

**HERITAGE PRESERVATION APPLICATION SUMMARY**

*Property Location:* 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North

*Project Name:* 6<sup>th</sup> Avenue North Reconstruction Project

*Prepared By:* Hilary Dvorak, Principal Planner, (612) 673-2639

*Applicant:* City of Minneapolis Public Works

*Project Contact:* Jeff Handeland, P.E.

*Ward:* 3 and 5

*Neighborhood:* North Loop

*Request:* To reconstruct 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North.

*Required Applications:*

<b>Certificate of Appropriateness</b>	For the reconstruction of 6 <sup>th</sup> Avenue North, between 5 <sup>th</sup> Street North and just north of Washington Avenue North, located in the Minneapolis Warehouse Historic District.
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**HISTORIC PROPERTY INFORMATION**

<b>Current Name</b>	6 <sup>th</sup> Avenue North
<b>Historic Name</b>	6 <sup>th</sup> Avenue North
<b>Historic Address</b>	Not applicable
<b>Original Construction Date</b>	1926 (brick installed)
<b>Original Architect</b>	Not applicable
<b>Original Builder</b>	City of Minneapolis
<b>Original Engineer</b>	City of Minneapolis
<b>Historic Use</b>	Street
<b>Current Use</b>	Street
<b>Proposed Use</b>	Street

<b>Date Application Deemed Complete</b>	November 4, 2015	<b>Date Extension Letter Sent</b>	Not applicable
<b>End of 60-Day Decision Period</b>	January 3, 2016	<b>End of 120-Day Decision Period</b>	Not applicable

**CLASSIFICATION**

<b>Local Historic District</b>	Minneapolis Warehouse Historic District
<b>Period of Significance</b>	1865 - 1930
<b>Criteria of Significance</b>	<p><i>Criteria 1:</i> The property is associated with significant events or with periods that exemplify broad patterns of cultural, political, economic or social history.</p> <p><i>Criteria 4:</i> The property embodies the distinctive characteristics of an architectural or engineering type or style, or method of construction.</p> <p><i>Criteria 6:</i> The property exemplifies works of master builders, engineers, designers, artists, craftsmen or architects.</p>
<b>Date of Local Designation</b>	1978
<b>Date of National Register Listing</b>	1989
<b>Applicable Design Guidelines</b>	<p><u>Minneapolis Warehouse Historic District Design Guidelines (2010)</u></p> <p><u>Warehouse District Heritage Street Plan (2011)</u></p>

**SUMMARY**

**BACKGROUND.** The primary form and general character of the Minneapolis Warehouse Historic District is set by the pattern of rail infrastructure such as rail corridors and rail spurs, interlaced with the public realm network of streets, loading docks, public sidewalks and alleys. While the buildings are often considered the character defining features of an area, their location, configuration and design are defined by the system of infrastructure they served and were served by. The system of infrastructure connects buildings and sites to one another within the district creating the overall spatial relationships that defines the character of the Minneapolis Warehouse Historic District as a whole. Each component of infrastructure, its location, materials, dimensions, and function plays a role in establishing this collective character of the district.

**APPLICANT’S PROPOSAL.** That portion of 6<sup>th</sup> Avenue north, from a point midblock between 3<sup>rd</sup> and 4<sup>th</sup> Streets through to the dead end north of Washington Avenue North, is located within the Minneapolis Warehouse Historic District and in the National Register district. The City of Minneapolis is proposing to reconstruct approximately 0.27 miles of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North. Currently, this segment of 6<sup>th</sup> Avenue North has several areas of broken or non-existent curb, and the driving surface provides a mixture of street pavers and asphalt patches. The current condition of the roadway requires frequent maintenance. Although sidewalk currently exists along the majority of the corridor, it varies in width and slope, and does not consistently meet ADA standards.

This project proposes to fully reconstruct the roadway and sidewalk through the corridor, while keeping the historic loading docks intact. The corridor will remain a two lane roadway with parking on both sides.

The project will involve the complete removal of the roadway section including historic pavers, the replacement of the existing storm sewer, the reconstruction of the roadway, the restoration and reinstallation of the historic pavers and the reconstruction of the sidewalks as ADA-compliant pedestrian walkways. As part of this project, the existing pavers will be removed, sorted and cleaned so that they can be used to re-pave the street as it is reconstructed. A percentage of the salvaged pavers

will be unusable and it is likely that pavers of the same brand and vintage may need to be purchased from outside the project to supplement the usable salvaged pavers.

**RELATED APPROVALS.** In August of 2011, the Heritage Preservation Commission approved the *Warehouse District Heritage Street Plan*. The purpose of the plan is to provide clear direction on the issue of how to protect the historic infrastructure of the District while promoting an accessible and pedestrian friendly environment.

**PUBLIC COMMENTS.** No comment letters have been received in regards to this application. Any correspondence received prior to the public meeting will be forwarded on to the Heritage Preservation Commission for consideration.

## ANALYSIS

### CERTIFICATE OF APPROPRIATENESS

The Department of Community Planning and Economic Development has analyzed the application to allow the reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North based on the following findings:

1. *The alteration is compatible with and continues to support the criteria of significance and period of significance for which the landmark or historic district was designated.*

The proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, is compatible with and supports the criteria of significance and period of significance for the Minneapolis Warehouse Historic District. The historic brick pavers were installed during the identified period of significance for the district. The proposed design will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines* and the *Warehouse District Heritage Street Plan*.

2. *The alteration is compatible with and supports the interior and/or exterior designation in which the property was designated.*

The proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, is compatible with and supports the designation of the Minneapolis Warehouse Historic District. The proposed reconstruction of the roadway, restoration and reinstallation of the historic pavers and reconstruction of the sidewalks as ADA-compliant pedestrian walkways will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines* and the *Warehouse District Heritage Street Plan*.

3. *The alteration is compatible with and will ensure continued integrity of the landmark or historic district for which the district was designated.*

Both the City of Minneapolis' Heritage Preservation Regulations and the National Register of Historic Places identify integrity as the authenticity of historic properties and recognize seven aspects that define a property's integrity: location, design, setting, materials, workmanship, feeling and association. The existing brick pavers will be removed, sorted and cleaned so that they can be used to re-pave the street as it is reconstructed. A percentage of the salvaged bricks may be unusable; in that case, pavers of the same brand and vintage may need to be purchased from outside the project to supplement the usable salvaged pavers. Reconstructing the street with primarily salvaged brick materials supplemented by additional matching brick pavers will allow for the feeling of the historic brick street to be retained. The proposed reconstruction of 6<sup>th</sup> Avenue North will

not negatively impact the district's location, design, setting, materials, workmanship, feeling, or association and will therefore ensure continued integrity.

4. *The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the applicable design guidelines adopted by the commission.*

Throughout the period of significance (1865-1930), the system of streets, blocks, and alleys in the area of the city that would become the Minneapolis Warehouse Historic District was modified by the introduction of various forms of infrastructure that were integrated into the neighborhood in order to accommodate the movement and distribution of raw materials and finished goods. A pattern of rail lines, rail spurs, public streets, alleys, loading docks and sidewalks evolved in such a way as to suit the specific location requirements of each business, while also joining properties together into a network of inter-related commerce. Three main patterns evolved in the ways in which buildings were constructed on city blocks to facilitate regular access to transportation networks, both the rail infrastructure and the street system. Taken together, this delineation of streets encompassed a unique character based on the interplay of industrial and commercial activity as embodied in the existing fabric of building orientation, location of loading docks, canopies, and building entrances.

In the 1880s, the system of streets, blocks, and alleys in the area was modified by the introduction of rail infrastructure. Construction of rail corridors, rail yards and rail spurs interrupted the otherwise deliberate pattern of blocks, streets and alleys as they were originally platted prior to the commercial development in the area. Many streets were closed to accommodate the new rail yards and rail corridors. The closure of the streets resulted in the formation of uninterrupted blocks, commonly referred to as superblocks. This interruption of the street grid is most evident in the area west of the Fourth Avenue North Rail Corridor where the later period of growth in the district occurred. The land adjacent to the rail yards and rail spur lines became prime real estate for the warehousing and manufacturing industries of the district. The building sites that adjoined rail yards and rail spur lines developed differently than sites that were not adjacent to any rail infrastructure.

Building sites along rail yards were built with direct rail access on the rear of the buildings to facilitate the transfer of materials from the rail cars into warehouse buildings. While adjacency to the rail yards allowed easy transfer of goods from rail cars; the businesses also required a location to transfer materials that were distributed by vehicles other than rail cars. On more traditional blocks, loading areas in the rear of buildings along alleys serve this purpose. However, due to the superblocks created by the rail yards only one facade of the buildings was accessible, the front facade. As a result the front of buildings served as the primary pedestrian entrance as well as the non-rail freight distribution access.

Building sites along rail spur lines presented similar challenges to sites along rail yards. However, the blocks retained their original pattern of being interrupted by intersecting streets. The intersecting streets allowed buildings to have frontage along two streets. Buildings oriented their primary pedestrian entrance along one street facade and their non-rail freight distribution access along the other street facade.

On more traditional blocks in the district (those that had not been interrupted by rail infrastructure), alleys continued to serve as the main access point for the transfer of goods between buildings and vehicles. In the areas of the district where rail lines had not interrupted the traditional form of city blocks, buildings were oriented so that their primary pedestrian entrance was located on the front facade of a major public street while freight and loading areas were located on the back of the building in the alley.

6<sup>th</sup> Avenue North is a freight street. Freight streets are indicative of the change in building orientation due to the block's adjacency to rail spur lines. These streets possess the non-rail freight distribution infrastructure of loading docks and canopies. Building access along these streets is traditionally less public and more commercial or industrial in nature.

The proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, is compatible with the following design guidelines for infrastructure and public realm:

*Requirement:*

- I.14. On freight streets, Street Design: The main aspects to be considered when improving freight streets shall include the preservation of historic loading docks and canopies to reflect their service function and proper management of vehicular and service access to the property.

*Advisory:*

- I.18. On all streets, the narrowing of vehicular right-of-way to accommodate sidewalks around loading docks to create more pedestrian friendly activity is encouraged.

*Requirement:*

- I.22. Original historic street paving materials shall be maintained and preserved.
- I.23. Existing railroad tracks located in streets, alleys, or corridors shall be maintained or reinstalled when improvements are made.
- I.24. Loading docks shall be preserved. Their heights or widths shall not be altered.
- I.25. The visual corridors created by the public and private roadways, bridges, alleys, and former rail corridors or other infrastructure are significant and shall be preserved.
- I.26. New or replacement street furnishings such as street lights and street furniture shall be compatible with the character of the historic district in terms of location, design, materials, color, and scale.

*Requirement:*

- I.35. Street trees shall not be located directly in front of entrances of historic buildings.
- I.36. The location of street trees shall be centered within or between bays of buildings.

*Other Considerations:*

- I.39. Landscape grass strips, planting beds, and grass boulevards are not recommended in most locations within the district. These features will be considered on a case by case basis.

- 5. *The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the recommendations contained in The Secretary of the Interior's Standards for the Treatment of Historic Properties.*

The proposed development will be consistent with the following *Secretary of the Interior's Standards for Rehabilitation*:

- 1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
- 2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.

3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.
5. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
6. *The certificate of appropriateness conforms to all applicable regulations of this preservation ordinance and is consistent with the applicable policies of the comprehensive plan and applicable preservation policies in small area plans adopted by the city council.*

The proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, will conform to all applicable regulations of this preservation ordinance and would be consistent with the following policies of the comprehensive plan:

**Heritage Preservation Policy 8.1: Preserve, maintain, and designate districts, landmarks, and historic resources which serve as reminders of the city's architecture, history, and culture.**

- 8.1.1 Protect historic resources from modifications that are not sensitive to their historic significance.
- 8.1.2 Require new construction in historic districts to be compatible with the historic fabric.
- 8.1.3 Encourage new developments to retain historic resources, including landscapes, incorporating them into new development rather than removal.

**Heritage Preservation Policy 8.3: Explore and protect potential archeological resources**

**Heritage Preservation Policy 8.5: Recognize and preserve the important influence of landscape on the cultural identity of Minneapolis.**

- 8.5.1 Identify and protect important historic and cultural landscapes.
- 8.5.2 Encourage planting and maintenance of street trees and other natural elements in historic districts to promote livability.
- 8.5.3 Preserve historic materials typically found in public spaces, such as street materials like pavers, lighting and other resources.

7. *Destruction of any property. Before approving a certificate of appropriateness that involves the destruction, in whole or in part, of any landmark, property in an historic district or nominated property under interim protection, the commission shall make findings that the destruction is necessary to correct an unsafe or dangerous condition on the property, or that there are no reasonable alternatives to the destruction. In determining whether reasonable alternatives exist, the commission shall consider, but not be limited to, the significance of the property, the integrity of the property and the economic value or usefulness of the existing structure, including its current use, costs of renovation and feasible alternative uses. The commission may delay a final decision for a reasonable period of time to allow parties interested in preserving the property a reasonable opportunity to act to protect it.*

The scope of work does not involve the destruction of the property.

Before approving a Certificate of Appropriateness, and based upon the evidence presented in each application submitted, the Commission shall make findings that alterations are proposed in a manner that demonstrates that the Applicant has made adequate consideration of the following documents and regulations:

8. *The description and statement of significance in the original nomination upon which designation of the landmark or historic district was based.*

The applicant has demonstrated adequate consideration for the description and statement of significance in the original nomination upon which the Minneapolis Warehouse Historic District was based. The proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines* and the *Warehouse District Heritage Street Plan*.

9. *Where applicable, adequate consideration of Title 20 of the Minneapolis Code of Ordinances, Zoning Code, Chapter 530, Site Plan Review.*

The scope of work in this application does not require site plan review under Title 20 of the Minneapolis Code of Ordinances, Zoning Code, Chapter 530.

10. *The typology of treatments delineated in the Secretary of the Interior's Standards for the Treatment of Historic Properties and the associated guidelines for preserving, rehabilitating, reconstructing, and restoring historic buildings.*

The applicant has made adequate consideration for the treatments delineated in *The Secretary of the Interior's Standards for the Treatment of Historic Properties*. The proposal complies with the rehabilitation guidelines of *The Secretary of the Interior's Standards for the Treatment of Historic Properties* as discussed in finding #5 above

Before approving a Certificate of Appropriateness that involves alterations to a property within an historic district, the Commission shall make findings based upon, but not limited to, the following:

11. *The alteration is compatible with and will ensure continued significance and integrity of all contributing properties in the historic district based on the period of significance for which the district was designated.*

The proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, is compatible with and will ensure continued significance and integrity of all contributing properties in the historic district based on the period of significance for the Minneapolis Warehouse Historic District. The proposed reconstruction of the roadway, restoration and reinstallation of the historic pavers and reconstruction of the sidewalks as ADA-compliant pedestrian walkways will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines* and the *Warehouse District Heritage Street Plan*. In addition, the historic loading docks along the street will not be modified as part of this project.

12. *Granting the certificate of appropriateness will be in keeping with the spirit and intent of the ordinance and will not negatively alter the essential character of the historic district.*

The spirit and intent of the City of Minneapolis' Heritage Preservation Regulations is to preserve historically significant buildings, structures, sites, objects, districts, and cultural landscapes of the community while permitting appropriate changes to be made to these properties. Granting the certificate of appropriateness will be in keeping with the spirit and intent of the ordinance and will not negatively alter the essential character of the historic district.

13. *The certificate of appropriateness will not be injurious to the significance and integrity of other resources in the historic district and will not impede the normal and orderly preservation of surrounding resources as allowed by regulations in the preservation ordinance.*

Granting the certificate of appropriateness will not be injurious to the significance and integrity of other resources in the historic district. The proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines* and the *Warehouse District Heritage Street Plan*.

## RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the Heritage Preservation Commission adopt staff findings for the application by the City of Minneapolis Public Works for the reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, located in the Minneapolis Warehouse Historic District:

### A. Certificate of Appropriateness.

Recommended motion: **Approve** the certificate of appropriateness to allow the reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North, subject to the following conditions:

1. By ordinance, approvals are valid for a period of two years from the date of the decision unless required permits are obtained and the action approved is substantially begun and proceeds in a continuous basis toward completion. Upon written request and for good cause, the planning director may grant up to a one year extension if the request is made in writing no later than December 1, 2017.
2. By ordinance, all approvals granted in this certificate of appropriateness shall remain in effect as long as all of the conditions and guarantees of such approvals are observed. Failure to comply with such conditions and guarantees shall constitute a violation of this Certificate of Appropriateness and may result in termination of the approval.

