designated a loading area on the ground floor for the residential and commercial uses.

Table 3. Vehicle Parking Requirements Per Use (Chapter 541)

	Minimum Parking Requirement	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Theater, Indoor	30	--	30	60	
General retail sales and services	4	--	6	24	
Restaurant, sit down	24	--	24	106	
Residential dwellings	82	Transit Incentives (9)	73	--	
Total	140	9	131	190	

Table 4. Bicycle Parking and Loading Requirements (Chapter 541)

	Minimum Bicycle Parking	Minimum Short-Term	Minimum Long-Term	Proposed	Total Loading Requirement	Proposed
Theater, Indoor	3	--	Not less than 50% (2)	14 short-term spaces	None	1 designated loading zone
General retail sales and services	3	Not less than 50% (2)	--			
Restaurant, sit down	3	Not less than 50% (2)	--			
Residential dwellings	41	--	Not less than 90% (37)	118		
Total	50	--	--	132	1	128

Building Bulk and Height – Requires conditional use permit

- The maximum floor area ratio (F.A.R.) in the C2 zoning district is 1.7. The development qualifies for the enclosed parking density bonus mixed residential-commercial density bonus, for a maximum of 2.38 in allowed F.A.R.

- The lot in question is 35,928 square feet in area. The applicant is proposing 84,106 square feet in gross floor area, for a total F.A.R. of 2.34.
- As previously mentioned, a conditional use permit is required to increase the development's allowed height in the C2 zoning district from 4 stories/56 feet, whichever is less, to 73 feet-6 inches.

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	35,928 sq. ft. / 0.82 acres
Gross Floor Area (GFA)	--	84,106 sq. ft.
Maximum Floor Area Ratio (GFA/Lot Area)	2.38	2.34
Maximum Building Height	4 stories or 56 feet, whichever is less	73 1/2 ft.

Residential Lot Requirements – Meets requirements

- The proposed dwelling units (DUs) are part of a mixed-use building and the residential lot dimension requirements for the C2 district do not apply to this development.

Table 6. Residential Lot Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	82 DUs
Density (DU/acre)	--	100 DU/acre
Minimum Lot Area	--	438 sq. ft. per DU

Yard Requirements – Requires variance(s)

- The front yard setback along Lyndale Avenue South and West Franklin Avenue is zero feet.
- The western property line is adjacent to the OR2 High Density Office Residence District and the R6 Multiple-Family District. The resulting minimum setback along the west property line is $5+2x$, where x equals the number of stories above the first floor. The resulting setback along this property line is 15 feet. The applicant has applied for a variance to reduce the rear setback to 3.5 feet.
- In the C2 zoning district, setback requirements do not typically apply when adjacent to commercially zoned properties. However, when the use is residential and there are windows facing an interior side or rear property line, a setback is required of $5+2x$, where x equals the number of stories above the first floor. This provision applies to the south interior property line. The resulting setback along these two property lines is 15 feet. The applicant is proposing to reduce the minimum setback to five feet along the south property line, which requires a variance.

Table 7. Minimum Yard Requirements

	Zoning District	Proposed
Front (East / Lyndale)	0 ft.	2 ft.
Corner Side (North / Franklin)	0 ft.	0 ft.
Interior Side (South / adjacent to C1)	15 ft.	5 ft.
Rear (West / adjacent to residential uses)	15 ft.	3.5 ft. at closest point; 7 feet to parking garage

Signs – *Not applicable*

- Signs are subject to Chapter [543](#) of the Zoning Code. No signage is proposed for the development at this time and the applicant will be required to submit a separate signage permit application for any signage that is proposed in the future.

Dumpster Screening – *Meets requirements*

- There will be trash and recycling rooms located within building on the first floor.

Screening of Mechanical Equipment – *Meets requirements*

- The transformer adjacent to Lyndale Avenue South would be fully screened.

Lighting – *Meets requirements with Conditions of Approval*

- A lighting plan showing footcandles shall be submitted to show compliance with the requirements of Chapter 535, Regulations of General Applicability.

Impervious Surface Area – *Not applicable*

Specific Development Standards – *Meets requirements*

- Indoor theaters are subject to the specific development standards in Chapter 536 of the zoning code if they are nonconforming as to parking. The proposed indoor theater would have sufficient off-street parking based on the applicant’s materials.
- The proposed sit-down restaurant would be subject to the following specific development standards:

Restaurant, sit down.

(1) Where alcoholic beverages are served, not less than sixty (60) percent of total gross sales revenue shall be from the sale of food and beverages not containing alcohol, and the use shall comply with the requirements of [Title 14](#), Liquor and Beer, of the Minneapolis Code of Ordinances and Chapter 4 of the Minneapolis City Charter.

(2) The premises, all adjacent streets, sidewalks and alleys, and all sidewalks and alleys within one hundred (100) feet shall be inspected regularly for purposes of removing any litter found thereon.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth identifies the site as Mixed Use on the future land use map. In addition to the policies listed in previous sections, the proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- 1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

- 1.4.2 Promote standards that help make commercial districts and corridors desirable, viable, and distinctly urban, including: diversity of activity, safety for pedestrians, access to desirable goods and amenities, attractive streetscape elements, density and variety of uses to encourage walking, and architectural elements to add interest at the pedestrian level.
- 1.4.4 Continue to encourage principles of traditional urban design including site layout that screens off-street parking and loading, buildings that reinforce the street wall, principal entrances that face the public sidewalks, and windows that provide “eyes on the street.”

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

- 1.5.2 Facilitate the redevelopment of underutilized commercial areas by evaluating possible land use changes against potential impacts on the surrounding neighborhood.
- 1.5.3 Promote the preservation of traditional commercial storefronts wherever feasible.

Land Use Policy 1.9: Through attention to the mix and intensity of land uses and transit service, the City will support development along Community Corridors that enhances residential livability and pedestrian access.

- 1.9.2 Support new small-scale retail sales and services, commercial services, and mixed uses where Community Corridors intersect with Neighborhood Commercial Nodes.
- 1.9.6 Promote more intensive residential development along Community Corridors near intersections with Neighborhood Commercial Nodes and other locations where it is compatible with existing character.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street’s character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- 1.10.2 Encourage commercial development, including active uses on the ground floor, where Commercial Corridors intersect with other designated corridors.
- 1.10.4 Encourage a height of at least two stories for new buildings along Commercial Corridors, in keeping with neighborhood character.

I.10.5 Encourage the development of high-density housing on Commercial Corridors.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

Urban Design Policy 10.16: Design streets and sidewalks to ensure safety, pedestrian comfort and aesthetic appeal.

10.16.1 Encourage wider sidewalks in commercial nodes, activity centers, along community and commercial corridors and in growth centers such as Downtown and the University of Minnesota.

10.16.2 Provide streetscape amenities, including street furniture, trees, and landscaping, that buffer pedestrians from auto traffic, parking areas, and winter elements.

10.16.3 Integrate placement of street furniture and fixtures, including landscaping and lighting, to serve a function and not obstruct pedestrian pathways and pedestrian flows.

10.16.4 Employ pedestrian-friendly features along streets, including street trees and landscaped boulevards that add interest and beauty while also managing storm water, appropriate lane widths, raised intersections, and high-visibility crosswalks.

The proposed development would include contain 82 dwelling units as well as ground-floor commercial uses. The building would be mixed use, as consistent with its designation in the future land use map, and would appropriately place high-density residential development along a Commercial Corridor.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The [Lyn-Lake Small Area Plan](#) was adopted by the City Council in 2009. The plan states that the intersection of West Franklin Avenue and Lyndale Avenue South is “a gateway to Southwest Minneapolis, and improvements should be made” to make the area comfortable for pedestrians and expand opportunities for development. The proposed development would widen the sidewalk along Lyndale Avenue South to reduce the pedestrian crossing across Lyndale Avenue South while creating a more pedestrian-friendly pathway along the west side of Lyndale Avenue South.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- **Parking garages.** The proposed parking garage is located to the rear of the building. Chapter 530 of the zoning code requires that sloped floors do not dominate the appearance of the walls and that vehicles be screened from view. The proposed facility contains large openings on the west façade that would not sufficiently screen the vehicles in the ramp from the residential properties to the west, and the sloped floors of the facility would be visible. This item requires alternative compliance. Staff finds that it would be practical for the Planning Commission to require compliance with this standard by requiring that the west and south walls of the parking facility be completely enclosed.

FOR REZONINGS ONLY

ZONING PLATE NUMBER. 18

LEGAL DESCRIPTION.

Parcel/Address	Legal Description
#1/2004 Lyndale Ave So., Minneapolis	The east 123.73 feet of Lots 1 and 2, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#2/2008 Lyndale Ave So., Minneapolis	Lot 3, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#3/2012 Lyndale Ave So., Minneapolis	The north 35 feet, front and rear, of Lot 4, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#4/2014 Lyndale Ave So., Minneapolis	The south 23 feet of Lot 4 and the north 18 feet of Lot 5, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#5/2018 Lyndale Ave So., Minneapolis	The south 40 feet of Lot 5, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota
All ABSTRACT property.	

RECOMMENDATIONS

Recommendation of the Department of Community Planning and Economic Development for the Rezoning:

The Department of Community Planning and Economic Development recommends that the City Planning Commission and City Council adopt the above findings and **approve** the rezoning petition to change the zoning classification at the properties located at 2008, 2012, 2014, and 2018 Lyndale Avenue South from the C1 Neighborhood Commercial District to the C2 Neighborhood Corridor Commercial District.

Recommendation of the Department of Community Planning and Economic Development for the Conditional Use Permit:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a conditional use permit to increase the maximum allowed height in the C2 Neighborhood Corridor Commercial District from 4 stories/56 feet to 6 stories/73 feet-6 inches, subject to the following condition:

- I. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning

administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the minimum rear yard setback from 15 feet to 3.5 feet at the properties located at 2004-2018 Lyndale Avenue South.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the south interior side yard setback from 15 feet to 5 feet at the properties located at 2004-2018 Lyndale Avenue South.

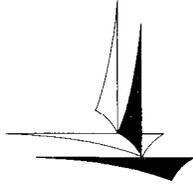
Recommendation of the Department of Community Planning and Economic Development for the Site Plan Review:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the site plan review application to allow a new, six-story, mixed-use building with 82 dwelling units at the properties located at 2004-2018 Lyndale Avenue South, subject to the following conditions:

1. Approval of the final site, elevation, landscaping, and lighting plans by CPED staff.
2. All site improvements shall be completed by November 15, 2016, unless extended by the zoning administrator, or the permit may be revoked for non-compliance.
3. All signs are expected to comply with Chapter 54I of the Zoning Code. All new signage requires a separate permit from CPED.
4. The west and south walls of the parking garage shall be completely enclosed,, shall comply with the blank wall limitations of section 530.120 of the zoning code, and shall include a densely planted landscaped area immediately to the west of the wall.
5. A minimum of 74 parking spaces shall be designated for the on-site residential units and shall be signed as such.
6. The final plans shall be revised to show that no fewer than two (2) long-term bicycle parking spaces will be provided for the theater as required by section 54I.180 of the zoning code.

ATTACHMENTS

1. PDR report
2. Written description and findings submitted by applicant
3. Zoning map
4. Survey
5. Site survey
6. Plans
7. Building elevations
8. Section and shadow study
9. Renderings
10. Photos
11. Travel Demand Management Plan
12. Public comments



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**
(612) 673-2726
don.zart@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001167
Applicant:	TGMA DEVELOPERS LLC 1221 NICOLLET MALL, SUITE 310 MINNEAPOLIS, MN 55403
Site Address:	2000 LYNDAL AVE S 2008 LYNDAL AVE S 2012 LYNDAL AVE S 2014 LYNDAL AVE S 2018 LYNDAL AVE S
Date Submitted:	28-JAN-2014
Date Reviewed:	07-FEB-2014

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.

Project Scope

Proposed 6 story mixed-use building consisting of 85 one to three bedroom apartments, approximately 600 sf of restaurant space, an updated 150 seat theatre space for the Theatre Garage, 212 parking stalls (approx 90 for general public use, a rooftop plaza for outdoor theatre use, general use and restaurant seating and a solar panel array to support the buildings energy needs.

***Approved:** You may continue to the next phase of developing your project.
***Resubmission Required:** You cannot move forward or obtain permits until your plans have been resubmitted and approved.

Review Findings (by Discipline)

□ Zoning - Planning

- Based on staff's preliminary review, the following land use applications have been identified:
 - Petition to rezone the properties located at 2008-2018 Lyndale Avenue South from the C1 Neighborhood Commercial District to the C2 Neighborhood Corridor Commercial District.
 - Conditional use permit to allow a principal district parking facility in the C2 Neighborhood Corridor Commercial District.
 - Conditional use permit to increase the maximum allowed height from 56 feet to 75.4 feet.
 - Variance to increase the maximum allowed floor area ratio.
 - Variance to reduce the rear yard setback from 15 to 3.2 feet.
 - Variance to reduce the south interior side yard setback from 15 to 3.1 feet.
 - Site plan review for a six-story, mixed-use building with 85 dwelling units.
- Additional applications may be required, depending on the plans that the applicant formally submits. Staff has reduced the minimum off-street parking requirement based on the assumption that the applicant will provide a transit shelter on-site. Staff would like the applicant to discuss this possibility with Metro Transit as soon as possible, preferably before submitting an application. Sign plans have not been submitted in this plan set and would be necessary to determine whether the proposal would need any applications related to signage. Similarly, a solar energy systems application would be necessary to determine whether the proposed solar panels would require a conditional use permit.
- The proposal exceeds the minimum landscaping requirements in the zoning code's site plan review standards. Staff is primarily concerned with the reduced rear yard setback and number of above-grade parking levels adjacent to residential uses.
- Staff would like to see the following changes in the proposed plans:
 - Ensure that the appearance of the rear and side walls of any building shall be similar to and compatible with the front of the building (Section 530.120).
 - Eliminate tandem stalls on the street level.
 - Address how the transformer will be screened or relocated.
 - Indicate how the development meets the minimum bicycle parking requirement for each use.

□ Addressing

- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each proposed address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- Starting on the corner of Lyndale and Franklin:
 - The Restaurant has a proposed address of 2004 Lyndale Ave
 - The Residential lobby has a proposed address of 2008 Lyndale Ave
 - The Theatre Garage lobby has a proposed address of 2010 Lyndale Ave
 - The 5 retail spaces will be, from North to South, 2012, 2014, 2016, 2018 & 2020 Lyndale
- When assigning suite numbers to the interior dwelling spaces the following guidelines apply:
- The first one to two digits of the suite sequence number will designate the floor number of the site.
- The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
- Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
- Please provide each condo, suite, unit or apartment number.
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

□ Parks - Forestry

- Contact Paul Martinson (612-499-9209) regarding removal or protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
 - <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,500 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$200 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your project, Theatre Garage & Marquee Apartments, the calculated dedication fee is as follows:
 - Total Park Dedication Fee =
 - Non-residential (\$1,800)+Residential(85 units x \$1,500 per unit) (\$127,500) = \$129,300
 - Total Park Dedication Administrative Fee: \$1,000
 - Total Park Dedication related fees = \$130,300
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Don Zart at 612-673-2726

□ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.

□ Street Design

- Note to the Applicant: The corner radius at Lyndale and Franklin does not appear to be graphically correct (possibly a CADD error).
- All driveway aprons shall be designed and constructed to City standards; driveway aprons shall be shown graphically correct on all plan sheets. Please refer to the details included on Sheet C5.0 of the plans.
- Public Works is reviewing the plans with Hennepin County staff; there is a chance that the taper along Lyndale could be removed and the curb alignment changed. Please contact Paul Miller at (612) 673-3603 for further information.
- Provide curb profile for any length of curb replacement greater than 50'
- Please work with Metro Transit if looking to relocate bus stop locations. The Metro Transit Contact is Maurice Roers (Manager - Facilities Planning, Engineering & Facilities) Direct: 612.349.7684

❑ Sidewalk

- The sidewalk layout shown on the Site Plan (Sheet C2.0) and the Landscaping Plan (Sheet L1.0) does not adequately detail all of the proposed encroachments in the Public right-of-way. The Applicant shall include the actual design details necessary to fully describe the streetscape and landscaping design. Details and standard plates for decorative paving, planters, benches, tree grates, tree planting shall be included in the plans. Decorative pavements and sidewalk features are generally not recommended within the pedestrian thru-zone of the Public sidewalk. Please contact Paul Miller at (612) 673-3603 for further information
- Review planter location adjacent to curb cut on Lyndale.
- It is recommended that streetscape details and tree planting in the Public right-of-way be similar to the standard established along Lyndale Ave. immediately south of the proposed development; this includes tree trenches, structured soils, and pervious pavers separating tree grates. Sidewalk layouts and landscaping in the Public right-of-way shall follow established design standards; refer to the following:
- City of Minneapolis Urban Forest Policy (http://www.minneapolismn.gov/www/groups/public/@cped/documents/webcontent/convert_282934.pdf)
- Access Minneapolis - Design Guidelines for Streets and Sidewalks (http://www.minneapolismn.gov/publicworks/transplan/comp/public-works_trans-plan_designguidelines)
- ADA compliant pedestrian ramps are required at each crosswalk at the intersection of Franklin Ave. and Lyndale Ave. S. Construct two (2) ADA compliant pedestrian ramps at this location. Include the appropriate details and standard plates in the site plan, refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>

❑ Traffic and Parking

- The nature of the proposed development is such that traffic impacts could be an issue; please contact Allen Klugman at (612) 673-2743 to discuss the potential requirements of a Travel Demand Management Plan (TDMP).
- The Applicant shall note the location of any existing Metro Transit "bus stops" on the site plan.
- The site plan indicates three (3) curb cut access points to the development; current ordinance states that all maneuvers associated with loading, parking or sanitation pick-up for a private development shall occur on private property. Please provide a narrative explaining the various vehicle access requirements, deliveries, and trash removal operations. Show turning maneuvers for all truck type vehicles on the site plans that will be using the parking entrance areas. More detail is required for the vehicle access points to the development such as garage door types, door operations, pedestrian safety/warning devices etc. Vehicle ramps into parking structures shall have no more than a 4% slope for the first 20 feet from the right of way (please indicate slope on the drawings).
- Please contact Bill Prince at (612) 673-3901 regarding existing and proposed street lighting. All street lighting (existing and proposed) shall be shown clearly on the site plan. Note to the Applicant: If decorative street lighting exists on the proposed site it must be preserved or replaced at existing levels. Street lighting will be strongly encouraged in areas immediately adjacent to existing lighting systems, in high density areas such as Uptown and the University of Minnesota, and along major pedestrian corridors and business nodes as identified in the Minneapolis Street Lighting Policy.
- Contact Nick Van Gunst at (612) 673-2172 prior to construction for the temporary removal and relocation of any City of Minneapolis signal system that maybe near the construction limits as the construction project will assume all costs for this work.

❑ Water

- Based upon past experience with developments of this size, Public Works would recommend that the service lines be reduced as follows: 6" for fire safety (unless expressly directed by the Minneapolis Fire Department) and a 4" domestic water service (see handout). Contact Rock Rodgers at 612-673-2286 for information.

□ Environmental Health

- No records are on file with the Minnesota Pollution Control Agency regarding these or any of these tanks or their condition. It is recommended the developer engage the Minnesota Pollution Control Agency Voluntary Investigation and Clean-up program to review the environmental condition of the soils on site and obtain a letter of no association for impacted soils if identified during investigation.
- If the project will be disturbing identified impacted soils on the site preapproval for removal, disposal and/or reuse of must occur from the MCPA and the City of Minneapolis prior to beginning excavation activities. If impacted soil is encountered during site activities call the MN State Duty officer at (615) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

□ Historical Preservation Committee

- There is no preservation flag on this property. However, whenever a structure is to be wrecked, a preservation review is required. A licensed wrecking contractor is required.

□ Sewer Design**Stormwater Management:**

- This area has experienced flooding issues in the past. Please provide stormwater modeling calculations for the existing and proposed conditions. It is requested that stormwater management be provided on-site to reduce the volume and maximum discharge as much as feasible. Please also identify the existing and proposed drainage patterns on the site.
- Based on the stormwater modeling calculations, requested above, the proposed storm sewer connection to the City main in Franklin Ave W will be further evaluated and may need to be revised.
- Please provide a copy of any geotechnical investigations performed at the site.

Utility Connections:

- It is recommended that the existing sanitary sewer services proposed to be connected to be televised and evaluated prior to construction to determine if they are suitable for reuse.
- Please identify the first floor elevation and the low floor elevation on the Grading Plan.

Utility:

- All existing and proposed public utilities (water, sanitary, and storm sewer) on and adjacent to the property shall be shown on the plans with corresponding pipe sizes, types and invert elevations. For City sanitary and storm sewer infrastructure records contact (612) 673-2405. Please include this information on the Utility Plan.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

❑ Fire Safety

- Provide required automatic fire suppression and fire detection systems throughout building
- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant
- Provide and maintain fire department access at all times
- Contact Mike Raeker, 612-673-2624 with any questions.

❑ Business Licensing

- Please contact Don Zart (612-673-2726) to discuss any Business Licensing or Health Plan review requirements that are needed.

❑ Construction Code Services

- Some footings along street sides may encroach into public space. Footing encroachments 8' or more below grade can project up to 12" into public space.
- The proposed method of earth retention requires review for permit purposes.
- Verify level of water table or perched water.
- One hour rated walls are required along the west and south sides. Openings in walls greater than 3' to 5' from the property line cannot exceed 15%. An open parking garage requires 20% openings on two or more sides.
- It is assumed that the basement, first floor and parking garage will be Type 1 or 2 construction. Verify the special provisions for parking above residential in Section 509.
- The northern most units on the 6th level are shown with one exit. Per Table 1019.2, a maximum of 3 stories of R-2 are allowed (emergency escape and rescue openings required) with one exit. Is it the intent for the 2nd exit on the 6th level to travel across the roof? Per 1024.1 , an exit discharge shall not re-enter a building.
- Exit doors from the roof deck and parking shall swing in the direction of travel.
- An accessible route from parking to the side restaurant entrance is required.
- Please contact the Metropolitan Council to have a SAC determination made. Please see this link for additional information.
http://www.minneapolismn.gov/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf

END OF REPORT



REAL ESTATE ■ DEVELOPMENT ■ CONSTRUCTION

Master Properties Minnesota, LLC
201 West Broadway Avenue
Minneapolis, MN 55411
Phone: 612.872.9200 Fax: 612.872.9201
www.masterpropertiesmn.com

July 10, 2014

Mei-Ling Anderson
City Planner
Land Use, Design & Preservation
250 South 4th Street – Room 300
Minneapolis MN 55415

**Re: 2004-2018 Lyndale Avenue South, Minneapolis (Theatre Garage and Marquee Apartments) –
Project Description / Statement of Proposed Use**

The subject property is located on the southwest quadrant of the intersection of Franklin and Lyndale Avenues in South Minneapolis in the Lowry Hill East Neighborhood. The property is comprised of five parcels containing two commercial buildings and a large, surface parking lot.

The multi-tenant commercial building situated on the northern part of the site was originally constructed in 1923 as an automotive sales and showroom building. This building is currently occupied by the Minneapolis Theatre Garage, Steeple People Thrift Store and a Barber Shop. The one story smaller multi-tenant commercial building located on the south property line of the site was constructed in 1958 and currently houses an art gallery and a dry-cleaners drop-off/pick-up and tailors. Situated between the two commercial buildings is a large, surface parking lot of approximately 54 stalls. This site and intersection is seen by many neighborhood residents and City officials as a gateway or entrance point to south Lyndale Avenue, Lyn-Lake, Uptown and South Minneapolis neighborhoods.

Master Properties Minnesota, on behalf of TGMA Developers, LLC, and Collage Architects, propose to construct a new, mixed-use building on the site that architecturally defines the intersection and establishes a prominent gateway to South Minneapolis. The proposed development would place a new, sit down restaurant at street level, anchoring the intersection. New retail spaces would front Lyndale Avenue as well as a lobby that will provide access to a new, 150-seat Theatre Garage space. Structured, surface parking to support the proposed retail space will be situated behind the retail spaces .

Above street level, four levels of market rate apartments will front Lyndale Avenue while five levels front Franklin Avenue and the intersection. Tucked behind the apartments is a four story, district parking structure to support the project's parking needs as well as provide public parking for neighborhood residents and local businesses.

The proposed Theatre Garage development project will bring life and vibrancy to an underutilized site and prominent commercial intersection. The project will establish a significant gateway into the neighborhood, as well as make the area more pedestrian friendly by replacing the current surface parking lot with vibrant storefronts, a performance theater and a restaurant. The regional parking

structure, tucked behind the retail and housing will help to solve a critical shortage of neighborhood parking and provide parking for neighborhood businesses and residents in the area.

The proposed plan includes:

- Construction of:
 - 82 one and two bedroom apartments
 - Approximately 7300sf of restaurant space
 - Residential street lobby for the residential units of the building along Lyndale Avenue
 - 7400sf of retail and theatre space along Lyndale Avenue
 - 160 parking stalls
 - A solar panel array to aid in offsetting the building's energy costs

Land Use Applications to be applied for include:

- Rezoning 2008-2018 Lyndale Avenue South from C1 to C2
- Conditional Use Permit to increase max allowable height from 56 to 73'5 3/8"
- Variance for a reduction in side and rear yard setbacks
 - Side yard: From 15' to 3.5'
 - Rear yard: From 15' to 3.8'



REAL ESTATE ■ DEVELOPMENT ■ CONSTRUCTION

Master Properties Minnesota, LLC
201 West Broadway Avenue
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July 10, 2014

Mei-Ling Anderson
Senior City Planner
City of Minneapolis
Community Planning & Economic Development
Planning Division – Development Services
250 South 6th Street, Room 300 PSC
Minneapolis, MN 55414-1393

RE: General Land Use Application: **CONDITIONAL USE PERMIT APPLICATION**

Subject Property: 2004-2018 Lyndale Avenue South, Minneapolis, MN

Conditional Use Permit (1): To allow for an increase in maximum height from 56' to 73'5 3/8"

Master Properties Minnesota, LLC, on behalf of TGMA Developers, LLC, is requesting a conditional use permit to allow for an increase in maximum building height from 56' to 73' 5 3/8".

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*
Granting of an increase in maximum height of the proposed project from 56' to 73'5 3/8" would not be detrimental nor endanger the public health, safety, comfort or general welfare. The topography at the subject site is such that the intersection is in a valley which rises steeply to the west and east along Franklin. When comparing the height of the proposed structure with structures in the immediate vicinity, taking into account the grade change, the structure is in line with the heights of the surrounding neighborhood. The intersection has also been identified as an area where the City would like to see increased density and mixed-use development. Allowing for increased height at the corner/intersection of this site would allow for compliance with the City's plans for increased density at commercial and transit corridors.
- 2. The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The properties to the west of the subject site are the only residential properties to directly abut the proposed project. The portion of the proposed structure which exceeds the existing maximum height code for C2 is along Franklin. Franklin's grade changes drastically from the intersection to both the west and east. The point at which the building abuts the adjacent residential uses, the grade is approximately seven feet higher than at the existing grade at the intersection. This grade change along Franklin Avenue gives the proposed building an appearance of being five stories rather than six, 66' rather than 73'.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Master has contracted with a civil engineering firm who has prepared a comprehensive utility and drainage plan for the subject site. The proposed project will have access both from Lyndale and Franklin Avenues.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Master has contracted with a private traffic engineering and urban planning consultant to prepare a Travel Demand Management Plan (TDMP) to assess the proposed design and its impacts on the public infrastructure as well as how the project itself will integrate with the existing infrastructure. The analysis has shown that the proposed design will not cause traffic congestion in the public streets, rather it will provide a more efficient intersection design by closing the existing curb cuts near the intersection and replacing them with two curb cuts furthest from the intersection. In addition, Hennepin County has agreed to allow for the reconstruction of the curb alignment along Lyndale Avenue at the intersection in order to eliminate the existing taper as well as reduce the pedestrian crossing length across Lyndale.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The City's comprehensive plan for sustainable growth identifies the intersection of Franklin and Lyndale as an area for density and mixed-use development. Allowing additional height at the intersection corner of the proposed project would allow for additional density at this location.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

The requested Conditional Use for increased height is consistent with the regulations in the C-1 district for a mixed-use development. The conditional use requested is directly related to the desire to provide additional density at the intersection of major commercial corridors and provides great benefit to the neighborhood by establishing housing at major transit intersections.

Additional findings for conditional use permit for INCREASING MAXIMUM HEIGHT:

1. *Access to light and air of surrounding properties.*

The architectural team has provided a shadowing and context study for the proposed project. The proposed project will produce minimal shadowing effects to neighboring properties through the various seasons and equinoxes.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*
There are no significant public spaces or existing solar energy systems in the area that would be adversely affected by shadowing. Any shadowing of neighboring properties has been minimized through architectural design elements of reduced height and step backs in building design in areas closest to residential neighbors.
3. *The scale and character of surrounding uses.*
The topography of the immediate area in which the subject site is located shows drastic grade changes with a valley, or low point, at the intersection and includes steep inclines to the west and east along Franklin Avenue. Taking into account these grade changes, the proposed height of the building fits appropriately within the scale and character of surrounding uses.
4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*
There are no designated landmark buildings, significant open spaces or water bodies that would be adversely affected by the requested additional height of the proposed project.



REAL ESTATE ■ DEVELOPMENT ■ CONSTRUCTION

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July 10, 2014

Mei-Ling Anderson
Senior City Planner
City of Minneapolis
Community Planning & Economic Development
Planning Division – Development Services
250 South 6th Street, Room 300 PSC
Minneapolis, MN 55414-1393

RE: General Land Use Application: **VARIANCE APPLICATION**

Subject Property: 2004-2018 Lyndale Avenue South, Minneapolis, MN

Variance (1): To allow for a reduction in rear and side yard setbacks from 15' to 3.8' and 3.5' respectively.

Master Properties Minnesota, LLC, on behalf of TGMA Developers, LLC, is requesting a variance to reduce the minimum rear and side yard setbacks from 15' to 3.8' and 3.5' respectively.

- 1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*
Several existing site conditions require Master Properties MN (Master) and TGMA Developers to request a variance to reduce the side and rear yard setbacks for this project. The most impactful condition being the soil conditions of the site. Master has completed geotechnical and environmental engineering soils analysis on the site. The existing soils are unsuitable and non-compactable and require 100% of the building to be constructed above ground. Due to the strict dimensions of stalls and drive aisles required to construct a parking facility, the architect has positioned the building as far from the west property line as possible to still accommodate code-required parking stalls and circulation/drive aisle dimensions, while still allowing for the major load bearing wall separating the parking structure from the residential structure. If the building were relocated any further from the west property line, a majority of the parking would be lost due to an inability to accommodate the required parking structure dimensions.

Keeping setbacks along the continuous retail wall along Lyndale will continue the policies outlined in the City's plan for sustainable growth for strong commercial and transit corridors.

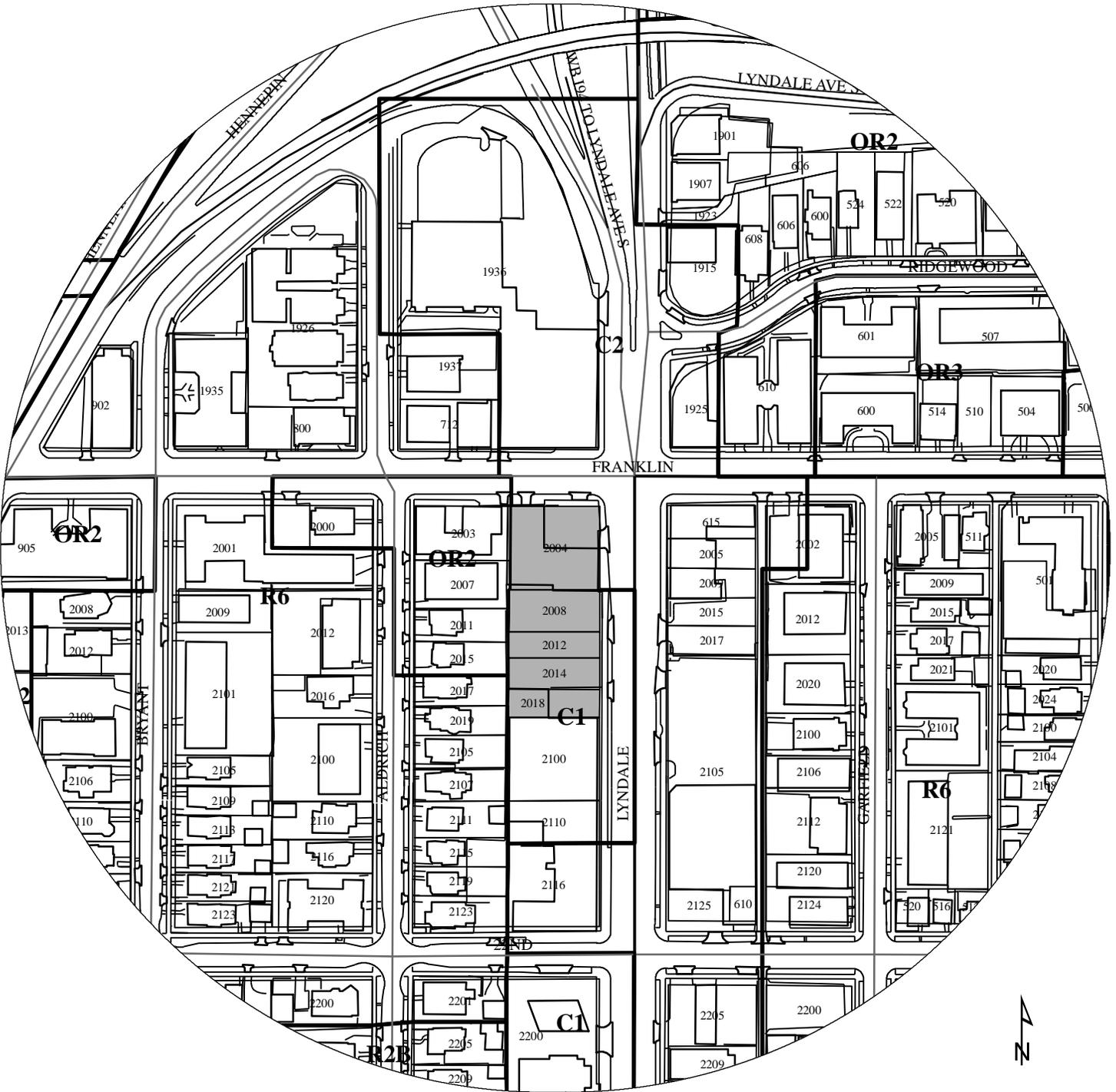
2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Master, in working with our architectural, civil, and landscaping consultants, has proposed to maximize the proposed side and rear yard setbacks to still keep with the spirit and intent of the zoning ordinance. Along the western property line, the parking structure is setback 7'. This dimension is in keeping with the code requirement for setbacks of $5+2(x)'$, with x being the number of stories above 1st floor. Due to the existing grade change that increases in height as one moves west, the parking structure will be 28 feet above grade from the residential properties to the west. Within the proposed 7' setback, Master proposes to fill the setback with extensive shrubs, trees, arborvitae and additional landscaping materials.