## ATTACHMENTS

1. Written description and findings submitted by applicant
2. Plan set for the proposed reconstruction of 6<sup>th</sup> Avenue North, between 5<sup>th</sup> Street North and just north of Washington Avenue North
3. Agency coordination letters and environmental documentation
4. Photos
5. Correspondence

November 3, 2015

## HERITAGE PRESERVATION APPLICATION PACKET

### 6<sup>th</sup> Avenue Reconstruction Project

#### Pre-Application Meeting

A pre-application meeting was held between the City of Minneapolis Planning department, the City Public Works department and the City's consultant engineer for the project on July 28, 2015. The meeting discussed timelines for the public hearing and what is required for the HPC Certificate of Appropriateness application process.

#### Completed Application Worksheet

See Attachment A.

#### Electronic Application Submittal

An electronic submittal has been created following the HPC's instructions.

#### List of Property Owners

See Attachment B for a map showing the property owners within 350 feet of the subject site (6<sup>th</sup> Avenue within the Historic Warehouse District), mailing labels and a list of property owners.

#### Statement of Proposed Use and Description of the Project

The City of Minneapolis is proposing to reconstruct approximately 0.27 miles of 6th Avenue North from 5th St N to the dead end north of Washington Ave N. Currently, this segment of 6th Avenue has several areas of broken or non-existent curb, and the driving surface provides a mixture of street pavers and asphalt patches. The current condition of the roadway requires frequent maintenance. Although sidewalk currently exists along the majority of the corridor, it varies in width and slope, and does not consistently meet ADA standards.

This project proposes to fully reconstruct the roadway and sidewalk through the corridor, while keeping the historic loading docks intact. The corridor will remain a two lane roadway with parking on both sides. The attached typical sections (Figures 2-6) in Attachment C compare the existing corridor and the proposed corridor.

The project will involve complete removal of the roadway section including historic pavers, replacement of the existing storm sewer, reconstruction of the roadway, restoration and reinstallation of the historic pavers and reconstruction of the sidewalk as an ADA-compliant pedestrian walkway. As part of this project, the existing pavers will be removed, sorted and cleaned so that they can be used to re-pave the street as it is reconstructed. A percentage the salvaged pavers will be unusable and it is likely that

pavers of the same brand and vintage may need to be purchased from outside the project to supplement the usable salvaged pavers.

A portion of 6th Avenue, from a point midblock between 3rd Street and 4th Street through to the dead end north of Washington Ave N, is located within the NRHP-listed Minneapolis Warehouse Historic District and has local designation by the City of Minneapolis. Figure 1 in Attachment C illustrates the limits of the proposed project in relation to the Historic District. The City has been awarded federal funding for this reconstruction project.

Written Statement Addressing Applicable Findings –  
Certificate of Appropriateness for property within a historic district: Findings #1-13

If applying for a Certificate of Appropriateness, provide a written statement which addresses the following required findings 1-10 for a landmark and 1-13 for a property within a historic district:

*(1) The alteration is compatible with and continues to support the criteria of significance and period of significance for which the landmark or historic district was designated.*

This reconstruction was first proposed as part of the Warehouse District Heritage Street Plan, in which the Minneapolis Heritage Preservation Commission (HPC) and the State Historic Preservation Office (SHPO) participated and was approved by the HPC in 2011. The Warehouse District Heritage Street Plan provides an approach to preserve and rehabilitate the historic infrastructure, improve pedestrian accessibility and increase sustainable practices through better stormwater management. The historic district also has local designation by the City of Minneapolis, and has specific design guidelines that were followed in the design of this project.

*(2) The alteration is compatible with and supports the interior and/or exterior designation in which the property was designated.*

The project will preserve the historic vitrified clay paver brick surface and loading docks, which are important historic fabric to the district, and will only introduce modern safety features, such as ADA compliant sidewalks and crossing, which will not negatively impact the historic district's integrity of location, design, setting, materials, workmanship, feeling and setting.

*(3) The alteration is compatible with and will ensure continued integrity of the landmark or historic district for which the district was designated.*

The Warehouse District Historic Street Plan (WDHSP) guides this area to maintain the historic pavers for the street surface. Historic pavers located along 6th Avenue will be salvaged and used within the thru-lane/ parking portions street. Pavers will be removed from the historic district as well as within the block between 5th St. N and 4th St. N., outside the historic district boundary. If salvageable, the pavers will be used in the drive/park lanes between 4th St. N and Washington Avenue.

The current street pavers were installed in 1926. The pavers have good longevity and will last for the expected life of the street going forward. Fully reconstructing the road will provide a stable base for the reinstalled pavers. Improving the infrastructure will ensure continued integrity for the road surface rather than being left in a state of deterioration. In addition to securing the integrity of the pavers, no work will occur on the existing loading docks which are a character-defining feature.

*(4) The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the applicable design guidelines adopted by the commission.*

This reconstruction was first proposed as part of the Warehouse District Heritage Street Plan, which provides an approach to preserve and rehabilitate the historic infrastructure, improve pedestrian accessibility and increase sustainable practices through better stormwater management that is based in and consistent with the historic design guidelines. Both the HPC and SHPO participated in the development of the Plan and the HPC approved it in 2011. The design guidelines and conceptual layout in the Heritage Street Plan are being adhered to in this reconstruction project. Within the Heritage Street Plan, 6th Avenue is considered a 'Freight Street'. The Minneapolis Warehouse District design guidelines list the following requirements for 'Freight Streets', which are met by this project:

1.11. Loading docks and canopies dating from the period of significance shall be preserved and retained.

1.14. On freight streets, Street Design: The main aspects to be considered when improving freight streets shall include the preservation of historic loading docks and canopies to reflect their service function and proper management of vehicular and service access to the property.

1.22. Original historic street paving materials shall be maintained and preserved.

1.24. Loading docks shall be preserved. Their heights or widths shall not be altered.

1.25. The visual corridors created by the public and private roadways, bridges, alleys, and former rail corridors or other infrastructure are significant and shall be preserved.

1.26. New or replacement street furnishings such as street lights and street furniture shall be compatible with the character of the historic district in terms of location, design, materials, color, and scale.

The project was designed to meet the requirements in the design guidelines and subsequently, will result in minimal change to the corridor. No change to the loading docks will occur with this project.

*(5) The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the recommendations contained in The Secretary of the Interior's Standards for the Treatment of Historic Properties.*

The project has been reviewed by the Minnesota Department of Transportation (MnDOT's) Cultural Resources Unit (CRU) and SHPO. As a result of the Section 106 process for Historic Properties, MnDOT's CRU has provided a letter stating that there are no anticipated permanent adverse physical impacts associated with this work and the SHPO has provided concurrence. The SHPO and MnDOT's CRU letters of 'no adverse effect' are included as Attachment D. The overall project is considered rehabilitation since many alterations have occurred in the district and to the streetscape since the period of significance. While all the Secretary of Interior's Standards for Rehabilitation apply, these two are most relevant to the proposed project.

2) The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.

5) Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize the preserved historic property shall be preserved.

This project falls within a historically designated area (Historic District) and design of the street follows guidance contained in the Warehouse District Heritage Streets Plan. The project has also been designed in coordination with the City of Minneapolis's Heritage Preservation Commission (HPC).

*(6) The certificate of appropriateness conforms to all applicable regulations of this preservation ordinance and is consistent with the applicable policies of the comprehensive plan and applicable preservation policies in small area plans adopted by the city council.*

The project has been reviewed by the MnDOT's CRU and the SHPO. As a result of the Section 106 process for Historic Properties, MnDOT's CRU has provided a letter stating that there are no anticipated permanent adverse physical impacts associated with this work and the SHPO has provided concurrence. The project is an implementation component of the North Loop Small Area Plan and Heritage Street Plan in accordance with the historic design guidelines.

The following findings must be addressed if approving a certificate of appropriateness that involves the destruction, in whole or in part, of any landmark, property in an historic district or nominated property under interim protection:

*(7) The destruction is necessary to correct an unsafe or dangerous condition on the property, or that there are no reasonable alternatives to the destruction. In determining whether reasonable alternatives exist, the commission shall consider, but not be limited to, the significance of the property, the integrity of the property and the economic value or usefulness of the existing structure, including its current use, costs of renovation and feasible alternative uses. The commission may delay a final decision for a reasonable period of time to allow parties interested in preserving the property a reasonable opportunity to act to protect it.*

Aside from the on-going maintenance and patching, this roadway was last reconstructed in 1926. The current street pavement surface is in poor condition and is currently a mix of bituminous patches and failing paver blocks. The road base under the driving surface is failing, leading to significant cracking and an uneven driving surface. The settling pavement provides areas for storm water to collect within the roadway. The roadway driving surface requires frequent maintenance and repairs.

Pedestrian and ADA compliant facilities along the corridor are also lacking. The sidewalk adjacent to the existing buildings does not have a consistent route for pedestrians to follow. Most intersections do not have ADA compliant ramps or facilities. Several corners have depressed curb that is cracked and uneven in lieu of pedestrian ramps.

A written statement by the applicant making the findings that alterations are proposed in a manner that demonstrates that the applicant has made adequate consideration of the following documents and regulations:

*(8) The description and statement of significance in the original nomination upon which designation of the landmark or historic district was based.*

The project is partially located within the NRHP-listed Minneapolis Warehouse Historic District (HE-MPC-0441). The district has local designation by the City of Minneapolis, and has design guidelines for the district. The historic district is listed under Criterion A as:

- a) an area of early commercial growth during the development of Minneapolis and
- b) for its role as Minneapolis became a major distribution and jobbing center in the upper Midwest.

The district is also listed under Criterion C as an intact concentration of commercial buildings designed by important architects during the late nineteenth and early twentieth centuries.

*(9) Where applicable, Title 20 of the Minneapolis Code of Ordinances, Zoning Code, Chapter 530, Site Plan Review.*

Not Applicable

*(10) The typology of treatments delineated in the Secretary of the Interior's Standards for the Treatment of Historic Properties and the associated guidelines for preserving, rehabilitating, reconstructing, and restoring historic buildings.*

Not Applicable. Refer to 'Required Finding #5 for a discussion of the Secretary of the Interior Standards for Rehabilitation.

In addition, the following findings must be addressed if approving a certificate of appropriateness that involves alterations to a property within an historic district:

*(11) The alteration is compatible with and will ensure continued significance and integrity of all contributing properties in the historic district based on the period of significance for which the district was designated.*

The reconstruction was first proposed as part of the Warehouse District Heritage Street Plan (2011), which provides an approach to preserve and rehabilitate the historic infrastructure, improve pedestrian accessibility and increase sustainable practices through better stormwater management. The design guidelines set forth in the Warehouse Historic District Design Guidelines and Warehouse District Heritage Street Plan were followed in the design of this project.

*(12) Granting the certificate of appropriateness will be in keeping with the spirit and intent of the ordinance and will not negatively alter the essential character of the historic district.*

The preservation of the historic vitrified clay paver brick surface and loading docks will not negatively impact the historic district's integrity of location, design, setting, materials, workmanship, feeling and setting.

*(13) The certificate of appropriateness will not be injurious to the significance and integrity of other resources in the historic district and will not impede the normal and orderly preservation of surrounding resources as allowed by regulations in the preservation ordinance.*

The project has been reviewed by way of the Section 106 process for Historic Properties. As a result, MnDOT's CRU has provided a letter stating that there are no anticipated permanent adverse physical impacts associated with this work and the SHPO has provided concurrence.

### Neighborhood Correspondence

See Attachment E for notification letter to neighborhood groups.

### Copy of Scaled and Dimensioned Site Plan

See Attachment F for construction plan sheets.

### Photos of the Property

See Attachment G for existing conditions photos of 6<sup>th</sup> Avenue within the Historic Warehouse District.

### Submittal of Progress Towards Approval of All Required State and Federal Reviews and Permits

This project is a federally funded, City administered roadway project. The project is required to meet all environmental clearances and permits through MnDOT's federal aid office prior to authorization of funds.

Environmental Review: This project utilizes federal funds authorized by the Federal Highway Department; therefore, the project is required to meet all National Environmental Policy Act (NEPA) regulations. NEPA requires the review and avoidance of potential impacts to the social and natural environment for proposed transportation projects.

Based on the evaluation of this project and the attached documentation, it is determined that the project meets the criteria of and is properly classified as a Federal Categorical Exclusion (Class II Action Category 23 CFR 771.117 (d) (3)). There are no extraordinary circumstances such as:

1. Significant impacts on the environment;
2. Substantial controversy on environmental grounds;
3. Significant impacts to Section 4(f) or 106 properties;
4. Inconsistency with any federal, state or local law or administrative determination relating to the environment.

The City of Minneapolis prepared a Project Memorandum to document the Categorical Exclusion request.

Since this is a locally-administered, federal aid project on local roads, MnDOT's State Aid Division is responsible for environmental review. The draft PM has been reviewed by MnDOT State Aid and the final PM is being revised and submittal for final approval.

See Attachment D for agency coordination letters and environmental documentation pertaining to the project.

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. ANY ERRORS OR OMISSIONS SHALL BE REPORTED TO STANTEC IMMEDIATELY. STANTEC SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS MADE BY THE CONTRACTOR. STANTEC SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS MADE BY THE CONTRACTOR. STANTEC SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS MADE BY THE CONTRACTOR.

**DESIGN DESIGNATION**

DESIGNATION	URBAN
FUNCTIONAL CLASS	URBAN COLLECTOR
NO. OF TRAFFIC LANES	2
NO. OF PARKING LANES	1
DRIVING LANE WIDTH	11 FT
SHOULDER WIDTH	N/A
R-VALUE	30
20 YEAR ESALS	1,400,000
ADT (PRESENT YEAR) 2016	5,402
ADT (FUTURE YEAR) 2036	5,942
HCA DT (FUTURE YEAR) 2036	600
DESIGN SPEED	30 MPH
DESIGN LOAD	9 TON
DESIGN SPEED BASED ON	
STOPPING SIGHT DISTANCE:	
HEIGHT OF EYE = 3.5 FT	
HEIGHT OF OBJECT = 2.0 FT	

DESIGN SPEED: 30MPH  
 6TH AVE (STA. 10+13.85-22+45.97)  
 6TH AVE NORTH (STA. 50+19.80-52+28.33)

DESIGN SPEED NOT ACHIEVED AT:  
 10+30-10+60 VERTICAL CURVE (14.7MPH)  
 21+40-21+55 VERTICAL CURVE (15.0MPH)

50+30-50+70 VERTICAL CURVE (17.5 MPH)

# MINNESOTA DEPARTMENT OF TRANSPORTATION

## CITY OF MINNEAPOLIS

### HENNEPIN COUNTY, MINNESOTA

#### 6TH AVENUE N STREET RECONSTRUCTION

CONSTRUCTION PLAN FOR: GRADING, BITUMINOUS PAVEMENT, CONCRETE PAVEMENT, BRICK PAVEMENT  
 CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, STORM SEWER

LOCATED ON 6TH AVENUE N (MSAS 220) FROM 5TH STREET N TO 216' NORTH OF WASHINGTON AVENUE (CSAH 152)

STATE PROJECT NO. SP 141-220-005

CITY PROJECT NO. 600D6699

6TH AVENUE N (MSAS 220)				6TH AVENUE N					
GROSS LENGTH	1232.12	FEET	0.233	MILES	GROSS LENGTH	208.53	FEET	0.040	MILES
BRIDGE LENGTH	0.00	FEET	0.000	MILES	BRIDGE LENGTH	0.00	FEET	0.000	MILES
EXCEPTION LENGTH	0.00	FEET	0.000	MILES	EXCEPTION LENGTH	0.00	FEET	0.000	MILES
NET LENGTH	1232.12	FEET	0.233	MILES	NET LENGTH	208.53	FEET	0.040	MILES
REF. PT.	TO REF. PT.				REF. PT.	TO REF. PT.			
LENGTH AND DESCRIPTION BASED ON: 6TH AVENUE N ALIGNMENT					LENGTH AND DESCRIPTION BASED ON: 6TH AVENUE N ALIGNMENT				

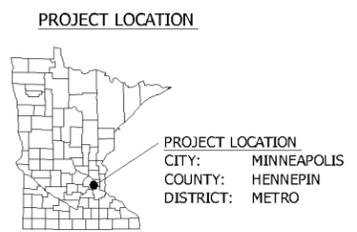
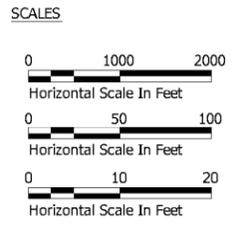
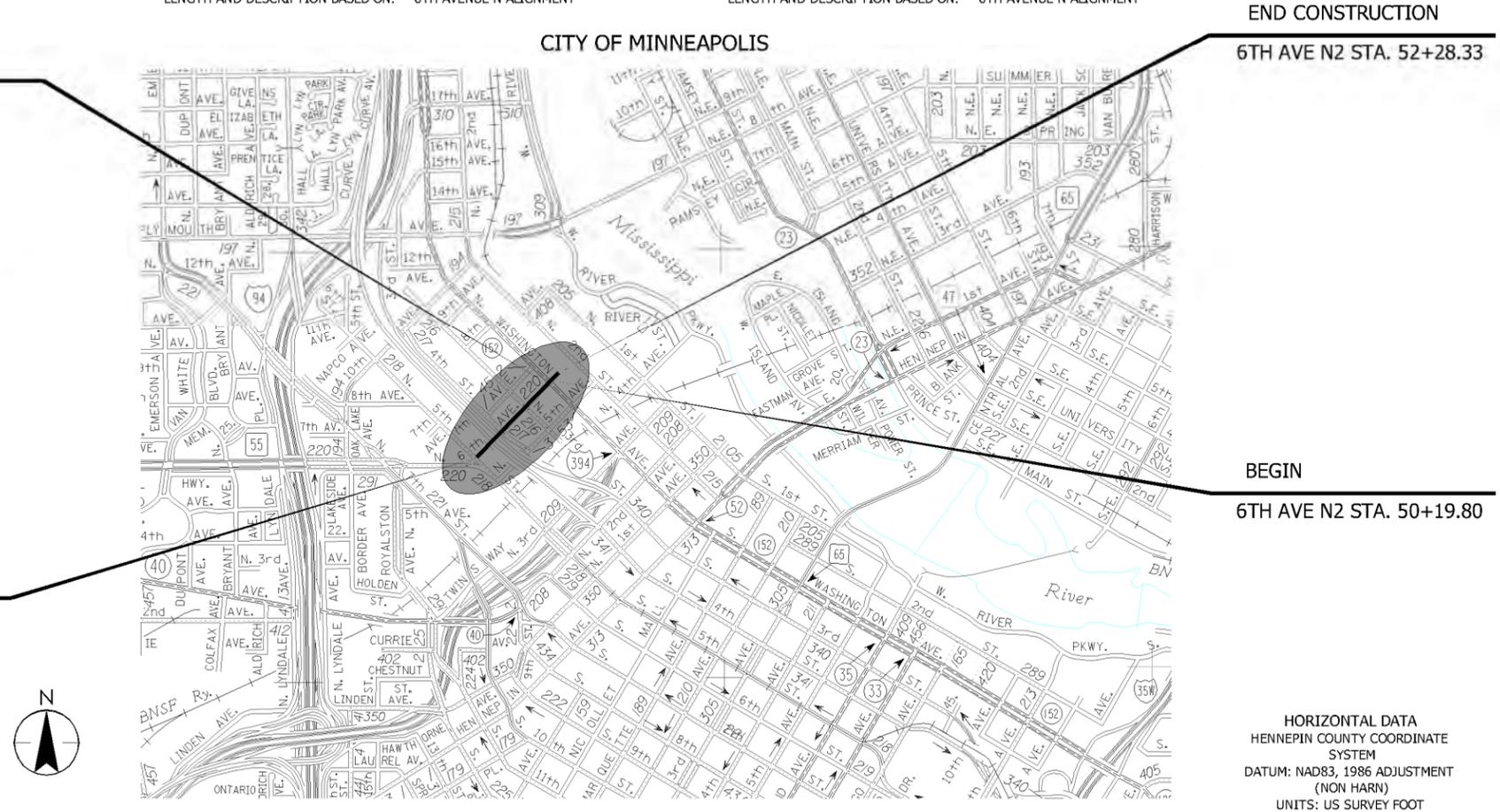
END  
 SP 141-220-005

6TH AVE N STA. 22+45.97

STATION EQUATION  
 6TH AVE N STA. 22+74.18 =  
 6TH AVE N2 STA 50+00

BEGIN CONSTRUCTION  
 SP 141-220-005

6TH AVE N STA. 10+13.85



A VARIANCE TO MNDOT STATE AID RULE 8820.9936, CHAPTER 8820, EFFECTIVE 2011, WAS GRANTED BY THE COMMISSIONER OF TRANSPORTATION ON \_\_\_\_\_, 2015 TO ALLOW A VERTICAL PROFILE BETWEEN STATION 10+30 AND 10+60 THAT MEETS A DESIGN SPEED OF 14.7 MPH INSTEAD OF 30 MPH AS REQUIRED BY RULE 8820.9936; AND WITH A VERTICAL PROFILE BETWEEN STATION 21+40 AND 21+55 THAT MEETS A DESIGN SPEED OF 15.0 MPH INSTEAD OF 30 MPH AS REQUIRED BY RULE 8820.9936.

STATE PROJ. NO. \_\_\_\_\_ CHARGE IDENTIFIER \_\_\_\_\_  
 SP 141-220-005 \_\_\_\_\_

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."

END CONSTRUCTION  
 6TH AVE N2 STA. 52+28.33

BEGIN  
 6TH AVE N2 STA. 50+19.80

**HORIZONTAL DATA**  
 HENNEPIN COUNTY COORDINATE SYSTEM  
 DATUM: NAD83, 1986 ADJUSTMENT (NON HARN)  
 UNITS: US SURVEY FOOT

**VERTICAL DATA**  
 DATUM: NGVD29

FEDERAL PROJECT NUMBER: TA 2716(051)

--- GOVERNING SPECIFICATIONS ---  
 THE 2014 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATION FOR CONSTRUCTION" AND THE 2014 EDITION OF THE "MATERIALS LAB SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN.

**INDEX**

SHEET NO.	SHEET DESCRIPTION
1	TITLE SHEET
2	STATEMENT OF ESTIMATED QUANTITIES
3	PROJECT CONTACTS
4	LEGEND
5	GENERAL LAYOUT
6	SOILS AND CONSTRUCTION NOTES
7	STANDARD PLATES AND INDEX OF TABULATIONS
8 - 11	EXISTING UTILITY TABULATIONS
12 - 15	TABULATIONS
16 - 28	CONSTRUCTION DETAILS AND STANDARD PLANS
29 - 31	TYPICAL SECTIONS
32	ALIGNMENT PLAN
33 - 34	INPLACE TOPOGRAPHY AND UTILITY PLAN
35 - 36	REMOVAL PLAN
37 - 39	CONSTRUCTION PLAN AND PROFILE
40 - 42	PROFILES
43 - 44	PAVING PLAN
45 - 46	SPOT ELEVATION PLAN
47 - 50	INTERSECTION DETAILS
51 - 53	PEDESTRIAN CURB RAMP DETAILS
54 - 55	PROPOSED UTILITY PLAN
56 - 58	DRAINAGE PROFILES AND TABULATIONS
59 - 61	SWPPP
62 - 64	TEMPORARY EROSION CONTROL PLAN
65 - 67	STREETSCAPE PLAN
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70 - 73	TRAFFIC CONTROL AND DETOUR PLAN
74 - 81	TRAFFIC SIGNAL PLAN
82 - 88	CROSS SECTIONS

THIS PLAN CONTAINS 88 SHEETS.



I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINTED NAME PHIL GRAVEL LIC. NO. 19864

SIGNATURE \_\_\_\_\_ DATE 8/6/2015

APPROVED: MINNEAPOLIS CITY ENGINEER OR DEPUTY CITY ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

APPROVED: DIRECTOR, TRAFFIC & PARKING SERVICES \_\_\_\_\_ DATE \_\_\_\_\_

APPROVED: DIRECTOR, TRANSPORTATION PLANNING & ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

APPROVED: DIRECTOR, TRANSPORTATION MAINTENANCE & REPAIR \_\_\_\_\_ DATE \_\_\_\_\_

APPROVED: DIRECTOR, SURFACE WATER & SEWERS \_\_\_\_\_ DATE \_\_\_\_\_

APPROVED: DIRECTOR, WATER TREATMENT & DISTRIBUTION \_\_\_\_\_ DATE \_\_\_\_\_

DISTRICT STATE AID ENGINEER: REVIEWED FOR COMPLIANCE WITH STATE AND FEDERAL AID RULES/POLICY \_\_\_\_\_ DATE \_\_\_\_\_

APPROVED FOR STATE AND FEDERAL AID FUNDING: STATE AID ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

Plot Date: 10/27/2015 9:29am  
 Drawing Name: V:\1936\active\193620857\CAD\Drawg\Plan\sheet1\193620857\_001.dwg  
 User: Minneapolis\_City

COMMENTS	DATE

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING.

CITY OF MINNEAPOLIS							
PROJECT MANAGER	STREET CONSTRUCTION	COMMUNICATIONS	GAS	COMMUNICATIONS	COMMUNICATIONS	COMMUNICATIONS	COMMUNICATIONS
309 2ND AVE S., ROOM 300 MINNEAPOLIS, MN 55401-2268 WEB PAGE: www.ci.minneapolis.mn.us CONTACT: JEFF HANDELAND TEL: 612-673-2363 EMAIL: jeff.handeland@minneapolismn.gov	<b>TRANSPORTATION MAINTENANCE &amp; REPAIR</b>  1858 27TH ST. E. MINNEAPOLIS, MN 55407-1265 WEB PAGE: www.ci.minneapolis.mn.us CONTACT: LARRY MATSUMOTO TEL: 612-673-5630 MOBILE: 612-919-1148 EMAIL: larry.matsumoto@minneapolismn.gov	<b>AT&amp;T</b>  CONTACT: KELLY FAILS EMAIL: kfails@att.com	<b>CENTER POINT ENERGY</b>  CONTACT: ALLA DENISOVA EMAIL: alla.denisova@centerpointenergy.com	<b>COMCAST</b>  CONTACT: TOM NIEDZIELSKI EMAIL: thomas_niedzielski@cable.comcast.com	<b>LEVEL 3 COMMUNICATIONS</b>  CONTACT: AURA BULARAN EMAIL: aura.bularan@level3.com	<b>XO COMMUNICATION INC</b>  CONTACT: ED PARKER TEL: 320-241-4029 EMAIL: ed.e.parker@xo.com	<b>WINDSTREAM COMMUNICATIONS</b>  CONTACT: JOEL SCHROEDER TEL: 763-682-3514 EMAIL: joel.schroeder@windstream.com
<b>PARK &amp; RECREATION BOARD</b>  <b>FORESTRY</b>  2117 WEST RIVER RD. MINNEAPOLIS, MN 55411-2227 WEB PAGE: www.minneapolisparcs.org CONTACT: TOM JOHNSON TEL: 612-370-4900 EMAIL: tjohanson@minneapolisparcs.org	<b>SEWER DESIGN</b>  <b>SURFACE WATER &amp; SEWERS</b>  309 2ND AVE S., ROOM 300 MINNEAPOLIS, MN 55401-2268 WEB PAGE: www.ci.minneapolis.mn.us CONTACT: KELLY MORIARITY TEL: 612-673-3617 FAX: 612-673-2048 EMAIL: kelly.moriarity@minneapolismn.gov	<b>COMMUNICATIONS</b>  <b>ROGERS COMMUNICATIONS</b>  CONTACT: VICKI MORAN EMAIL: vmoran@gabes.com	<b>COMMUNICATIONS</b>  <b>QWEST COMMUNICATIONS</b>  CONTACT: TODD HEYER TEL: 612-408-9357 EMAIL: todd.heyer@centurylink.com	<b>COMMUNICATIONS</b>  <b>TW TELECOM</b>  5480 FELTL RD. MINNETONKA, MN 55343 WEB PAGE: www.twtelecom.com CONTACT: BOB STRONG TEL: 612-805-6827 MOBILE: 612-805-6827 FAX: 720-225-5909 EMAIL: bob.strong@twtelecom.com	<b>POWER</b>  <b>XCEL ENERGY</b>  CONTACT: MATT HEATH EMAIL: MATTHEW.A.HEATH@XCELENERGY.COM	<b>COMMUNICATIONS</b>  <b>MCI</b>  CONTACT: VINCE JOHNSON EMAIL: vince.johnson@verizon.com	<b>COMMUNICATIONS</b>  <b>ZAYO BANDWIDTH</b>  CONTACT: MIKE DAHLE EMAIL: michael.dahle@zayo.com
<b>TRAFFIC DESIGN/CONSTRUCTION</b>  <b>TRAFFIC &amp; PARKING SERVICES</b>  300 BORDER AVE. N. MINNEAPOLIS, MN 55405-1528 WEB PAGE: www.ci.minneapolis.mn.us CONTACT: STEVE MOSING TEL: 612-673-5746 EMAIL: steve.mosing@minneapolismn.gov CONTACT: DOUG MADAY TEL: 612-673-5755 MOBILE: 612-221-5274 EMAIL: doug.maday@minneapolismn.gov	<b>SEWER CONSTRUCTION</b>  <b>SURFACE WATER &amp; SEWERS</b>  PAVING CONSTRUCTION OFFICE, ROOM MAIN 3607 44TH ST. E. MINNEAPOLIS, MN 55406 WEB PAGE: www.ci.minneapolis.mn.us CONTACT: KEVIN DANEN TEL: 612-673-5627 EMAIL: kevin.danen@minneapolismn.gov	<b>COMMUNICATIONS</b>  <b>CENTURYLINK</b>  CONTACT: TODD PAULSON EMAIL: todd.paulson@centurylink.com	<b>TRANSPORTATION</b>  <b>MNDOT</b>  CONTACT: JIM DEENY TEL: 651-234-7762 EMAIL: jim.deeny@dot.state.mn.us	<b>WATER/WASTEWATER</b>  <b>MCES</b>  CONTACT: MANNY LEE EMAIL: manking.lee@metc.state.mn.us	<b>BUS/RAIL/SEWER</b>  <b>METROPOLITAN COUNCIL</b>  560 6TH AVE. N MINNEAPOLIS, MN 55411 WEB PAGE: www.metrocouncil.org CONTACT: DAWN SKOOG TEL: 612-249-7547 EMAIL: dawn.skoog@metc.state.mn.us	<b>GENERAL NOTES:</b> 1. ALL WORK PERFORMED AND ALL MATERIALS FURNISHED SHALL BE IN CONFORMANCE WITH THE LINES, GRADES, CROSS SECTIONS, DIMENSIONS, AND MATERIAL REQUIREMENTS, INCLUDING TOLERANCE, SHOWN IN THE PLANS OR INDICATED IN THE SPECIFICATIONS. 2. THE PLAN INDICATES THE APPROXIMATE LOCATION OF THE KNOWN UTILITIES ON THE PROJECT 3. IF THE CONTRACTOR DISCOVERS UTILITY PROPERTY WHOSE EXISTENCE WAS NOT KNOWN, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY OWNER AND THE ENGINEER. 4. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL UTILITY OWNERS AND TO ASCERTAIN THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES BEFORE PERFORMING EXCAVATION OPERATIONS. THE CONTRACTOR SHALL CONDUCT THE OPERATIONS IN THE VICINITY OF EXISTING UNDERGROUND UTILITIES IN A MANNER THAT WILL PREVENT DAMAGE TO ANY OF THEM. 5. STATE LAW REQUIRES ANYONE EXCAVATING OR DRILLING IN THE GROUND IN THE STATE OF MINNESOTA WITH POWER EQUIPMENT TO CALL GOPHER STATE "ONE CALL" AT 651-454-0002 FOR UTILITY LOCATIONS AT LEAST 48 HOURS BEFORE STARTING WORK. 6. SAWCUT SIDEWALKS TO NEAREST JOINT, 3' MINIMUM FOR CONCRETE WALK 7. ASSUMPTIONS UNDERGROUND UTILITIES 7.1. WATER MAIN: -7.5 FT 7.2. GAS MAIN: -3.0 FT 7.3. XCEL: -3.0 FT 7.4. TELEPHONE: -3.0 FT 7.5. CABLE: -3.0 FT	
<b>STREET DESIGN</b>  <b>TRANSPORTATION PLANNING &amp; ENGINEERING</b>  309 2ND AVE. S., ROOM 300 MINNEAPOLIS, MN 55401-2268 WEB PAGE: www.ci.minneapolis.mn.us CONTACT: JEFF HANDELAND TEL: 612-673-2363 EMAIL: jeff.handeland@minneapolismn.gov	<b>WATER DESIGN/CONSTRUCTION</b>  <b>WATER TREATMENT &amp; DISTRIBUTION</b>  935 5TH AVE. SE. MINNEAPOLIS, MN 55414-1205 WEB PAGE: www.ci.minneapolis.mn.us CONTACT: BOB ERVIN TEL: 612-673-5682 EMAIL: robert.ervin@minneapolismn.gov	<b>STEAM</b>  <b>NRG ENERGY CENTER MINNEAPOLIS</b>  2600 IDS CENTER MINNEAPOLIS, MN 55402 CONTACT: GREG OLSON TEL: 612-349-6087 EMAIL: greg.olson@nrgenergy.com	<b>TRANSIT</b>  <b>METRO TRANSIT</b>  CONTACT: DAN LUECK EMAIL: dan.lueck@metrotransit.org				

Plot Date: 10/27/2016 9:30am  
 Program: 10/27/2016 9:30am  
 User: 10/27/2016 9:30am