Additionally, the building design provides for residential window fenestrations above the commercial first floor, the architectural team has proposed a series of setbacks and insets to eliminate any commercial buildings from residential buildings future setback code requirements.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

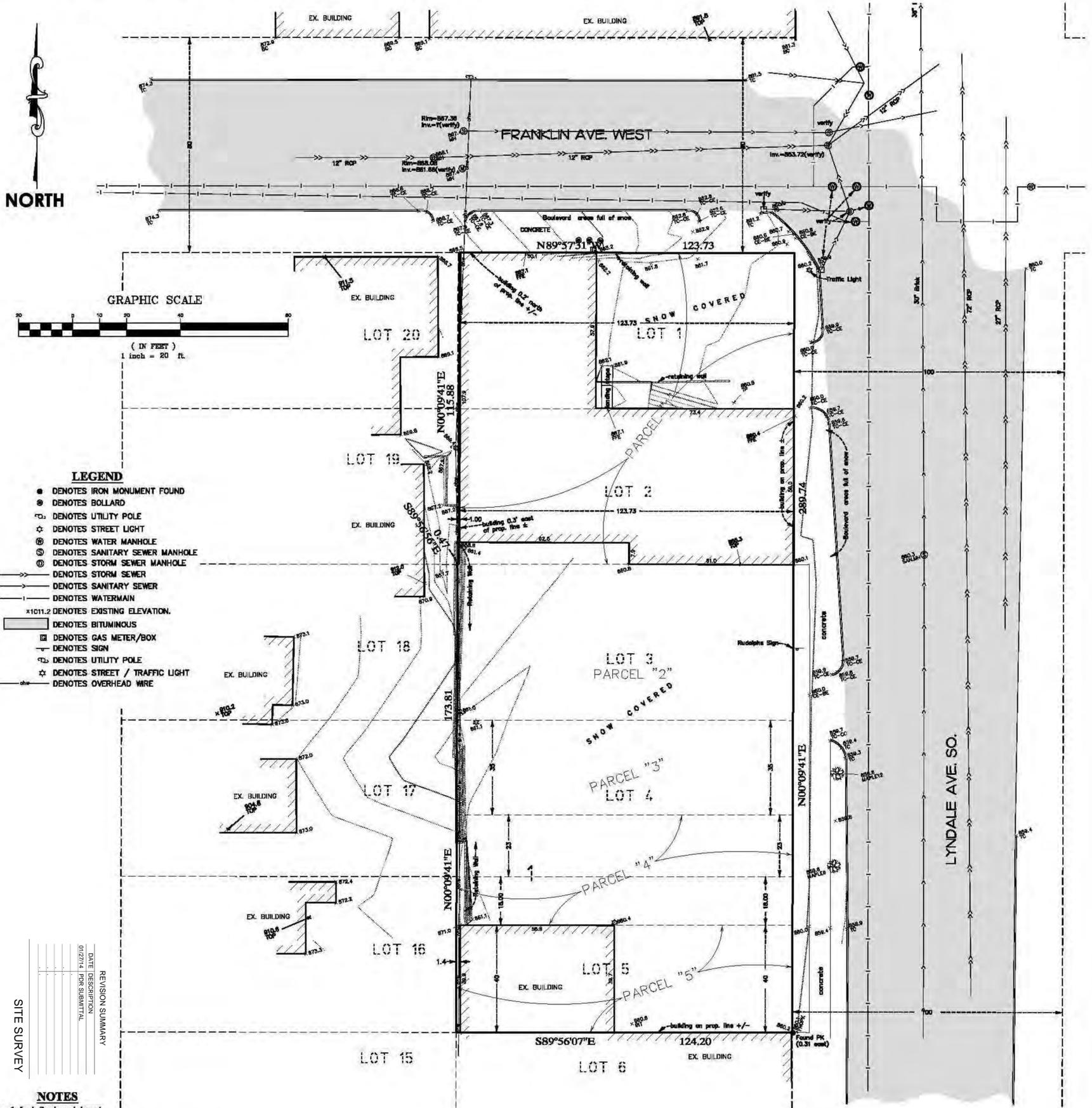
The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity nor will it be injurious or detrimental to the health, safety, or welfare of neither the general public nor those utilizing the property or nearby properties. As outlined above, Master plans to utilize the reduced setback by planting various shrubs, trees and other landscaping, in order to provide a buffer between the subject site and the adjacent sites. In keeping with the City's policies in the Minneapolis plan for sustainable growth, eliminating and minimizing "gaps" in the proposed building (commercial wall) design will eliminate "dark" or "dead" spots for pedestrians utilizing public sidewalks along these commercial corridors. The continuous wall will make for a more enjoyable and safe pedestrian experience as well as further foster vibrant commercial activity at the street level.



ALTA/ACSM LAND TITLE SURVEY

PROPERTY ADDRESS: #2400 - #2018 LYNDALE AVE. SO., MINNEAPOLIS, MN

~for~ 2004 Real Estate Company & Theros Realty II, LLC



LEGEND

- DENOTES IRON MONUMENT FOUND
- DENOTES BOLLARD
- DENOTES UTILITY POLE
- ⊙ DENOTES STREET LIGHT
- ⊕ DENOTES WATER MANHOLE
- ⊗ DENOTES SANITARY SEWER MANHOLE
- ⊖ DENOTES STORM SEWER MANHOLE
- DENOTES STORM SEWER
- DENOTES SANITARY SEWER
- DENOTES WATERMAIN
- x1011.2 DENOTES EXISTING ELEVATION.
- DENOTES BITUMINOUS
- DENOTES GAS METER/BOX
- DENOTES SIGN
- DENOTES UTILITY POLE
- ★ DENOTES STREET / TRAFFIC LIGHT
- DENOTES OVERHEAD WIRE

NOTES

- 1 foot Contour Interval
- N.G.V.D. Vertical Datum
- This survey was completed Dec. 18th, 2013, in the middle of the Minnesota snow/winter season. There may be improvements/features in addition to those shown hereon that were not located due to snow/ice cover.

BENCHMARK

BASIS FOR ELEVATION: NAVD83 (VIA REAL TIME GPS MEASUREMENTS UTILIZING MINNESOTA DEPARTMENT OF TRANSPORTATION VRS NETWORK)

BASIS FOR BEARINGS: NAD83 (1996) (VIA REAL TIME GPS MEASUREMENTS UTILIZING MINNESOTA DEPARTMENT OF TRANSPORTATION VRS NETWORK)

PROPERTY DESCRIPTION

- Parcel 1:
The east 123.73 feet of Lots 1 and 2, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#2004 Lyndale Ave. So., Minneapolis, MN
- Parcel 2:
Lot 3, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#2008 Lyndale Ave. So., Minneapolis, MN
- Parcel 3:
The north 35 feet, front and rear, of Lot 4, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#2012 Lyndale Ave. So., Minneapolis, MN
- Parcel 4:
The south 23 feet of Lot 4 and the north 18 feet of Lot 5, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#2014 Lyndale Ave. So., Minneapolis, MN
- Parcel 5:
The south 40 feet of Lot 5, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.
#2018 Lyndale Ave. So., Minneapolis, MN

All ABSTRACT property.

GENERAL NOTES

- Bearings shown hereon are on assumed datum.
- Boundary area of surveyed premises: 35,828± sq. ft. (0.82± acres)
- The surveyed premises has access to Lyndale Ave. So. & Franklin Ave. W., both publicly dedicated right-of-ways.
- Utilities shown hereon are observed. Excavations were not made during the process of this survey to locate underground utilities and/or structures. The location of underground utilities and/or structures may vary from locations shown hereon and underground utilities and/or structures may be encountered. Contact Gopher State One Call Notification Center at (651) 434-0022 for verification of utility type and field location prior to excavation.
- Subsurface and environmental conditions were not examined or considered during the process of this survey. No statement is made concerning the existence of underground or overhead containers or facilities that may affect the use or development of the surveyed property.
- This survey was completed 12/18/2013.
- There is no observable evidence of building construction or building additions within recent months. There is no observable evidence of site use as a solid waste dump, sump or sanitary landfill. There is no observable evidence of recent street or sidewalk repairs within recent months.
- Property Identification Number 33-028-24-11-0017, 33-028-24-11-0018, 33-028-24-11-0020, 33-028-24-11-0021 & 33-028-24-11-0022.
- Property FEMA flood map #2705300359E (unprinted map status).

I hereby certify to "2004 Real Estate Company & Theros Realty II, LLC" to their heirs, successors and assigns, that I have surveyed the property legally described hereon; that this survey is a true, correct and accurate drawing and representation of said property and the boundaries thereof; that this map or plat and the survey on which it is based were made in accordance with "Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys," as jointly established and adopted by ALTA and ACSM in 2011, and includes Items 2, 3, 4, 5, 7 (a), 8, 11 (a & b), 16 and 18 of Table A thereof; and meets the Accuracy Standards (as adopted by ALTA and ACSM and in effect on the date of this certification), and that in locating or identifying recorded easements or other recorded documents I have relied upon the Commitment for Title Insurance issued by Old Republic National Title Insurance Company, dated Dec. 4th, 2013 (commitment no. ORTE740790).

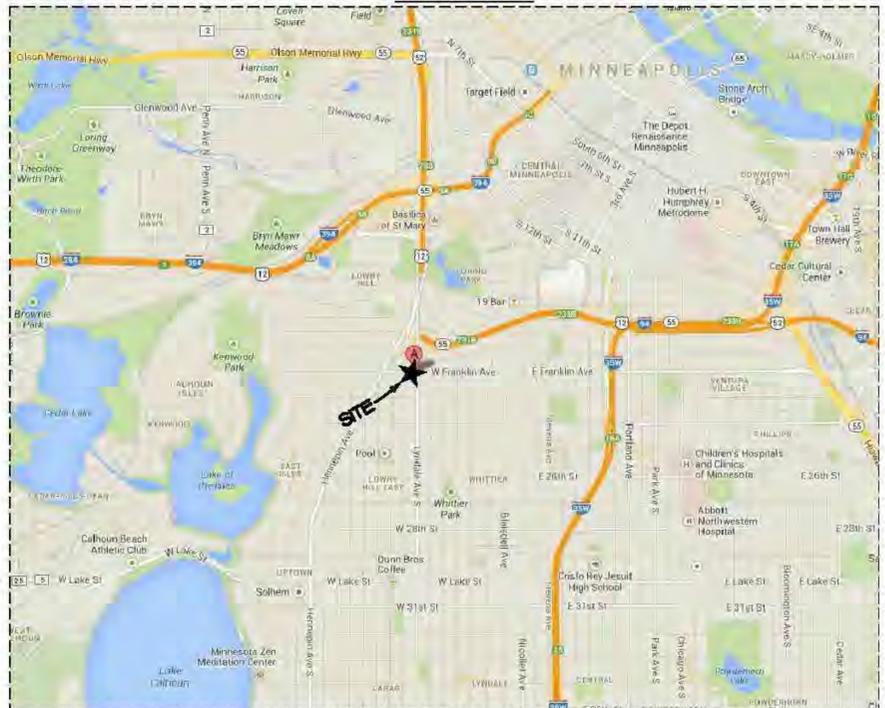
Dated January 3rd, 2014.

Eric R. Vickaryous, P.L.S.#44125

DLand Projects 2008\13838\mpls-civil\dwg\13838a1a.dwg 1/27/2014 10:05 AM CST

JOB #13838

VICINITY MAP



City of Minneapolis, Minnesota (No Scale)

NO.	DATE	DESCRIPTION	BY
1	01/07/14	revise utility	ERV
2	01/22/14	revise utility	ERV
3			

ACRE LAND SURVEYING
Serving Twin Cities Metro area and beyond
763-458-2997 acrelandsurveying@gmail.com

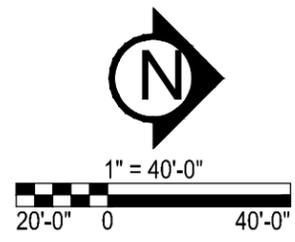
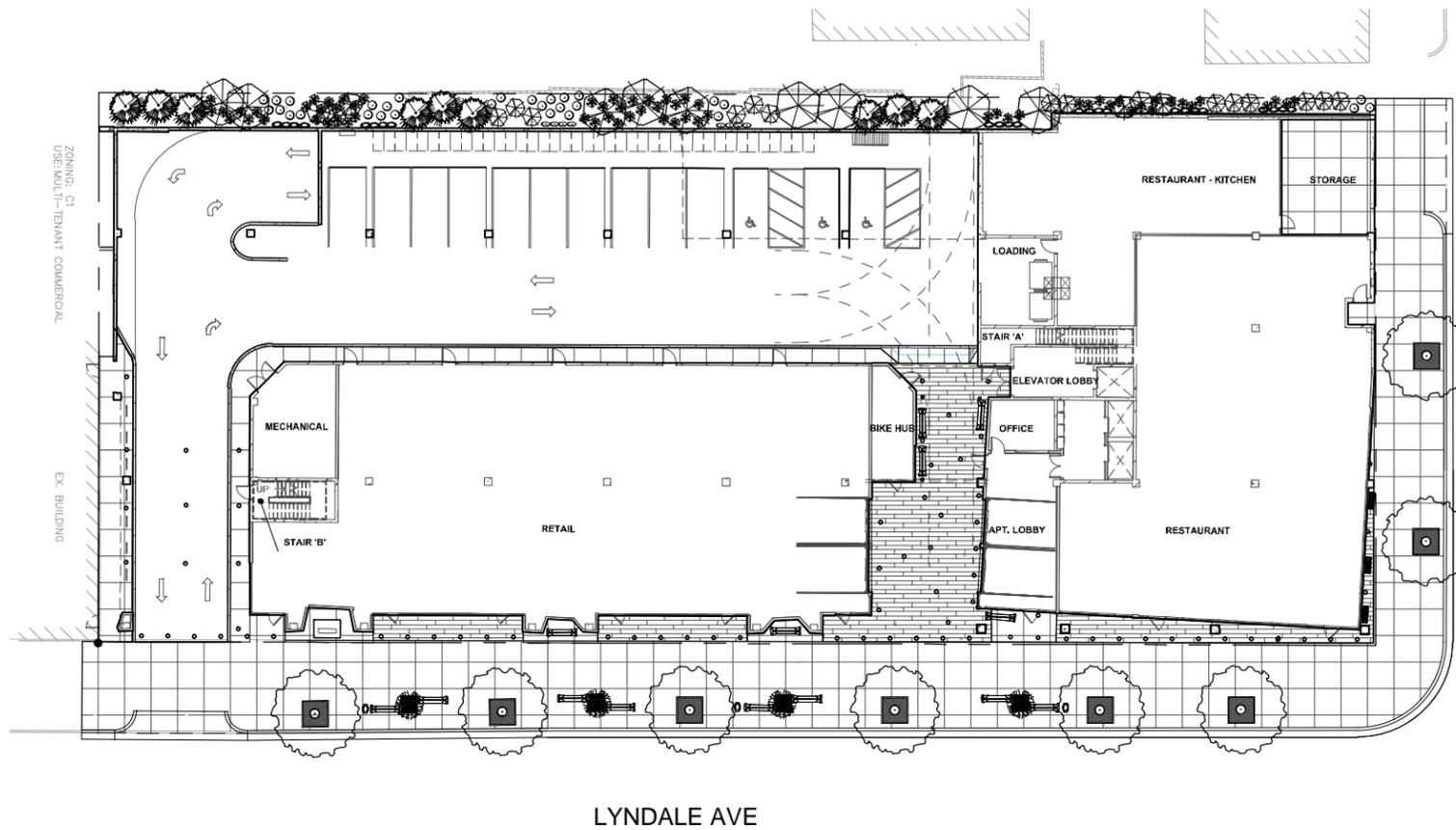
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SITE SURVEY

REVISION SUMMARY

DATE DESCRIPTION

01/27/14 FOR SUBMITTAL

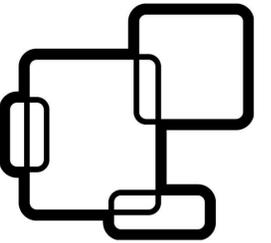


**Theater Garage
Marquee Apartments**
Minneapolis, MN
APR. 11. 2014



**TGMA
Developers LLC**

THEATER GARAGE



www.collagearch.com



MASTER DEVELOPMENT



THEATER GARAGE /
MARQUEE APARTMENTS

2004-2018 LYNDALE
AVENUE, MINNEAPOLIS

Theater Garage Marquee Apartments
Developers, LLC.
612.872.9200
1221 Nicollet Mall, Suite 310
Minneapolis, MN 55403

Collage | architects
Architects
Pete Keely
651.472.0050
705 Raymond Avenue, Suite #200
St. Paul, Minnesota 55114

Ericksen Roeds & Associates
651.251.7570
2550 University Avenue West, Suite #201
St. Paul, Minnesota 55114

CivilSITE Group
Patrick Sarver
952-250-2003
4931 W. 35th Street, Suite #200
St. Louis Park, Minnesota 55416

DATE: 07.10.2014
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION
OR REPORT WAS PREPARED BY ME OR UNDER MY
DIRECT SUPERVISION AND THAT I AM A DULY
LICENSED ARCHITECT UNDER THE LAWS OF THE
STATE OF MINNESOTA.

PETER KEELY, REG. NO.: 23570
DATE: 07.10.2014

LAND USE APPLICATION SET

REVISIONS:		
NO.	DATE	DESCRIPTION

PROJECT NO: 14_001
DRAWN BY: BGAO
CHK'D BY: Checker

SHEET TITLE

TITLE SHEET AND CODE REVIEW

T100

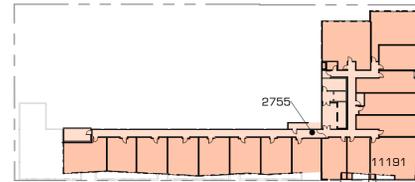


PROJECT LOCATION

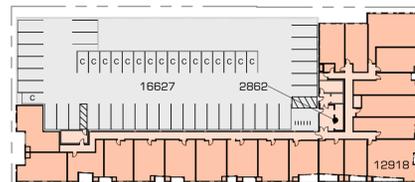
DRAWING INDEX			
Current Revision	ISSUED	#	SHEET NAME
	07/10/14	T100	TITLE SHEET AND CODE REVIEW
	07/10/14	C0.0	SURVEY
	07/10/14	C1.0	REMOVALS PLAN
	07/10/14	C2.0	SITE PLAN
	07/10/14	C3.0	GRADING & EROSION CONTROL PLAN
	07/10/14	C4.0	UTILITY PLAN
	07/10/14	C5.0	SITE DETAILS
	07/10/14	L1.0	LANDSCAPE PLAN
	07/10/14	AS100	SHADOW STUDIES
	07/10/14	AS101	LIGHTING PLAN
	07/10/14	A101	FIRST LEVEL
	07/07/14	A102	SECOND LEVEL
	07/10/14	A103	THIRD LEVEL
	07/10/14	A104	FOURTH LEVEL
	07/10/14	A105	FIFTH LEVEL
	07/10/14	A106	SIXTH LEVEL
	07/10/14	A107	ROOF PLAN
	07/10/14	A200	ELEVATIONS
	07/10/14	A200-A	COLORLED ELEVATIONS
	07/10/14	A201	ELEVATIONS
	07/10/14	A201-A	COLORLED ELEVATIONS



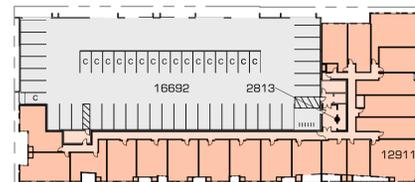
SIXTH LEVEL



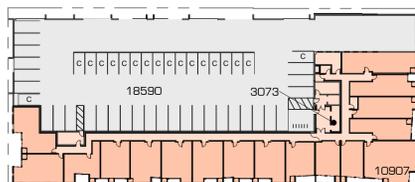
FIFTH LEVEL



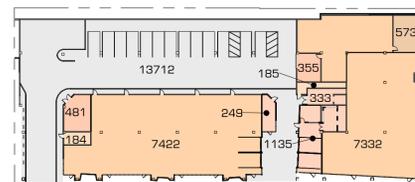
FOURTH LEVEL



THIRD LEVEL



SECOND LEVEL



FIRST LEVEL

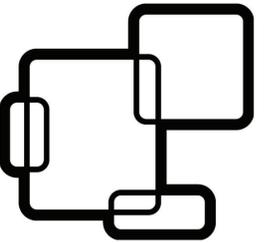
UNIT SCHEDULE			
Name	Area	BD	BA
UNIT 440 SF	1	1	
UNIT 459 SF	1	1	
UNIT 517 SF	1	1	
UNIT 538 SF	1	1	
UNIT 569 SF	1	1	
UNIT 591 SF	1	1	
UNIT 594 SF	2	1	
UNIT 623 SF	1	1	
UNIT 623 SF	1	1	
UNIT 627 SF	1	1	
UNIT 627 SF	1	1	
UNIT 631 SF	1	1	
UNIT 647 SF	1	1	
UNIT 691 SF	1	1	
UNIT 700 SF	1	1	
UNIT - TYPE 'A'	792 SF	1	1
UNIT 1023 SF	2	2	
SECOND LEVEL: 17			
UNIT 440 SF	1	1	
UNIT 459 SF	1	1	
UNIT 529 SF	1	1	
UNIT 530 SF	1	1	
UNIT 531 SF	1	1	
UNIT 553 SF	1	1	
UNIT 561 SF	1	1	
UNIT 591 SF	1	1	
UNIT 604 SF	1	1	
UNIT 619 SF	1	1	
UNIT 625 SF	1	1	
UNIT 627 SF	1	1	
UNIT 630 SF	1	1	
UNIT 636 SF	1	1	
UNIT 637 SF	1	1	
UNIT 639 SF	1	1	
UNIT 691 SF	1	1	
UNIT 700 SF	1	1	
UNIT 943 SF	2	2	
UNIT 1023 SF	2	2	
THIRD LEVEL: 20			

UNIT SCHEDULE			
Name	Area	BD	BA
UNIT 440 SF	1	1	
UNIT 459 SF	1	1	
UNIT 517 SF	1	1	
UNIT 531 SF	1	1	
UNIT 538 SF	1	1	
UNIT 553 SF	1	1	
UNIT 555 SF	1	1	
UNIT 591 SF	1	1	
UNIT 605 SF	1	1	
UNIT 611 SF	1	1	
UNIT 623 SF	1	1	
UNIT 623 SF	1	1	
UNIT 625 SF	1	1	
UNIT 640 SF	1	1	
UNIT 653 SF	1	1	
UNIT 657 SF	1	1	
UNIT 691 SF	1	1	
UNIT 700 SF	1	1	
UNIT 943 SF	2	2	
UNIT 1010 SF	2	2	
FOURTH LEVEL: 20			
UNIT 440 SF	1	1	
UNIT 506 SF	1	1	
UNIT 508 SF	1	1	
UNIT 527 SF	1	1	
UNIT 531 SF	1	1	
UNIT 553 SF	1	1	
UNIT 555 SF	1	1	
UNIT 558 SF	1	1	
UNIT 563 SF	1	1	
UNIT 565 SF	1	1	
UNIT 583 SF	1	1	
UNIT 583 SF	1	1	
UNIT 594 SF	1	1	
UNIT 633 SF	1	1	
UNIT 683 SF	1	1	
UNIT 943 SF	2	2	
UNIT 960 SF	2	2	
FIFTH LEVEL: 17			
UNIT 559 SF	1	1	
UNIT 569 SF	1	1	
UNIT 594 SF	1	1	
UNIT 633 SF	1	1	
UNIT 646 SF	1	1	
UNIT 933 SF	2	1	
UNIT 1028 SF	2	2	
UNIT 1029 SF	2	2	
SIXTH LEVEL: 8			
Grand total: 82			

AREA SCHEDULE - GROSS BUILDING				
Area	FAR Calc	Level	BIKE	Name
249 SF	249 SF	FIRST LEVEL		BIKE
355 SF	355 SF	FIRST LEVEL		LOADING
1467 SF	1467 SF	FIRST LEVEL		LOBBY
481 SF	481 SF	FIRST LEVEL		MECHANICAL
13712 SF	0 SF	FIRST LEVEL		PARKING
7332 SF	7332 SF	FIRST LEVEL		RESTURANT
7422 SF	7422 SF	FIRST LEVEL		RETAIL
369 SF	369 SF	FIRST LEVEL		STAIR
573 SF	573 SF	FIRST LEVEL		STORAGE
31962 SF	18250 SF			
18590 SF	0 SF	SECOND LEVEL		PARKING
10907 SF	10907 SF	SECOND LEVEL		RESIDENTIAL
3073 SF	3073 SF	SECOND LEVEL		UTILITY & CIRCULATION
32571 SF	13981 SF			
16692 SF	0 SF	THIRD LEVEL		PARKING
12911 SF	12911 SF	THIRD LEVEL		RESIDENTIAL
2813 SF	2813 SF	THIRD LEVEL		UTILITY & CIRCULATION
32416 SF	15724 SF			
16627 SF	0 SF	FOURTH LEVEL		PARKING
12918 SF	12918 SF	FOURTH LEVEL		RESIDENTIAL
2862 SF	2862 SF	FOURTH LEVEL		UTILITY & CIRCULATION
32408 SF	15780 SF			
11191 SF	11191 SF	FIFTH LEVEL		RESIDENTIAL
2755 SF	2755 SF	FIFTH LEVEL		UTILITY & CIRCULATION
13946 SF	13946 SF			
6242 SF	6242 SF	SIXTH LEVEL		RESIDENTIAL
1476 SF	1476 SF	SIXTH LEVEL		UTILITY & CIRCULATION
7718 SF	7718 SF			
151021 SF	85400 SF	GROSS FAR		

PARKING SCHEDULE			
Count	Level	Type	
1	FIRST LEVEL	ADA - 8'0 x 18' - 90 deg	
2	FIRST LEVEL	ADA - 8' x 18' (8' Aisle)	
10	FIRST LEVEL	Standard - 8'6 x 18' - 90 deg	
FIRST LEVEL: 13			
1	SECOND LEVEL	ADA - 8' x 18' (8' Aisle)	
18	SECOND LEVEL	Compact - 8' x 15'	
1	SECOND LEVEL	Standard - 8'6 x 18' - 90 Deg	
27	SECOND LEVEL	Standard - 8'6 x 18' - 90 deg	
SECOND LEVEL: 47			
1	THIRD LEVEL	ADA - 8' x 18' (8' Aisle)	
17	THIRD LEVEL	Compact - 8' x 15'	
1	THIRD LEVEL	Standard - 8'6 x 18' - 90 Deg	
30	THIRD LEVEL	Standard - 8'6 x 18' - 90 deg	
THIRD LEVEL: 49			
1	FOURTH LEVEL	ADA - 8' x 18' (8' Aisle)	
17	FOURTH LEVEL	Compact - 8' x 15'	
1	FOURTH LEVEL	Standard - 8'6 x 18' - 90 Deg	
32	FOURTH LEVEL	Standard - 8'6 x 18' - 90 deg	
FOURTH LEVEL: 51			
GRAND TOTAL: 160			

NOTE: THESE AREAS CALCULATIONS ARE PRELIMINARY ONLY. NOT INTENDED FOR LEGAL OR FINANCIAL PURPOSES



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THEATER GARAGE / MARQUEE APARTMENTS

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CivILSITE Group
Patrick Sarver
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4831 W. 33rd Street, Suite #200
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DATE: 07.10.2014
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PETER KEELY, REG. NO.: 23570

DATE: 07.10.2014

LAND USE APPLICATION SET

REVISIONS:

NO.	DATE	DESCRIPTION

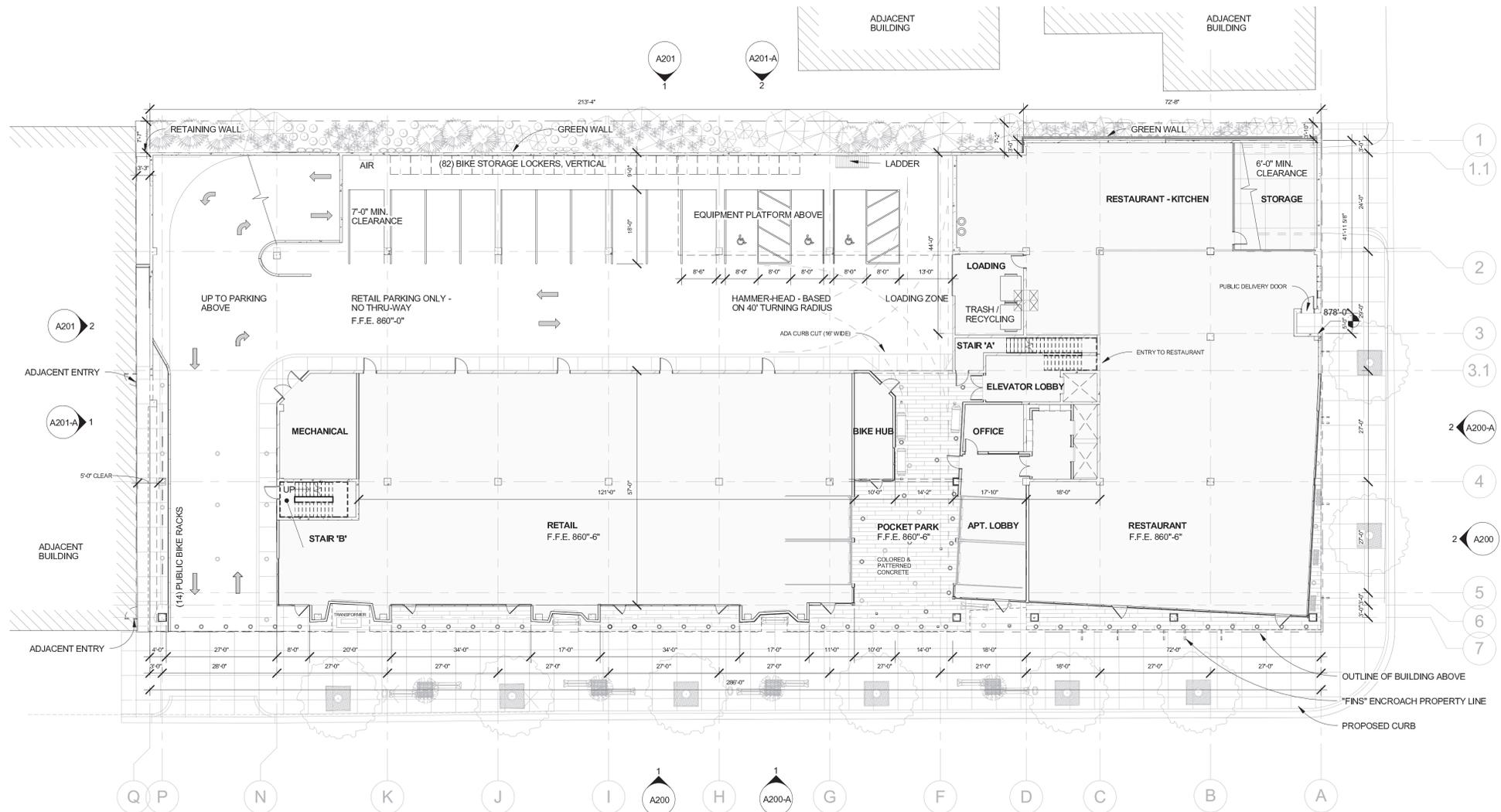
PROJECT NO: 14_001
DRAWN BY: BGAO
CHK'D BY: Checker
SHEET TITLE

FIRST LEVEL

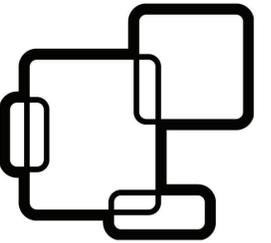
A101

GENERAL PLAN NOTES

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1 FIRST LEVEL
1/16" = 1'-0"



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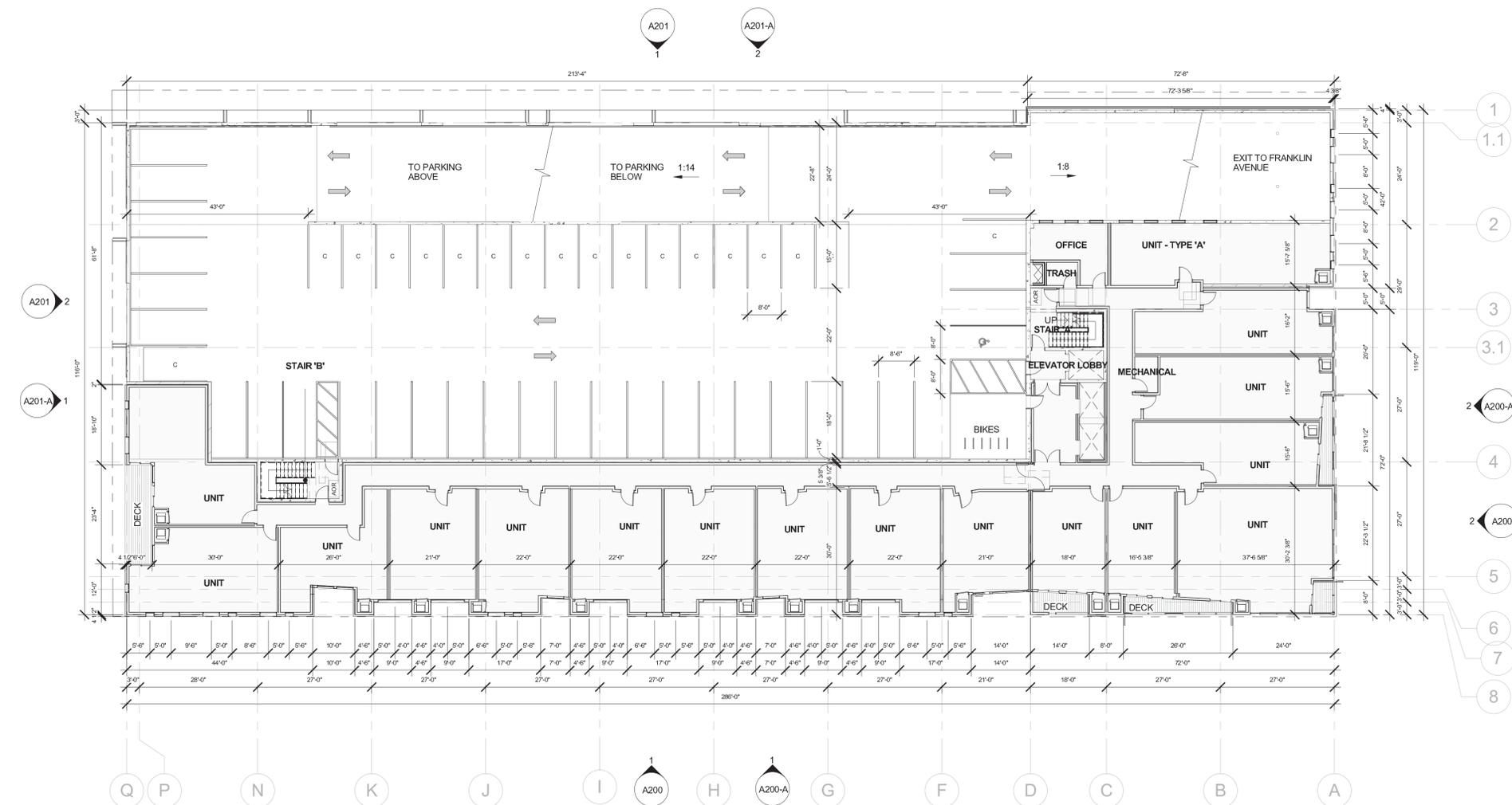
PROJECT NO: 14.001
DRAWN BY: BGAO
CHK'D BY: Checker
SHEET TITLE

SECOND LEVEL

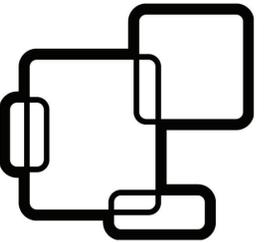
A102

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1 SECOND LEVEL
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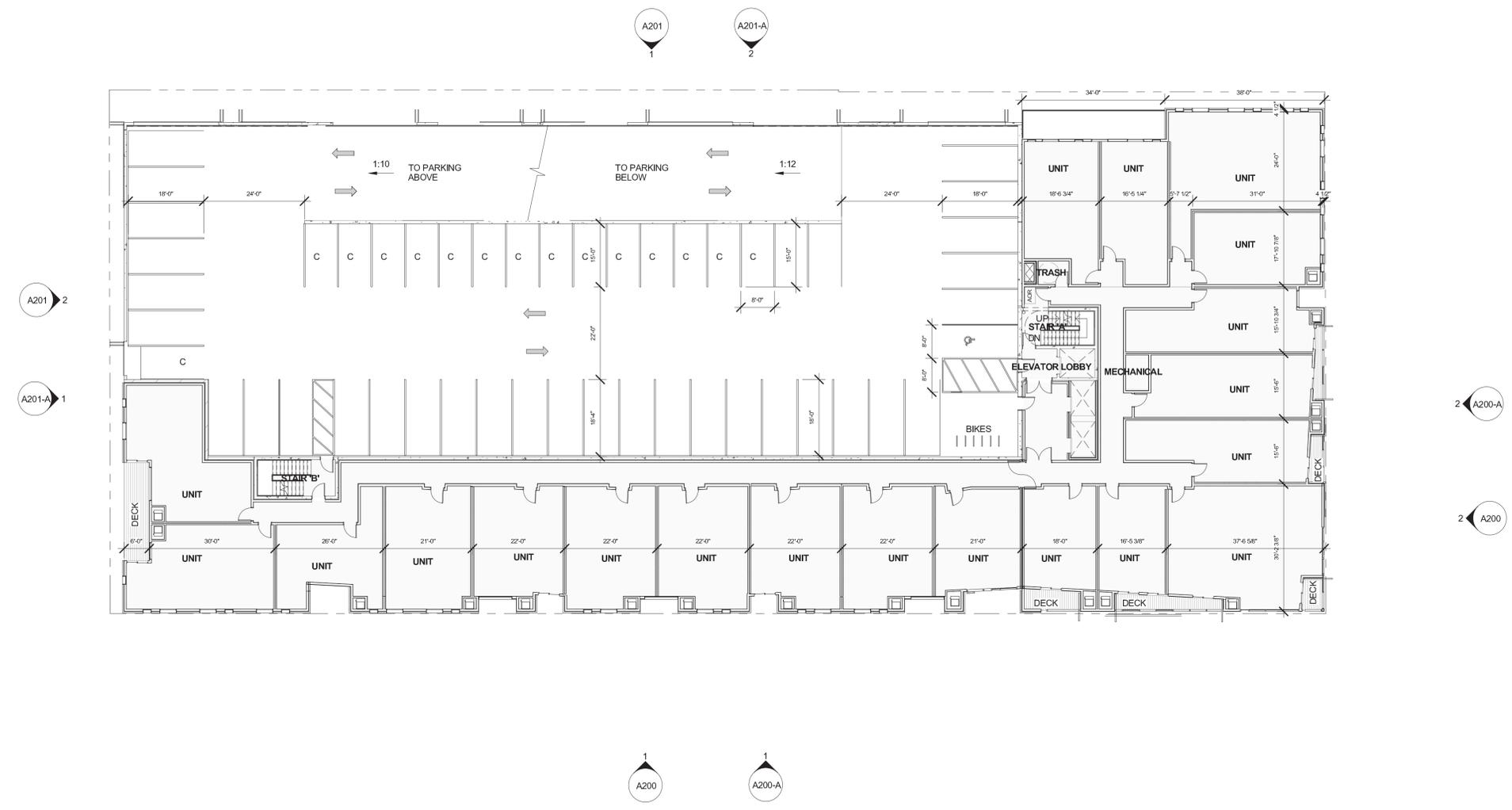
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DRAWN BY: BGAO
CHK'D BY: Checker
SHEET TITLE

THIRD LEVEL

A103

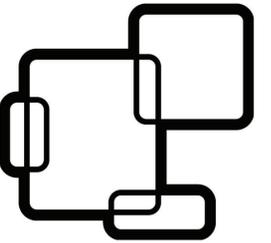
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1 THIRD LEVEL
1/16" = 1'-0"

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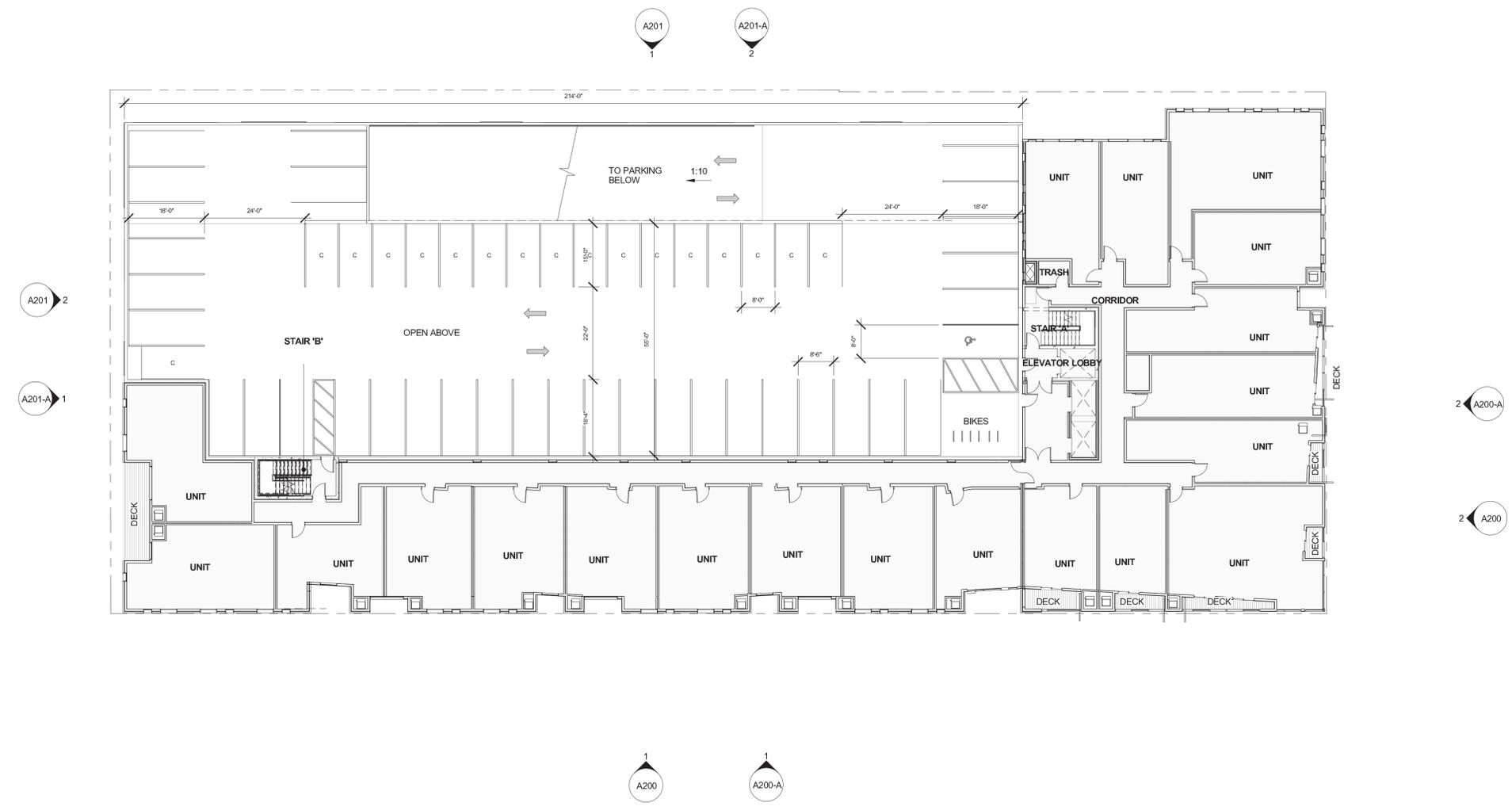
PROJECT NO: 14_001
DRAWN BY: BGAO
CHK'D BY: Checker
SHEET TITLE

FOURTH LEVEL

A104

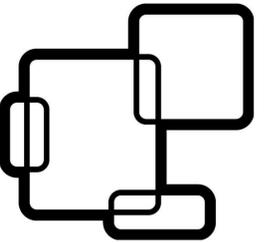
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1 FOURTH LEVEL
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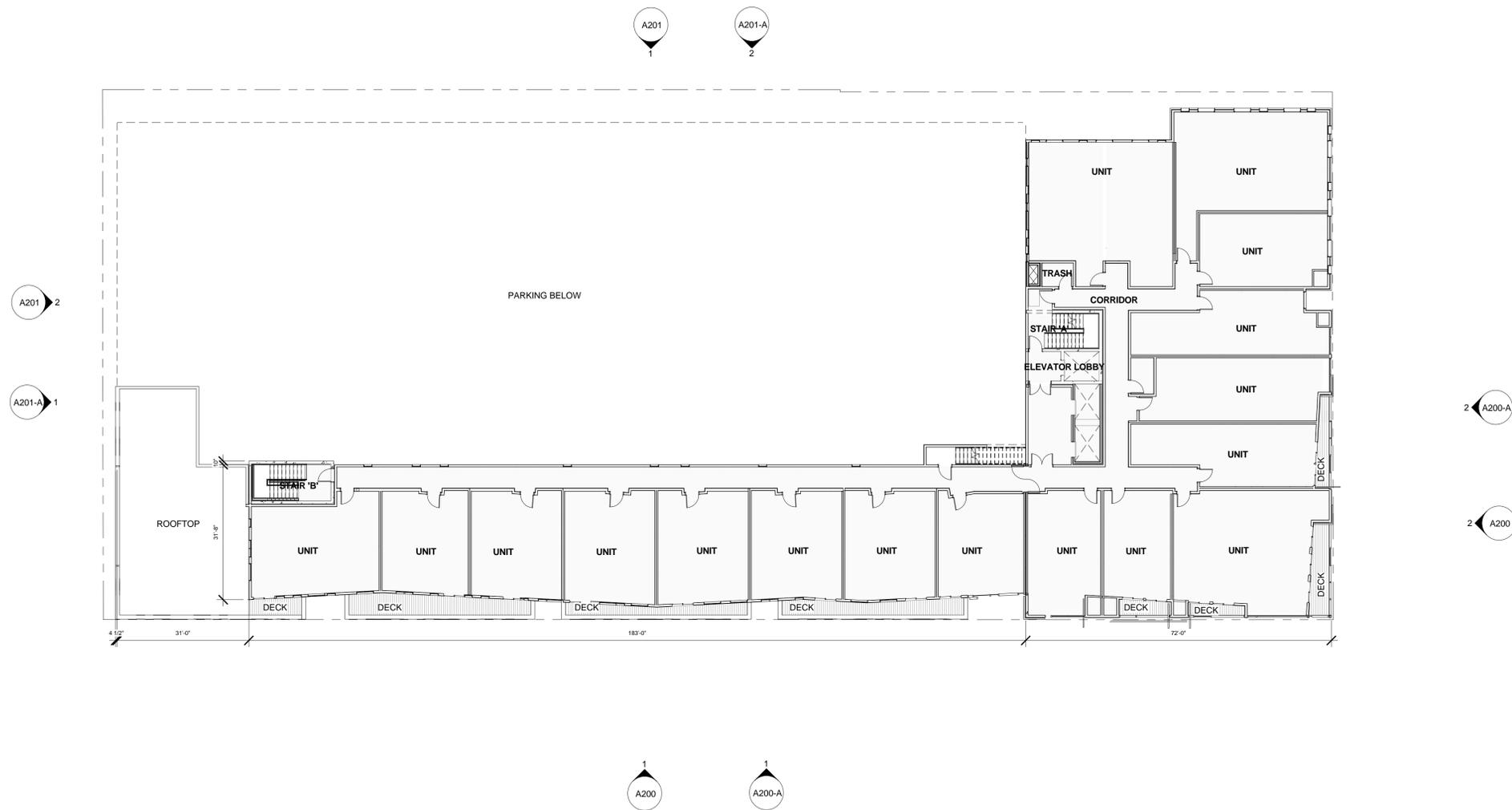
PROJECT NO: 14_001
DRAWN BY: BGAO
CHKD BY: Checker
SHEET TITLE

FIFTH LEVEL

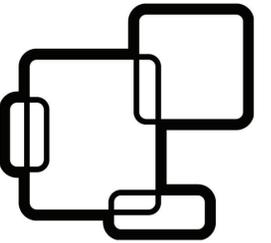
A105

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1 FIFTH LEVEL
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PROJECT NO: 14_001
DRAWN BY: Author
CHK'D BY: Checker

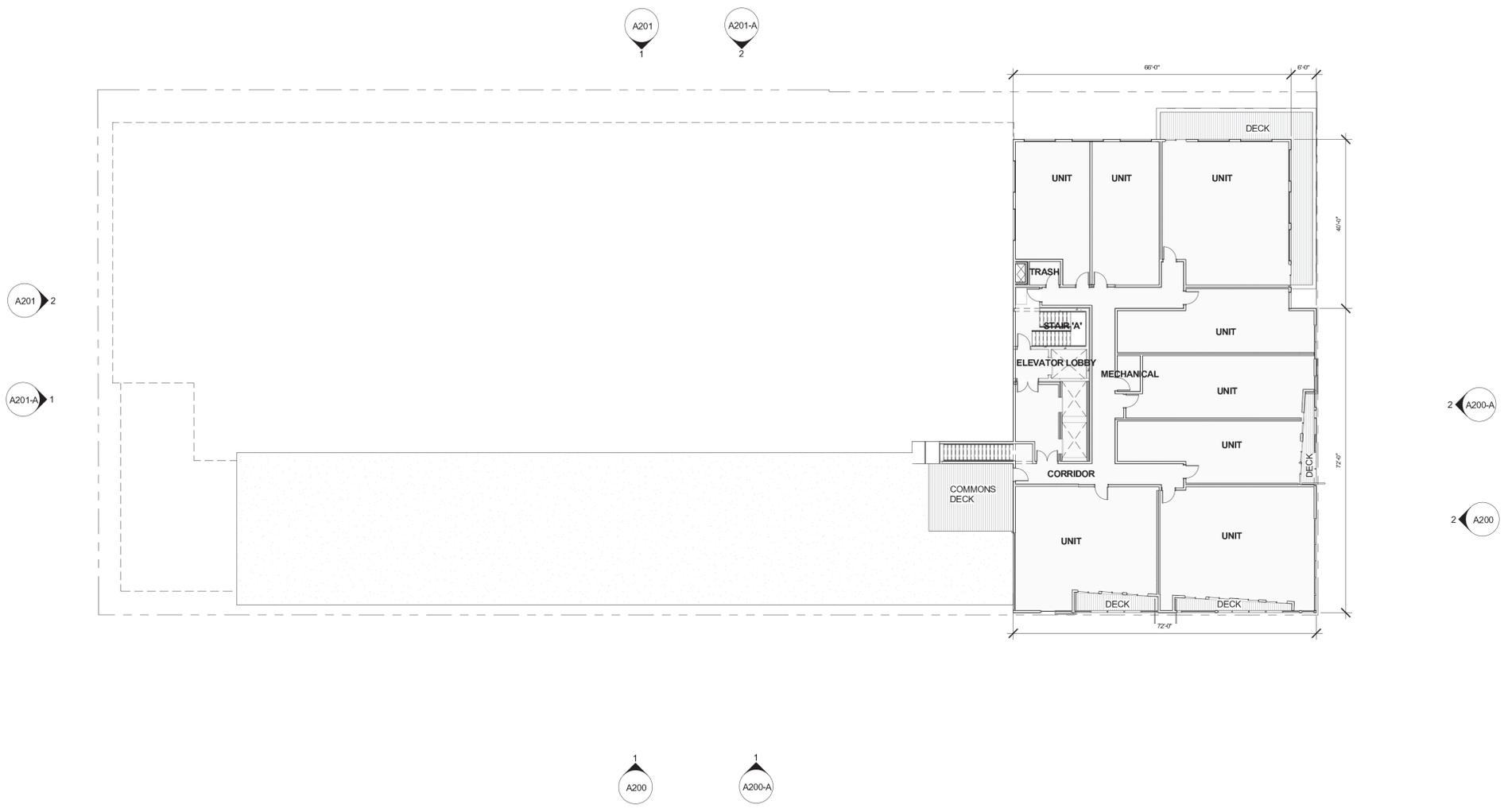
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SIXTH LEVEL

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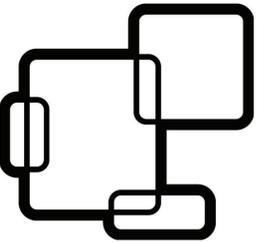
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3. IF DIMENSIONAL DISCREPANCIES OCCUR, NOTIFY THE ARCHITECT IMMEDIATELY.
4. VERIFY SLAB EDGE LOCATIONS, BRICK AND MASONRY FACE WITH ARCHITECTURAL AND STRUCTURAL DETAILS.
5. WINDOW SIZES ARE BASED ON A GENERIC MODULAR AND MUST BE ADAPTED TO ACTUAL MANUFACTURER SIZES.
6. PROVIDE ACOUSTH-MAT IN ALL AREAS NOT CARPETED. VERIFY CPT LOCATIONS, TYPICALLY ALL BEDROOMS AND BEDROOM CLOSETS.
7. SEE STRUCTURAL PLANS FOR BEAM, JOIST, HEADER LOCATIONS AND SIZING.
8. SEE SHEET T100 FOR WALL FIRE RATINGS AND LOCATIONS. THIS INCLUDES ALL PENETRATIONS THROUGH THESE ASSEMBLIES.
9. PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES.
10. PROVIDE WOOD BLOCKING BEHIND ALL WALL HUNG CASEWORK, MILLWORK, SHELVING, AND FIXTURES AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
11. GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, DUCTWORK, ETC. NOTIFY ARCHITECT OF ANY NECESSARY CHANGES.
12. SEE G-100 FOR ALL WALL TYPES AND ASSEMBLIES.
13. NOT IN CONTRACT INDICATED BY DASHED LINE INCLUDES: WITHIN UNIT DOORS AND FRAMS, BATHROOM CASEWORK, FIXTURES & ACCESSORIES, KITCHEN CASEWORK, APPLIANCES, LAUNDRY FIXTURES, UNIT FINISHES AND ROOF PAVER SYSTEM.



1 SIXTH LEVEL
1/16" = 1'-0"

7/9/2014 2:29:22 PM



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MASTER DEVELOPMENT



THEATER GARAGE / MARQUEE APARTMENTS

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CivilSITE Group
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PETER KEELY, REG. NO.: 23570

DATE: 07.10.2014

LAND USE APPLICATION SET

REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO: 14_001

DRAWN BY: BGAO

CHK'D BY: Checker

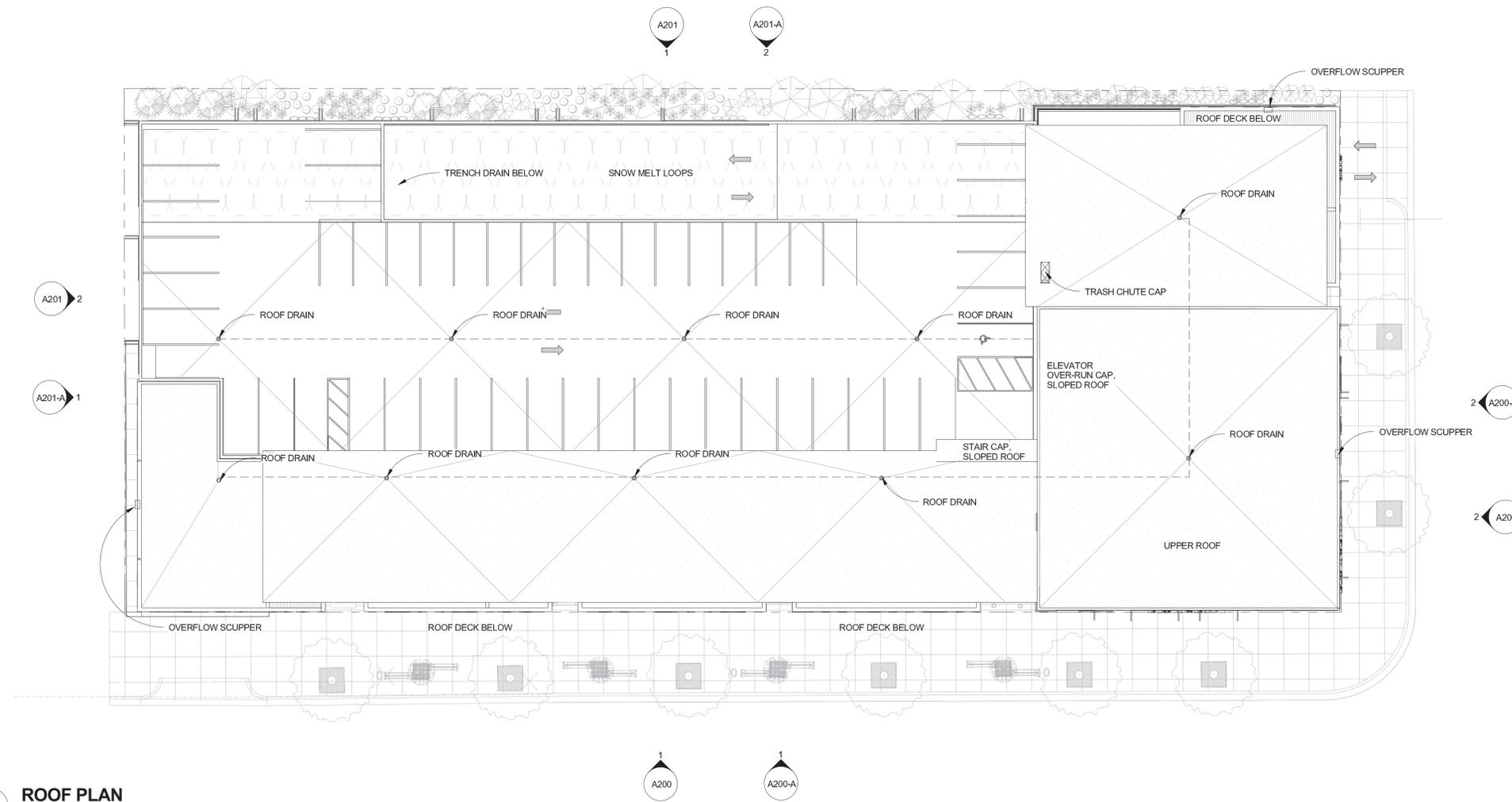
SHEET TITLE

ROOF PLAN

A107

GENERAL PLAN NOTES

1. ALL PLAN DIMENSIONS ARE NOMINAL TO FACE OF EXTERIOR SHEATHING OR CONCRETE BLOCK AT EXTERIOR WALLS AND CENTERLINE OF WALL ASSEMBLY AT INTERIOR WALLS. UNLESS NOTED OTHERWISE, SEE PARTITION SCHEDULE FOR ACTUAL PARTITIONS. WHERE CLEAR DIMENSIONS NOTED, THESE MUST BE MAINTAINED.
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1 ROOF PLAN
1/16" = 1'-0"

7/8/2014 1:31:19PM



ANOTHER PROJECT BY:

Master Properties Minnesota LLC
Elden Building North Loop Gateway
201 W Broadway Avenue
Minneapolis, MN 55411
P: 612-872-9200 F: 612-872-9201
www.masterpropertiesmn.com

THEATER GARAGE
2004-2018 LYNDALE AVE S, MINNEAPOLIS, MINNESOTA
TGMA DEVELOPERS LLC
1221 NICOLLET MALL, SUITE 310, MINNEAPOLIS, MN 55403

City of Minneapolis Standard Erosion Control Notes



City of Minneapolis Standard Erosion and Sediment Control Notes for Plans
These notes may be used as performance standards or requirements for projects not meeting the threshold for a formal plan.

- Contractor must call a construction start 48 hours prior to any land disturbances 612-673-3867. Failure to do so may result in fines, the revocation of permit and a stop work order being issued.
- Install perimeter erosion control at the locations shown on the plans prior to the commencement of any land disturbance or construction activities.
- Before beginning construction, install a temporary rock sediment entrance at each point where vehicles exit the construction site. Use 2 inch or greater diameter rock in a layer at least 6 inches thick across the entire width of the entrance. Extend the rock entrance at least 50 feet into the construction zone using a geo-textile fabric beneath the aggregate to prevent migration of soil into the rock from below.
- Remove all soils and sediments tracked or otherwise deposited onto public and private pavement areas. Removal shall be on a daily basis when tracking occurs and may be ordered by Minneapolis inspectors at any time if conditions warrant. Sweeping shall be maintained throughout the duration of the construction and done in a manner to prevent dust being blown to adjacent properties.
- Install inlet protection at all public and private catch basin inlets, which receive runoff from the disturbed area. Contractor shall clean, remove sediment or replace storm drain inlet protection devices on a routine basis such that the devices are fully functional for the next rain event. Sediment deposited in and/or plugging drainage systems is the responsibility of the contractor. Hay bales or filter fabric wrapped grates are not allowed for inlet protection.
- Locate soil or dirt stockpiles no less than 25 feet from any public or private roadway or drainage channel. If remaining for more than seven days, stabilize the stockpiles by mulching, vegetative cover, tarps, or other means. Control erosion from all stockpiles by placing silt barriers around the piles. Temporary stockpiles located on paved surfaces must be no less than two feet from the drainage/gutter line and shall be covered if left more than 24 hours.
- Maintain all temporary erosion and sediment control devices in place until the contributing drainage area has been stabilized. Inspect temporary erosion and sediment control devices on a daily basis and replace deteriorated, damaged, or rotated erosion control devices immediately.
- Temporarily or permanently stabilize all construction areas which have undergone final grading, and all areas in which grading or site building construction operations are not actively underway against erosion due to rain, wind and running water within 7-14 days. Use seed and mulch, erosion control matting, and/or sodding and staking in green space areas. Remove all temporary synthetic, structural, non-biodegradable erosion and sediment control devices after the site has undergone final stabilization with permanent vegetation establishment. Final stabilization for purposes of this removal is 70% established cover over denuded area.
- Ready mixed concrete and concrete batch/mix plants are prohibited within the public right of way. All concrete related production, cleaning and mixing activities shall be done in the designated concrete mixing/washout locations as shown in the erosion control plan. Under no circumstance may washout water drain onto the public right of way or into any public or private storm drain conveyance.
- Changes to approved erosion control plan must be approved by the erosion control inspector prior to implementation. Contractor to provide installation and details for all proposed alternate type devices.
- If dewatering or pumping of water is necessary, the contractor is responsible for obtaining any necessary permits and/or approvals prior to discharge of any water from the site. If the discharge from the dewatering or pumping process is turbid or contains sediment laden water, it must be treated through the use of sediment traps, vegetative filter strips, or other sediment reducing measures such that the discharge is not visibly different from the receiving water. Additional erosion control measures may be required at the discharge point to prevent scour erosion. The contractor shall provide a dewatering/pumping plan to the Erosion Control Inspector prior to initiating dewatering activities.

PREPARED BY: THE CITY OF MINNEAPOLIS PUBLIC WORKS SURFACE WATER AND SEWERS WWW.CIVILSITEGROUP.COM/FILES/STANDARDNOTES
JANUARY 7, 2011

SUPPLEMENTARY REMOVALS EROSION CONTROL NOTES:

PROJECT NARRATIVE:
PROJECT IS A REDEVELOPMENT OF AN EXISTING COMMERCIAL SITE, INCLUDING THE REMOVAL OF EXISTING BUILDINGS AND INTRODUCTION OF A NEW MULTI-USE COMMERCIAL/RESIDENTIAL BUILDING. PUBLIC ROOF GARDEN PARK SHALL BE PROVIDED WITH DIRECT ACCESS FROM STREET LEVEL. PUBLIC PARKING FACILITY IS PART OF THE DEVELOPMENT.

CONSTRUCTION REMOVALS SEQUENCING IS PLANNED AS FOLLOWS:

- INSTALL SILT FENCE/CONSTRUCTION FENCE AROUND SITE, & INLET PROTECTION IN AND AROUND CATCH BASINS WHERE SITE RUNOFF IS FLOWING.
- REMOVE EXISTING DEBRIS
- PREP AND TRANSITION TO NEW CONSTRUCTION ACTIVITIES

REMOVALS DISTURBANCE LENGTH OF TIME:

THE REMOVALS TIMING FOR THIS PROJECT IS EXPECTED TO LAST 30 DAYS UNTIL THE START OF NEW CONSTRUCTION.

TEMPORARY AND PERMANENT STABILIZATION:

SITE WILL BE ENTIRELY HARD-SCAPED AND STABILIZED WITH THOSE ELEMENTS.

SEE GRADING & EROSION CONTROL PLAN (SHEET C3.0) FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN AFTER DEMOLITION AND REMOVALS ARE COMPLETE.