NO.	DATE	APPR.	REVISION

SURVEY	MPV
DRAWN	TJM
DESIGNED	TJM
CHECKED	CIVL
APPROVED	LEP
PROJ. NO.	1936288

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: TYLER J MOLETTE  
 SIGNATURE:   
 DATE: 8-4-15 LIC. NO. 51774





2335 Highway 56 W  
 St. Paul, MN 55113  
 www.stantec.com

PROJECT CONTACTS

**CITY OF MINNEAPOLIS, MN**  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

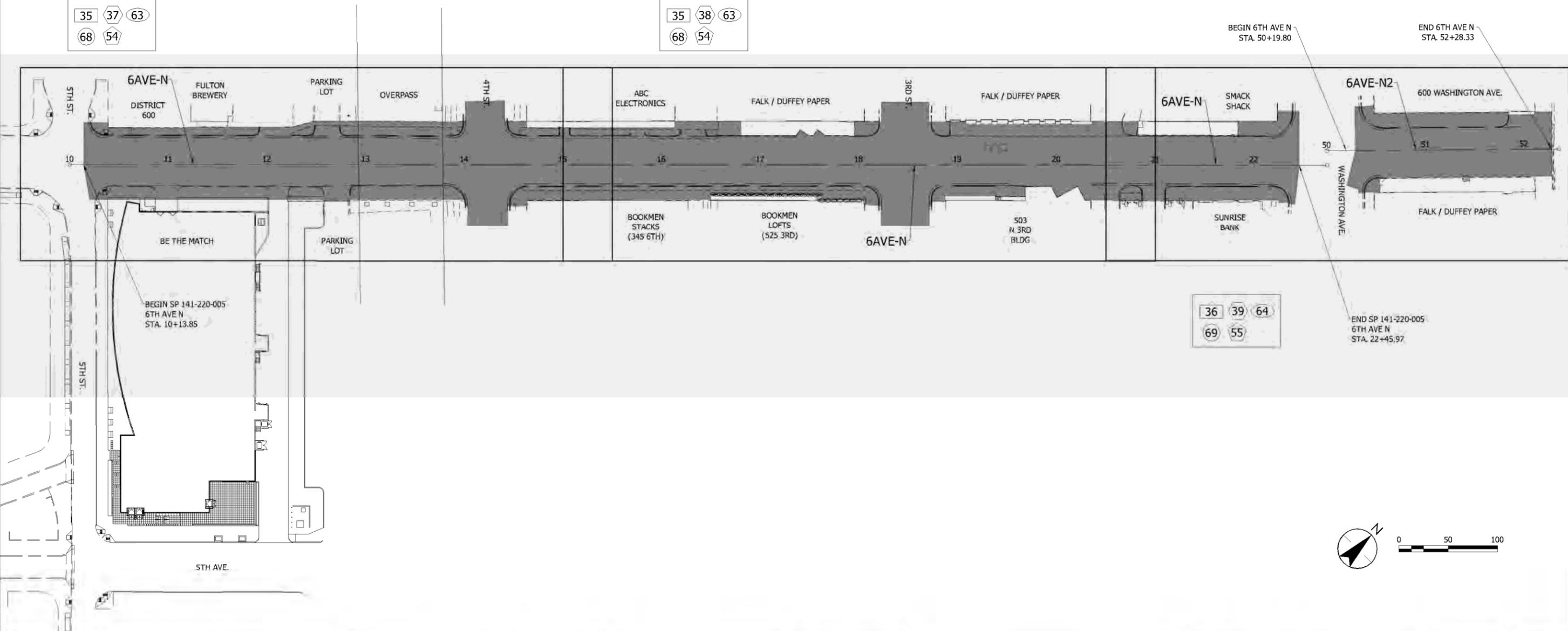
THE CONTRACTOR SHALL VERIFY THE REFLECTIONS FOR ACCURACY AND SCALE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF MINNEAPOLIS AND THE STATE OF MINNESOTA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF MINNEAPOLIS AND THE STATE OF MINNESOTA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF MINNEAPOLIS AND THE STATE OF MINNESOTA.

LEGEND	
	CONSTRUCTION AREA
	REMOVAL PLAN
	CONSTRUCTION PLAN AND PROFILE
	DRAINAGE PLAN
	TEMPORARY EROSION CONTROL PLAN
	SIGNING AND PAVEMENT MARKING PLAN

35 37 63  
68 54

35 38 63  
68 54

36 39 64  
69 55



Plot Date: 10/27/2016 9:33am  
Drawing Path: \\s:\projects\141-220-005\6th Ave N\Drawings\141-220-005-005.dwg  
User: jgarcia  
Printer: 141-220-005-005.ctb  
Plot Date: 10/27/2016 9:33am  
Drawing Path: \\s:\projects\141-220-005\6th Ave N\Drawings\141-220-005-005.dwg  
User: jgarcia  
Printer: 141-220-005-005.ctb

NO.	DATE	APPR.	REVISION

SURVEY	MPV
DESIGN	JDP
CHECKED	CWL
APPROVED	LRG
PROJECT NO.	141-220-005

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: JEFFREY DANESON  
 SIGNATURE: *[Signature]*  
 DATE: 8/15/16 LIC. NO.: 41964

Minneapolis  
City of Lakes

2335 Highway 36 W  
St. Paul, MN 55113  
www.stantec.com

GENERAL LAYOUT

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET  
 5  
 OF  
 88

# SOILS AND CONSTRUCTION NOTES

1. THE GRADING GRADE IS DEFINED AS THE BOTTOM OF THE CLASS 5 AGGREGATE BASE.
2. SELECT GRADING MATERIAL ON THIS PROJECT SHALL CONSIST OF ALL SOILS ENCOUNTERED EXCEPT TOPSOIL, DEBRIS, ORGANIC MATERIAL AND OTHER UNSTABLE MATERIAL.
3. GRANULAR MATERIAL SHALL MEET THE REQUIREMENTS OF SPEC. 3149.2B.
4. SELECT GRANULAR MATERIAL SHALL MEET THE REQUIREMENTS OF SPEC. 3149.2B.
5. STABILIZING AGGREGATE SHALL MEET THE REQUIREMENTS OF SPEC. 3149.2C.
6. SLOPE DRESSING IS DEFINED AS THE INPLACE TOPSOIL OR OTHER SOIL PLACED DURING PRIOR CONSTRUCTION TO PROVIDE A MEDIUM FOR ESTABLISHING TURF.
7. DISPOSITION OF EXCAVATED MATERIAL SHALL BE IN ACCORDANCE WITH SPEC. 2105.3D.
8. ALL TOPSOIL SHALL BE CONSIDERED COMMON EXCAVATION.
9. ANY DEBRIS WHICH MAY BE ENCOUNTERED BY THE CONTRACTOR DURING GRADING SHALL BE DISPOSED OF BY THE CONTRACTOR OFF THE PROJECT RIGHT OF WAY IN A SUITABLE DISPOSAL AREA AS APPROVED BY THE ENGINEER.
10. ALL EXCAVATION AND EMBANKMENT SHALL BE PERFORMED IN ACCORDANCE WITH SPEC. 2105.
11. BITUMINOUS AND CONCRETE ITEMS DISTURBED BY CONSTRUCTION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE RECYCLED TO THE EXTENT ALLOWED IN THE BASE AND SURFACING ITEMS OR DISPOSED OF OFF OF THE RIGHT OF WAY IN ACCORDANCE WITH MNDOT SPECIFICATION 2104.3C.
12. THE CONTRACTOR SHALL INVESTIGATE AND MAKE HIS OWN DETERMINATION OF THE EXISTING PAVEMENT THICKNESS. SEE PROJECT SOIL BORINGS FOR MORE INFORMATION.
13. THE NEW UTILITY INSTALLATIONS AND IMPROVEMENTS WHICH ARE DEEPER THAN THE SUBGRADE IMPROVEMENT ZONE SHOULD BE PERFORMED PRIOR TO SUBGRADE PREPARATION. BACKFILL COMPACTION SHALL MEET THE REQUIREMENTS OF MNDOT 2105.3F1 (SPECIFIED DENSITY METHOD). (INCIDENTAL)
14. MATERIALS DEFINED AS UNSUITABLE (TOPSOIL, ORGANIC, DEBRIS) MUST BE REMOVED FROM THE EMBANKMENT FOUNDATION. THE UNSUITABLE MATERIALS SHALL BE DISPOSED OF IN ACCORDANCE WITH SPEC. 2104 AND 2105. MATERIALS SUCH AS PEAT, MARL, MULCH AND OTHER UNSTABLE MATERIALS NOT TO BE USED IN THE ROADBED EMBANKMENT SHALL BE DEPOSITED IN AREAS INDICATED IN THE PLAN, OR DISPOSED OF ELSEWHERE AS APPROVED BY THE ENGINEER.
15. WHERE SOILS ARE DRY AND STABLE, THE SUBCUT BOTTOM SHALL BE COMPACTED BY THE QUALITY COMPACTION METHOD AND INCLUDE A MINIMUM OF 4 PASSES WITH AN APPROVED ROLLER. COMPACTION SHALL BE COMPLETED TO THE SATISFACTION OF THE ENGINEER AND ACCORDING TO MNDOT SPECIFICATION 2105.3F.
16. COMPACTION OF SUITABLE GRADING MATERIAL SHALL BE BY THE QUALITY COMPACTION METHOD, MNDOT SPECIFICATION 2105.3F2.
17. COMPACTION OF AGGREGATE BASE SHOULD BE IN ACCORDANCE WITH MNDOT "MODIFIED PENETRATION INDEX METHOD".
18. PROVIDE FOR A UNIFORM BITUMINOUS TACK COAT BETWEEN ALL BITUMINOUS COURSES. THE TACK SHALL BE APPLIED IN ACCORDANCE WITH MNDOT SPECIFICATION 2357. THE ROADWAY SURFACE SHALL BE CLEAN AND FREE OF DEBRIS PRIOR TO APPLYING TACK. BITUMINOUS TACK COAT MATERIAL SHALL BE APPLIED AT A UNIFORM RATE OF 0.03 TO 0.05 GAL/SQ YD BETWEEN NEW BITUMINOUS LAYERS PLACED THAT HAVE BEEN SUBJECTED TO TRAFFIC AND A RATE OF 0.07 TO 0.10 GAL/SQ YD ON TRAFFICKED BITUMINOUS SURFACES TO BE OVERLAID. THESE APPLICATION RATES ARE FOR UNDILUTED EMULSION (AS SUPPLIED FROM THE REFINERY) OR MC LIQUID ASPHALT. (INCIDENTAL)
19. ALL BITUMINOUS MIXTURES SHALL MEET THE REQUIREMENTS OF SPEC. 2360.
20. THE BITUMINOUS AFT SPECIFICATION SHALL BE USED ON THIS PROJECT.
21. BITUMINOUS SURFACES WILL NOT BE SUBJECT TO THE IRI PAVEMENT SMOOTHNESS SPECIFICATION.
22. ALL BITUMINOUS FINAL SURFACES WILL BE SUBJECT TO STRAIGHT EDGE SMOOTHNESS REQUIREMENTS AS FOLLOWS:
  - THE SURFACE OF THE FINAL TWO LIFTS PLACED SHALL SHOW NO VARIATION GREATER THAN 1/4 INCH FROM THE EDGE OF A 10 FOOT STRAIGHTEDGE LAID PARALLEL TO OR AT RIGHT ANGLES TO THE CENTERLINE.
25. PROVIDE A SAWCUT TO ENSURE A UNIFORM JOINT WHERE PLACING NEW PAVEMENT NEXT TO INPLACE PAVEMENT.
26. WHERE CONNECTING TO INPLACE ROADWAYS AT THE TERMINI OF PROPOSED NEW CONSTRUCTION, CUT VERTICALLY TO THE BOTTOM OF THE INPLACE SURFACING OR TO THE BOTTOM OF THE NEW PROPOSED SURFACING, WHICHEVER IS DEEPER, THEN AT A 1:20 TAPER TO THE BOTTOM OF THE RECOMMENDED EXCAVATION.
27. PROVIDE 1:20 TAPERS WHERE APPLICABLE FOR CHANGES IN MATERIAL TYPES TO PREVENT AN ABRUPT SOIL DIFFERENTIAL. TAPERS BETWEEN PLASTIC AND GRANULAR SOILS SHOULD BE CONSTRUCTED SO THAT THE GRANULAR SOIL OVERLAYS THE PLASTIC SOIL.
28. AS A PRECAUTIONARY MEASURE FROM A SOILS STANDPOINT, TRAFFIC LANES TO BE USED DURING CONSTRUCTION MUST BE DELINEATED TO KEEP VEHICLES A SAFE DISTANCE AWAY FROM THE ADJACENT EXCAVATION. THE DELINEATION SHOULD COINCIDE WITH POINTS ESTABLISHED BY PROJECTING 1:2 OR GREATER (FLATTER) SLOPE BETWEEN THE EDGE OF THE TRAFFIC SURFACE AND THE BOTTOM OF THE EXCAVATION. A 1:3 OR FLATTER SLOPE WILL BE REQUIRED IN SWAMP EXCAVATION AREAS.
29. THE TOP OF BACKSLOPES AND THE TOE OF FILL SLOPES SHALL BE ROUNDED TO NATURALIZE THE CONSTRUCTION EVEN THOUGH THE CROSS SECTIONS DO NOT SHOW ROUNDING.
30. THE CONTRACTOR IS HEREBY REMINDED OF THE RESPONSIBILITY UNDER STATE LAW TO CONTACT ALL UTILITIES THAT MAY HAVE FACILITIES IN THE AREA. CONTACT MUST BE MADE THROUGH GOPHER STATE ONE-CALL.
31. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SEE THAT THE TEMPORARY EROSION ITEMS ARE USED THROUGHOUT THE PROJECT. WHENEVER PRACTICAL, EVEN THOUGH SPECIFIC LOCATIONS FOR THEIR USE MAY NOT BE DESIGNATED DUE TO CONSTRUCTION STAGING.
32. NO EXTRA PAYMENT WILL BE MADE FOR TEMPORARY STOCKPILING OF EXCAVATION, EMBANKMENT AND/OR BORROW MATERIAL.
33. WATER WILL BE REQUIRED FOR DUST CONTROL ON ALL GRAVEL SURFACES.
34. CONTRACTOR SHALL MATCH EXISTING ELEVATIONS AT TIE IN POINTS.
35. WHERE SIDEWALK MEETS EXISTING STAIRS, THE STEPS SHALL MATCH EXISTING RISER AND TREAD UNLESS OTHERWISE DIMENSIONED ON PLANS OR AS DIRECTED BY THE ENGINEER.
36. ALL STREET DIMENSIONS ARE TO FACE OF CURB. ALL CURB ELEVATIONS ARE TO TOP BACK OF CURB UNLESS OTHERWISE NOTED. STRUCTURE LOCATIONS ARE TO CENTER OF STRUCTURE. CASTING ELEVATIONS ARE GIVEN TO FLOW LINE OR SURFACE.
37. THE PITCH OF THE GUTTER, HEIGHT OF THE CURB, PEDESTRIAN RAMP GRADES MAY BE ADJUSTED AND MINOR CHANGES MADE TO MEET EXISTING CONDITIONS AT THE DISCRETION OF THE PROJECT ENGINEER.
38. SAWCUT AT NEAREST JOINT OR AS DIRECTED BY ENGINEER.
39. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES OUTSIDE THE CONSTRUCTION LIMITS RESULTING FROM NEGLIGENCE.

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING.

Plot Date: 10/27/2016 9:34am  
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 User: jason@stantec.com  
 Title: 13002667\_000.dwg

NO.	DATE	APPR.	REVISION

SURVEY	REV
DRAWN	JOP
DESIGNED	JOP
CHECKED	CWL
APPROVED	JRG
PROJECT NO.	13002667

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: JEFFREY DANESON  
 SIGNATURE: *[Signature]*  
 DATE: 8/15/16 LIC. NO. 41964



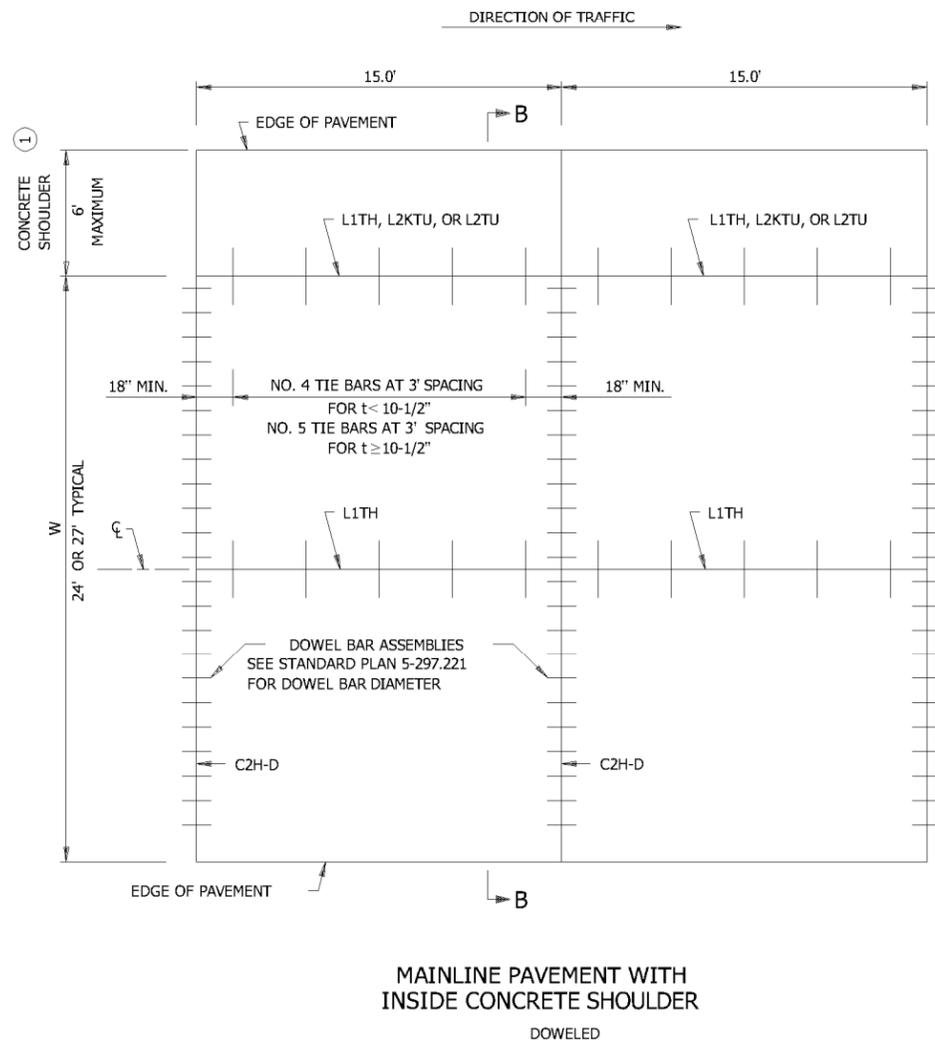
Minneapolis  
City of Lakes



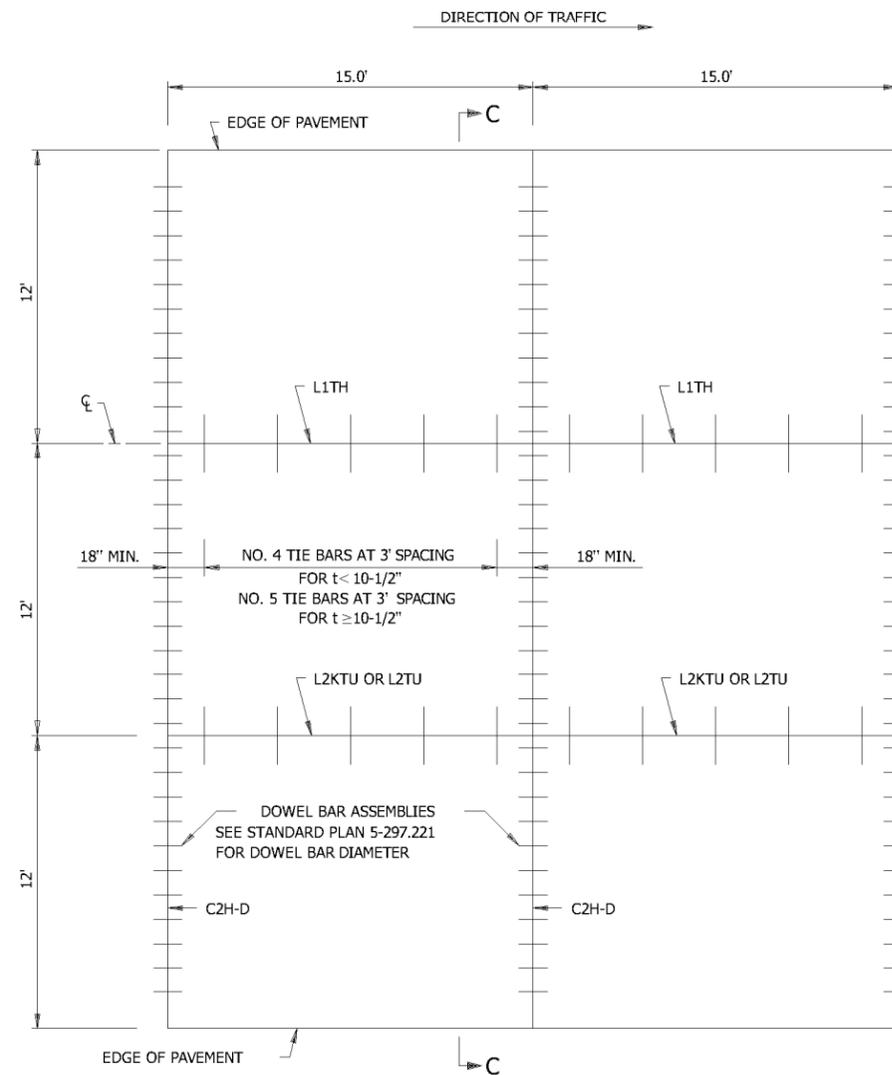
Stantec  
2335 Highway 56 W  
St. Paul, MN 55113  
www.stantec.com

SOILS AND CONSTRUCTION NOTES

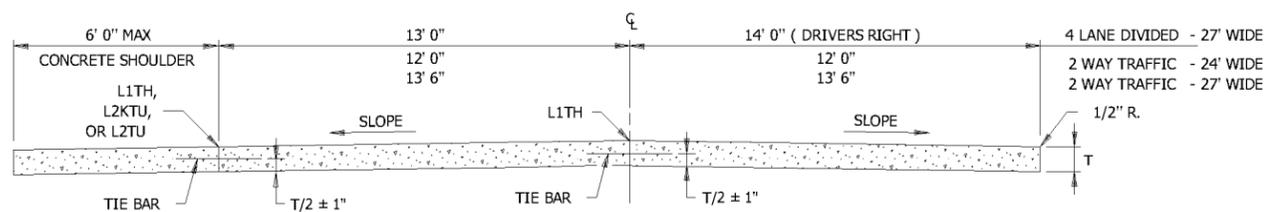
CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005



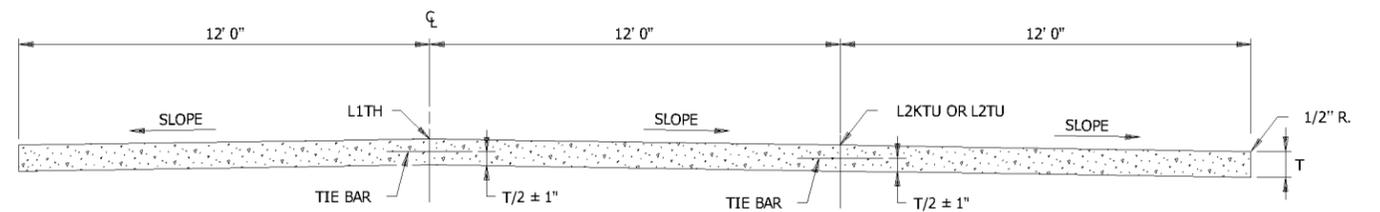
MAINLINE PAVEMENT WITH INSIDE CONCRETE SHOULDER  
DOWELED



MAINLINE PAVEMENT URBAN  
DOWELED



SECTION B-B



SECTION C-C

GENERAL NOTES:

SEE TYPICAL SECTIONS AND PLAN SHEETS FOR CROSS SLOPES AND PAVEMENT THICKNESS, T.

DOWEL BAR ASSEMBLIES, WHEN REQUIRED, SHALL BE SIMILAR TO THOSE SHOWN ON STANDARD PLATE 1103.

ALL REINFORCING BARS SHALL BE EPOXY COATED AND COMPLY WITH SPEC. 3301.

FOR SUPPLEMENTAL PAVEMENT REINFORCEMENT, SEE STANDARD PLATE 1070.

① CONTACT THE CONCRETE ENGINEER TO DISCUSS WHETHER TIE BARS AND SAWED JOINTS ARE NEEDED BASED ON CONCRETE SHOULDER WIDTH AND DEPTH.

REVISION:

APPROVED: 8-6-2014

*[Signature]*  
DIRECTOR, OFFICE OF MATERIALS AND ROAD RESEARCH



*[Signature]*  
STATE DESIGN ENGINEER

REVISED:

APPROVED:

8-6-2014

STANDARD PLAN 5-297.217

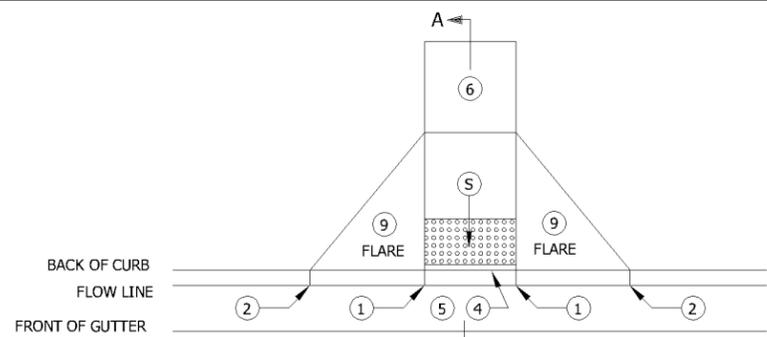
2 OF 2

CONCRETE MAINLINE PAVEMENT

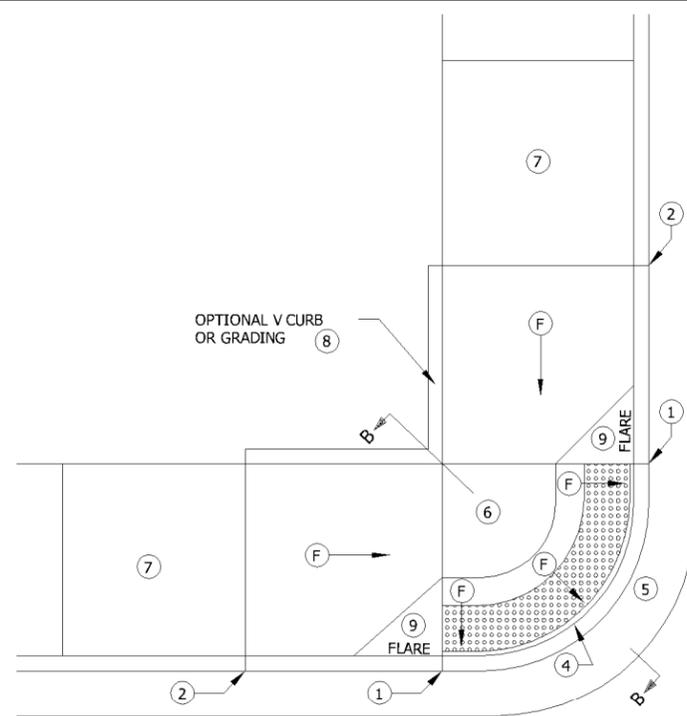
15.0 FT. PANEL LENGTH  
URBAN OR CONCRETE SHOULDERS

SP 141-220-005

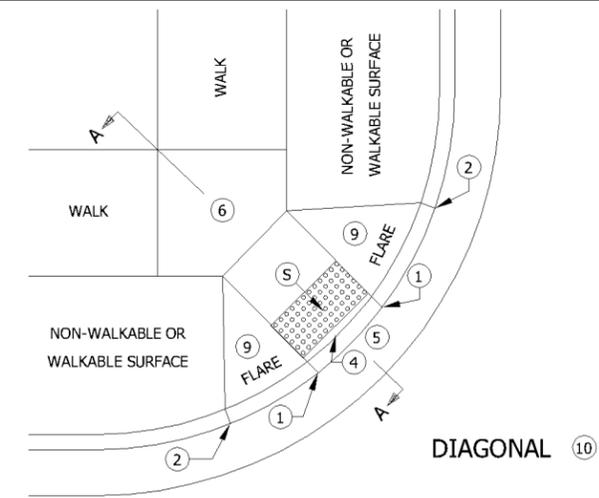
SHEET 21 OF 88



PERPENDICULAR



FAN



DIAGONAL 10

NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.

TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS MAY BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 5 WHEN LANDINGS ARE CAST SEPARATELY.

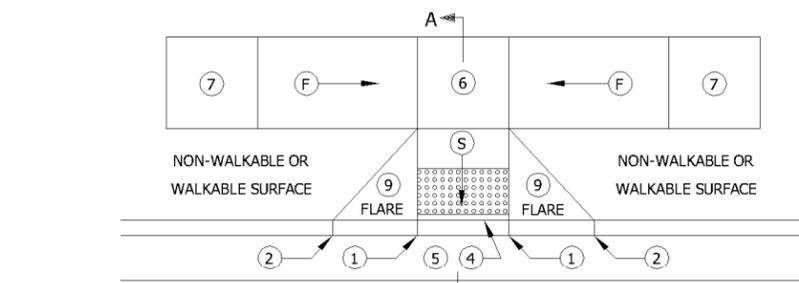
ALL SLOPES ARE ABSOLUTE, RATHER THAN RELATIVE TO SIDEWALK/ROADWAY GRADES.

TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.

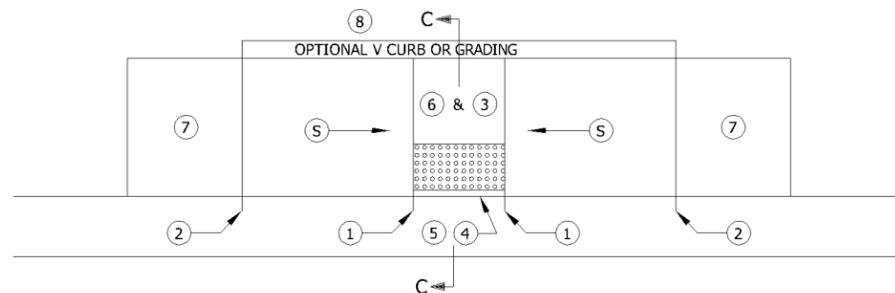
4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MINIMUM OF 24" IN THE PATH OF TRAVEL. SHARED USE PATHS SHALL HAVE DETECTABLE WARNING ACROSS THE ENTIRE WIDTH OF PATH WHEN THE PATH CROSSES A ROAD.

SEE STANDARD PLATE 7038 AND SHEET 4 OF 5 FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.

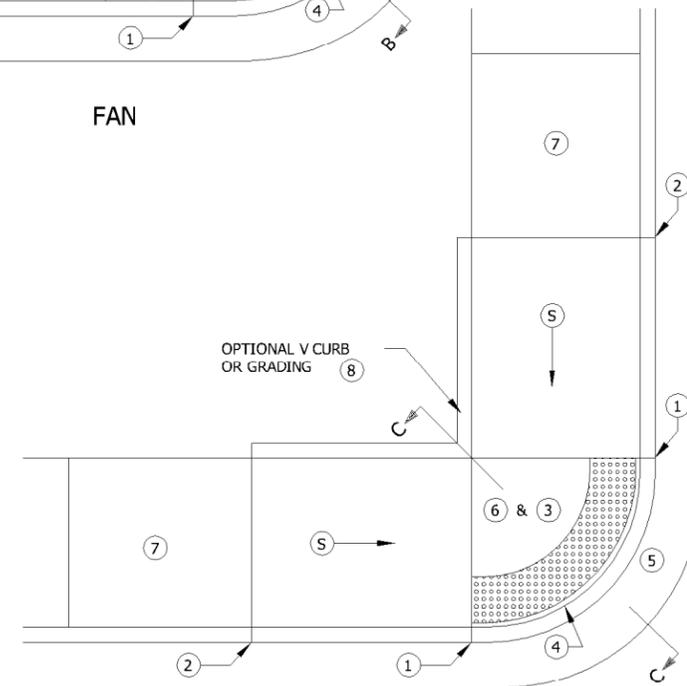
- 1 0" CURB HEIGHT.
- 2 FULL CURB HEIGHT.
- 3 DETECTABLE WARNINGS MAY BE PART OF 4' X 4' LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
- 4 1/2" PREFORMED JOINT FILLER MATERIAL AASHTO M 213. JOINT FILLER SHALL BE PLACED FLUSH WITH THE BACK OF CURB AND ADJACENT SIDEWALK. JOINT SHALL BE FREE OF DEBRIS. RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.
- 5 SEE PEDESTRIAN ACCESS ROUTE CURB AND GUTTER DETAIL FOR INFORMATION ON CONSTRUCTING CURB AND GUTTER AT CURB OPENINGS. SEE SHEET NO. 3 OF 5.
- 6 4' BY 4' MIN. LANDING WITH MAX. 2.0% SLOPE IN ALL DIRECTIONS.
- 7 IF LONGITUDINAL SLOPE IS GREATER THAN 5.0%, 4' X 4' MIN. LANDING WITH MAX 2.0% SLOPE IN ALL DIRECTIONS REQUIRED.
- 8 V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. SEE SHEET 5 OF 5.
- 9 SEE SHEET 4 OF 5, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
- 10 DIAGONAL RAMPS SHOULD ONLY BE USED AFTER ALL OTHER CURB RAMP TYPES HAVE BEEN EVALUATED AND DEEMED IMPRACTICAL.