REMOVALS LEGEND:

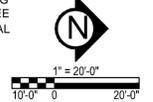
- EX. 1' CONTOUR ELEVATION INTERVAL
- COMBINATION BIO-ROLL, CHAIN LINK CONSTRUCTION FENCE
- STABILIZED CONSTRUCTION ENTRANCE
- INLET PROTECTION - AT ALL EXIST. INLETS
- TREE REMOVAL - INCLUDING ROOTS AND STUMPS
- TREE PROTECTION
- REMOVE BUILDING/STRUCTURE, INCLUDING ALL FOOTINGS FOUNDATIONS AND APPURTANANCES
- REMOVE EXIST. PVMT., INCLUDING ALL BITUMINOUS, CONCRETE AND GRAVEL MATERIAL, ALL BASE MATERIALS, AND ALL PVMT. AND PARKING LOT APPURTANANCES (SIGNS, BOLLARDS, ETC)
- REMOVE EXIST. PVMT. IN PUBLIC RIGHT-OF-WAY, INCLUDING ALL BITUMINOUS, CONCRETE AND GRAVEL MATERIAL, ALL BASE MATERIALS, AND ALL PUBLIC APPURTANANCES (SIGNS, BOLLARDS, ETC), SALVAGE ALL MATERIAL AND COORDINATE REMOVALS WITH CITY.

OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

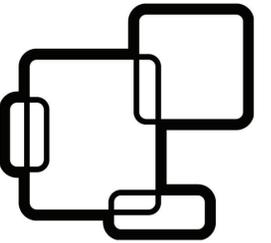
OWNER: _____
DEVELOPER: _____
CONTRACTOR: _____

GOPHER STATE ONE CALL
WWW.GOPHERSTATEONECALL.ORG
(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL



REMOVALS PLAN

C1.0



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MASTER DEVELOPMENT



THEATER GARAGE / MARQUEE APARTMENTS

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PETER KEELY, REG. NO.: 23570

DATE: 07.10.2014

LAND USE APPLICATION SET

REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO: 14_001
DRAWN BY: BGAO
CHK'D BY: PK
SHEET TITLE

LIGHTING PLAN

AS101

GENERAL LIGHTING PLAN NOTES

DISCLAIMER: EXISTING LIGHTING IS UNKNOWN AT THIS TIME. THEREFORE THIS LIGHTING PLAN IS ONLY FOR CONCEPT TO SHOW APPROXIMATE LOCATIONS AND TYPE OF LIGHTING.

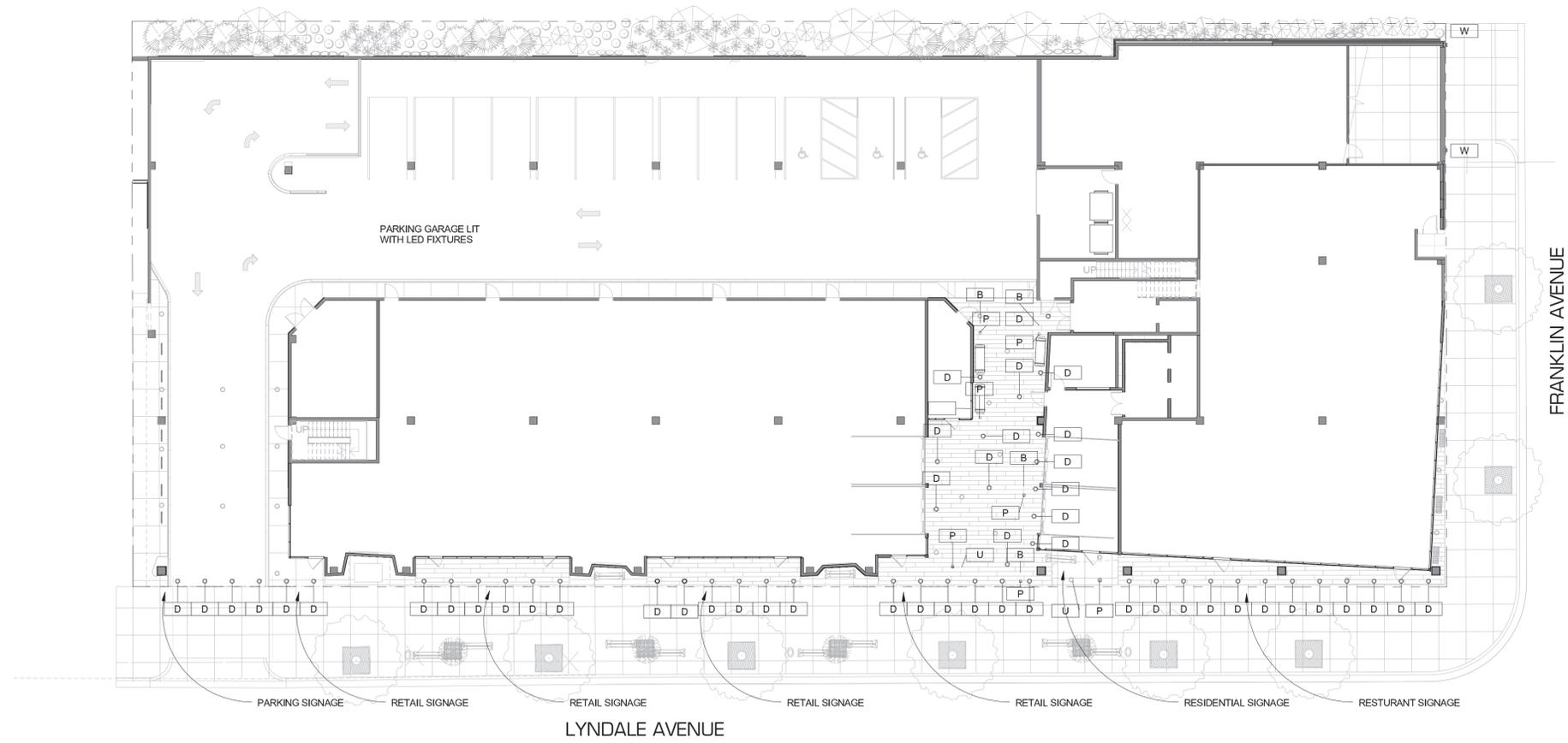
THE FIXTURES IDENTIFIED IN THIS DOCUMENT ARE FOR CONCEPT AND INTENT ONLY AND ARE NOT THE FINAL FIXTURE TO BE USED.

RETAIL SIGNAGE AND BUILDING BLADE SIGNS ARE UNKNOWN AT THIS TIME.

THE MARQUEE LIGHTING ELEMENT FOR THE THEATER IS UNKNOWN AT THIS TIME.



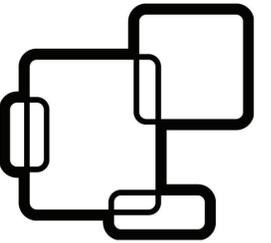
LIGHT FIXTURES - CONCEPT ONLY



1 LIGHTING PLAN
1/16" = 1'-0"



7/9/2014 1:32:01 PM



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REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO: 14_001
DRAWN BY: Author
CHK'D BY: Checker

SHEET TITLE

ELEVATIONS

A200

GENERAL ELEVATION NOTES

1. ALL MATERIALS AT FACE TO RETURN AT CORNER. TERMINATE MATERIALS AT INSIDE CORNERS, UNLESS NOTED OTHERWISE.
2. ALL PLAN DIMENSIONS AT WINDOWS ARE TO THE CENTERLINE, UNLESS OTHERWISE NOTED. VERIFY ALL WINDOW ROUGH OPENING DIMENSIONS WITH MANUFACTURER.
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MATERIALS INDEX

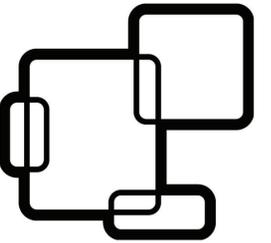
- 1 FACE BRICK - NORMAN BRICK, COLOR TBD
- 2 PREFINISHED METAL PANEL - COLOR TBD
- 3 STONE SILL - SMOOTH FACE KASOTA STONE
- 4 STONE PANEL - STOREFRONT BASE
- 5 PRECAST WALL PANEL - COLOR AND TEXTURE TBD
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- 21 VINYL WINDOWS
- 22 PREFINISHED METAL SCUPPER
- 23 COMPOSITE WOOD DECORATIVE SOFFIT / "LINER" - COLOR TBD



2 NORTH ELEVATION
1/16" = 1'-0"



1 EAST ELEVATION
1/16" = 1'-0"



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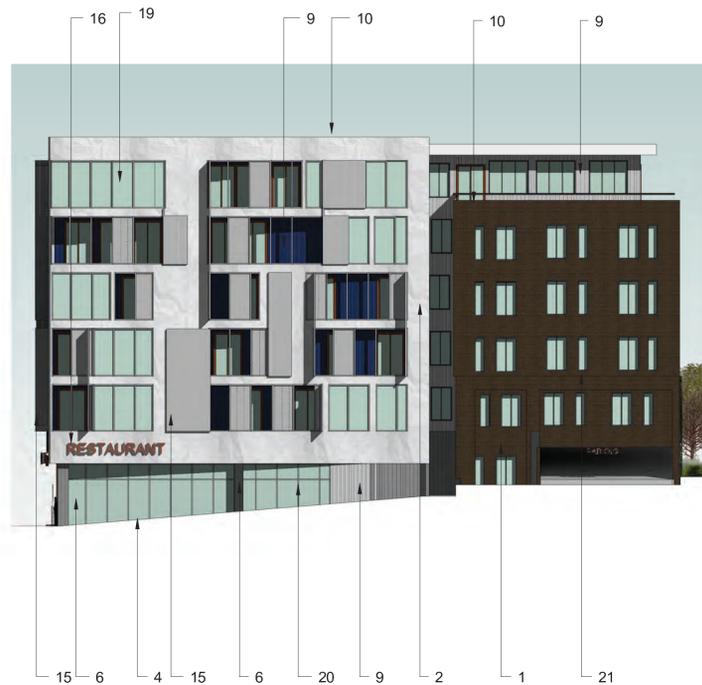
PROJECT NO: 14_001
DRAWN BY: BGAO
CHK'D BY: Checker
SHEET TITLE

COLORED ELEVATIONS

A200-A

MATERIALS INDEX

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2 NORTH ELEVATION
1/16" = 1'-0"

MATERIAL CALCULATION - EAST

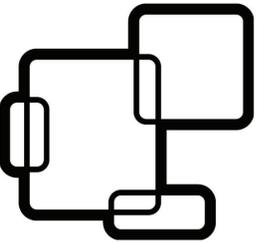
Orientation	Material: Name	Material: Area	Percent
East	Exterior - Brick	6076 SF	27.4%
East	Exterior - Glass	6315 SF	28.4%
East	Exterior - Metal Panel Type 1	4590 SF	20.7%
East	Exterior - Metal Panel Type 2A	4067 SF	18.3%
East	Exterior - Metal Panel, Blue	1155 SF	5.2%
East: 308		22205 SF	

MATERIAL CALCULATION - NORTH

Orientation	Material: Name	Material: Area	Percent
North	Exterior - Brick	1589 SF	17.7%
North	Exterior - Glass	2627 SF	29.3%
North	Exterior - Metal Panel Type 1	3369 SF	37.5%
North	Exterior - Metal Panel Type 2	119 SF	1.3%
North	Exterior - Metal Panel Type 2A	452 SF	5.0%
North	Exterior - Metal Panel, Blue	618 SF	9.1%
North: 143		8974 SF	



1 EAST ELEVATION
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PETER KEELY, REG. NO.: 23570

DATE: 07.10.2014

**LAND USE
APPLICATION SET**

REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO: 14_001
DRAWN BY: Author
CHK'D BY: Checker

SHEET TITLE

ELEVATIONS

A201

GENERAL ELEVATION NOTES

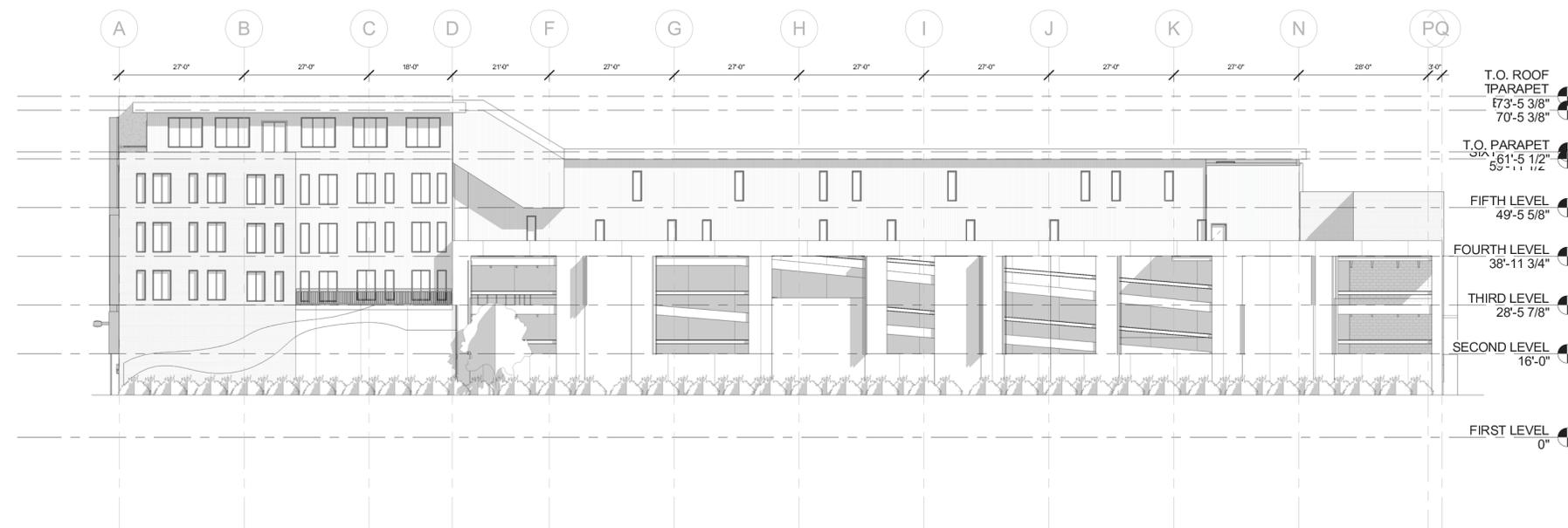
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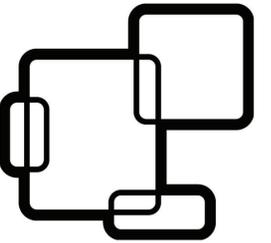
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- 17 BENT METAL GUARDRAIL/CABLE - COLOR TBD
- 18 PHOTOVOLTAIC METAL CANOPY - COLOR TBD (NOT USED)
- 19 RESIDENTIAL ALUMINUM GLAZING SYSTEM WITH METAL INFILL PANEL
- 20 RETAIL/COMMERCIAL ENTRY ALUMINUM STOREFRONT SYSTEM
- 21 VINYL WINDOWS
- 22 PREFINISHED METAL SOUPPER
- 23 COMPOSITE WOOD DECORATIVE SOFFIT / "LINER" - COLOR TBD



2 SOUTH ELEVATION
1/16" = 1'-0"



1 WEST ELEVATION
1/16" = 1'-0"



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MASTER DEVELOPMENT



THEATER GARAGE /
MARQUEE APARTMENTS

2004-2018 LYNDALE
AVENUE, MINNEAPOLIS

Theater Garage Marquee Apartments
Developers, LLC.
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1221 Nicollet Mall, Suite 310
Minneapolis, MN 55403

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CivilSITE Group
Patrick Sarver
952-250-2003
4931 W. 33rd Street, Suite #200
St. Louis Park, Minnesota 55416

DATE: 07.10.2014
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION
OR REPORT WAS PREPARED BY ME OR UNDER MY
DIRECT SUPERVISION AND THAT I AM A DULY
LICENSED ARCHITECT UNDER THE LAWS OF THE
STATE OF MINNESOTA.

PETER KEELY, REG. NO.: 23570
DATE: 07.10.2014

**LAND USE
APPLICATION SET**

REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO: 14_001
DRAWN BY: BGAO
CHK'D BY: Checker
SHEET TITLE

**COLORED
ELEVATIONS**

A201-A

MATERIALS INDEX

- 1 FACE BRICK - NORMAN BRICK, COLOR TBD
- 2 PREFINISHED METAL PANEL - COLOR TBD
- 3 STONE SILL - SMOOTH FACE KASOTA STONE
- 4 STONE PANEL - STOREFRONT BASE
- 5 PRECAST WALL PANEL - COLOR AND TEXTURE TBD
- 6 WRAPPED PRECAST CONCRETE PIER
- 7 FIBER CEMENT BOARD SIDING - COLOR TBD (NOT USED)
- 8 SPECIAL METAL WALL PANEL - COLOR TBD
- 9 PREFINISHED METAL PANEL - COLOR TBD
- 10 PREFINISHED METAL CORNICE/CAP FLASHING - COLOR TO MATCH
- 11 METAL GROWING MESH
- 12 CONTROL JOINT - COLOR TBD
- 13 EXPANSION JOIST - COLOR TBD
- 14 PAINTED STEEL SIGNAGE BAND - COLOR TBD
- 15 PREFINISHED METAL MESH FIN - COLOR TBD
- 16 ILLUMINATED SIGNAGE - DESIGN TBD
- 17 BENT METAL GUARDRAIL/CABLE - COLOR TBD
- 18 PHOTOVOLTAIC METAL CANOPY - COLOR TBD (NOT USED)
- 19 RESIDENTIAL ALUMINUM GLAZING SYSTEM WITH METAL INFILL PANEL
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- 21 VINYL WINDOWS
- 22 PREFINISHED METAL SCUPPER
- 23 COMPOSITE WOOD DECORATIVE SOFFIT / "LINER" - COLOR TBD

MATERIAL CALCULATION - WEST

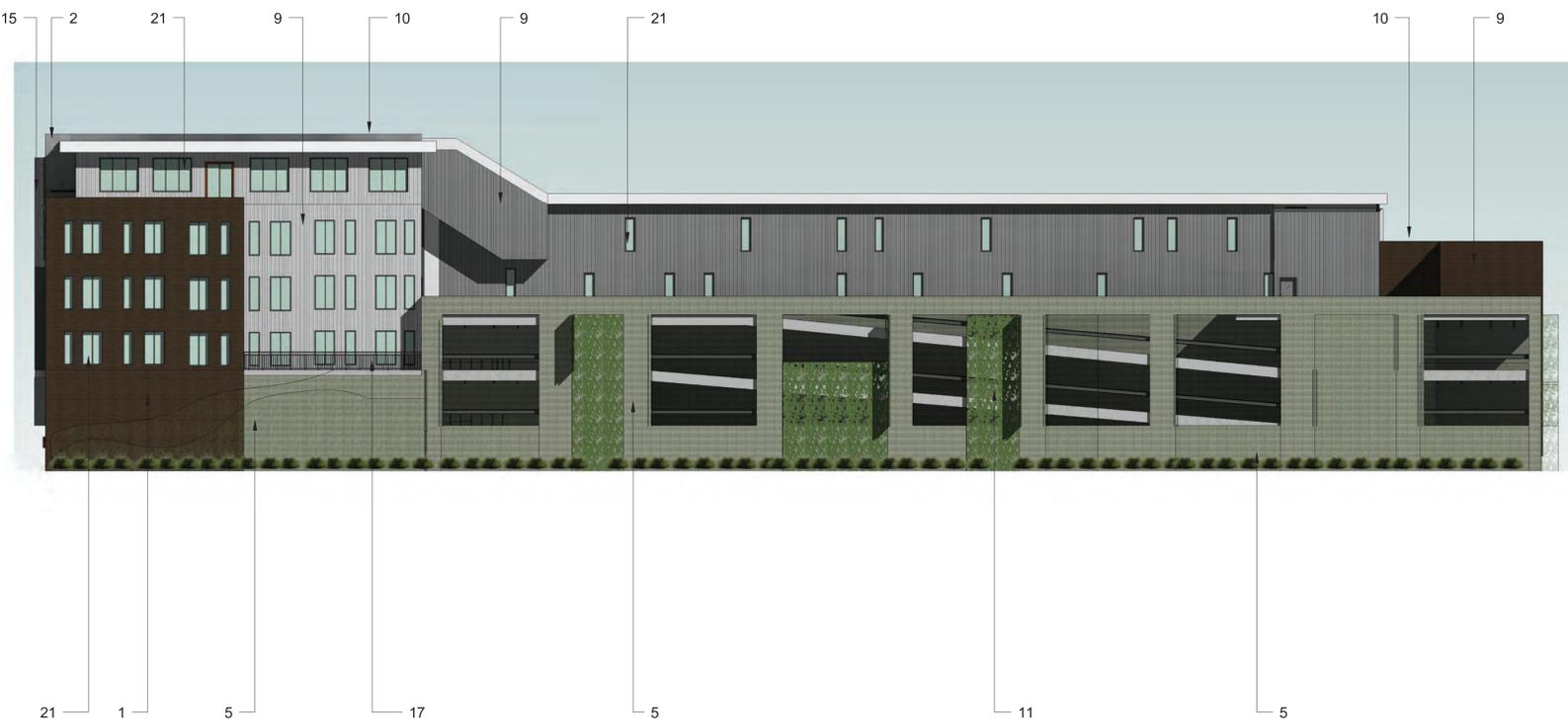
Orientation	Material Name	Material Area	Percent
West	Exterior - Brick	3226 SF	11.2%
West	Exterior - Concrete, Precast Wall	7569 SF	26.4%
West	Exterior - Ivy	7613 SF	26.5%
West	Exterior - Metal Panel Type 2	7548 SF	26.3%
West	Exterior - Metal Panel Type 2A	2755 SF	9.6%
West: 48		28711 SF	

MATERIAL CALCULATION - SOUTH

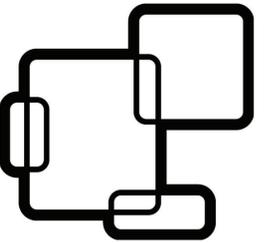
w/wt	Material Name	Material Area	Percent
South	Exterior - Brick	1345 SF	12%
South	Exterior - Concrete Masonry Units	31 SF	0.3%
South	Exterior - Concrete, Precast Wall	2354 SF	20.9%
South	Exterior - Glass	193 SF	1.7%
South	Exterior - Ivy	1336 SF	11.9%
South	Exterior - Metal Panel Type 1	3836 SF	34.1%
South	Exterior - Metal Panel Type 2	1483 SF	13.2%
South	Exterior - Metal Panel Type 2A	662 SF	5.9%
South: 31		11243 SF	



1 SOUTH ELEVATION
1/16" = 1'-0"



2 WEST ELEVATION
1/16" = 1'-0"



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PETER KEELY, REG. NO.: 23570
DATE: 07.10.2014

**LAND USE
APPLICATION SET**

REVISIONS:

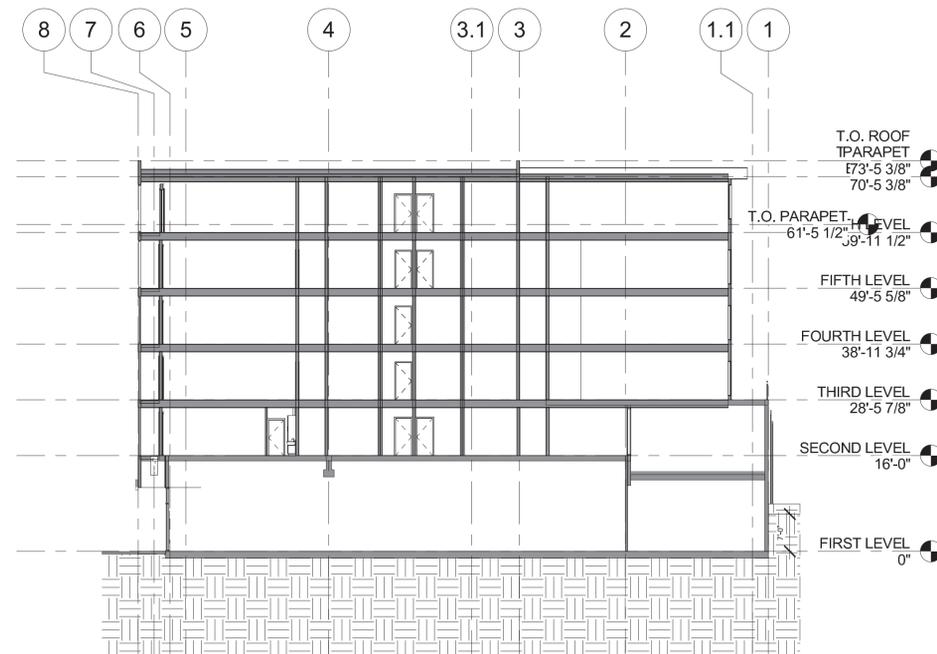
NO.	DATE	DESCRIPTION

PROJECT NO: 14_001
DRAWN BY: Author
CHK'D BY: Checker

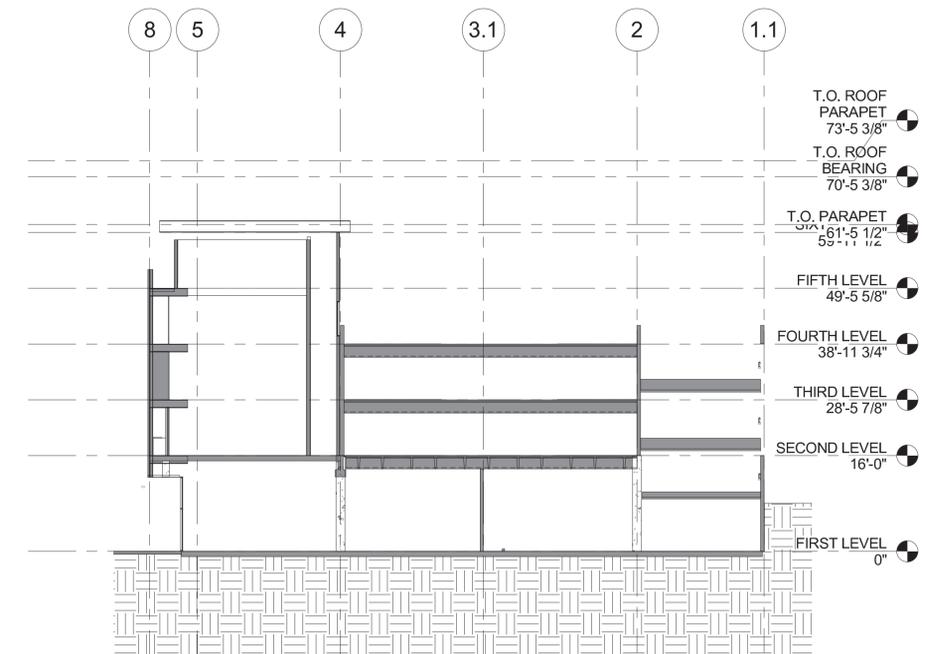
SHEET TITLE

BUILDING SECTIONS

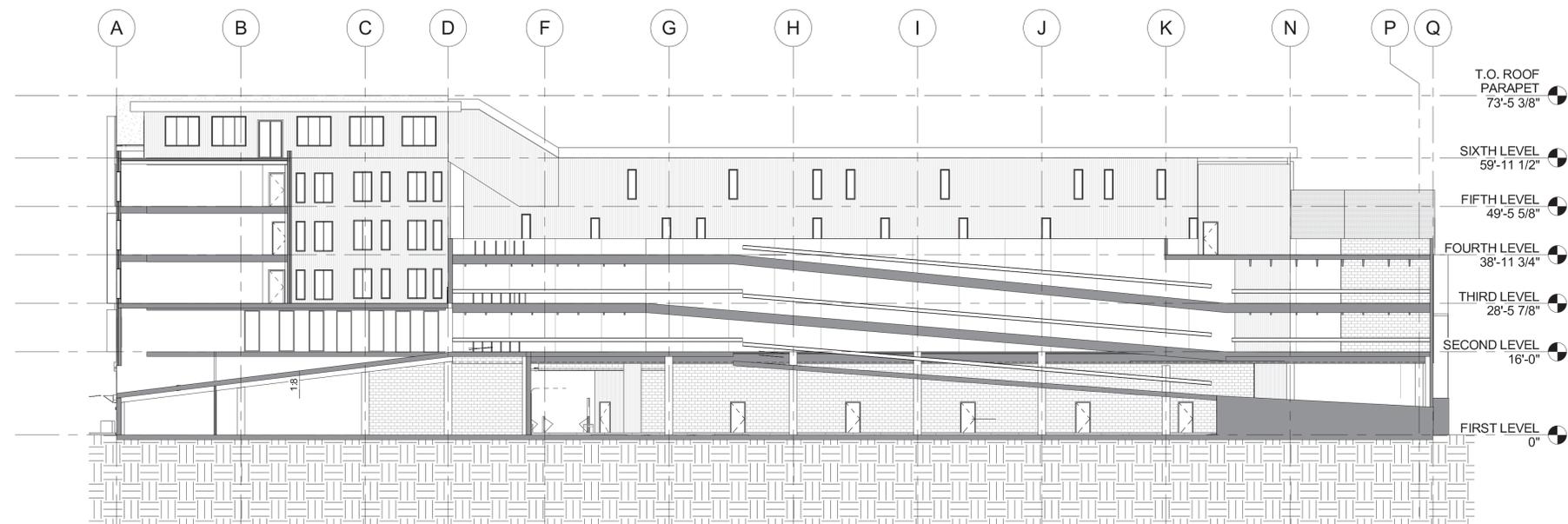
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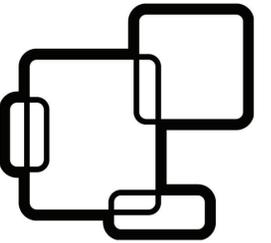
3 SECTION - TRANSVERSE RESTURANT
1/16" = 1'-0"



2 SECTION - TRANSVERSE RETAIL
1/16" = 1'-0"



1 SECTION - LONGITUDINAL
1/16" = 1'-0"



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MASTER DEVELOPMENT



THEATER GARAGE / MARQUEE APARTMENTS

2004-2018 LYNDALE AVENUE, MINNEAPOLIS

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DATE: 07.10.2014

LAND USE APPLICATION SET

REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO: 14_001

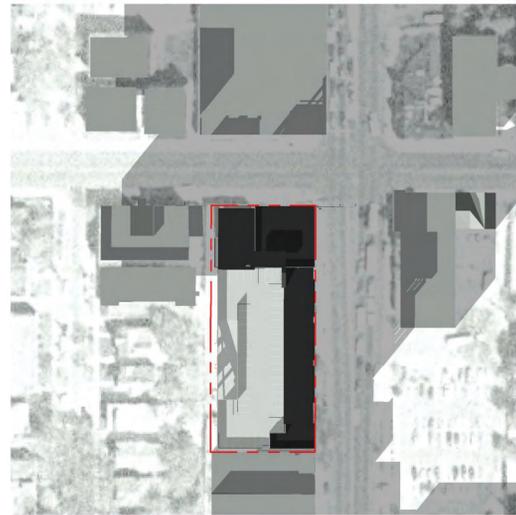
DRAWN BY: BGAO

CHK'D BY: Checker

SHEET TITLE

SHADOW STUDIES

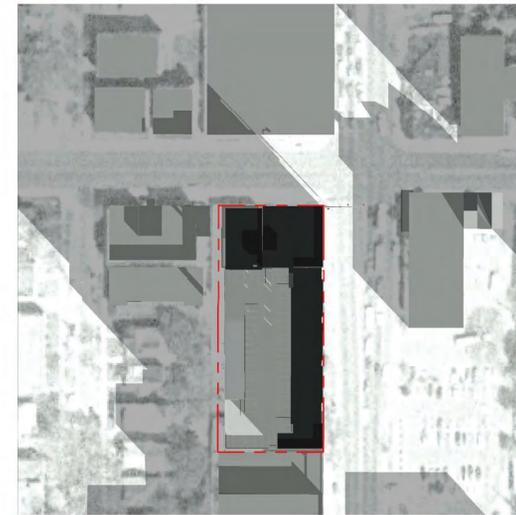
AS100



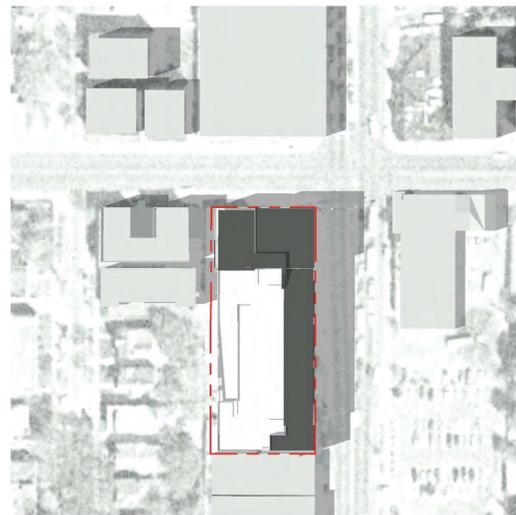
WINTER SOLSTICE - 4PM



WINTER SOLSTICE - 12PM



WINTER SOLSTICE - 10AM



SUMMER SOLSTICE - 4PM



SUMMER SOLSTICE - 12PM



SUMMER SOLSTICE - 9AM



SPRING EQUINOX - 4PM



SPRING EQUINOX - 12PM



SPRING EQUINOX - 9AM

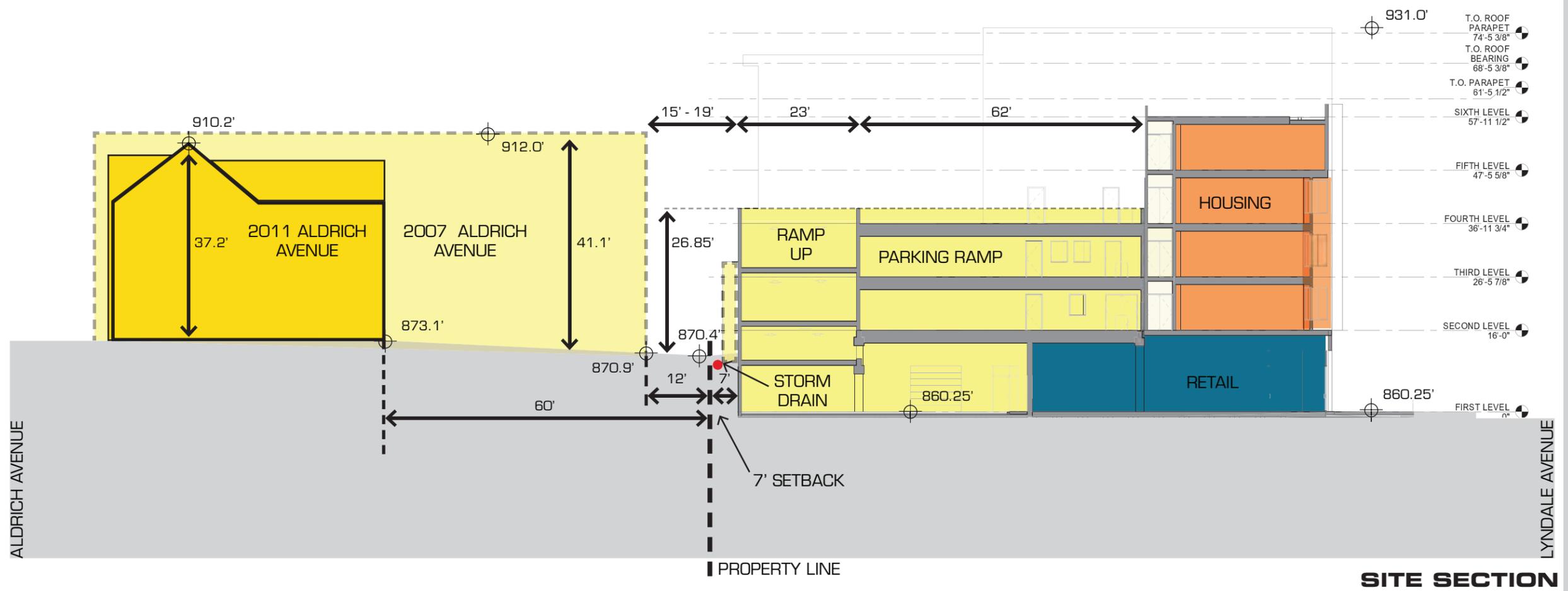
FRANKLIN AVENUE

LYNDALE AVENUE



**Theater Garage
 Marquee Apartments**
 Minneapolis, MN
 JULY.10.2014

FRANKLIN AVENUE COMPARATIVE SECTION

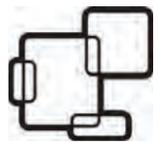


SITE SECTION

**TGMA
 Developers LLC**







Collage | architects



W Franklin Ave

W Franklin Ave

S Aldrich Ave

5

22

Ridgewood Ave

W 22nd St

W 22nd St



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BOX DOGS

Steeple People
Surplus Store
Recycling Quality Used Goods
612-871-8305

MINNEAPOLIS
THEATRE GARAGE
20% THEATRE
THAT FACE
JUNE 6 - 19
612 227 118

THEATRE
GARAGE

MINNEAPOLIS
THEATRE GARAGE
TICKETS



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THEATRE GARAGE
711 W. FRANKLIN

MINNEAPOLIS
THEATRE GARAGE
711 W. FRANKLIN

Living Faith Ministries
Service 10:30 AM





Structure 1 – 2004 Lyndale Ave S, west wall from Franklin Avenue



Structure 1 – 2004 Lyndale Ave S, North Wall from Franklin Avenue



Structure 1 – 2004 Lyndale Ave S, East Wall from Lyndale Avenue



Structure 1: 2004 Lyndale Ave S, East and North walls from intersection



Structure 1: 2004 Lyndale Ave, Loading dock facing West from Lyndale



Structure 1: 2004 Lyndale Ave, South wall from Parking Lot



Retaining Wall: Facing north from parking lot



Retaining Wall: Facing south in parking lot



Structure 2: 2018 Lyndale Ave, North and East walls from parking lot



Structure 2: 2018 Lyndale Ave, North wall from parking lot

TRAVEL DEMAND MANAGEMENT PLAN

for the

THEATER GARAGE MARQUEE APARTMENTS



TGMA
Developers LLC



July 10, 2014

TRAVEL MANAGEMENT PLAN
for the
THEATER GARAGE MARQUEE APARTMENTS

Prepared for:

**TGMA Developers, LLC
Keg House Arts Building
34 - 13th Avenue Northeast
Suite 102A
Minneapolis, MN 55413**

Prepared by:

**Biko Associates, Inc.
79 - 13th Avenue Northeast
Studio 107
Minneapolis, MN 55413**

July 10, 2014

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INTRODUCTION

PURPOSE

This report presents a Travel Demand Management Plan (TDMP) for the Theater Garage Marquee Apartments, a mixed-use residential and commercial development proposed to be constructed in the southwest quadrant of the Lyndale Avenue/Franklin Avenue intersection in south Minneapolis. The project proposer is Theater Garage Marquee Apartments (TGMA) Developers, LLC.

TDMPs are an element of the City of Minneapolis' development review process. TDMPs address the following issues.

1. Potential traffic and parking impacts that may result from project implementation,
2. Opportunities for the development's end-users to access alternative transportation modes, such as transit, bicycle and pedestrian facilities, and
3. TDMP action steps the development's owners and/or property managers will follow to ensure that the completed project will contribute to the city's transportation goals (refer to Chapter 8 (Movement) of the *Minneapolis Plan*). The goals are that for a minimum of three days of the week, a maximum of 50 percent of the residents will drive alone, 35 percent will use transit, and 15 percent will bike or walk.

Four key issue categories were identified by the City of Minneapolis Public Works Department for the Theater Garage Marquee Apartments TDMP.

- Design Review --- A design review should be conducted to analyze the project's design elements that are transportation related. Of interest would be: a) the location and number of driveways serving the site, b) sidewalk width adjacent to the development, and c) pedestrian circulation and bicycle and transit accessibility.
- Traffic Impact Analysis --- Determine how the proposed development will affect the public's system of streets and intersections. Specifically attention should be given to the Lyndale/Franklin intersection and the Lyndale/22nd intersection.
- Parking Generation and Required Supply --- This concern includes analyses of on-site parking generation. The parking generation analysis should include justifications for the parking ratios that are recommended for the proposed development. As will be explained in this document, it is the developers' intention that any excess parking capacity (beyond the calculated demand for on-site uses) will be made available for other uses in the vicinity of the development.
- Travel Demand Management Strategies --- A major component of the Travel Demand Management Plan are strategies that the owner/manager of the development will agree to implement to help ensure that alternative travel modes can be accessed by the development's users.

REGIONAL AND SITE LOCATIONS

Regional Location:

The Theater Garage Marquee Apartments (TGMA) development site is located in south Minneapolis on the southwest corner of the intersection formed by Franklin and Lyndale Avenues. The development site is within an area known as the Wedge, and the Lyndale/Franklin intersection is built out as a 100 percent commercial corner.

The proposed site is ideally located to capture both local and regional markets. Sub-regional and regional roads serving the site include:

- Hennepin, Lyndale, and Franklin Avenues are three Hennepin County roads with direct connections to the interstate highway system. They also provide cross-Minneapolis transportation service.
- I-94 and I-35W are principal arterials, with ramps north of the development site on Lyndale Avenue and east of the site on Franklin Avenue. I-94 provides access to Saint Paul and I-394 and I-694 two facilities that provide transportation service to the Twin Cities' western and northwestern suburbs. I-35W links the northern and southern suburbs to downtown Minneapolis
- I-394 is an east/west Principal Arterial that provides access between downtown Minneapolis and the western suburbs.

Figure 1 shows the location of the site relative to the roadways described above and downtown Minneapolis.



Theater Garage Marquee Apartments
Travel Demand Management Plan

Figure 1
Regional Location



Site Location Zoning, Parking, Transit, and Bicycle Services :

The TGMA development site is within a split zone, where the northern parcel of the site is zoned C2 for uses within a neighborhood corridor commercial district, and the southern parcels are zoned C1 for neighborhood commercial uses. The developer of the project is proposing to rezone the C1 parcels to C2. No overlay zoning districts were identified for the development site.

Existing uses at the Franklin/Lyndale corner include:

- Rudolph’s Barbeque on the northeast corner
- Vision Loss Resources on the northwest corner
- Theater Garage, Steeple People Surplus Store, and a hair salon and art gallery on the southwest corner
- Mortimer’s Bar and Restaurant on the southeast corner

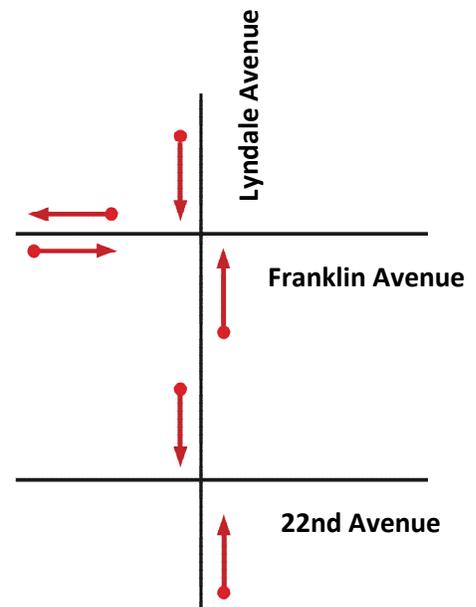
With the exception of Steeple People, these businesses largely attract a customer base that is both local and regional. Steeple People’s customer base is predominantly local.

Parking within the Study Area:

On-street parking is not permitted along Franklin Avenue nor along Lyndale Avenue adjacent to the development site. Existing uses that occupy the development site (Theater Garage, Steeple People Surplus Store, the hair salon, and art gallery) rely on off-street parking facilities. Theater Garage and Steeple People both have small surface lots that are in the immediate southwest corner of the Franklin/Lyndale intersection. They, and other existing uses on the southwest corner and some existing uses along the west side of Lyndale Avenue, are also permitted to park in the Rudolph’s Barbeque surface lot.

Transit Routes Serving the Study Area:

The study area is well served by transit. Bus stops serving the project area, shown to the right, are located both on the near side and far side of intersections. The north/south routes have near side bus stops along Lyndale Avenue at Franklin and 22nd Avenues. The eastbound route (Route 2) that runs along Franklin Avenue has a near side bus stop, and westbound Route 2 along Franklin Avenue has a far side stop. Table 1 on the following page outlines bus routes serving the site and identifies the locations of bus stops. Figure 2 illustrates the routes.



These routes provide a high level of transit service to the proposed development. All of the routes listed above, except Route 2, serve the Minneapolis CBD. Other trip destinations served by these routes, either directly or by transfer, include downtown Saint Paul, the University of Minnesota, the Midway area, and the airport/Mall of America.

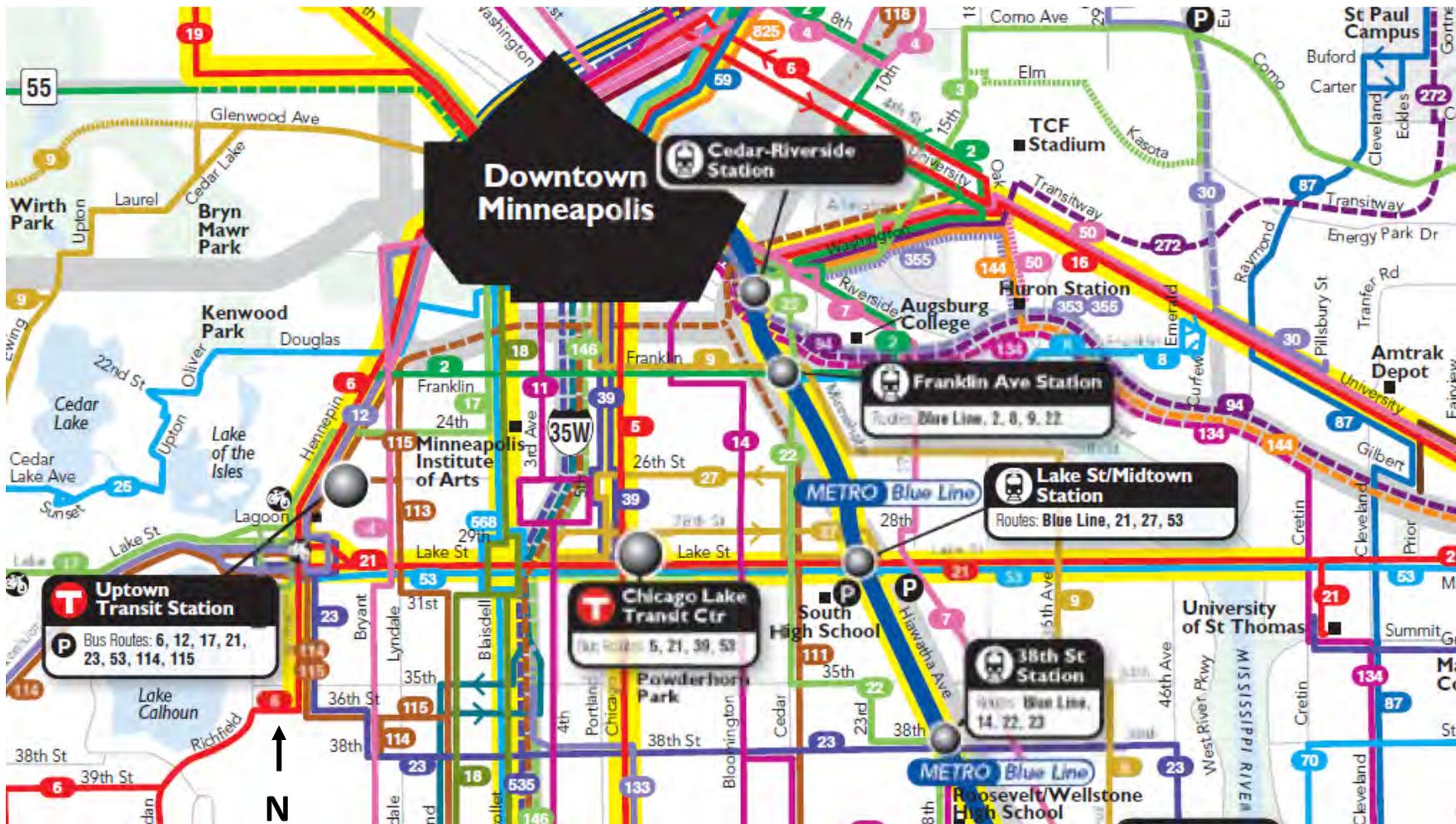
Bicycle Routes Serving the Study Area:

Figure 3 shows existing and proposed bike paths that serve the project area. The project area is shown to be served by a large number of on-street bike paths. It should be noted, however, that Lyndale Avenue is not an official bike path.

TABLE 1
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
TRANSIT ROUTES

TRANSIT ROUTE	Type of Service	Nearest Bus Stop to TGMA Development	Route Characteristics		
			Major Line-Haul Street and Destination	Peak Hour Frequency	Off Peak Frequency
Route 2	Local	Franklin/Lyndale Ave	Hennepin Ave/22nd St to Hennepin Ave/6th St SE	10 to 17 min	20 to 30 min
Route 4	Local	Franklin/Lyndale Ave	1st Ave NW/Old Hwy 8 to 82nd St/I-35W	20 min	20 min
Route 6	Local	Franklin/Lyndale Ave	Ontario/Beacon St SE to Picture Dr/78th St SW	20 min	30 min
Route 12	Local	Franklin/Hennepin Ave	Opportunity Partners (Hopkins) to Hennepin/Washington Ave	20 min	30 min
Route 17	Local	24th St/Lyndale Ave	Lake St NE (Hopkins) to 27th NE/Washington St NE	12 - 15 min	20 min
Route 113	Limited Stop	Franklin/Lyndale Ave	Oak/4th St SE to Lyndale Ave/50th St	1 hour	1 hour
Route 115	Limited Stop	Franklin/Lyndale Ave	Oak/4th St SE to Grand Ave/46th St	30 min	30 min

Source: Metro Transit 2014 Service Map.

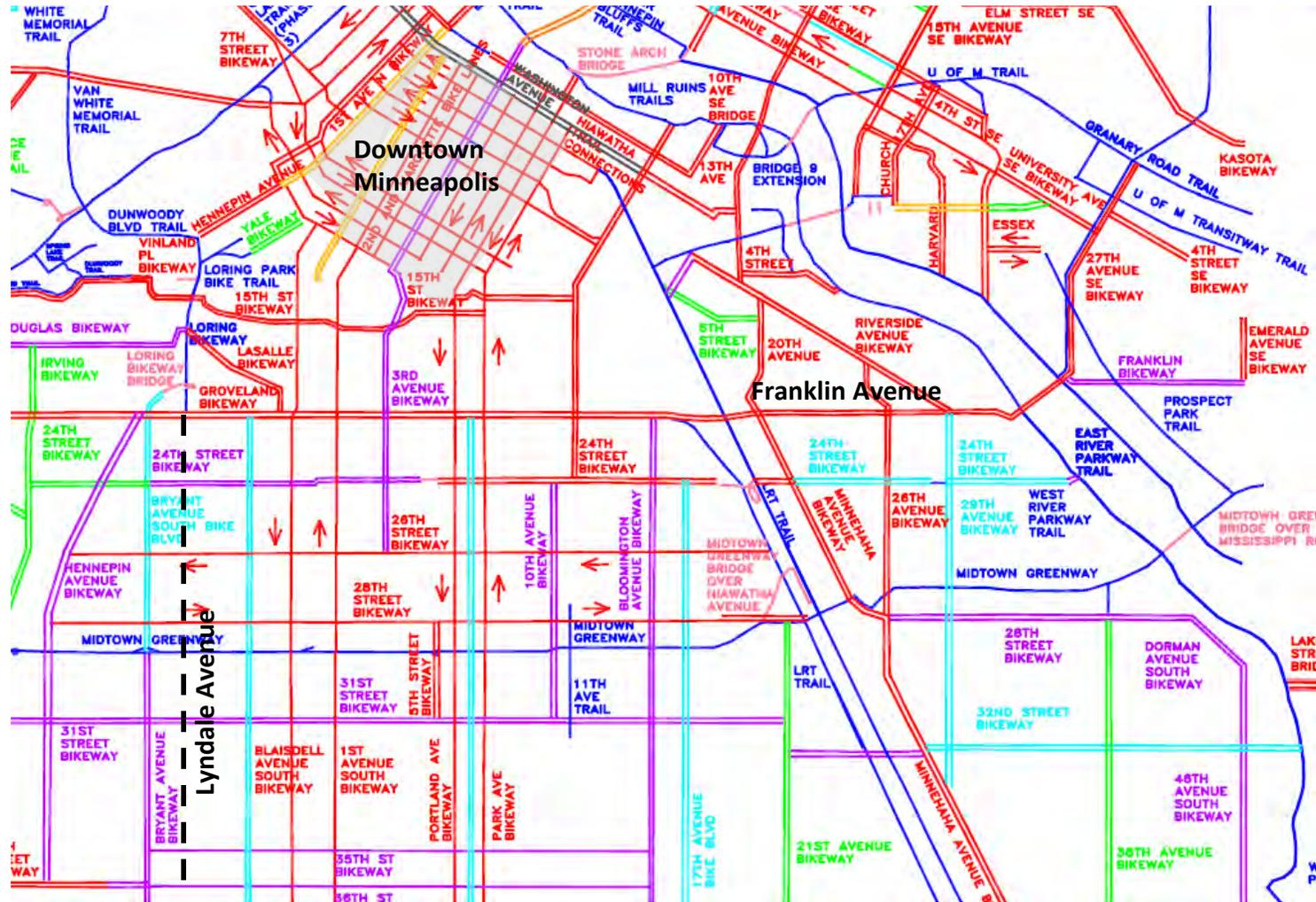


Theater Garage Marquee Apartments
Travel Demand Management Plan



Figure 2
Project Area Transit Routes

Source: Metro Transit 2014
Service Map



Theater Garage Marquee Apartments
Travel Demand Management Plan



Figure 3
Project Area Bicycle Routes

Source: City of Minneapolis
Bicycle Map

TGMA DEVELOPMENT PROGRAM

The proposed development will be constructed on the southwest corner of the block formed by Franklin and Lyndale Avenues. Existing uses on the corner are Theater Garage, Steeple People Surplus Store, a hair salon, Rudolph's Barbeque surface lot, a dry cleaner and an art gallery. Each of these uses will be demolished to make way for the development. The 2100 Building, which is attached to and south of the dry cleaner, will not be demolished. The 2100 Building houses four retail shops and consists of approximately 13,800 SF.



Uses programmed for the new development are detailed in Table 2, and illustrations presenting floor plans, a section, and perspectives are shown on Figures 4A through 4E.

**TABLE 2
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
PROPOSED DEVELOPMENT PROGRAM**

USE	QUANTITY	LOCATION
Theater	4,275 SF 150 seats	Below ground and at-grade
Quality restaurant (front of the house)	7,331 SF 300 seats	Ground level
Retail shops	4,000 SF	Ground level
Bike transit hub	250 SF	Ground level
Apartments	82 DU 84,006 GSF 66,319 Usable SF	2nd floor through 6th floor

Source: Collage Architects

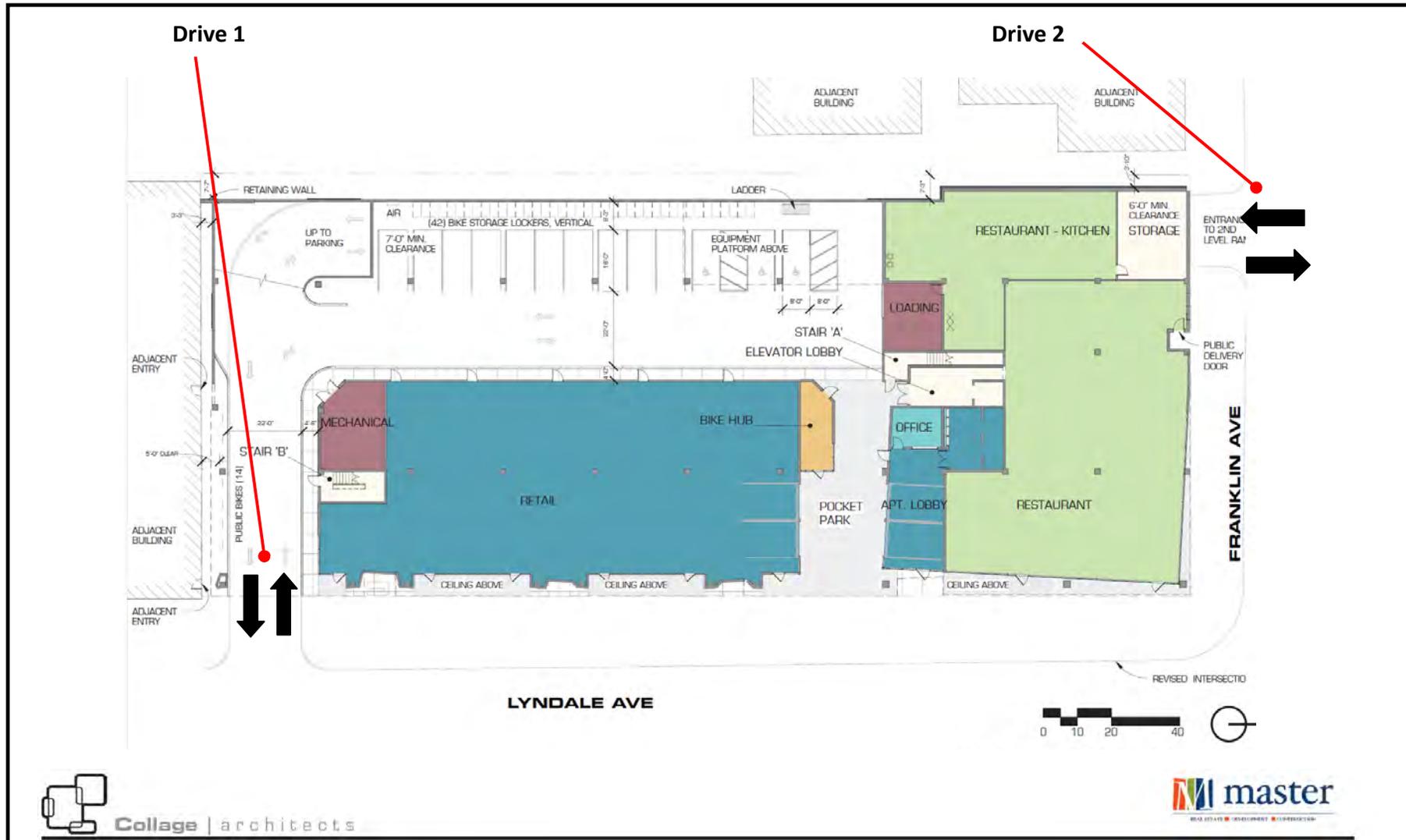
On-site parking for the new development will be accommodated with a 14-stall ground-level lot and a 149-stall, above ground parking ramp. As shown on Figure 4A (Street Level Footprint), the surface lot will be accessed via a two-way driveway at the southern edge of the project. This driveway will access Lyndale Avenue. The parking ramp will be accessed via the southernmost driveway and a two-way driveway that links the development to Franklin Avenue. Primary users of the 14-stall ground-level lot will be customers of the retail shops identified in Table 2 and existing retail shops in the 2100 Building.

The parking ramp will be provided for the:

- 82 apartment dwelling units
- 300-seat restaurant
- 150-seat theater
- 4,000 SF retail shops
- Others in need of parking stalls within the Wedge District

The Bike/Transit Hub shown at the southeast corner of the proposed development will house vending machines for bike tire repair and replacement and free bike route maps and transit route maps that the public can access. It will also provide bike parking stalls for on-site residents and the general public.¹ In addition, bike parking for the public (including patrons of on-site uses and retail shops in the 2100 Building) is proposed to be provided on the south side of the development, as shown on Figure 4A.

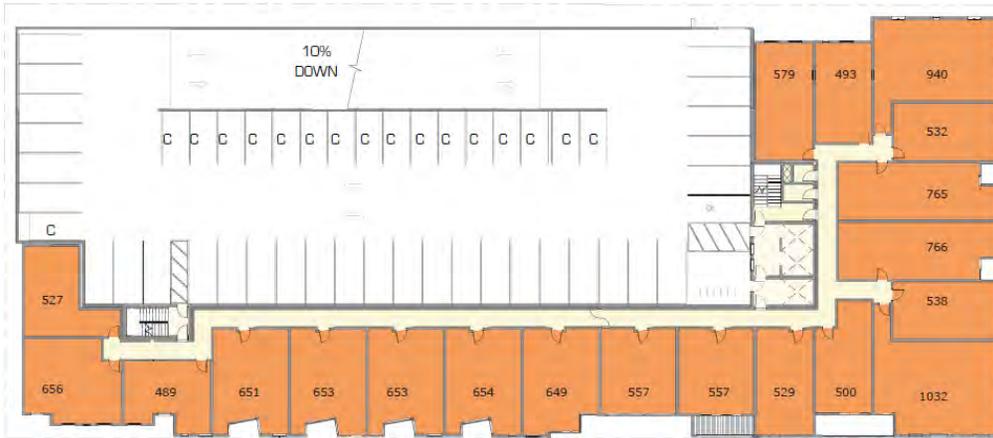
¹ The major supply of bike parking stalls for residents will be provided at a 1:1 ratio and will be located along the west wall of the ground level parking area and at key locations convenient to users in the parking ramp.



Theater Garage Marquee Apartments
Travel Demand Management Plan



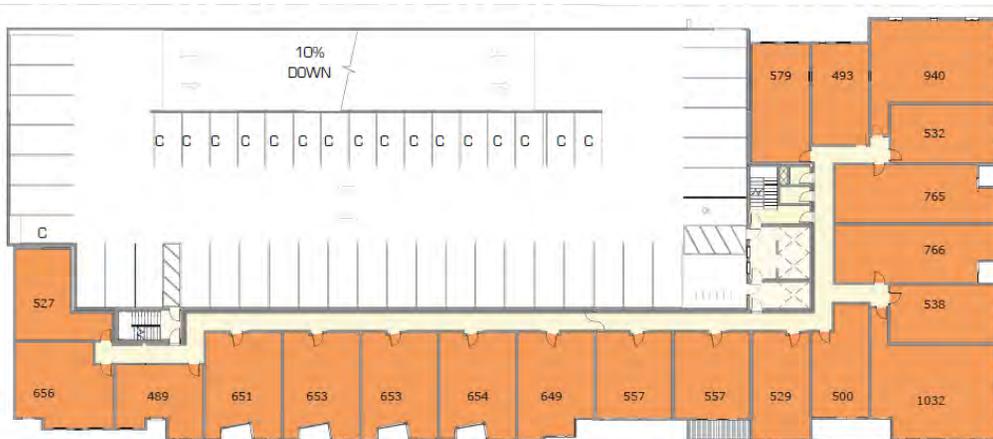
Figure 4A
Street Level Footprint



Theater Garage Marquee Apartments
 Minneapolis, MN
 MAY.27.2014

49 PARKING STALLS
 20 UNITS

Second Level Floor Plan



Theater Garage Marquee Apartments
 Minneapolis, MN
 MAY.27.2014

49 PARKING STALLS
 20 UNITS

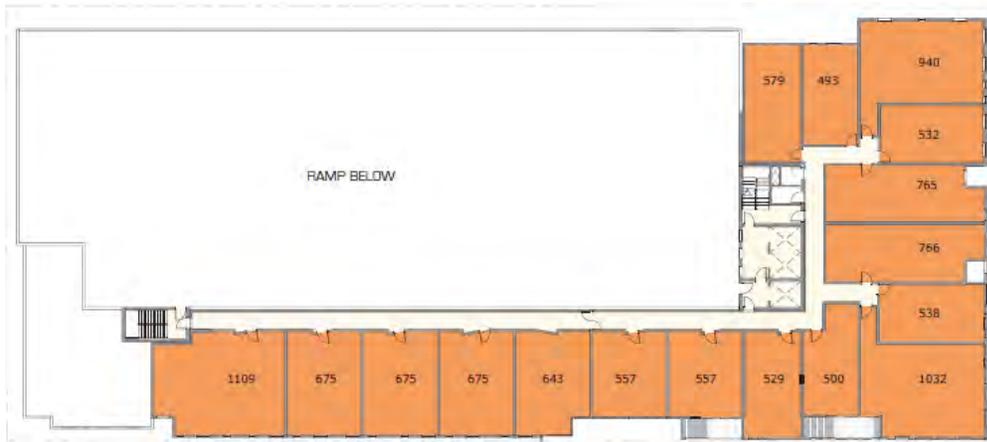
Third and Fourth Level Floor Plans



Theater Garage Marquee Apartments
 Travel Demand Management Plan



Figure 4B
 Second, Third and Fourth Level
 Floor Plans



**Theater Garage
Marquee Apartments**
Minneapolis, MN
MAY.27.2014

17 UNITS

Fifth Level Floor
Plan



**Theater Garage
Marquee Apartments**
Minneapolis, MN
MAY.27.2014

0 PARKING STALLS

8 UNITS

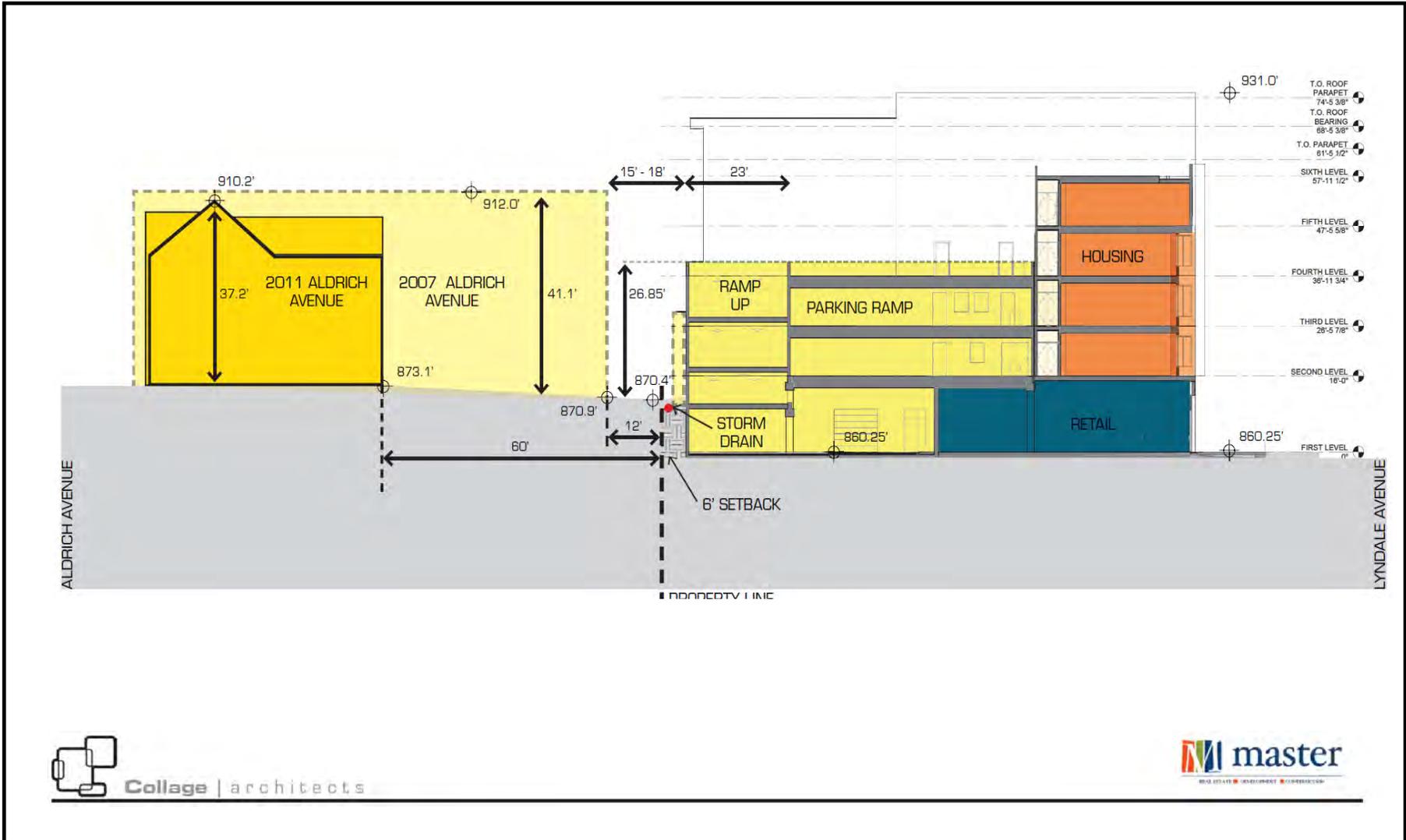
Sixth Level Floor
Plans



**Theater Garage Marquee Apartments
Travel Demand Management Plan**



**Figure 4C
Fifth and Sixth Level Floor Plans**



Theater Garage Marquee Apartments
Travel Demand Management Plan



Figure 4D
Section through the Site



Theater Garage Marquee Apartments
Travel Demand Management Plan



Figure 4E
Site Perspective
(Looking South along Lyndale
Avenue)

DESIGN REVIEW

This section of the TDMP addresses design-related issues that bear on transportation, both for the general travelling public using the system of sidewalks, streets and intersections and travelers who will access the development site for the purposes of shopping, dining, entertainment, residing, and providing services to on-site uses for deliveries and on-site trash/garbage collection

PEDESTRIAN CIRCULATION AND TRANSIT ACCESSIBILITY

Data collected by the City of Minneapolis shows that the proposed development area has a high level of pedestrian and bicycle traffic. Table 3 details non-motorized (pedestrian and bicycle) circulation during the AM and PM peak periods at two intersections with crosswalks in the immediate project area.