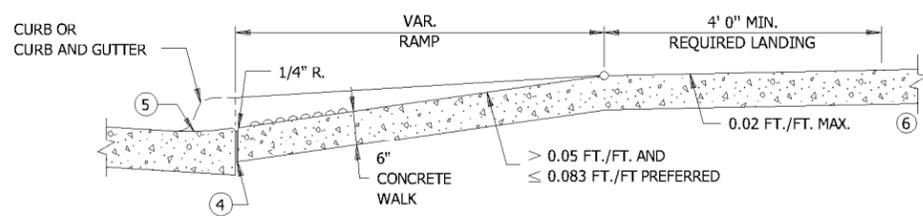
TIERED PERPENDICULAR



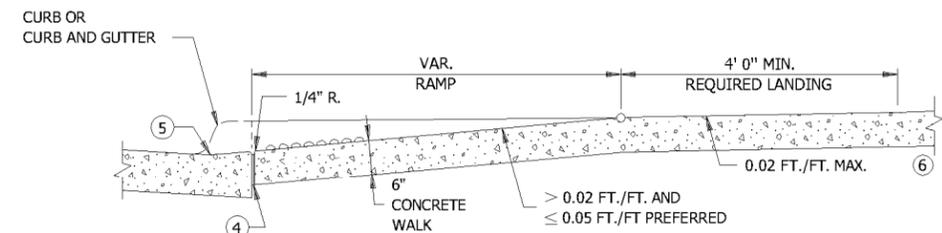
PARALLEL



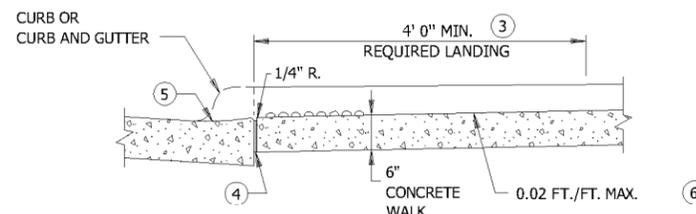
DEPRESSED CORNER



SECTION A-A  
PERPENDICULAR/TIERED/DIAGONAL



SECTION B-B  
FAN



SECTION C-C  
PARALLEL/DEPRESSED CORNER

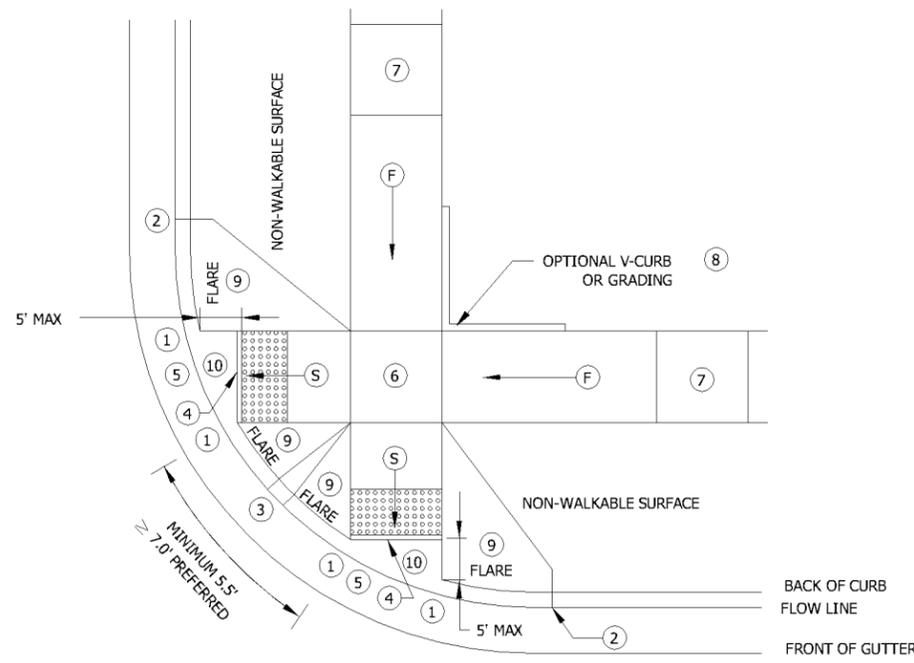
STANDARD PLAN 5-297.250  
1 OF 5

LEGEND	
THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.	
S	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%
F	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

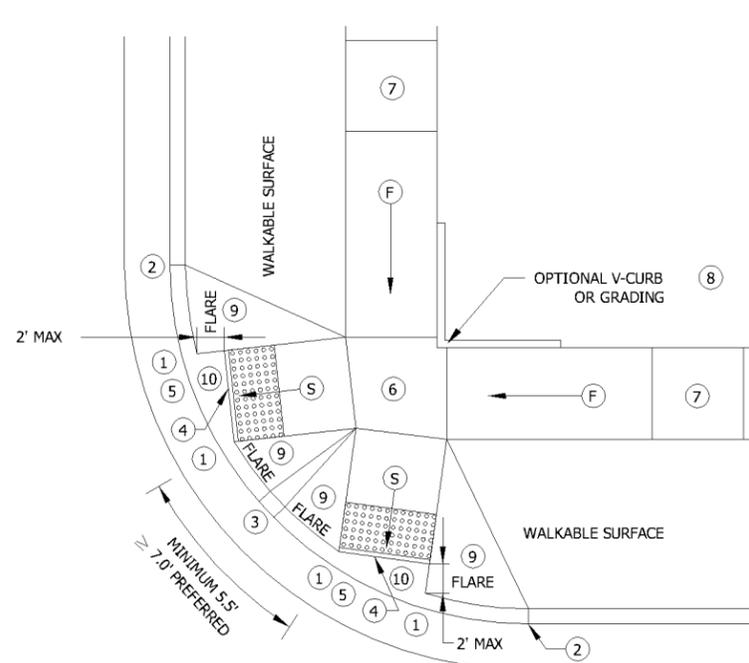
REVISION:  
APPROVED: 8-6-2014  
*[Signature]*  
OPERATIONS ENGINEER

REVISOR:  
APPROVED: 8-6-2014  
*[Signature]*  
STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS  
SP 141-220-005  
SHEET 24 OF 88

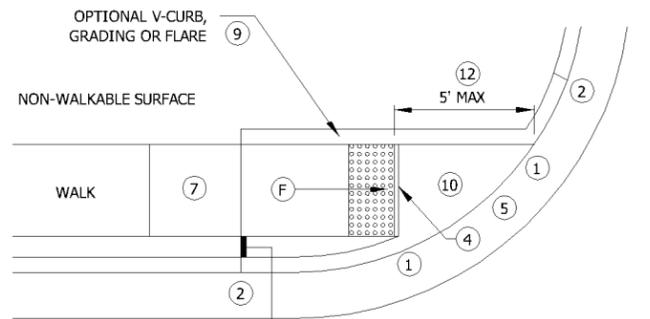


ADJACENT TO NON-WALKABLE SURFACE



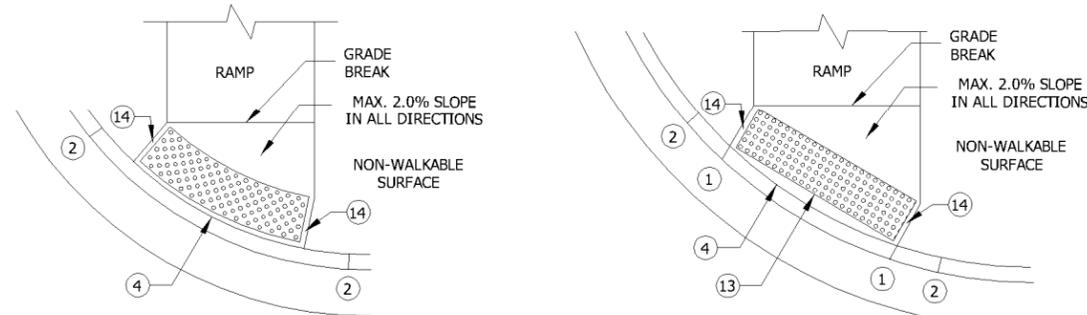
ADJACENT TO WALKABLE SURFACE

COMBINED DIRECTIONAL 15

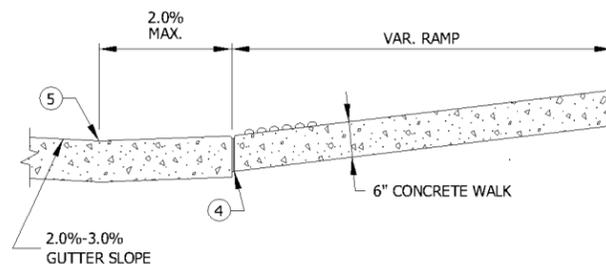
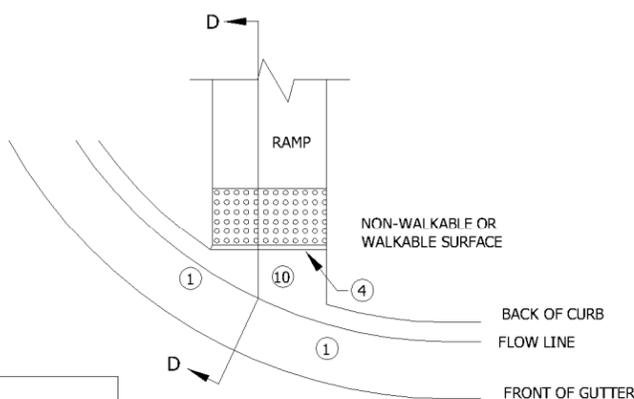


ONE-WAY DIRECTIONAL

IF NON-CONCRETE BLVD. IS CONSTRUCTED AND IS LESS THAN 2' IN WIDTH AT TOP OF CURB TRANSITION, PAVE CONCRETE RAMP WIDTH TO ADJACENT BACK OF CURB.



DETECTABLE WARNING PLACEMENT WHEN SETBACK CRITERIA IS EXCEEDED



SECTION D-D

NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.

TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS MAY BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 5 WHEN LANDINGS ARE CAST SEPARATELY.

ALL SLOPES ARE ABSOLUTE, RATHER THAN RELATIVE TO SIDEWALK/ROADWAY GRADES.

TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.

4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MINIMUM OF 24" IN THE PATH OF TRAVEL. SHARED USE PATHS SHALL HAVE DETECTABLE WARNING ACROSS THE ENTIRE WIDTH OF PATH WHEN THE PATH CROSSES A ROAD.

SEE STANDARD PLATE 7038 AND SHEET 4 OF 5 FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.

- 1 0" CURB HEIGHT.
- 2 FULL CURB HEIGHT.
- 3 3" MINIMUM CURB HEIGHT, 4" PREFERRED.
- 4 1/2" PREFORMED JOINT FILLER MATERIAL AASHTO M 213. JOINT FILLER SHALL BE PLACED FLUSH WITH THE BACK OF CURB AND ADJACENT SIDEWALK. JOINT SHALL BE FREE OF DEBRIS. RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MIN. TO 6" MAX. FROM THE BACK OF CURB.
- 5 SEE PEDESTRIAN ACCESS ROUTE CURB AND GUTTER DETAIL FOR INFORMATION ON CONSTRUCTING CURB AND GUTTER AT CURB OPENINGS. SEE SHEET NO. 3 OF 5.
- 6 4' BY 4' MIN. LANDING WITH MAX. 2.0% SLOPE IN ALL DIRECTIONS.
- 7 IF LONGITUDINAL SLOPE IS GREATER THAN 5.0%, 4' X 4' MIN. LANDING WITH MAX 2.0% SLOPE IN ALL DIRECTIONS REQUIRED.
- 8 V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.
- 9 SEE SHEET 4 OF 5, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
- 10 MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTEGRAL WITH CURB AND GUTTER.
- 11 TO BE USED FOR ALL DIRECTIONAL RAMPS.
- 12 PLACE DOMES AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
- 13 RECTANGULAR DETECTABLE WARNINGS MAY BE SETBACK 9" FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
- 14 WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- 15 FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2' MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE, AND 5' MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3" FROM BACK OF CURB. WHETHER A SURFACE IS WALKABLE OR NOT SHALL BE DETERMINED BY THE ENGINEER

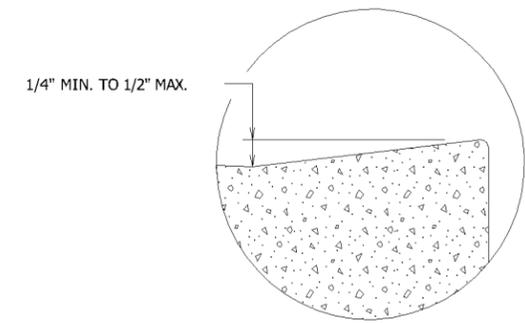
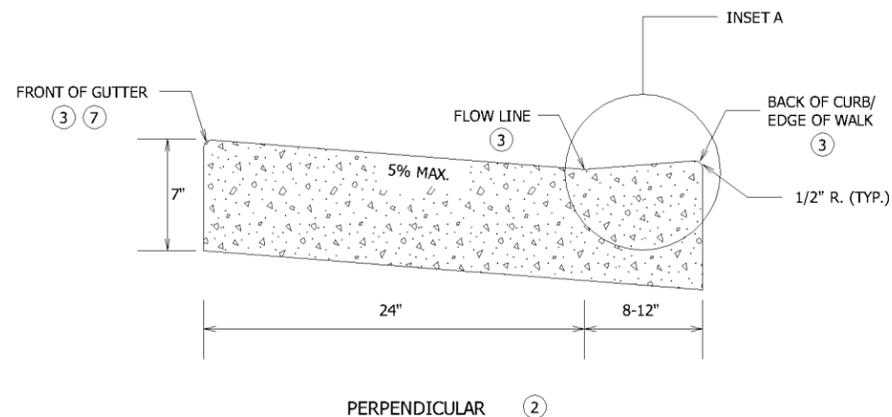
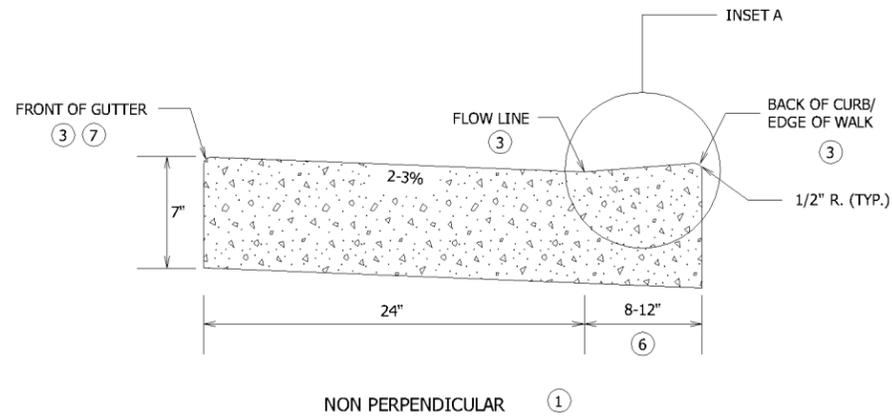
LEGEND

THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.