**TABLE 3
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
NON-MOTORIZED CIRCULATION ADJACENT TO THE PROPOSED DEVELOPMENT DURING PEAK TRAVEL PERIODS**

Intersection	Year	Peak Period	Northside East/West Movement	Eastside North/South Movement	Southside East/West Movement	Westside North/South Movement	Total Non-Motorized Movements
Franklin/Lyndale	1994	AM	0	0	0	0	0
Franklin/Lyndale	1994	PM	6	0	0	0	6
Franklin/Lyndale	2005	AM	15	23	20	13	71
Franklin/Lyndale	2005	PM	31	23	59	42	155
Franklin/Lyndale	2011	AM	3	7	9	31	50
Franklin/Lyndale	2011	PM	44	48	42	78	212
22nd/Lyndale	1995	AM	29	15	17	13	74
22nd/Lyndale	1995	PM	0	0	0	0	0
22nd/Lyndale	2011	AM	21	11	6	4	42
22nd/Lyndale	2011	PM	79	63	131	91	364
22nd/Lyndale	2012	AM	20	14	21	23	78
22nd/Lyndale	2012	PM	142	30	43	44	259

Source: City of Minneapolis Traffic Count Management System

These data show the importance of the two intersections within the project area to a growing number of travelers who are not using automobiles. Keeping in mind the fact that transit riders are pedestrians, the importance of the bus stops in the immediate project area is also brought to light by the data in Table 3.

LYNDALE AVENUE SIDEWALK WIDTH

Shown in the aerial photograph to the right is the existing southwest corner of the Franklin Avenue/Lyndale Avenue intersection. Field measurements showed that the sidewalk on the west side of Lyndale Avenue, south of Franklin Avenue, is approximately 9 feet wide at the curb. This current width likely resulted from the construction of the northbound, exclusive left-turn lane or the construction of a bus pull-out when a far-side bus stop was located on Lyndale Avenue, south of Franklin Avenue.



The 9 foot width presents an unacceptable condition for an area where pedestrian activity has been counted and observed to be at a very high level. In addition to the narrow sidewalk, two traffic signal poles, a parking regulation sign and pole, a bus stop bench for eastbound transit riders on Route 2, and other street furniture (newspaper vending machines and a mail box) further impede pedestrian circulation in the area.

Moving south from the intersection, the sidewalk on the west side of Lyndale Avenue widens to a maximum width of 15 feet at the south end of the 2100 Building. It then abruptly narrows to 8 feet.

Given its location in the Wedge (an area served by transit routes), its mix of ground level entertainment, restaurant, and retail/commercial uses, and its above ground dwelling units, the proposed development will be oriented toward a high level of pedestrian patronage. This orientation will require sidewalk widths that are wide enough to ensure that pedestrian circulation can be accommodated.

As shown on the street level floor plan (Figure 4A on page 10), the sidewalk on Lyndale Avenue will be reconstructed as part of the proposed development project to provide a sidewalk that is consistently 20 feet wide from the intersection of Franklin/Lyndale to the southern edge of the project, a distance of approximately 350 feet. Figure 4A shows that the sidewalk on Franklin Avenue will be reconstructed to a width of 15 feet adjacent to the project. The Franklin Avenue sidewalk is currently only 8 feet wide.

DRIVEWAY LOCATIONS

Two driveways are included in the proposed design. (Refer to Figure 4A on page 10.) The first, Drive 1, is a two-way drive that will provide access between Lyndale Avenue and the ground-level parking lot and also to a ramp up to the parking garage. It is proposed to be located approximately 273 feet south of the

Franklin/Lyndale intersection (corner clearance). The second, Drive 2, is a two-way driveway providing access between the parking ramp and Franklin Avenue and is proposed to be located approximately 125 feet west of the Franklin/Lyndale intersection (corner clearance). The second drive will only provide access to the parking garage.

The locations of these driveways, as proposed, will help ensure efficient traffic operations, pedestrian circulation and safety, and transit accessibility on both Lyndale and Franklin Avenues, because:

- Their locations do not interfere with transit boarding and alighting operations.
- Both Lyndale and Franklin Avenues have two travel lanes in each direction. This configuration will allow through vehicles to overtake vehicles turning left into the site at Drives 1 and 3

ON-SITE CIRCULATION

As shown on Figure 4A, vehicles destined for uses in the 2100 Building and on-site uses will be able to enter and exit the ground-level parking lot and the parking ramp via Drive 1, a two-way driveway. Long wheelbase vehicles destined for receiving/loading facilities and trash bins on the north end of the development will also enter an exit the site at Drive 1.

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ANALYSIS OF TRAFFIC CONDITIONS

HISTORICAL TRAFFIC GROWTH RATES AND CALCULATED 2014 TRAFFIC VOLUMES

Findings from an analysis of historical turning movement counts collected by the City of Minneapolis at the Franklin/Lyndale and 22nd/Lyndale intersection are outlined below.

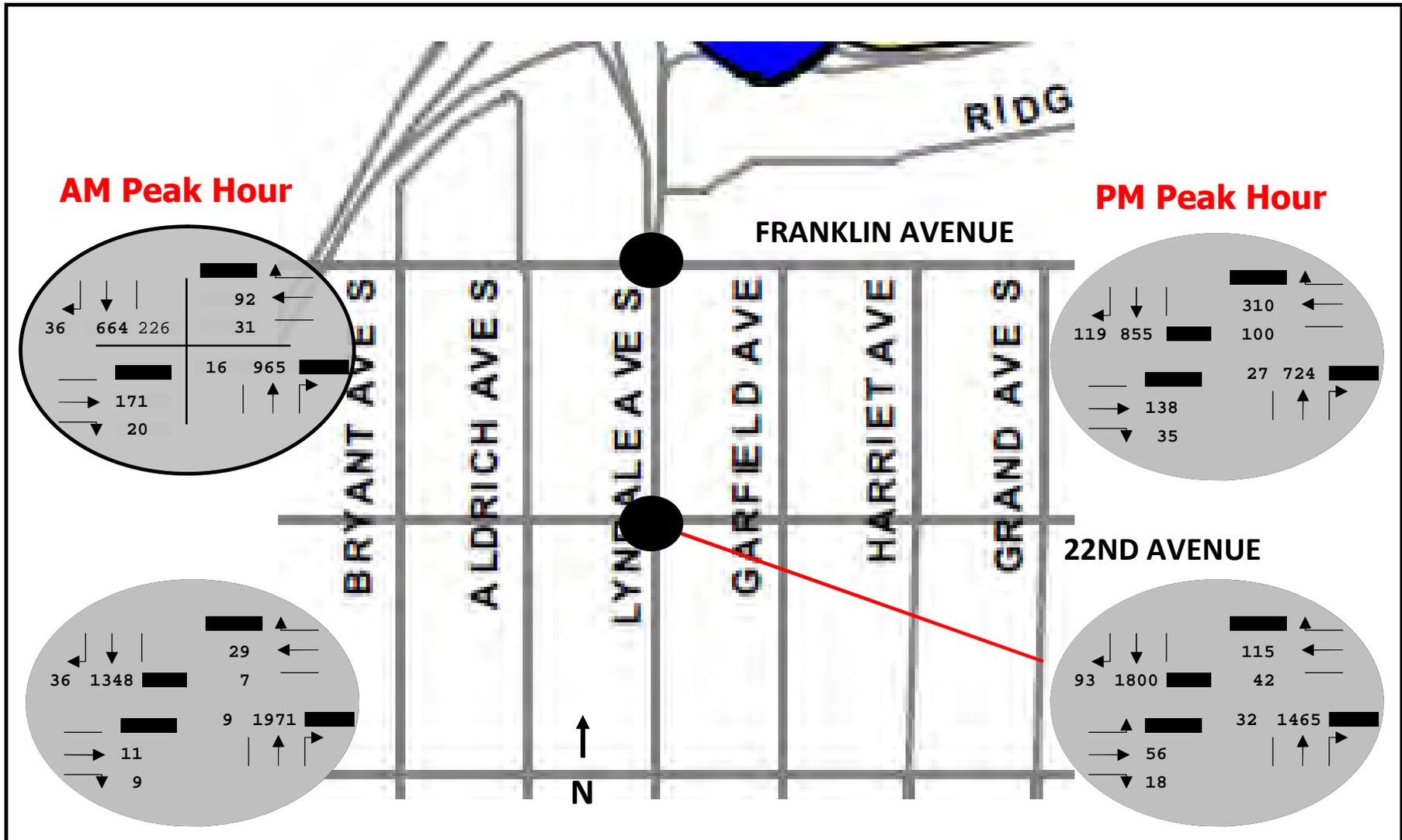
- Franklin/Lyndale intersection (comparison of 2005 and 2011 data):
 - East/west and north/south traffic volumes moving through the Franklin/Lyndale intersection have decreased, both during the AM and PM peak hours.
 - East/west traffic during the AM peak hour has decreased by approximately 40 percent between 2005 and 2011, approximately 7 percent per year.
 - North/south traffic during the AM peak hour has decreased by approximately 25 percent, approximately 4 percent per year.
 - East/west traffic during the PM peak hour has decreased by approximately 26 percent, approximately 4 percent per year.
 - North/south traffic during the PM peak hour has decreased by approximately 19 percent, approximately 3 percent per year.
- 22nd/Lyndale intersection (comparison of 1995 and 2012 data):
 - East/west traffic volumes through the 22nd/Lyndale intersection are growing slowly (less than 1 percent per year).
 - North/south traffic volumes through the 22nd/Lyndale intersection have increased, both during the AM and PM peak hours.
 - North/south traffic during the AM peak hour has grown by approximately 82 percent, approximately 5 percent per year.
 - North/south traffic during the PM peak hour has grown by approximately 25 percent, approximately 2 percent per year.

The annual growth rates outlined above were applied to the most current turning movements at the two intersections to derive calculated 2014 turning movements. Table 4 presents calculated 2014 AM and PM peak hour turning movements. Figure 5 illustrates the calculated AM and PM turning movements.

TABLE 4
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
CALCULATED 2014 AM AND PM PEAK HOUR TURNING MOVEMENTS

Intersection	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	226	664	36	31	92	232	16	965	40	90	171	20
Franklin/Lyndale PM	249	855	119	100	310	331	27	724	28	62	138	35
22nd/Lyndale AM	74	1348	36	7	29	63	9	1971	23	88	11	9
22nd/Lyndale PM	42	1800	93	42	115	75	32	1465	49	90	56	18

Source: Biko Associates, Inc.



Theatre Garage Marquee Apartments
Travel Demand Management Plan



Figure 5
Calculated 2014
Turning Movements

EXISTING INTERSECTION CAPACITY ANALYSIS

Analysis Inputs:

Inputs in the intersection capacity analysis included:

- AM and PM peak hour turns shown above in Table 4 and illustrated on Figure 5 and
- Intersection lane configurations shown in Table 5.

**TABLE 5
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
EXISTING INTERSECTION LANE CONFIGURATIONS**

Intersection	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale	1	2	++	>	2	++	1	2	++	>	2	++
22nd/Lyndale	>	2	++	>	1	<	>	2	++	>	1	<

> : Left-turn lane is shared with adjacent through lane.

< : Right-turn lane is shared with adjacent through lane.

++ : Defacto right-turn lane.

Source : Field reconnaissance conducted by Biko Associates, Inc.

2014 Intersection Level of Service:

Intersection capacity is a measurement of average vehicle delay and is rated in terms of Level of Service (LOS). LOS A, B, C, and D are acceptable operational conditions. LOS E and F, which represent long periods of average vehicle delay and congestion, are unacceptable during the majority of a 24-hour day.

By policy, the City of Minneapolis has identified LOS E as an acceptable condition for the AM and PM peak travel periods. An intersection that displays LOS E or LOS F operations during non-peak periods and LOS F operations during peak periods is identified for mitigation through: a) travel demand reduction strategies, b) improved intersection control devices (installation or modification of signage, installation of a traffic signal, or improved timing and/or phasing for an existing traffic signal), c) addition of turn lanes, or d) a combination of the three.

Table 6 presents results of the intersection capacity analysis for the existing, 2014 condition. Computer printouts that detail results of the analysis can be found in the Appendix. As shown in Table 6, both of the intersections currently operate at acceptable, peak hour LOS, both for the AM and PM peak hours.

**TABLE 6
 TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
 2014 AM AND PM PEAK HOUR INTERSECTION CAPACITY**

Intersection	Overall LOS	Signal Timing (sec)	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	B	60	B	B	B	B	B	B	A	B	B	C	C	C
Franklin/Lyndale PM	B	55	B	B	B	B	B	A	B	B	B	B	B	B
22nd/Lyndale AM	B	90	B	B	B	D	D	D	A	A	A	D	D	D
22nd/Lyndale PM	C	90	C	C	C	D	D	D	B	B	B	D	D	D

Source: Biko Associates, Inc.,

FORECAST 2017 NO-BUILD CONDITIONS

Introduction:

The No-Build analysis was conducted for the purpose of identifying traffic conditions within the influence area of the proposed development at a future time (Year 2017), where the proposed redevelopment has not been implemented. Moreover, the No-Build condition is a future extension of the existing condition where any growth in traffic volume would be attributed to the general growth in traffic and other factors not related to the proposed development. As such, the No-Build analysis can then be used as a base condition, against which the Build condition can be evaluated. This approach to preparing traffic studies enables analysts and decision-makers to accurately evaluate the incremental traffic impacts of a proposed project.

Year 2017 was selected as the analysis forecast year for this traffic study in accordance with rules for preparing Environmental Assessment Worksheets. These rules state that traffic impacts should be evaluated to reflect conditions that will occur one-year after the proposed development's build-out. As the TGMA development project will be completed and ready for occupancy in 2016, forecast 2017 conditions will be evaluated for the No-Build and Build conditions in this traffic study.

Forecast 2017 No-Build Peak Hour Traffic:

Although the growth rates identified on page 19 indicated a reduction in peak hour traffic at the Franklin/Lyndale intersection, it was assumed for the purposes of forecasting 2017 peak hour traffic that the peak hour volumes would grow at a slow rate (1 percent to 2 percent per year). This conservative approach, which may actually overstate peak hour traffic, was used to rigorously test forecast 2017 conditions. Table 7 shows AM and PM peak hour turning movements for TGMA development' 2017 No-Build condition, and Figure 6 illustrates them.

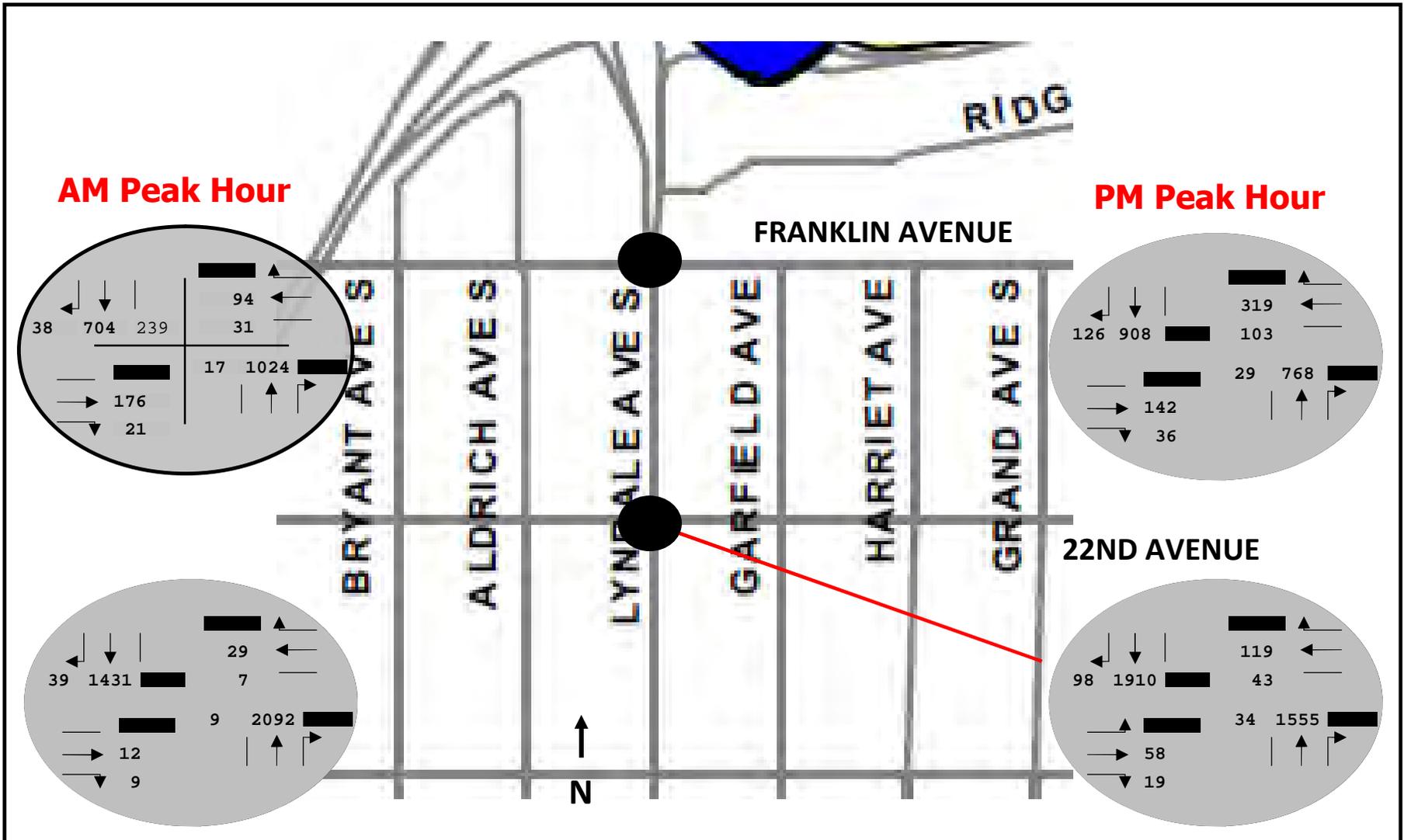
**TABLE 7
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
FORECAST 2017 NO-BUILD AM AND PM PEAK HOUR TURNS**

Intersection	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	239	704	38	31	94	239	17	1024	42	93	176	21
Franklin/Lyndale PM	264	908	126	103	319	341	29	768	30	64	142	36
22nd/Lyndale AM	78	1431	39	7	29	65	9	2092	25	90	12	9
22nd/Lyndale PM	44	1910	98	43	119	78	34	1555	52	92	58	19

Source: Biko Associates, Inc.

Forecast 2017 No-Build Intersection Capacity Analysis:

Table 8, on page 25, shows results of the capacity analysis conducted for the forecast 2017 No-Build condition. As shown, the intersections are forecast to operate at acceptable LOS under this condition.



Theatre Garage Marquee Apartments
Travel Demand Management Plan



Figure 6
Forecast 2017
No-Build Turning Movements

**TABLE 8
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
FORECAST 2017 NO-BUILD AM AND PM PEAK HOUR INTERSECTION CAPACITY**

Intersection	Overall LOS	Signal Timing (sec)	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	B	60	B	A	A	B	C	C	A	B	B	C	C	C
Franklin/Lyndale PM	B	60	B	B	B	C	C	B	B	B	B	B	B	B
22nd/Lyndale AM	C	90	E	E	E	D	D	D	B	B	B	D	D	D
22nd/Lyndale PM	C	90	C	C	C	D	D	D	B	B	B	D	D	D

Source: Biko Associates, Inc.,

FORECAST 2017 BUILD CONDITIONS

A trip generation analysis was conducted to quantify the volume of traffic that will be generated by the proposed development. Table 9 shows the results of the analysis.

The proposed development includes the addition of two new uses: 1) the apartment use, which includes 82 dwelling units, which will be constructed on floors two through five on Lyndale Avenue and two through 6 at the intersection and 2) retail shops, which front on Lyndale Avenue and will consist of 4,000 square feet. Other uses proposed for the new development (the theater) and the quality sit-down restaurant) are not new uses but, instead, relocated and rebuilt uses. The new theater is proposed to occupy the same area as the existing theater, and the sit-down restaurant is roughly the same size as the existing restaurant on the northeast corner. As a result, the trip generation analysis only includes two uses, the 82 dwelling unit apartment use and 4,000 square feet of retail shops.

As shown in Table 9, after taking a 10 percent credit because of the development's proximity to bus stops, the new uses proposed for the site will generate a total of 600 daily trips, 27 AM peak hour trips, and 63 PM peak hour trips.

**TABLE 9
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
ESTIMATED TRIP GENERATION**

Land Use	Size	Daily Trips	AM Peak Hour			PM Peak Hour		
			In	Out	Total	In	Out	Total
Residential Apartments	82 DUs	366	8	12	20	18	12	30
Retail	4,000 sf	300	5	5	10	20	20	40
Subtotal		666	13	17	30	38	32	70
Reductions:								
10 % Transit Accessibility		(66)	(1)	(2)	(3)	(4)	(3)	(7)
Grand Total Phase I		600	12	15	27	34	29	63

Biko Associates, Inc., *Trip Generation, 8th Edition*, Institute of Transportation Engineers.

Forecast 2017 Build Peak Hour Traffic:

Trips estimated to be generated by the development were assigned to the network of streets and intersections serving the site, and forecast 2017 Build traffic assignments were derived. These are detailed in Table 10 and illustrated on Figure 7.

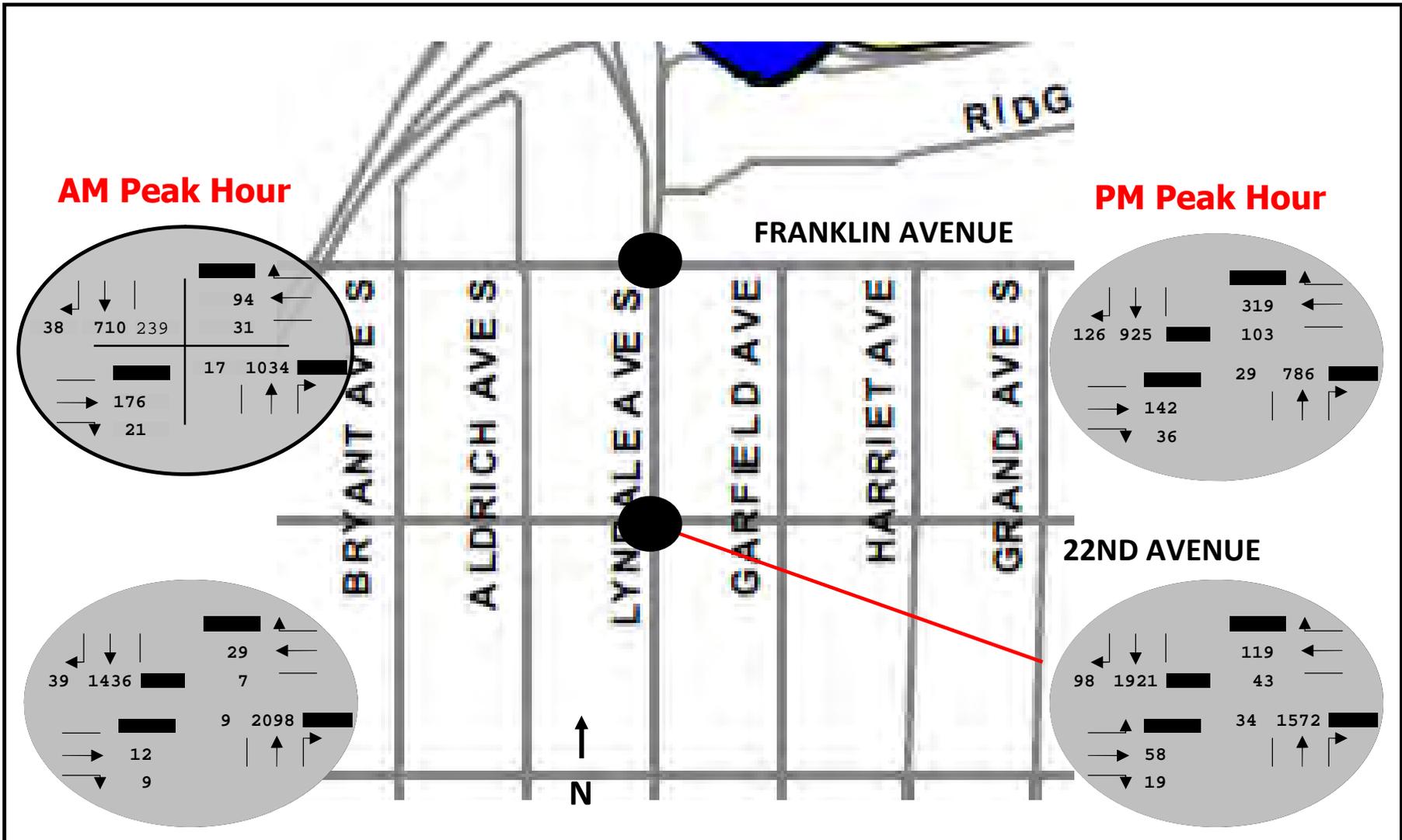
Forecast 2017 Build Intersection Capacity:

Table 11 shows results of the intersection capacity analysis that was conducted for the forecast 2017 Build condition. As shown, both intersections will operate with acceptable LOS after the proposed development has been implemented.

**TABLE 10
 TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
 FORECAST 2017 BUILD AM AND PM PEAK HOUR TURNS**

Intersection	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	239	710	38	31	94	239	17	1034	42	93	176	21
Franklin/Lyndale PM	264	925	126	103	319	341	29	786	30	64	142	36
22nd/Lyndale AM	78	1436	39	7	29	65	9	2098	25	90	12	9
22nd/Lyndale PM	44	1921	98	43	119	78	34	1572	52	92	58	19

Source: Biko Associates, Inc.



Theatre Garage Marquee Apartments
Travel Demand Management Plan



Figure 7
Forecast 2017
Build Turning Movements

**TABLE 11
 TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
 FORECAST 2017 BUILD AM AND PM PEAK HOUR INTERSECTION CAPACITY**

Intersection	Overall LOS	Signal Timing (sec)	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	B	60	B	A	A	B	B	B	A	B	B	C	C	C
Franklin/Lyndale PM	B	60	B	B	B	C	C	B	B	B	B	B	B	B
22nd/Lyndale AM	C	90	E	E	E	D	D	D	B	B	B	D	D	D
22nd/Lyndale PM	C	90	D	D	D	D	D	D	B	B	B	D	D	D

Source: Biko Associates, Inc.,

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PARKING CHARACTERISTICS OF THE PROPOSED DEVELOPMENT

CITY OF MINNEAPOLIS PARKING REQUIREMENTS

According to the Municipal Code, a minimum of 146 and a maximum of 247 off-street parking stalls will be required for proposed uses on the site. Table 12 details the code-specified, minimum and maximum parking supplies for each on-site use and presents a proposed parking supply for each use. As shown in Table 12, a total of 163 stalls are proposed for the development.

**TABLE 12
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN
PROPOSED PARKING SUPPLY**

On-Site Use	Independent Variable	Required Parking Supply		Proposed Supply
		Minimum	Maximum	
Restaurant	300 seats 7,331 SF	22	98	30
Theater	150 seats	30	60	30
New retail stores	4,000 SF	0	20	5
Existing retail in 2100 Building	13,800 SF	20	69	20
Apartments	82 dus	74	82**	78
Total		146	247	163

** No maximum number of parking stalls is identified in the Municipal Code. The assumed maximum parking ratio for retail is 1 stall per dwelling unit.

Source: Biko Associates, Inc.

BICYCLE PARKING

According to the Municipal Code, bicycle parking stalls are required for the proposed development. The Code specifies a minimum of 9 bicycle parking stalls, as detailed below, for the restaurant, theater, and retail uses.

- 3 bicycle parking stalls for the restaurant use
- 3 stalls for the theater use
- 3 stalls for the retail uses

The developers are proposing to install 14 stalls for these uses. In addition, the developers have decided to supply 82 bike parking stalls for residents in the apartment dwelling units, at a 1:1 ratio. The 82 stalls will be supplied on-site for the exclusive use by residents of the apartment dwelling units. These will be located along the western edge of the ground-level parking lot and in the parking ramp.

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CONCLUSIONS

OBSERVATIONS

Observations from the TDMP are:

1. The Traffic Impact Analysis showed that the two intersections that were analyzed will exhibit acceptable Levels of Service (LOS) under all conditions, including:
 - 2014 AM and PM peak hours
 - 2017 No-Build AM and PM peak hours
 - 2017 Build AM and PM peak hours
2. In order to enhance safe and efficient traffic operations, the northbound left-turn bay on the south approach to the Franklin/Lyndale intersection should be extended to the south. This can be accomplished by painting an extension onto the existing northbound left-turn bay.

While this is not necessary, because Lyndale Avenue has been constructed with two northbound lanes for general traffic, and vehicles accomplishing left-turns from Lyndale Avenue into the site can be overtaken by vehicles travelling north on Lyndale, provision of an extended northbound left-turn bay would enhance safety and efficiency.

3. Driveways defined for the site (one on Lyndale Avenue and one on Franklin Avenue) are located so as to ensure safety and efficiency for automobile traffic and pedestrians. As explained on page 17, both Lyndale and Franklin Avenues have two travel lanes in each direction. This configuration will allow through vehicles to overtake vehicles turning left into the site at Drives 1 and 2.
4. A trip generation analysis showed that the proposed development will generate a low volume of trips. A 10 percent alternative mode allowance was applied to the gross trip generation to account for the availability of transit routes directly adjacent to the development on Lyndale and Franklin Avenues
5. The parking supply proposed for the development, 163 off-street stalls, exceeds City of Minneapolis minimum requirements. According to the Municipal Code, a minimum of 146 and a maximum of 247 parking stalls will be required.

TRAVEL DEMAND MANAGEMENT PLAN

The following Travel Demand Management Plan presents action steps for management at TGMA development.

1. The management team will support bicycle transportation for residents of the 82 apartment dwelling units by providing bicycle racks along the western edge of the ground-level parking lot and in the parking ramp. A minimum of 82 bike parking spaces will be provided. If there is more demand for bike parking, the management team will provide bike racks to meet demand in the parking ramp. The management team will further support bicycle use by providing residents with maps of bicycle routes, facilities, and services.

Bike parking will also be provided on site for patrons of on-site businesses. A total of 14 bike parking spaces will be maintained in the public bike parking area, which is located along the south side of the development.

2. The management team will support transit use by providing information on transit services to residents in Welcome Packets. In addition to distributing this information in Welcome Packets, the management team will maintain this information in packets in conspicuous locations in the lobby of the apartment building. The packets shall include information on:
 - Transit routes and schedules
 - Guaranteed Ride Home (GRH) program sponsored by Metro Commuter Services
 - Forming and joining car-pool programs
 - High Occupancy Vehicle (HOV) facilities
 - New employee incentives to ride the bus
 - Metro Mobility
3. The management team will enroll TGMA development as an associate in the Transit Works Bus Pass Program, which allows associates to purchase bus passes at discounted prices, and inform residents that they are eligible to participate in the program.
4. The management will not subsidize single occupant vehicle use. Parking spaces will not be free but, instead, sold to residents with or in addition to the cost of a dwelling unit.