- S INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%
- F INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

REVISION:  
APPROVED: 8-6-2014  
*[Signature]*  
OPERATIONS ENGINEER

REVISOR:  
APPROVED: 8-6-2014  
*[Signature]*  
STATE DESIGN ENGINEER

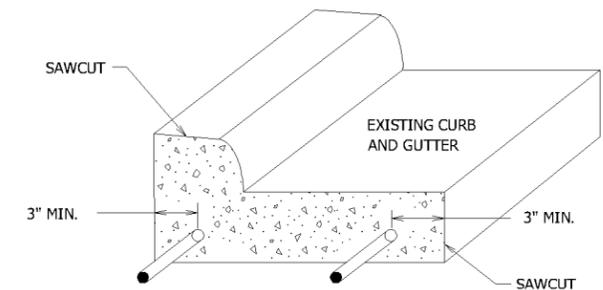
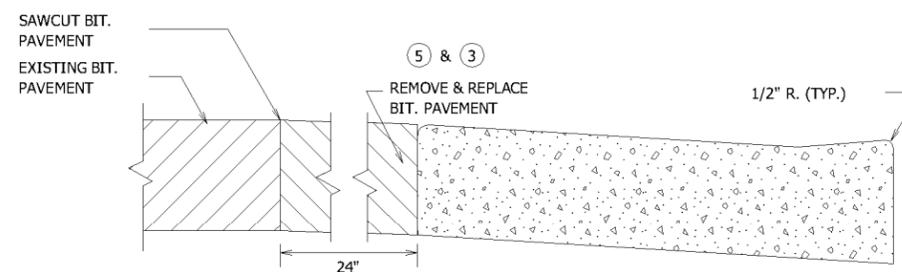
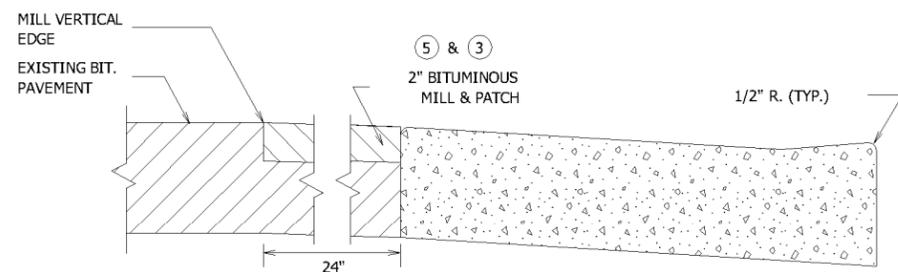


NON PERPENDICULAR ①

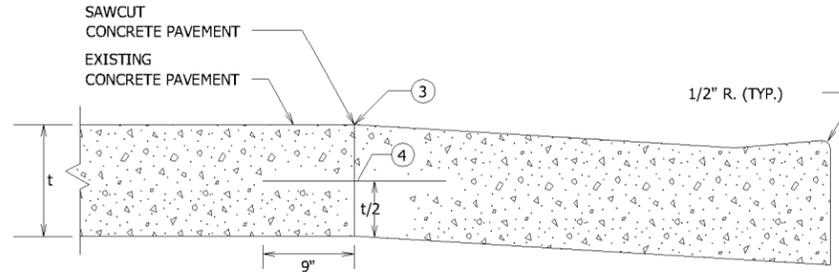
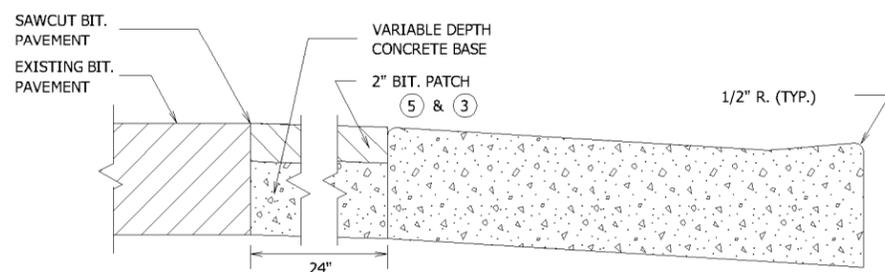
PERPENDICULAR ②

INSET A

PEDESTRIAN ACCESS ROUTE CURB & GUTTER DETAIL



CURB AND GUTTER REINFORCEMENT ⑧  
FOR USE ON CURB RAMP RETROFITS



PAVEMENT TREATMENT OPTIONS IN FRONT OF CURB & GUTTER  
FOR USE ON CURB RAMP RETROFITS

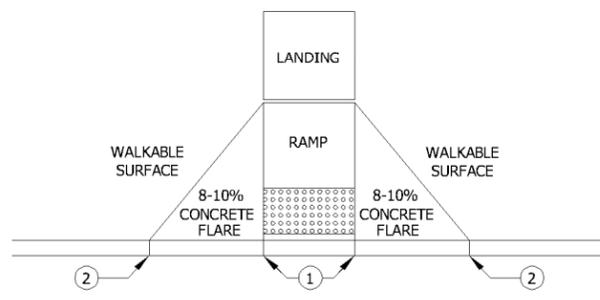
NOTES:

- POSITIVE FLOW LINE DRAINAGE SHALL BE MAINTAINED THROUGH THE PEDESTRIAN ACCESS ROUTE (PAR) AT A 2% MAXIMUM.
- NO PONDING SHALL BE PRESENT IN THE PAR.
- ANY VERTICAL LIP THAT OCCURS AT THE FLOW LINE SHALL NOT BE GREATER THAN 1/4 INCH.
- ① FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED NON PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: FANS, DEPRESSED CORNERS, & ONE WAY AND COMBINED DIRECTIONALS.
- ② FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: PERPENDICULAR, TIERED PERPENDICULAR, PARALLEL, AND DIAGONAL RAMP.
- ③ THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4".
- ④ DRILL AND GROUT NO. 4 EPOXY-COATED 18" LONG TIE BARS AT 30" CENTER TO CENTER INTO EXISTING CONCRETE PAVEMENT.
- ⑤ ELEVATION CHANGE TAKES PLACE FROM THE EXISTING TO NEW FRONT OF GUTTER. PATCH IS USED TO MATCH THE NEW GUTTER FACE INTO THE EXISTING ROADWAY.
- ⑥ VARIABLE WIDTH FOR DIRECTIONAL CURB APPLICATIONS.
- ⑦ TOP FRONT OF GUTTER SHALL BE CONSTRUCTED FLUSH WITH PROPOSED ADJACENT PAVEMENT ELEVATION. PAR GUTTER SHALL NOT BE OVERLAID.
- ⑧ WHERE PLAN SPECIFIES, DRILL AND GROUT 2 - NO. 4 X 12" LONG REINFORCEMENT BARS (EPOXY COATED).

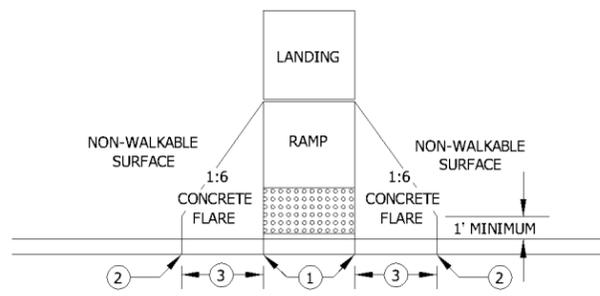
REVISION:  
APPROVED: 8-6-2014  
*[Signature]*  
OPERATIONS ENGINEER

REVISOR:  
APPROVED: 8-6-2014  
*[Signature]*  
STATE DESIGN ENGINEER

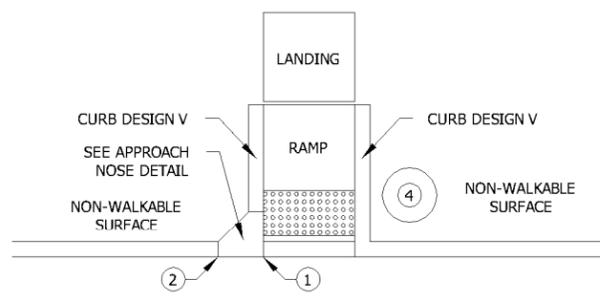
PEDESTRIAN CURB RAMP DETAILS  
SP 141-220-005 SHEET 26 OF 88



PAVED FLARES  
ADJACENT TO WALKABLE SURFACE

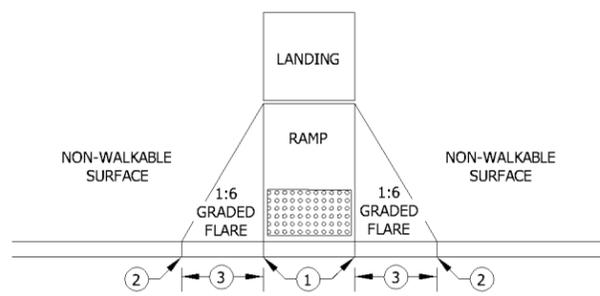


PAVED FLARES  
ADJACENT TO NON-WALKABLE SURFACE



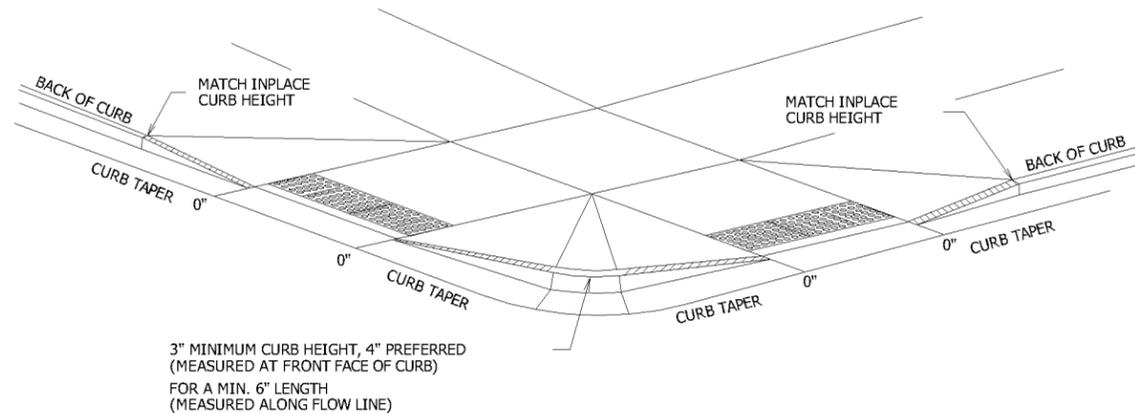
DIRECTION OF TRAFFIC

RETURNED CURB



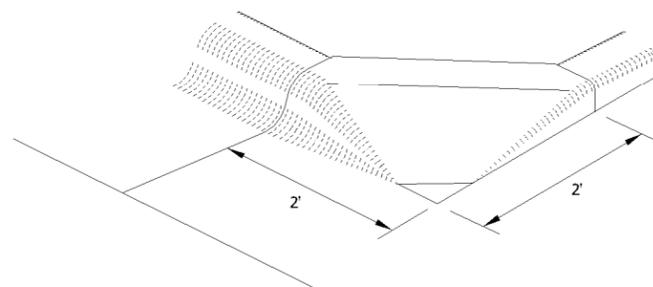
GRADED FLARES

TYPICAL SIDE TREATMENT OPTIONS ⑤



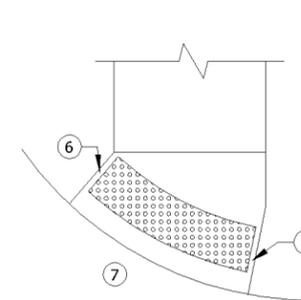
3" MINIMUM CURB HEIGHT, 4" PREFERRED  
(MEASURED AT FRONT FACE OF CURB)  
FOR A MIN. 6' LENGTH  
(MEASURED ALONG FLOW LINE)

DETECTABLE EDGE WITH  
CURB AND GUTTER ⑧

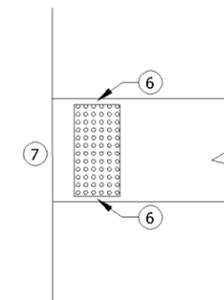


SECTION A-A

APPROACH NOSE DETAIL  
FOR DOWNSTREAM SIDE OF TRAFFIC



RADIAL DETECTABLE WARNING



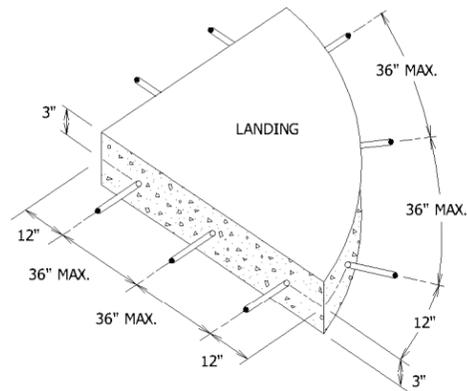
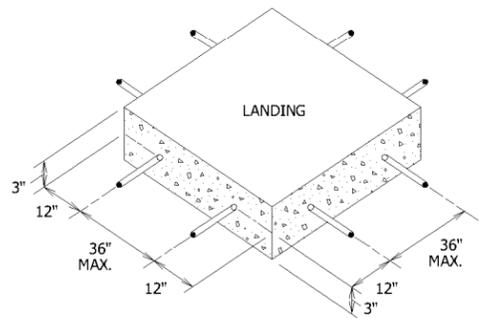
RECTANGULAR DETECTABLE WARNING

DETECTABLE EDGE WITHOUT CURB AND GUTTER

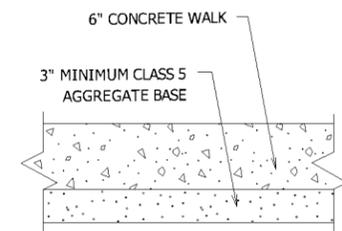
NOTES:

SEE STANDARD PLATE 7038 AND THIS SHEET FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.  
WHETHER A SURFACE IS WALKABLE OR NOT SHALL BE DETERMINED BY THE ENGINEER.  
CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8' LONG  
MEASURED ALONG THE RAMP FROM THE BACK OF CURB.

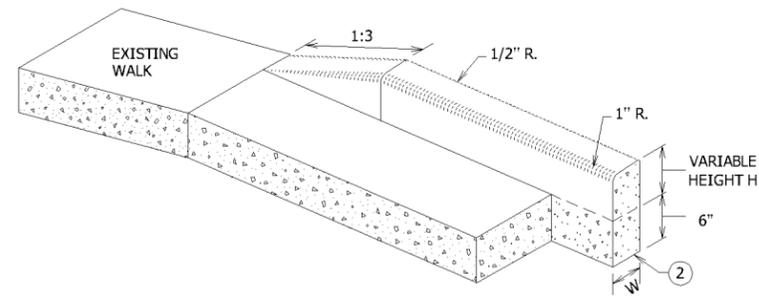
- ① 0" CURB HEIGHT.
- ② FULL CURB HEIGHT.
- ③ 2' - 3' FLARE.
- ④ IMMOVABLE OBJECT OR OBSTRUCTION.
- ⑤ SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED ON ALL RAMP TYPES AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS.
- ⑥ WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑦ IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED 1' FROM THE EDGE OF ROADWAY TO PROVIDE VISUAL CONTRAST.
- ⑧ ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE EDGE REQUIRES DETECTABLE WARNINGS WHEREVER THERE IS ZERO-INCH HIGH CURB. CURB TAPERS ARE CONSIDERED A DETECTABLE EDGE WHEN THE TAPER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARNINGS AND UNIFORMLY RISES TO A 3-INCH MINIMUM CURB HEIGHT. ANY CURB NOT PART OF A CURB TAPER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.