Residents of TGMA development will not be able to participate in future Critical Parking Areas (CPA). Extra parking stalls may be sold to residents of the TGMA development at the going market rate.

5. A Transportation Coordinator (TC) must be designated by the management. The TC will be responsible for monitoring travel demand and parking measures and will act as a liaison between the project and the Commuter Connection, a downtown Minneapolis Transportation Management Organization that provides information and services on commuting to/from the downtown area.
6. Provisions must be made for monitoring the travel and parking behavior of residents in the development. The purpose of monitoring will be to: a) track residents' modal choices and resulting mode splits in relation to the project's goals, b) identify additional travel demand measures that may be necessary to implement in the future to ensure goals are met, and c) control the volume of parking spillover in adjacent neighborhoods from residents owning more than one car.

SIGNATURE PAGE

TGMA DEVELOPERS, LLC

Dated: _____

By: _____

Its: _____

City of Minneapolis Community Planning and Economic Development Department

Dated: _____

By: _____

Its: _____

City of Minneapolis Public Works Department – Traffic and Parking Services

Dated: _____

By: _____

Its: _____

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APPENDIX

- I. **2014 Calculated 2014 Capacity Analysis Computer Printouts**
- II. **2017 Forecast No-Build Intersection Capacity Analysis Computer Printouts**
- III. **2017 Forecast Build Intersection Capacity Analysis Computer Printouts**

22nd/Lyndale 2014_AM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔			↔	
Volume (vph)	88	11	9	7	29	63	9	1971	23	74	1348	36
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.0			4.0			4.0			4.0	
Lane Util. Factor		0.95			0.95			0.95			0.95	
Frt		0.99			0.91			1.00			1.00	
Flt Protected		0.96			1.00			1.00			1.00	
Satd. Flow (prot)		3358			3193			3532			3517	
Flt Permitted		0.72			0.93			0.95			0.63	
Satd. Flow (perm)		2528			2985			3339			2225	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	96	12	10	8	32	68	10	2142	25	80	1465	39
RTOR Reduction (vph)	0	0	0	0	0	0	0	1	0	0	1	0
Lane Group Flow (vph)	0	118	0	0	108	0	0	2176	0	0	1583	0
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	4	0	0	4
Turn Type	Perm		Perm		Perm		Perm		Perm		Perm	
Protected Phases		4			4			2				2
Permitted Phases	4			4			2			2		
Actuated Green, G (s)		9.6			9.6			72.4			72.4	
Effective Green, g (s)		9.6			9.6			72.4			72.4	
Actuated g/C Ratio		0.11			0.11			0.80			0.80	
Clearance Time (s)		4.0			4.0			4.0			4.0	
Vehicle Extension (s)		3.0			3.0			3.0			3.0	
Lane Grp Cap (vph)		270			318			2686			1790	
v/s Ratio Prot												
v/s Ratio Perm		c0.05			0.04			0.65			c0.71	
v/c Ratio		0.44			0.34			0.81			0.96dl	
Uniform Delay, d1		37.7			37.3			4.9			6.0	
Progression Factor		1.00			1.00			1.00			1.00	
Incremental Delay, d2		1.1			0.6			2.8			6.8	
Delay (s)		38.8			37.9			7.7			12.7	
Level of Service		D			D			A			B	
Approach Delay (s)		38.8			37.9			7.7			12.7	
Approach LOS		D			D			A			B	

Intersection Summary

HCM Average Control Delay	11.5	HCM Level of Service	B
HCM Volume to Capacity ratio	0.83		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	8.0
Intersection Capacity Utilization	111.8%	ICU Level of Service	H
Analysis Period (min)	15		

dl Defacto Left Lane. Recode with 1 though lane as a left lane.

c Critical Lane Group

22nd/Lyndale 2014 PM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕			↕↕			↕↕	
Volume (vph)	90	56	18	42	115	75	32	1465	49	42	1800	93
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.5			5.5			5.5			5.5	
Lane Util. Factor		0.95			0.95			0.95			0.95	
Frt		0.98			0.95			1.00			0.99	
Flt Protected		0.97			0.99			1.00			1.00	
Satd. Flow (prot)		3387			3337			3519			3510	
Flt Permitted		0.70			0.85			0.78			0.84	
Satd. Flow (perm)		2420			2871			2762			2957	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	98	61	20	46	125	82	35	1592	53	46	1957	101
RTOR Reduction (vph)	0	0	0	0	0	0	0	2	0	0	3	0
Lane Group Flow (vph)	0	179	0	0	253	0	0	1678	0	0	2101	0
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	4	0	0	4
Tum Type	Perm			Perm			Perm			Perm		
Protected Phases		4			4			2			2	
Permitted Phases	4			4			2			2		
Actuated Green, G (s)		13.1			13.1			65.9			65.9	
Effective Green, g (s)		13.1			13.1			65.9			65.9	
Actuated g/C Ratio		0.15			0.15			0.73			0.73	
Clearance Time (s)		5.5			5.5			5.5			5.5	
Vehicle Extension (s)		3.0			3.0			3.0			3.0	
Lane Grp Cap (vph)		352			418			2022			2165	
v/s Ratio Prot												
v/s Ratio Perm		0.07			0.09			0.61			0.71	
v/c Ratio		0.51			0.61			0.83			0.97	
Uniform Delay, d1		35.5			36.0			8.2			11.1	
Progression Factor		1.00			1.00			1.00			1.00	
Incremental Delay, d2		1.2			2.5			4.1			13.5	
Delay (s)		36.6			38.5			12.3			24.6	
Level of Service		D			D			B			C	
Approach Delay (s)		36.6			38.5			12.3			24.6	
Approach LOS		D			D			B			C	

Intersection Summary			
HCM Average Control Delay	21.1	HCM Level of Service	C
HCM Volume to Capacity ratio	0.91		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	11.0
Intersection Capacity Utilization	108.5%	ICU Level of Service	G
Analysis Period (min)	15		

c Critical Lane Group

Franklin-Lyndale 2014 AM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔↔			↔↔	↔	↔	↔↔		↔	↔↔	
Volume (vph)	90	171	20	31	92	232	16	965	40	226	664	36
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.0			4.0	4.0	4.0	4.0		4.0	4.0	
Lane Util. Factor		0.95			0.95	1.00	1.00	0.95		1.00	0.95	
Frt		0.99			1.00	0.85	1.00	0.99		1.00	0.99	
Flt Protected		0.98			0.99	1.00	0.95	1.00		0.95	1.00	
Satd. Flow (prot)		3446			3495	1558	1770	3518		1770	3512	
Flt Permitted		0.81			0.83	1.00	0.29	1.00		0.15	1.00	
Satd. Flow (perm)		2850			2951	1558	535	3518		280	3512	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	98	186	22	34	100	252	17	1049	43	246	722	39
RTOR Reduction (vph)	0	0	0	0	0	0	0	4	0	0	6	0
Lane Group Flow (vph)	0	306	0	0	134	252	17	1088	0	246	755	0
Bus Blockages (#/hr)	0	0	4	0	0	4	0	0	4	0	0	4
Turn Type	Perm			Perm		pt+ov	pm+pt			pm+pt		
Protected Phases		4			4	4 1	1	2		1	2	
Permitted Phases	4			4			2			2		
Actuated Green, G (s)		13.1			13.1	25.4	34.9	26.6		34.9	26.6	
Effective Green, g (s)		13.1			13.1	25.4	34.9	26.6		34.9	26.6	
Actuated g/C Ratio		0.22			0.22	0.42	0.58	0.44		0.58	0.44	
Clearance Time (s)		4.0			4.0		4.0	4.0		4.0	4.0	
Vehicle Extension (s)		3.0			3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)		622			644	660	482	1560		369	1557	
v/s Ratio Prot						0.16	0.00	c0.31		c0.09	0.22	
v/s Ratio Perm		c0.11			0.05		0.02			0.30		
v/c Ratio		0.49			0.21	0.38	0.04	0.70		0.67	0.49	
Uniform Delay, d1		20.5			19.2	11.9	5.6	13.5		8.3	11.8	
Progression Factor		1.00			1.00	1.00	1.00	1.00		1.00	1.00	
Incremental Delay, d2		0.6			0.2	0.4	0.0	2.6		4.5	1.1	
Delay (s)		21.2			19.4	12.3	5.6	16.1		12.8	12.9	
Level of Service		C			B	B	A	B		B	B	
Approach Delay (s)		21.2			14.7			15.9			12.9	
Approach LOS		C			B			B			B	

Intersection Summary

HCM Average Control Delay	15.2	HCM Level of Service	B
HCM Volume to Capacity ratio	0.64		
Actuated Cycle Length (s)	60.0	Sum of lost time (s)	12.0
Intersection Capacity Utilization	65.2%	ICU Level of Service	C
Analysis Period (min)	15		

c Critical Lane Group

Franklin/Lyndale 2014 PM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	62	138	35	100	310	331	27	724	28	249	855	119
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.0			4.0	4.0	4.0	4.0		4.0	4.0	
Lane Util. Factor		0.95			0.95	1.00	1.00	0.95		1.00	0.95	
Frt		0.98			1.00	0.85	1.00	0.99		1.00	0.98	
Flt Protected		0.99			0.99	1.00	0.95	1.00		0.95	1.00	
Satd. Flow (prot)		3415			3496	1558	1770	3520		1770	3474	
Flt Permitted		0.77			0.80	1.00	0.25	1.00		0.19	1.00	
Satd. Flow (perm)		2652			2841	1558	462	3520		353	3474	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	67	150	38	109	337	360	29	787	30	271	929	129
RTOR Reduction (vph)	0	0	0	0	0	0	0	4	0	0	17	0
Lane Group Flow (vph)	0	255	0	0	446	360	29	813	0	271	1041	0
Bus Blockages (#/hr)	0	0	4	0	0	4	0	0	4	0	0	4
Turn Type	Perm			Perm		pt+ov	pm+pt			pm+pt		
Protected Phases		4			8	8	1	5	2	1	6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)		14.3			14.3	26.7	21.9	20.3		32.7	27.1	
Effective Green, g (s)		14.3			14.3	26.7	21.9	20.3		32.7	27.1	
Actuated g/C Ratio		0.26			0.26	0.49	0.40	0.37		0.59	0.49	
Clearance Time (s)		4.0			4.0		4.0	4.0		4.0	4.0	
Vehicle Extension (s)		3.0			3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)		690			739	756	222	1299		426	1712	
v/s Ratio Prot						c0.23	0.00	0.23		c0.10	0.30	
v/s Ratio Perm		0.10			c0.16		0.05			c0.28		
v/c Ratio		0.37			0.60	0.48	0.13	0.63		0.64	0.61	
Uniform Delay, d1		16.7			17.9	9.5	10.2	14.2		7.2	10.1	
Progression Factor		1.00			1.00	1.00	1.00	1.00		1.00	1.00	
Incremental Delay, d2		0.3			1.4	0.5	0.3	2.3		3.1	1.6	
Delay (s)		17.0			19.3	9.9	10.4	16.5		10.3	11.7	
Level of Service		B			B	A	B	B		B	B	
Approach Delay (s)		17.0			15.1			16.3			11.4	
Approach LOS		B			B			B			B	

Intersection Summary			
HCM Average Control Delay	14.1	HCM Level of Service	B
HCM Volume to Capacity ratio	0.58		
Actuated Cycle Length (s)	55.0	Sum of lost time (s)	4.0
Intersection Capacity Utilization	66.2%	ICU Level of Service	C
Analysis Period (min)	15		

c - Critical Lane Group

22nd/Lyndale 2017 NB AM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SEL	SBT	SBR
Lane Configurations		↔			↔			↔			↔	
Volume (vph)	90	12	9	7	29	65	9	2092	25	78	1431	39
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.5			5.5			5.5			5.5	
Lane Util. Factor		0.95			0.95			0.95			0.95	
Flt		0.99			0.90			1.00			1.00	
Flt Protected		0.96			1.00			1.00			1.00	
Satd. Flow (prot)		3359			3188			3532			3517	
Flt Permitted		0.72			0.93			0.94			0.57	
Satd. Flow (perm)		2525			2962			3334			2026	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	98	13	10	8	32	71	10	2274	27	85	1555	42
RTOR Reduction (vph)	0	0	0	0	0	0	0	1	0	0	2	0
Lane Group Flow (vph)	0	121	0	0	111	0	0	2310	0	0	1680	0
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	4	0	0	4
Turn Type	Perm			Perm			Perm			Perm		
Protected Phases		4			4			2			2	
Permitted Phases	4			4			2			2		
Actuated Green, G (s)		9.7			9.7			69.3			69.3	
Effective Green, g (s)		9.7			9.7			69.3			69.3	
Actuated g/C Ratio		0.11			0.11			0.77			0.77	
Clearance Time (s)		5.5			5.5			5.5			5.5	
Vehicle Extension (s)		3.0			3.0			3.0			3.0	
Lane Grp Cap (vph)		272			319			2567			1560	
v/s Ratio Prot												
v/s Ratio Perm		c0.05			0.04			0.69			c0.83	
v/c Ratio		0.44			0.35			0.90			1.08	
Uniform Delay, d1		37.6			37.2			7.8			10.4	
Progression Factor		1.00			1.00			1.00			1.00	
Incremental Delay, d2		1.2			0.7			5.6			46.7	
Delay (s)		38.8			37.9			13.4			57.1	
Level of Service		D			D			B			E	
Approach Delay (s)		38.8			37.9			13.4			57.1	
Approach LOS		D			D			B			E	
Intersection Summary												
HCM Average Control Delay			32.1									
HCM Volume to Capacity ratio			1.00									
Actuated Cycle Length (s)			90.0									
Intersection Capacity Utilization			119.8%									
Analysis Period (min)			15									
c Critical Lane Group												

22nd/Lyndale 2017 NB PM

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	92	58	19	43	119	78	34	1555	52	44	1910	98
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.0			4.0			4.0			4.0	
Lane Util. Factor		0.95			0.95			0.95			0.95	
Flt		0.98			0.95			1.00			0.99	
Flt Protected		0.97			0.99			1.00			1.00	
Satd. Flow (prot)		3387			3336			3519			3510	
Flt Permitted		0.66			0.86			0.78			0.84	
Satd. Flow (perm)		2283			2896			2734			2937	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	100	63	21	47	129	85	37	1690	57	48	2076	107
RTOR Reduction (vph)	0	0	0	0	0	0	0	2	0	0	4	0
Lane Group Flow (vph)	0	184	0	0	261	0	0	1782	0	0	2227	0
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	4	0	0	4
Turn Type	Perm			Perm			Perm			Perm		
Protected Phases		4			4			2			2	
Permitted Phases	4			4			2			2		
Actuated Green, G (s)		13.2			13.2			68.8			68.8	
Effective Green, g (s)		13.2			13.2			68.8			68.8	
Actuated g/C Ratio		0.15			0.15			0.76			0.76	
Clearance Time (s)		4.0			4.0			4.0			4.0	
Vehicle Extension (s)		3.0			3.0			3.0			3.0	
Lane Grp Cap (vph)		335			425			2090			2245	
v/s Ratio Prot												
v/s Ratio Perm		0.08			0.09			0.65			0.76	
v/c Ratio		0.55			0.61			0.85			0.99	
Uniform Delay, d1		35.6			36.0			7.2			10.3	
Progression Factor		1.00			1.00			1.00			1.00	
Incremental Delay, d2		1.8			2.6			4.7			17.2	
Delay (s)		37.5			38.6			11.8			27.6	
Level of Service		D			D			B			C	
Approach Delay (s)		37.5			38.6			11.8			27.6	
Approach LOS		D			D			B			C	
Intersection Summary												
HCM Average Control Delay			22.3									
HCM Volume to Capacity ratio			0.93									
Actuated Cycle Length (s)			90.0									
Intersection Capacity Utilization			109.7%							8.0		
Analysis Period (min)			15									
c Critical Lane Group												

Franklin/Lyndale 2017 NB AM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	93	176	21	31	94	239	17	1024	42	239	704	38
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.0			4.0	4.0	4.0	4.0		4.0	4.0	
Lane Util. Factor		0.95			0.95	1.00	1.00	0.95		1.00	0.95	
Flt		0.99			1.00	0.85	1.00	0.99		1.00	0.99	
Flt Protected		0.98			0.99	1.00	0.95	1.00		0.95	1.00	
Satd. Flow (prot)		3445			3496	1558	1770	3518		1770	3512	
Flt Permitted		0.81			0.84	1.00	0.35	1.00		0.13	1.00	
Satd. Flow (perm)		2843			2957	1558	647	3518		251	3512	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	101	191	23	34	102	260	18	1113	46	260	765	41
RTOR Reduction (vph)	0	0	0	0	0	0	0	5	0	0	5	0
Lane Group Flow (vph)	0	315	0	0	136	260	18	1154	0	260	801	0
Bus Blockages (#/hr)	0	0	4	0	0	4	0	0	4	0	0	4
Turn Type	Perm			Perm		Prot	pm+pt			pm+pt		
Protected Phases		4			8	8	5	2		1	6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)		13.8			13.8	13.8	26.5	25.7		38.2	33.4	
Effective Green, g (s)		13.8			13.8	13.8	26.5	25.7		38.2	33.4	
Actuated g/C Ratio		0.23			0.23	0.23	0.44	0.43		0.64	0.56	
Clearance Time (s)		4.0			4.0	4.0	4.0	4.0		4.0	4.0	
Vehicle Extension (s)		3.0			3.0	3.0	3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)		654			680	358	301	1507		375	1955	
v/s Ratio Prot						c0.17	0.00	c0.33		c0.10	0.23	
v/s Ratio Perm		0.11			0.05		0.03			0.34		
v/c Ratio		0.48			0.20	0.73	0.06	0.77		0.69	0.41	
Uniform Delay, d1		20.0			18.6	21.4	9.4	14.6		9.5	7.6	
Progression Factor		1.00			1.00	1.00	1.00	1.00		1.00	1.00	
Incremental Delay, d2		0.6			0.1	7.2	0.1	3.8		5.5	0.6	
Delay (s)		20.6			18.8	28.5	9.5	18.4		15.0	8.3	
Level of Service		C			B	C	A	B		B	A	
Approach Delay (s)		20.6			25.2			18.2			9.9	
Approach LOS		C			C			B			A	
Intersection Summary												
HCM Average Control Delay			16.4									HCM Level of Service B
HCM Volume to Capacity ratio			0.74									
Actuated Cycle Length (s)			60.0									Sum of lost time (s) 12.0
Intersection Capacity Utilization			68.0%									ICU Level of Service C
Analysis Period (min)			15									

c Critical Lane Group

Franklin/Lyndale 2017 NB PM

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	64	142	36	103	319	341	29	768	30	264	908	126
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.5			5.5	5.5	5.5	4.0		4.0	4.0	
Lane Util. Factor		0.95			0.95	1.00	1.00	0.95		1.00	0.95	
Frt		0.98			1.00	0.85	1.00	0.99		1.00	0.98	
Flt Protected		0.99			0.99	1.00	0.95	1.00		0.95	1.00	
Satd. Flow (prot)		3415			3497	1558	1770	3519		1770	3475	
Flt Permitted		0.70			0.80	1.00	0.20	1.00		0.17	1.00	
Satd. Flow (perm)		2424			2831	1558	364	3519		316	3475	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	70	154	39	112	347	371	32	835	33	287	987	137
RTOR Reduction (vph)	0	0	0	0	0	0	0	4	0	0	16	0
Lane Group Flow (vph)	0	263	0	0	459	371	32	864	0	287	1108	0
Bus Blockages (#/hr)	0	0	4	0	0	4	0	0	4	0	0	4
Turn Type	Perm			Perm		pt+ov	pm+pt			pm+pt		
Protected Phases		4			8	8	5	2		1	6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)		14.7			14.7	29.5	24.1	22.5		35.8	28.7	
Effective Green, g (s)		14.7			14.7	29.5	24.1	22.5		35.8	28.7	
Actuated g/C Ratio		0.24			0.24	0.49	0.40	0.38		0.60	0.48	
Clearance Time (s)		5.5			5.5		5.5	4.0		4.0	4.0	
Vehicle Extension (s)		3.0			3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)		594			694	766	184	1320		414	1662	
v/s Ratio Prot						c0.24	0.00	0.25		c0.11	0.32	
v/s Ratio Perm		0.11			c0.16		0.07			c0.31		
v/c Ratio		0.44			0.66	0.48	0.17	0.65		0.69	0.67	
Uniform Delay, d1		19.2			20.4	10.2	11.2	15.5		8.4	12.0	
Progression Factor		1.00			1.00	1.00	1.00	1.00		1.00	1.00	
Incremental Delay, d2		0.5			2.4	0.5	0.5	2.5		5.0	2.1	
Delay (s)		19.7			22.8	10.7	11.6	18.1		13.4	14.1	
Level of Service		B			C	B	B	B		B	B	
Approach Delay (s)		19.7			17.4			17.8			14.0	
Approach LOS		B			B			B			B	
Intersection Summary												
HCM Average Control Delay			16.3			HCM Level of Service					B	
HCM Volume to Capacity ratio			0.62									
Actuated Cycle Length (s)			60.0			Sum of lost time (s)					5.5	
Intersection Capacity Utilization			71.4%			ICU Level of Service					C	
Analysis Period (min)			15									

c Critical Lane Group

22nd/Lyndale 2017 Build AM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	90	12	9	7	29	65	9	2098	25	78	1436	39
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.5			5.5			5.5			5.5	
Lane Util. Factor		0.95			0.95			0.95			0.95	
Frt		0.99			0.90			1.00			1.00	
Flt Protected		0.96			1.00			1.00			1.00	
Satd. Flow (prot)		3359			3188			3532			3517	
Flt Permitted		0.72			0.93			0.94			0.57	
Satd. Flow (perm)		2525			2962			3334			2024	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	98	13	10	8	32	71	10	2280	27	85	1561	42
RTOR Reduction (vph)	0	0	0	0	0	0	0	1	0	0	2	0
Lane Group Flow (vph)	0	121	0	0	111	0	0	2316	0	0	1686	0
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	4	0	0	4
Turn Type	Perm			Perm			Perm			Perm		
Protected Phases		4			4			2			2	
Permitted Phases	4			4			2			2		
Actuated Green, G (s)		9.7			9.7			69.3			69.3	
Effective Green, g (s)		9.7			9.7			69.3			69.3	
Actuated g/C Ratio		0.11			0.11			0.77			0.77	
Clearance Time (s)		5.5			5.5			5.5			5.5	
Vehicle Extension (s)		3.0			3.0			3.0			3.0	
Lane Grp Cap (vph)		272			319			2567			1558	
v/s Ratio Prot												
v/s Ratio Perm		c0.05			0.04			0.69			c0.83	
v/c Ratio		0.44			0.35			0.90			1.08	
Uniform Delay, d1		37.6			37.2			7.8			10.4	
Progression Factor		1.00			1.00			1.00			1.00	
Incremental Delay, d2		1.2			0.7			5.7			48.7	
Delay (s)		38.8			37.9			13.5			59.0	
Level of Service		D			D			B			E	
Approach Delay (s)		38.8			37.9			13.5			59.0	
Approach LOS		D			D			B			E	
Intersection Summary												
HCM Average Control Delay			33.0					HCM Level of Service		C		
HCM Volume to Capacity ratio			1.00									
Actuated Cycle Length (s)			90.0					Sum of lost time (s)		11.0		
Intersection Capacity Utilization			119.9%					ICU Level of Service		H		
Analysis Period (min)			15									

c Critical Lane Group

22nd/Lyndale 2017 Build PM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		↔			↔			↔			↔		
Volume (vph)	92	58	19	43	119	78	34	1572	52	44	1921	98	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Total Lost time (s)		5.5			5.5			5.5			5.5		
Lane Util. Factor		0.95			0.95			0.95			0.95		
Frt		0.98			0.95			1.00			0.99		
Flt Protected		0.97			0.99			1.00			1.00		
Satd. Flow (prot)		3387			3336			3519			3510		
Flt Permitted		0.69			0.85			0.74			0.82		
Satd. Flow (perm)		2393			2872			2622			2881		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Adj. Flow (vph)	100	63	21	47	129	85	37	1709	57	48	2088	107	
RTOR Reduction (vph)	0	0	0	0	0	0	0	2	0	0	4	0	
Lane Group Flow (vph)	0	184	0	0	261	0	0	1801	0	0	2239	0	
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	4	0	0	4	
Turn Type	Perm			Perm			Perm			Perm			
Protected Phases		4			4			2			2		
Permitted Phases	4			4			2			2			
Actuated Green, G (s)		12.3			12.3			66.7			66.7		
Effective Green, g (s)		12.3			12.3			66.7			66.7		
Actuated g/C Ratio		0.14			0.14			0.74			0.74		
Clearance Time (s)		5.5			5.5			5.5			5.5		
Vehicle Extension (s)		3.0			3.0			3.0			3.0		
Lane Grp Cap (vph)		327			393			1943			2135		
v/s Ratio Prot													
v/s Ratio Perm		0.08			c0.09			0.69			c0.78		
v/c Ratio		0.56			0.66			0.93			1.05		
Uniform Delay, d1		36.3			36.9			9.6			11.6		
Progression Factor		1.00			1.00			1.00			1.00		
Incremental Delay, d2		2.2			4.2			9.2			33.7		
Delay (s)		38.5			41.1			18.8			45.4		
Level of Service		D			D			B			D		
Approach Delay (s)		38.5			41.1			18.8			45.4		
Approach LOS		D			D			B			D		
Intersection Summary													
HCM Average Control Delay			34.2									HCM Level of Service	C
HCM Volume to Capacity ratio			0.99										
Actuated Cycle Length (s)			90.0									Sum of lost time (s)	11.0
Intersection Capacity Utilization			113.8%									ICU Level of Service	H
Analysis Period (min)			15										

c Critical Lane Group

Franklin/Lyndale 2017 Build AM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	93	176	21	31	94	239	17	1034	42	239	710	38
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.5			5.5	5.5	5.5	4.0		4.0	4.0	
Lane Util. Factor		0.95			0.95	1.00	1.00	0.95		1.00	0.95	
Fr't		0.99			1.00	0.85	1.00	0.99		1.00	0.99	
Flt Protected		0.98			0.99	1.00	0.95	1.00		0.95	1.00	
Satd. Flow (prot)		3445			3496	1558	1770	3518		1770	3512	
Flt Permitted		0.81			0.81	1.00	0.34	1.00		0.14	1.00	
Satd. Flow (perm)		2850			2862	1558	642	3518		252	3512	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	101	191	23	34	102	260	18	1124	46	260	772	41
RTOR Reduction (vph)	0	0	0	0	0	0	0	5	0	0	5	0
Lane Group Flow (vph)	0	315	0	0	136	260	18	1165	0	260	808	0
Bus Blockages (#/hr)	0	0	4	0	0	4	0	0	4	0	0	4
Turn Type	Perm			Perm		pt+ov	pm+pt			pm+pt		
Protected Phases		4			8	8	1	5	2	1	6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)		12.5			12.5	26.4	26.4	25.6		38.0	31.7	
Effective Green, g (s)		12.5			12.5	26.4	26.4	25.6		38.0	31.7	
Actuated g/C Ratio		0.21			0.21	0.44	0.44	0.43		0.63	0.53	
Clearance Time (s)		5.5			5.5		5.5	4.0		4.0	4.0	
Vehicle Extension (s)		3.0			3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)		594			596	686	298	1501		372	1856	
v/s Ratio Prot						0.17	0.00	c0.33		c0.10	0.23	
v/s Ratio Perm		c0.11			0.05		0.03			0.35		
v/c Ratio		0.53			0.23	0.38	0.06	0.78		0.70	0.44	
Uniform Delay, d1		21.1			19.7	11.3	9.5	14.7		9.6	8.7	
Progression Factor		1.00			1.00	1.00	1.00	1.00		1.00	1.00	
Incremental Delay, d2		0.9			0.2	0.4	0.1	4.0		5.7	0.7	
Delay (s)		22.1			19.9	11.6	9.6	18.8		15.3	9.4	
Level of Service		C			B	B	A	B		B	A	
Approach Delay (s)		22.1			14.5			18.6			10.8	
Approach LOS		C			B			B			B	
Intersection Summary												
HCM Average Control Delay			15.6			HCM Level of Service				B		
HCM Volume to Capacity ratio			0.70									
Actuated Cycle Length (s)			60.0			Sum of lost time (s)				13.5		
Intersection Capacity Utilization			70.7%			ICU Level of Service				C		
Analysis Period (min)			15									

c Critical Lane Group

Franklin/Lyndale 2017 Build PM

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕	↗	↗	↕↕		↗	↕↕	
Volume (vph)	64	142	36	103	319	341	29	786	30	264	925	126
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.5			5.5	5.5	5.5	4.0		4.0	4.0	
Lane Util. Factor		0.95			0.95	1.00	1.00	0.95		1.00	0.95	
Frt		0.98			1.00	0.85	1.00	0.99		1.00	0.98	
Flt Protected		0.99			0.99	1.00	0.95	1.00		0.95	1.00	
Satd. Flow (prot)		3415			3497	1558	1770	3519		1770	3476	
Flt Permitted		0.70			0.80	1.00	0.19	1.00		0.16	1.00	
Satd. Flow (perm)		2424			2831	1558	350	3519		301	3476	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	70	154	39	112	347	371	32	854	33	287	1005	137
RTOR Reduction (vph)	0	0	0	0	0	0	0	4	0	0	16	0
Lane Group Flow (vph)	0	263	0	0	459	371	32	883	0	287	1126	0
Bus Blockages (#/hr)	0	0	4	0	0	4	0	0	4	0	0	4
Turn Type	Perm			Perm		pt+ov	pm+pt			pm+pt		
Protected Phases		4			8	8	1	5	2	1	6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)		14.7			14.7	29.5	24.1	22.5		35.8	28.7	
Effective Green, g (s)		14.7			14.7	29.5	24.1	22.5		35.8	28.7	
Actuated g/C Ratio		0.24			0.24	0.49	0.40	0.38		0.60	0.48	
Clearance Time (s)		5.5			5.5		5.5	4.0		4.0	4.0	
Vehicle Extension (s)		3.0			3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)		594			694	766	178	1320		407	1663	
v/s Ratio Prot						c0.24	0.00	0.25		c0.11	0.32	
v/s Ratio Perm		0.11			c0.16		0.07			c0.31		
v/c Ratio		0.44			0.66	0.48	0.18	0.67		0.71	0.68	
Uniform Delay, d1		19.2			20.4	10.2	11.2	15.6		8.6	12.1	
Progression Factor		1.00			1.00	1.00	1.00	1.00		1.00	1.00	
Incremental Delay, d2		0.5			2.4	0.5	0.5	2.7		5.5	2.2	
Delay (s)		19.7			22.8	10.7	11.7	18.3		14.1	14.3	
Level of Service		B			C	B	B	B		B	B	
Approach Delay (s)		19.7			17.4			18.1			14.3	
Approach LOS		B			B			B			B	
Intersection Summary												
HCM Average Control Delay			16.5			HCM Level of Service				B		
HCM Volume to Capacity ratio			0.63									
Actuated Cycle Length (s)			60.0			Sum of lost time (s)				5.5		
Intersection Capacity Utilization			71.9%			ICU Level of Service				C		
Analysis Period (min)			15									

c Critical Lane Group

Anderson, Mei-Ling C.

From: Bender, Lisa
Sent: Wednesday, July 30, 2014 1:21 PM
To: Anderson, Mei-Ling C.
Cc: Somogyi, Ben
Subject: FW: Ward 10 Contact Form

For record

Lisa Bender
10th Ward City Council Member

From: no-reply@minneapolismn.gov [<mailto:no-reply@minneapolismn.gov>]
Sent: Wednesday, July 30, 2014 12:33 PM
To: Crockett, Matthew; Somogyi, Ben; Bender, Lisa
Subject: Ward 10 Contact Form

City of Minneapolis

Name * Gary Dombouy
Email * gdombouy@aol.com
Phone (612) 871-0344
Phone Type Home
Address 110 East 25th Street
City Minneapolis
State MN
Zip 55404

Question/Comment * Master's Theater Garage and Marquee Apartments is a great proposal for the Franklin-Lyndale intersection. Master under Don Gerberding has contributed some very attractive multi-unit developments on Nicollet Avenue (at Franklin and at 26th). However I am concerned about Master's history of construction quality: aesthetically admirable while structurally unsettling. Owners at Artist Quarter Lofts experienced roof-leaking problems only a year after completion! Common with new construction but. . . The later built Master Greenleaf Lofts foreclosed and now is under complete reconstruction (including brick facing) because of bad materials. While Don Gerberding has admirably modified the Theater Garage and Marquee Apartments by decreasing the height through reducing the number of and increasing the price of units, how will he address this reputation of construction practices?

This is an email generated from the City of Minneapolis website. * Required fields are indicated with an asterisk.

Anderson, Mei-Ling C.

From: Peter Kim <sukkikim@gmail.com>
Sent: Sunday, February 09, 2014 4:11 PM
To: Anderson, Mei-Ling C.; Smoley, John
Subject: Fwd: Ward 10 Contact Form

I am copying you for public record.

----- Forwarded message -----

From: Peter Kim <sukkikim@gmail.com>
Date: Wed, Feb 5, 2014 at 10:53 AM
Subject: Re: Ward 10 Contact Form
To: "Bender, Lisa" <Lisa.Bender@minneapolismn.gov>
Cc: "Palmer, Leah" <Leah.Palmer2@minneapolismn.gov>, "Somogyi, Ben" <Ben.Somogyi@minneapolismn.gov>

SUBJ: Proposed Development 2004-2018 Lyndale Avenue South at Franklin

Dear Ms. Bender

I have to take back my support for new 2004 - 2018 Lyndale Avenue South development.

I liked an idea from newspaper yet after reviewing their proposal from city website, developer lacks a logic to change city zoning only for them.

If developer wants more height and less set-back, they need to provide counters offer to the area (such as parks, art, high end building material and ammenities that make logical sense to nearby residents). Otherwise, if granted, city is making an exception and favor for this particular developer which you cannot justiy.

If develpoer thinks City Ordinance and Zoning are made to be broken, they should bring their business to other cities.

Also, We (LHENA) had several damaging issues due to incompetent city staff. As an example, staff **wrongfully** issued a permit to the house for 3rd floor remodeling and work as done.

City was desperate to settle down and I(LHENA Z&P chair) asked if city has an insurance to recover damages done.

City did not have and city staff was not fired either. Lawsuit was only way to resolve this issue and LHENA did not take that road since we know it cost our tax dollars.

I think samething is going on with KNOX development.

<http://www.southwestjournal.com/news/news/luxury-apartment-owners-face-costly-groundwater-problem>

Minneapolis residents lost credit on city staff performance and **Staff need to face consequences/have ownership like any other job in America.**

City staff should fight for residents to make better projects. Their job is not given without performance.

I will leave it to you how to resolve this critical issue yet personally I will not hire such incompetent staff for my business.

Best.

--
Peter Kim

Anderson, Mei-Ling C.

From: Anderson, Mei-Ling C.
Sent: Friday, March 14, 2014 10:57 AM
To: 'Elizabeth Liebhard'
Cc: Don Gerberding
Subject: RE: Franklin-Lyndale project

Thank you! I will make sure it is included in correspondence if/when there is a formal application.

Mei-Ling

From: Elizabeth Liebhard [<mailto:ElizabethL@masterpropertiesmn.com>]
Sent: Friday, March 14, 2014 10:16 AM
To: Anderson, Mei-Ling C.
Cc: Don Gerberding
Subject: FW: Franklin-Lyndale project

Mei-Ling,
Just forwarding to you as I saw your email address was incorrectly typed below.
Thanks,

We've Moved! Please note our new address:

Elizabeth Liebhard
Project Coordinator

Master Properties Minnesota LLC
34 – 13th Ave NE
Suite 102A
Minneapolis, MN 55413

Direct Phone: (612) 236-1633
Fax: (612) 872-9201
Email: ElizabethL@masterpropertiesmn.com

<http://www.masterpropertiesmn.com>



From: William Wells [<mailto:wellsandcompany@yahoo.com>]
Sent: Friday, March 14, 2014 10:13 AM
To: Don Gerberding
Cc: Greg Ottum; Jason Haugen; Elizabeth Liebhard; meiling.anderson@minneapolismn.gov; mie-ling.anderson@minneapolismn.gov
Subject: Franklin-Lyndale project

Don Gerberding — Principal

I live in the Whittier Neighborhood - South Minneapolis- and I was at the Neighborhood Meeting on Wednesday night.

I wanted to offer you and your team, my full support for your project at Franklin-and-Lyndale.

The site you are seeking to develop is an area of blight and your new building is vibrant and beautiful. I like all the parking options and different material choices on the building. I also like the attention to urban planning and sidewalk improvements.

The notion that everyone in the neighborhood is against your project **is false**. We just have 3 or 4 angry people that yell louder than other intelligent people. I am sorry some of the neighborhood residents were rude to you.

Some of the points made about the large blank parking ramp wall facing the residential neighborhood on the West Property line are valid.

But I am sure you can work with your design team and City Staff to address this issue.

Please don't give up.

I will be there to speak in **support of your project**, when you present it to the City Planning Commission.

William Wells
2838 Fremont Ave S. Apt 616
Minneapolis, MN 55408
612-669-2052

William Wells is 30 years old and has a Master's Degree in Architecture and Urban Planning from the U of M. William Wells has live in Uptown for 5 years. He is a licensed architect and Real Estate Agent.
www.wellsandcompanyarchitects.com

Anderson, Mei-Ling C.

From: Trilby Busch <trilbybusch@yahoo.com>
Sent: Tuesday, February 04, 2014 5:39 PM
To: Hodges, Betsy A.; Bender, Lisa
Cc: Goodman, Lisa R.; Glidden, Elizabeth A.; Cano, Alondra; Quincy, John; Johnson, Andrew; linea.palmisano@minneapolis.gov; Johnson, Barbara A. - City Council; Yang, Blong; Warsame, Abdi; Reich, Kevin A.; Gordon, Cam A.; Frey, Jacob; Glidden, Elizabeth A.; Anderson, Mei-Ling C.
Subject: Proposed Development 2004-2018 Lyndale Avenue South
Attachments: Lyn-Franklin Development.doc

Dear Mayor Hodges and Council Member Bender:

Attached and pasted below please find my comments regarding the mixed-use building proposed for the Wedge, at 2004-2018 Lyndale Avenue South.

Thank you.

Sincerely yours,

Trilby Busch
2648 Emerson Ave. S.
Minneapolis, MN 55408

To Mayor Betsy Hodges, the City Council, and the Minneapolis Planning Department

SUBJ: Proposed Development 2004-2018 Lyndale Avenue South at Franklin

In its current form, this development is enormously out of scale for the neighborhood. I visit that block frequently to shop at the Wedge Co-op. When I was there today, I looked around at the buildings on the surrounding blocks. The proposed building would dwarf all around it for blocks.

Too high. At over 75 feet high, it's the tallest building along Lyndale south of I-94. Even the new-ish "European" apartment building on the east side of the next block (mostly vacant, I might add) is five stories, not seven. The proposed building would cast the neighboring buildings on Aldrich Avenue in shadow all morning. Residents on Aldrich would look out on a huge blind wall.

Too big. Its massive half-block footprint is more suitable to a warehouse district project or suburban development such as Excelsior and Grand. A new building this immense would destroy the distinctive character of the neighborhood--a huge, nondescript suburban building hulking over the 1-3 story older commercial buildings, apartment buildings, and houses around it. A building this size and design does not belong in the Wedge.

Increased traffic congestion. Traffic snarls are already a serious problem at this intersection. During rush hour and often on weekends Lyndale Avenue is backed up for a block or more in both directions. What will happen when vehicles try to get in or out of this building's parking area with the streets already packed with gridlocked traffic? In addition, the adjacent streets will have increased traffic and more cars parked there from the people using the development. The City needs to address the issues of traffic congestion and increased off-site parking before it encourages developments such as this in areas not equipped to handle them. Despite assurances that the new buildings on the Wedge Greenway have adequate parking to serve the new residents, those of us who live on the blocks just north of the Greenway know that there is quite a bit of spillover parking from these buildings, especially on weekends.

Increased noise level. The addition of a rooftop plaza and garden is an invitation for people to hang out up there in warm weather. Remember the war that recently raged between local residents and the rooftop bar owners in Uptown? The sound of people laughing and talking far above the street carries for blocks. Add alcohol and you have even more noise.

Deterioration of property values in nearby residences. People do not want to live behind a gargantuan building. Homeowners will have a harder time selling, and their homes will lose value if this building goes up. Landlords, too, will have a harder time renting and keeping rents at their current level. The City should be encouraging, not oppressing, the current property owners who have made considerable investments in their homes and rentals.

Before the City gives this proposed project serious consideration, it needs to be significantly scaled back by the developer.

Trilby Busch
2648 Emerson Ave. S.

LINDA HUHIN

2553 DUPONT AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55405
(612) 374-1435

February 10, 2014

Ms. Lisa Bender
Council Member, Ward 10
Minneapolis City Council
City Hall
350 South 5th Street
Minneapolis, MN 55415

Dear Ms. Bender,

I join many neighbors and friends in the Wedge neighborhood in asking you to reconsider your support for the proposed, but **unsustainable development by Mr. Don Gerberding** for the northwest corner of Lyndale Avenue South and Franklin Avenue. For 37 years I have owned and lived in the duplex at the above address, a fixer-upper when I purchased it in 1977. I have improved and maintained this property, which I bought as a young single woman, and which is the only property I have ever owned.

Our neighborhood is already heavily populated, 7,000 as I understand. Attracting more people here to live in a high density development might be good them and for the developer who I understand does not live in the Wedge, but it has no pluses for homeowners/tenants/taxpayers already established. We know the Wedge is somewhat transitional and that most residents are single renters. We homeowners would **like to see more families and/or long-term home ownership here, in order to build on our base of caring owners with vested interests in the quality of the existing housing stock, the historical nature of our architecture, and other amenities of our beautiful neighborhood.** This will only happen if we encourage it, but not by erecting slick, quickie, high density structures, that attract congestion into the neighborhood and degrade the quality of life for those who have already invested in nearby homes.

In consideration of homeowners in the coop on Aldrich behind the proposed five-story garage, I feel somewhat outraged at the prospect of our **city council cavalierly voting to block out their sunshine with the height of this structure,** which will also hold a rooftop public park overlooking their properties and **violating their privacy.**

I look forward to the traffic study, but **common sense tells us this development will only add more traffic to an important and already busy corridor, heavily used by people like me from the nearby area but also commuters from four directions of our city.**

Please think carefully before signing on to this unsustainable and unwise project. I'm not against development per se, but a building of fewer stories with businesses that serve the neighborhood and would not attract large numbers of people from outside would be more supportive of our neighborhood. Thank you for your consideration.

Linda Huhin

Anderson, Mei-Ling C.

From: Smoley, John
Sent: Thursday, January 30, 2014 2:16 PM
To: Deanna.Hagg@capella.edu
Cc: Anderson, Mei-Ling C.
Subject: FW: Franklin Lyndale development
Attachments: Cpla_new home_10.9.13.jpg; Cpla_view from South Aldrich Apts 1.jpg; Cpla_view from South Aldrich Apts 1.2.jpg

Deanna,

It was nice to meet you on Tuesday evening. Thanks for your thoughts on our conservation district ordinance and this project. I'm forwarding your letter to my colleague, Ms. Mei-Ling Anderson, the planner assigned to this project.

John

John Smoley, Ph.D.
City of Minneapolis
Community Planning and Economic Development
250 South 4th Street, Room 300
Minneapolis, MN 55415
612-673-2830
John.Smoley@minneapolismn.gov

From: Hagg, Deanna [mailto:Deanna.Hagg@capella.edu]
Sent: Wednesday, January 29, 2014 4:54 PM
To: Bender, Lisa; Smoley, John; don@masterpropertiesmn.com
Cc: paulruckerart@gmail.com; vegetariandragon@yahoo.com; pcjryan@gmail.com; helen.romanishan@yahoo.com; wedgecoordinator@gmail.com; sararomanishan@yahoo.com; arthur lyons; cynthia durushia; maipenrai67@msn.com; b'jones; 'Beth Kendrick'; mmbarthelémy@gmail.com
Subject: Franklin Lyndale development

Greetings Ms. Bender, Mr. Smoley, and Mr. Geberding,

My name is Deanna, and I currently reside at the South Aldrich Apartments on Aldrich and Franklin. (2009 Aldrich Avenue South, #11) I've recently been made aware of the proposed new development on the corner of Franklin and Lyndale where the Steeple People and Theater Garage are located. As you may realize, this is directly in my back yard. Not figuratively, but literally.

I moved to Minneapolis 6 years ago, have lived in this neighborhood for the last 5 years, and just bought my first home at the end of October, 2013. Please see the attached photo of me the day the SOLD sign went up. I was so excited! As a single gal, finding an affordable property to purchase on my own in my neighborhood was a dream come true!

I found out last week that my current view from my living room (see attached photos) that overlooks the Theater Garage may be changing to either a brick wall or a parking garage. I was so upset, I couldn't sleep for

three nights in a row. I had so many concerns running through my head – I can't look into a brick wall! Will I continue to have sunlight? Will I be able to sell my place for what I paid for it? What if I can't handle it? Will I have to file bankruptcy? What about the noise pollution? And the car exhaust? Will I be able to have my windows open?

I literally went into a panic. Here I am, a first time home-owner, and my home is already being destroyed just months after I bought it. But, I am not only concerned about myself, my view, my lack of sunlight, my resale value, my health, my safety, but I also worry about our neighborhood as a whole and the other members of my co-op. Many of us there would never be able to afford buying a home in Uptown on our own and we have so much love for our homes and community.

A few of my other concerns include:

- Traffic congestion. They already have a traffic cop at the Wedge. How much more can we handle?
- In addition to traffic congestion comes pollution - air pollution, noise pollution, etc.
- Increased violence. That area already has issues with gang violence. Higher density would possibly add to the crime rate, increasing the number of muggings and/or shootings in the area.
- Weight of the new structure. It is my understanding that this area is swamp land. Would it be able to hold the weight of the structure without causing damage to neighboring buildings? South Aldrich Apartments already has structural issues that we are in the process of fixing this year, but if the new construction will cause more damage, we don't want to waste the money and/or who would pay for the increased damages?
- Loss of unique and historical structures. I enjoy my end of Uptown because of its originality. If we add new cookie-cutter developments, we'll just be like the other end of Uptown.
- Will these retail spaces fill? Will the apartments rent? There is a newer building on Lyndale and 24th where the two stories of retail space have been sitting empty for at least the last 4 years when it was originally constructed. I have never seen anything in it.

And to reiterate my initial concerns:

- My view of Minneapolis will become a brick wall, or parking garage.
- Air pollution in my house. (Car exhaust, etc.)
- Noise pollution in my home – not only from the increased car traffic, but also foot traffic from the restaurant/bar/retail shops and apartments.
- Will I have any sunshine again? Or will my place be constantly in a cast shadow?
- My resale value wouldn't even come close to what I paid for my place. I don't think that anyone would ever want to buy a home that looks into a brick wall or a parking garage.
- Affordability of purchasing a new home/condo in that area for a single person who makes an average salary.
- Safety of our neighborhood. You increase density, add another restaurant or bar, which elevates the number of intoxicated people, and people in general, and we then turn into the other end of Uptown down by Lake Street. No thank you!

I would like to respectfully ask that you, and others who are supporting this development, take my valid concerns into consideration.

In addition, I would like to request that a traffic study be conducted, as well as one that would verify that the weight of a 5 or 6 story building would be able to be properly supported on the land and without causing harm

to nearby dwellings. Also, are there studies being conducted regarding increased density in respect to increased crime rates, and environmental resource over-use?

I am aware that a request has already been made to conduct a shadow study, so thank you.

I understand the desire and need for progress, but can we not compromise and build a new structure that is the same height and weight as the current building? Can we take into consideration the concerns of the neighborhood?

Thank you for your time, and I look forward to hearing back from you!

Best,
Deanna

Deanna E. Hagg, M.A. | Academic Associate | **SoSBS Counseling & Psychology** | LSO - Faculty & School Support | 612.977.5730 | **Capella University** | www.capella.edu | Please do not print this e-mail unless necessary

Our greatest glory is not in never falling, but in rising every time we fall. ~ Confucius

August 11, 2014

To: Planning Commission, City of Minneapolis
CM Lisa Bender, Chair
Mei-Ling C. Anderson, City Planner

Re: TGMA Proposal for Franklin-Lyndale

Request

- 1) Do not approve parking garage proposal, due to setback and impacts on neighbors of an unenclosed structure.
- 2) Appraise the setback/height variance request according to our suggested standard (in relation to the property rights of directly impacted neighbors).
- 3) If proposal is approved or approved with conditions, place strict requirements (including threat of financial penalties and permit revocations) to prevent bait and switch changes in accordance with the deep distrust that has developed with Master Properties.

Summary

We live at 2105 Aldrich Ave. S., within the range for which notice of this hearing was required; the proposed garage will end opposite our neighbor's backyard, and will be visible from our own. Thank you for encouraging testimony by those who have the potential to be adversely impacted.

The Commission should remain cognizant that this Development asks you to make extraordinary accommodations to allow the project to proceed. *Both* substantial additions to height *and* reduction of setback are requested. (Often the request for one is offset by *greater* accommodation for the other.) Difficult soil conditions at the location negate the possibility of underground parking or the use of basement space, leading the developer to request an above ground parking structure that (at the minimal setback) towers over neighboring backyards. The soil conditions, in combination with the structure proposed, require deep footings, for which neighbors uphill on the west have every right to be concerned about potential construction vibration and damage to their homes.

It is true that density at good transit spots is a worthy city goal – one that we support, albeit with a preference for requiring mixed-income access – and that the intersection would benefit by redevelopment. But such benefits must also consider impact on neighbors. A City Council Member expressed this well in regard to your recent advisory changes concerning teardowns: *Palmisano said the changes are designed to protect the rights of neighbors as much as the new homeowner who wants to build a large house next door. "It's saying they have property rights too, and it gives them a little more comfort in where they live."* (*Star Tribune, 7/29/14*)

This Development is like a McMansion in how it serves the needs of one owner by deeply encroaching on a neighbor – in this case several neighbors. In a city and neighborhood that is adding rental housing units by the hundreds, rejection or substantial revisioning of this project need only be understood as protecting property rights in Lowry Hill East to the same degree they may be protected in Linden Hills or anywhere else in the city. Such a vote on your part should not be seen as:

- a referendum on density

- bowing to pressure from Neighborhood Boards (if applicable),
- discouraging new rental housing construction, nor
- a signal that the city is disinterested in redevelopment of this specific parcel.

Specifics

Setback/Height: We are concerned about the request for reduced setback. A standard we would suggest, if not already in use, is that the city should not allow new structures that, if in existence prior to neighboring structures, would have realistically prevented the neighboring structures from having been built. If the TGMA proposal was built while Aldrich was all empty lots, would an apartment building with rear-facing lots abutting a property with minimal setback have had salable (i.e. livable) units in the rear? In this regard, we are not taking a position about the results as much as urging the standard your deliberations should be based upon. Such a standard gives practical interpretation for how to administer review per CM Palmisanos's statement.

Parking Garage: Using the same standard for the houses further south, it should be obvious that the rear and side walls of the parking ramp should be enclosed. Houses would not be built on Aldrich that have yards a few feet away from an open parking structure. We're not aware of any other city parking ramp that is so close to a residential property line, and it is unfathomable that one would be allowed that also has its ramps exposed, allowing noise, fumes and indirect headlights into our yards, decks, and rear windows. It could be argued that one benefit to neighbors of having a parking structure is the elimination of the current surface lot, and its late night noise. The open design worsens a situation that it could otherwise improve.

Construction: That TGMA proposes such a parking structure after previously indicating it would abide by neighbor preferences regarding enclosure has created substantial trust problems that create doubt about other promises, such as the particulars about measurements, building materials and, most importantly, construction impacts (given the poor soil and the construction on a hillside). If the project is approved, the city must protect other properties by: requiring significant bonding and inspection measures, disallowing change orders that stray from what has been promised, and establishing a financial penalty for TGMA (including potential for permit revocation) if the city needs to enforce adherence to the project's conditions.

We have a long chain of emails with the lead for Master Properties which demonstrate both that they have reversed their interest in building the parking garage in a manner that minimizes effects on neighbors and that they in fact do not think *it matters* if they respond to neighbor concerns. (We are happy to share these emails upon request.) As property owners in Minneapolis, we depend upon city officials to fairly protect our interests even while pursuing worthy goals. It appears such protection now exists in Linden Hills; thank you for similar consideration.



Michael Friedman & Susan Hasti
2105 Aldrich Ave. S.

Anderson, Mei-Ling C.

From: Bender, Lisa
Sent: Friday, July 25, 2014 8:23 AM
To: Rachael Bergan
Cc: Anderson, Mei-Ling C.
Subject: Re: The Marquee Apartments OPEN PARKING GARAGE

Rachel,

Thank you for your message. Your comments will be entered into the public record. If you are able to attend there is a public hearing scheduled for August 11 at the planning commission which starts at 4:30pm. But if you can't make it your email will be given to the planning commission.

I think there was a misunderstanding about the back wall when some of the neighbors met with the developer but the planning commission can add conditions of approval to actions taken if they choose to address remaining concerns.

all my best,
Lisa

Sent from my iPhone

On Jul 24, 2014, at 10:43 PM, "Rachael Bergan" <juniorballoon@gmail.com> wrote:

Hello,

I am a property owner/ resident of 2009 Aldrich Ave S. I am horrified to note that the development as currently envisioned includes an OPEN PARKING GARAGE mere feet from my place of residence. Yes, we objected at outset to a several-story solid concrete wall pressing close to our faces, but this is NOT A SOLUTION. This is so much worse!

Auto noise and exhaust, headlights, slamming doors, loud conversation within feet of our building at all hours - - we beg you to go back to the drawing board.

My heart is sick. My building is taking a huge hit on this. At least half of us in the building live at or below the poverty line, and you are devaluing dramatically our only financial asset, our little piece of real estate bought cheaply in the 80's or 90's. And you are rendering our homes uninhabitable.

Please consider our plight as you proceed. Respectfully

Rachael Bergan
2009 Aldrich Ave S #5

Anderson, Mei-Ling C.

From: Heather Baker <pearl.anita.baker@gmail.com>
Sent: Friday, August 01, 2014 4:00 PM
To: Anderson, Mei-Ling C.
Subject: Proposed Development 2004-2018 Lyndale Ave S

Follow Up Flag: Follow up
Flag Status: Flagged

Ms. Mei-Ling Anderson
City Planner
Minneapolis, MN 55415

Dear Ms. Anderson:

This is to express my great concern and displeasure with the proposed development at Lyndale and Franklin South. I live at 2019 Aldrich, directly behind the building and parking ramp. If approved, it would negatively impact all of us in every way.

It would not only change the character of our quiet residential block, it would be extremely detrimental to our peace, health and well-being. I am in my 80's and in poor health, and am certain that this project would very adversely affect my condition.

We would lose our sun and light, our views and our peace and quiet in exchange for air and noise pollution and blocked views with this pedestrian building imposing itself on us up to our doorsteps. We would no longer be able to enjoy our decks and backyards in privacy and peace, something we all deserve, and that many of us have worked all our lives achieve.

If the Commission does approve this disasturous plan, the very least they could do is require that a closed parking ramp be built with a mechanical ventilation system, as well as a way to block headlights from cars shining in our windows.

Best of all, it would be preferable if the city would keep any density plans to the downtown area where there are still empty parking lots and leave our area green and residential. No one now or in the future will want a city that looks like East Berlin during the Cold War or the frightening dystyopia depicted in the 1920's sci-fi movie "Metropolis."

I visited East Berlin in the 1960's and remember well the drab and dreary cement blocks of apartment houses where they warehoused as many poor souls as possible. Is Lyndale on its way to this dreary prospects?

We ask you to consider how greatly our of life would be impacted, and suggest that the developer find a more suitable spot. Let's strive to make Minneapolis a more beautiful city and not destroy the best things about it, all in the name of density. Why not aim for something more like the City Fathers planned in 1914?

Sincerely,
Pearl Anita Baker
612-872-6173

Anderson, Mei-Ling C.

From: kristi nerem <maipenrai67@msn.com>
Sent: Wednesday, July 30, 2014 4:42 PM
To: Bender, Lisa; Anderson, Mei-Ling C.
Cc: kristi nerem
Subject: Just some thoughts about life and the Lyndale / Franklin construction

I have lived at South Aldrich Avenue Co-operative at 2007 / 2009 Aldrich Ave. S. for 32 years. I love this building and this neighborhood. I wish to express several concerns about the proposed construction at Franklin and Lyndale. I present them in order of the severity (from minor to major) of the issues I have.

1. Additional retail space when numerous empty store fronts line Lyndale. The mixed use condominium / retail space on 24th and Lyndale has existed for years and has never filled its retail space, probably because of the cost of rental.
2. Affordable housing. Condominiums and apartment buildings are being built all around the Uptown / Greenway / Wedge area, but all seem to be high end and many are not filled. I would be happier if the project included affordable housing.
3. Electrical grid - can the existing infrastructure handle more businesses and residences. We experience flickers, brown-outs and brief outages now independent of storms.
4. Shadow effect. I mourn the fact that I will never see the sun or moon rise again from my home. What effect will the height of the building to the east of numerous homes have on heating and vegetation?
5. Storm drains and sewer. All the surrounding land slopes down toward Franklin and Lyndale. I have seen two occasions where people have been canoeing in over 6 feet of water at the intersection. Can the current infrastructure handle the increased use?
6. Parking. Finding a place to park in the neighborhood is a nightmare as things are. What plans are in place for Rudolph's parking during the construction? Where will residents park if users of the new building, i.e. The Theatre Garage, choose not to pay for parking.
7. Air quality. Since the parking garage will be feet from our windows, will we ever be able to open our homes again without fear of exhaust / carbon dioxide poisoning. Is a garage exhaust / ventilation system part of the plan? If so where will the exhaust be discharged?
8. The ground under the Franklin / Lyndale intersection is "swampy" and transmits movement. Our building shakes if a bus drives up Franklin. Our building is over 100 years old, and is experiencing subsidence toward the east due to the instability of the ground. Assuming that pylons will have to be sunk to support the structure, does the project have insurance to cover damage to adjacent housing due to pile driving or other construction issues that may cause vibrations?
9. Crime. Rudolph's has a long history of not being a good neighbor concerning crime. My windows overlook Rudolph's current parking lot. I have watched drug deals behind the catering truck, called 911 after shootings, and heard screams, arguments, and disruptions that are too numerous to count. The vast majority of the crimes and disturbances have taken place in full view of Lyndale Avenue. Years ago, I would call Rudolph's at 1 am (now the disturbances occur at 2 or 3 because of later closing hours) and report a disturbance. I thought I was helping their business by not simply calling the police to their property again and again. At first they promised to send someone to the parking lot. No one ever appeared. Then their response was simply: "Call 911". I spoke to the manager on duty several times. The response was always the same. I gave up calling them for assistance, but I hate to call 911 for a disturbance on Rudolph's parking property when I might be calling a squad away from a deadly event elsewhere. Now with the new construction drug dealing, disputes, disturbances and other crimes will be inside a structure and hidden from easy view. Will someone staff the parking garage at all times? A few nights ago I was reading at 2am when I heard a woman scream twice. I tried to listen to determine if the screams were in play or fear. I could not tell. It was quiet after that, so I did not call 911. It is not my job to police the parking property. I cannot call 911 with every disturbance. I hope the screams were from drunken play, but perhaps a woman was raped and could not scream more than twice. It is not my job to protect people on parking property when

the owner has abdicated concern. I don't want to decide night after night whether or not someone is in danger and if I do nothing, fear I have made the wrong decision. Will the owner of the new project choose to be a responsible citizen concerning crime on his property, or continue the policy of a blind eye? Thank you for your consideration. Kristi Nerem

Anderson, Mei-Ling C.

From: Bender, Lisa
Sent: Wednesday, July 30, 2014 1:21 PM
To: Anderson, Mei-Ling C.
Cc: Somogyi, Ben
Subject: FW: Ward 10 Contact Form

For record

Lisa Bender
10th Ward City Council Member

From: no-reply@minneapolismn.gov [<mailto:no-reply@minneapolismn.gov>]
Sent: Wednesday, July 30, 2014 12:33 PM
To: Crockett, Matthew; Somogyi, Ben; Bender, Lisa
Subject: Ward 10 Contact Form

City of Minneapolis

Name * Gary Dombouy
Email * gdombouy@aol.com
Phone (612) 871-0344
Phone Type Home
Address 110 East 25th Street
City Minneapolis
State MN
Zip 55404

Question/Comment * Master's Theater Garage and Marquee Apartments is a great proposal for the Franklin-Lyndale intersection. Master under Don Gerberding has contributed some very attractive multi-unit developments on Nicollet Avenue (at Franklin and at 26th). However I am concerned about Master's history of construction quality: aesthetically admirable while structurally unsettling. Owners at Artist Quarter Lofts experienced roof-leaking problems only a year after completion! Common with new construction but. . . The later built Master Greenleaf Lofts foreclosed and now is under complete reconstruction (including brick facing) because of bad materials. While Don Gerberding has admirably modified the Theater Garage and Marquee Apartments by decreasing the height through reducing the number of and increasing the price of units, how will he address this reputation of construction practices?

This is an email generated from the City of Minneapolis website. * Required fields are indicated with an asterisk.

From: Saralyn Romanishan <sararomanishan@yahoo.com>
Sent: Friday, August 01, 2014 6:21 PM
To: Bender, Lisa; Somogyi, Ben; Anderson, Mei-Ling C.
Subject: 2008-2018 Lyndale Theater Marquee Apartments

We would like to address the following concerns regarding the 2008-2018 Lyndale application:

Petition to rezone the properties located at 2009-2018 Lyndale Ave S from C1 Neighborhood Commercial District to C2 Neighborhood Corridor Commercial District.

1) We ask that you stay within the current zoning ordinances and do not upzone this property to C2. There is no reason why the developer cannot build two separate buildings, separating the garage from the residential or commercial. Therefore, there is no need for a Conditional use permit (CUP) to increase the maximum allowed height in the C2 Neighborhood Corridor Commercial District from 4 stories/56 feet to 73 feet-6 inches.

Variance to reduce the rear yard setback from 15 feet to 3 feet and Parking Structure.

1) Variances were originally meant for small items not to be an easy way for a developer to buck the system. Please keep this development within the current code.

2) This is unprecedented. There is NO ALLEY.

3) The application plans do not "promote development that is compatible with nearby properties, neighborhood character, natural features and plans adopted by the city council, to minimize pedestrian and vehicular conflict, to reinforce public spaces, to promote public safety, and to visually enhance development." (Municipal code: 530.10)

4) We do not feel that this is the appropriate location for a garage. But if there must be a garage, enclose and install a mechanical ventilation system.

5) We prefer more attention be paid to public transportation at this site (including more space for bus stops and shelters including a cutout for buses to pull in) instead of cars.

6) An open garage and the placement of this garage will create a public nuisance to the neighbors adjacent to the property. "No use or structure shall be operated or occupied as to constitute a dangerous, injurious or noxious condition because of fire...noise, vibration, smoke, dust, fumes, odor or other air pollution, light, glare...or other substance or condition, or as to unreasonably interfere with the use and enjoyment of property by any person of normal sensitivities, or otherwise as to create a public nuisance. In addition, no use or structure shall be operated or occupied in a manner not in compliance with any performance standard contained in this article or with any other applicable regulation." (Municipal code 535.570)

Variance to reduce the south interior side yard setback from 15 feet to 5 feet.

1) Stay within the current code as that is why it exists.

2) This is detrimental to the businesses on the south side of the new construction, essentially relegating their customer entrance to an unsafe alley.

Six story, mixed-use building with 82 dwelling units.

1) We do not need more empty storefronts. They are everywhere in Uptown.

2) This building design is not a "Gateway" to the area. The vision for Linden Hills Corner makes much more sense than the vision presented by this plan and could easily be adjusted to fit within the current zoning of this block.

3) There is no reason to make the building (and the garage) so large if there is less retail.

4) This building is designed to reflect the old standard of cars for transportation (with a little bike parking thrown in to try to minimize the reality of the garage).

5) This is a stick structure built for the short-term. It is not built to last. This development plan, in its current form, is a short-term moneymaker for the developer with no clear eye to the real future needs of this city.

Green space

1) The developer has opted to pay a fee instead of including green space in the building plan. This does not fall within the city plan or the city goals even though there is an ordinance allowing a pass. We are preparing our city for a future of density and climate change. We are a city proud of our green spaces. Allowances such as this strongly affect our future as a livable city. It is the opposite to current urban design trends.

2) We request that the boulevard along Franklin and Lyndale be treated as real green space (not just a few small trees in a sidewalk) expanding it "...to enhance and improve aesthetic appearance of city streets... as an aid to maintaining the quality and livability of the residential neighborhoods of the city." (Municipal code 427.10 Boulevards) This public space should not be used simply for outdoor restaurant seating.

West elevation

Remove the terrace facing the apartments and enclose the west wall and add arbor vitae not a green screen that is alive for only three months out of the year.

Traffic study

More thorough discussion with Hennepin County is needed regarding the intersection of Franklin and Lyndale. The reconstruction north of Franklin will do little to effect the bottleneck in this area. In the city, the demolition and reconstruction of a building is the only opportunity to enlarge a street, add barriers, bike, car, mass transit lanes, bus shelters, green space, and to apply a new urban design strategy. This is a chance to really study this location and redesign it.

We would like to include some notes from the recently adopted City Vision, Values, and Goals.

1) "Growth...While preserving a city's character..."

2) "Living well: Minneapolis is safe and livable..."

3) "Great Places: Natural and built spaces work together and our environment is protected..."

4) "One Minneapolis: Disparities are eliminated so all Minneapolis residents can participate and prosper..."

The treatment of this neighborhood compared to other neighborhoods where the city is making it a priority "...to protect the rights of neighbors...they have property rights too..." is appalling. (see recent article <http://m.startribune.com/local/?id=269150251&c=y>)

We have one final request. "The city planning commission or zoning administrator may require a performance bond or letter of credit to be supplied by the applicant guaranteeing completion of required site improvements as a condition of site plan approval. The amount of such bond shall be set at one hundred twenty-five (125) percent of the estimated cost of the improvements." If this development is to go through, we request this bond or letter of credit be required. (Municipal Code 530.90)

Thank you,

Helen Romanishan & Saralyn Romanishan
2111 Aldrich Ave S

Minneapolis, MN 55405

Cell 612-310-8022 / sararomanishan@yahoo.com