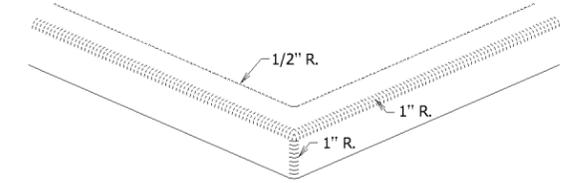
SIDEWALK REINFORCEMENT (6) (7)



TYPICAL SIDEWALK SECTION WITHIN INTERSECTION CORNER

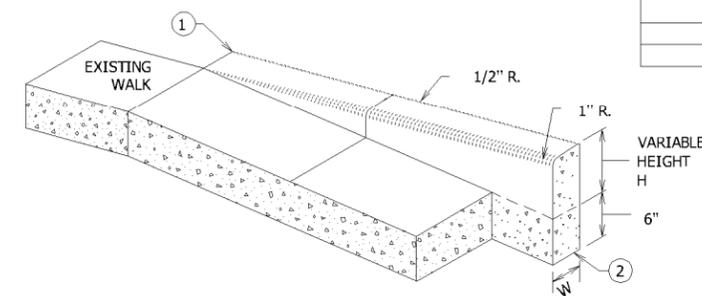


V CURB ADJACENT TO LANDSCAPE  
CURB WITHIN SIDEWALK LIMITS

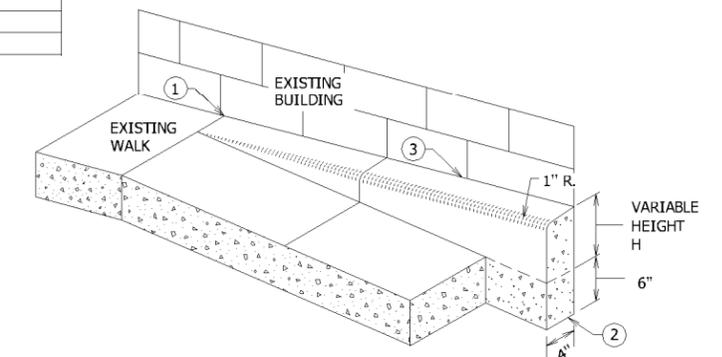


V CURB INTERSECTION

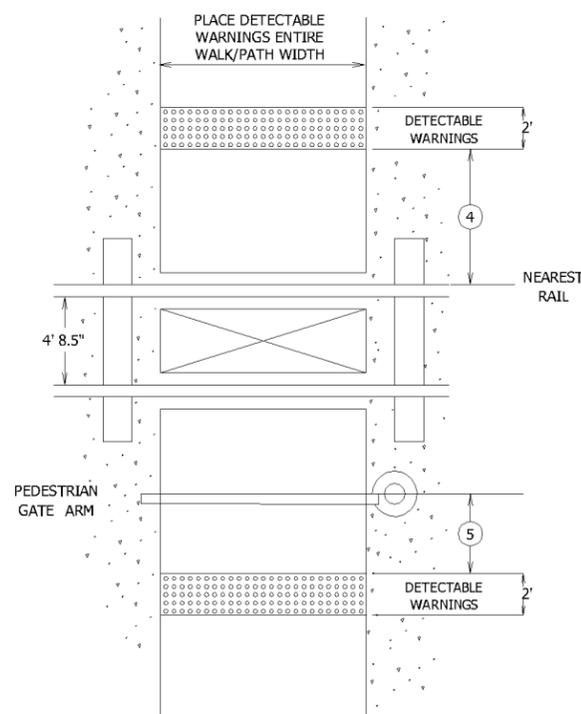
CONCRETE CURB DESIGN V	
CURB HEIGHT H	CURB WIDTH W
< 6"	4"
≥ 6"	6"



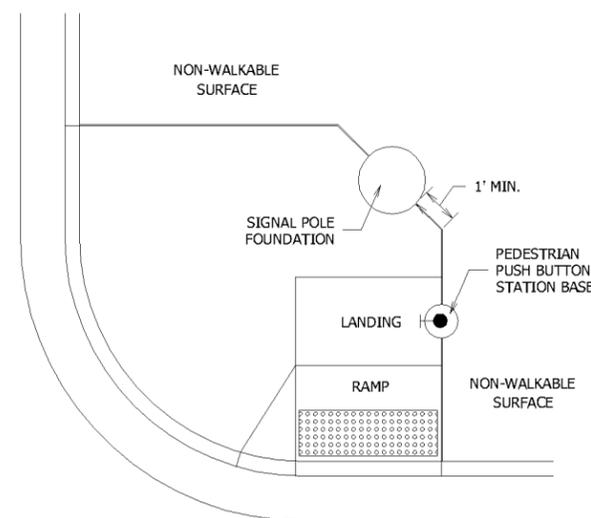
V CURB ADJACENT TO LANDSCAPE  
CURB OUTSIDE SIDEWALK LIMITS



V CURB ADJACENT TO BUILDING  
OR BARRIER



RAILROAD CROSSING  
PLAN VIEW



CONCRETE WALK EDGES ADJACENT  
TO CONCRETE STRUCTURES

NOTES:

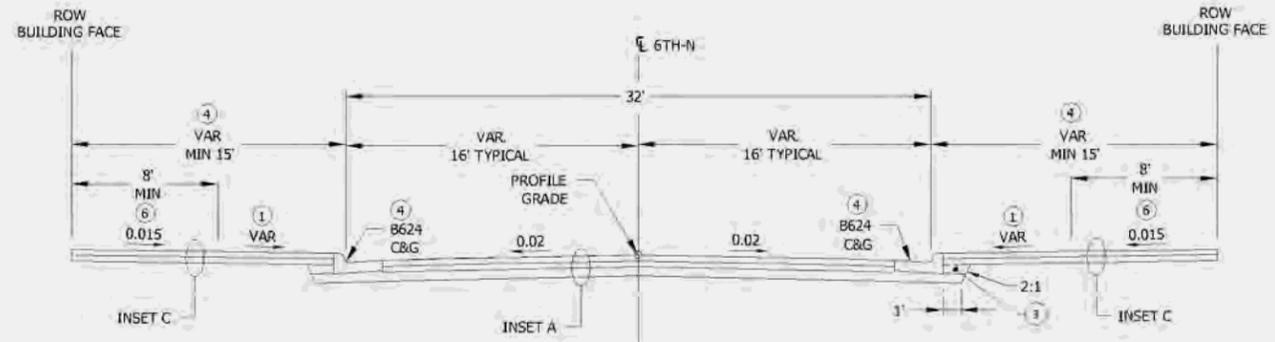
- ALL V CURB CONTRACTION JOINTS SHALL MATCH CONCRETE WALK JOINTS.
- WHERE RIGHT-OF-WAY ALLOWS, USE OF V CURB SHOULD BE MINIMIZED. GRADING ADJACENT TURF OR SLOPING ADJACENT PAVEMENT IS PREFERRED.
- V CURB SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.
- V CURB NEXT TO BUILDING SHALL BE A 4" WIDTH AND SHALL MATCH PREVIOUS TOP OF SIDEWALK ELEVATIONS.
- (1) END TAPERS AT TRANSITION SECTION SHALL MATCH INPLACE SIDEWALK GRADES.
- (2) ALL V CURB SHALL MATCH BOTTOM OF ADJACENT WALK.
- (3) EDGE BETWEEN NEW V CURB AND INPLACE STRUCTURE SHALL BE SEALED AND BOND BREAKER SHALL BE USED BETWEEN EXISTING STRUCTURE AND PLACED V-CURB.
- (4) Nearest EDGE OF DETECTABLE WARNING SURFACES SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST RAIL. for skewed railways IN NO INSTANCE SHALL the DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.
- (5) WHEN PEDESTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE APPROACHING SIDE OF THE GATE ARM.
- (6) WHEN PLAN SPECIFIES, DRILL AND GROUT NO. 4 12" LONG REINFORCEMENT BARS AT 36" MAX. CENTER TO CENTER (EPOXY COATED).
- (7) TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS MAY BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON THIS SHEET WHEN LANDINGS ARE CAST SEPARATELY.

REVISION:  
APPROVED: 8-6-2014  
*[Signature]*  
OPERATIONS ENGINEER

REVISOR:  
APPROVED: 2-9-2015  
*[Signature]*  
STATE DESIGN ENGINEER

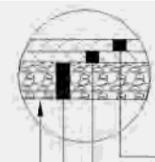
PEDESTRIAN CURB RAMP DETAILS  
SP 141-220-005  
SHEET 28 OF 88

THE INFORMATION ON THIS SHEET WAS PREPARED BY THE CONSULTING ENGINEER OR ARCHITECT. SCALE THE INFORMATION TO THE ORIGINAL DRAWING. THE CONSULTING ENGINEER OR ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION ON THIS SHEET. THE CONSULTING ENGINEER OR ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION ON THIS SHEET.



1 TYPICAL SECTION  
6TH AVENUE N

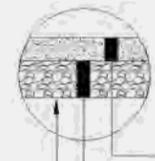
10+13.85 - 10+74.30



INSET A - BITUMINOUS PAVEMENT

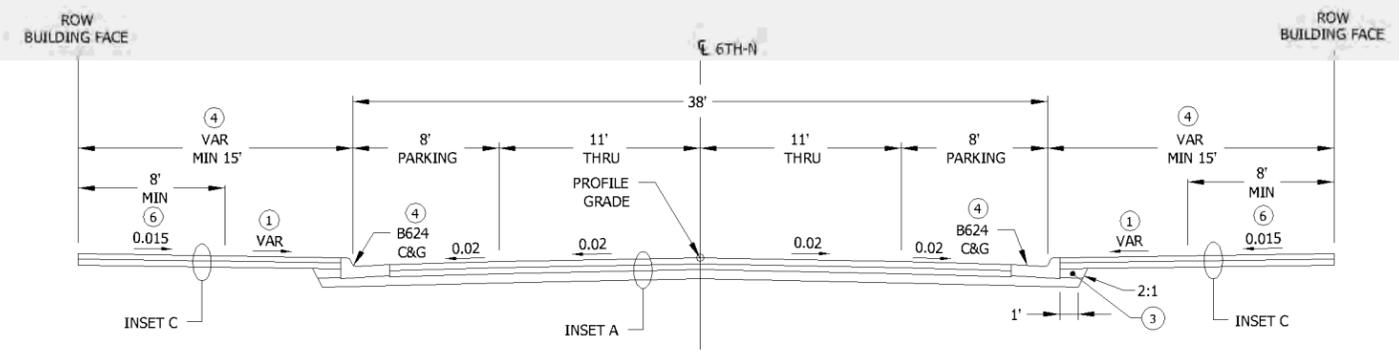
- 4.0" WEAR COURSE MIXTURE SPWEBS40L (PLACE IN TWO LIFTS) TYPE SP 12.5 WEARING COURSE MIXTURE (5,L) MNDOT SPEC 2360
- 4.0" NON WEAR COURSE MIXTURE SPIWB430L (PLACE IN TWO LIFTS) TYPE SP 12.5 NON-WEARING COURSE MIXTURE (4,L) MNDOT SPEC 2360
- 6.0" CLASS 5 AGGREGATE BASE MNDOT SPEC 2211
- GRADING GRADE

NOT TO SCALE



INSET C - CONCRETE WALK

- 3.5" CONCRETE WALK (2)
- 4.0" CLASS 5 AGGREGATE BASE MNDOT SPEC 2211
- GRADING GRADE



2 TYPICAL SECTION  
6TH AVENUE N

10+74.30 - 13+80.63

STREET CROSS SLOPE

STATION	LEFT LANE	RIGHT LANE
6TH AVENUE		
10+00	-2.00%	-2.00%
16+55	-2.00%	-2.00%
16+75	-3.00%	-1.50%
18+00	-3.00%	-1.50%
18+20	-2.00%	-2.00%
18+95	-2.00%	-2.00%
19+15	-3.00%	-1.50%
22+00	-3.00%	-1.50%
22+20	-2.00%	-2.00%
22+74	-2.00%	-2.00%

GENERAL NOTES:

1. ALL CROSS SLOPES ARE IN FT/FT.
2. UNLESS OTHERWISE SPECIFIED, THE GRADING GRADE CROSS SLOPES WILL BE THE SAME AS THE PROPOSED DRIVING SURFACE.
3. DRIVEWAY APRONS INSTEAD OF CURBS AT LOCATIONS SHOWN IN PLANS.

NOTES:

- ① CROSS SLOPES VARY. SEE CROSS SECTIONS.
- ② 6 INCH CONCRETE WALK REQUIRED ADJACENT DRIVEWAYS AND AT INTERSECTIONS. SEE CONSTRUCTION PLAN FOR LOCATIONS. SEE SHEETS 37-39.
- ③ BACKFILL WITH SUITABLE GRADING MATERIAL.
- ④ SEE CONSTRUCTION PLANS FOR DRIVEWAY LOCATIONS. DRIVEWAYS SHALL BE CONSTRUCTED PER MINNEAPOLIS STANDARD DETAILS.
- ⑤ CROSS SLOPE CAN VARY. SEE CROSS SLOPE CHART ON SHEET 29.
- ⑥ PEDESTRIAN ZONE. 1.5% SLOPE (1% MIN & 2% MAX)

Plot Date: 10/27/2016 9:45am  
 Program: 10/27/2016 9:45am  
 User: 10/27/2016 9:45am  
 File: 10/27/2016 9:45am  
 Path: C:\Users\tyler.j.mollete\AppData\Local\Temp\10/27/2016 9:45am\10/27/2016 9:45am.dwg  
 Plot: 10/27/2016 9:45am  
 Scale: 1:1  
 Sheet: 88 of 88

NO.	DATE	APPR.	REVISION

SURVEY	APPV
DESIGNED	TJM
CHECKED	CIVL
APPROVED	LPG
PROJ. NO.	1936288

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: TYLER J. MOLETTE  
 SIGNATURE: *[Signature]*  
 DATE: 8-4-15 LIC. NO. 51774

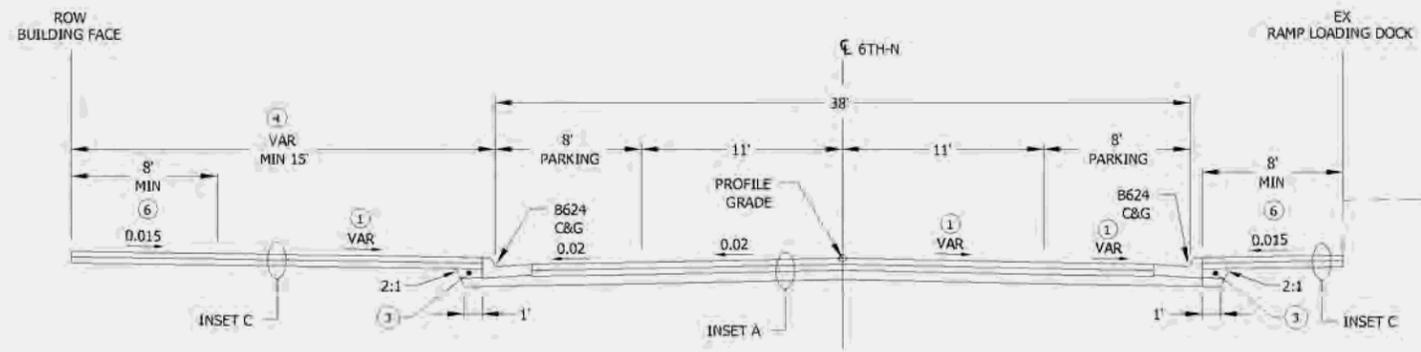
2335 Highway 56 W  
 St. Paul, MN 55113  
 www.stantec.com

TYPICAL SECTIONS

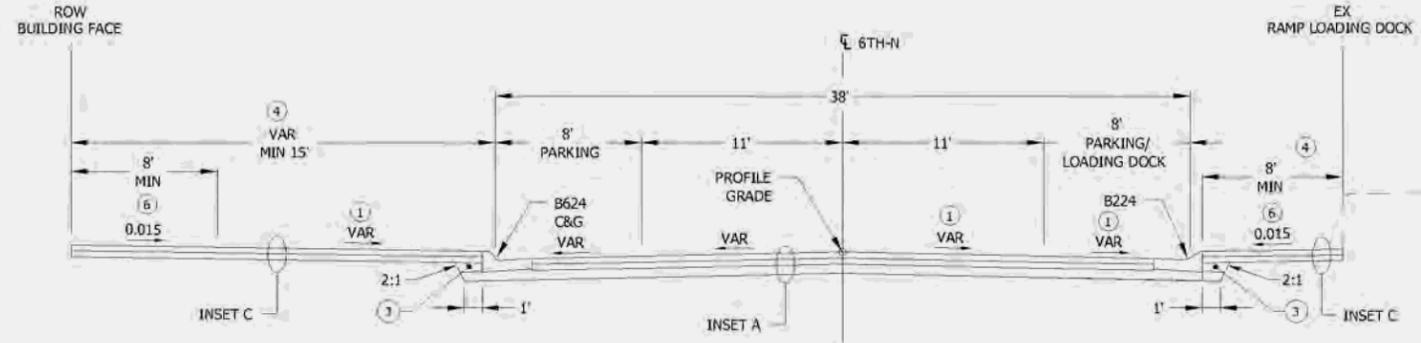
CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005



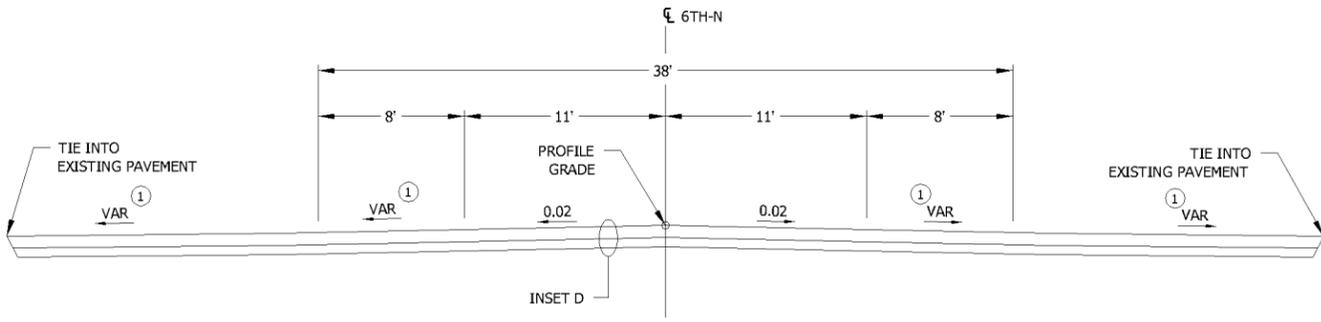
THE INFORMATION ON THIS SHEET WAS PREPARED BY THE CONSULTING ENGINEER OR ARCHITECT. THE CONSULTING ENGINEER OR ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION ON THIS SHEET. THE CONSULTING ENGINEER OR ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION ON THIS SHEET.



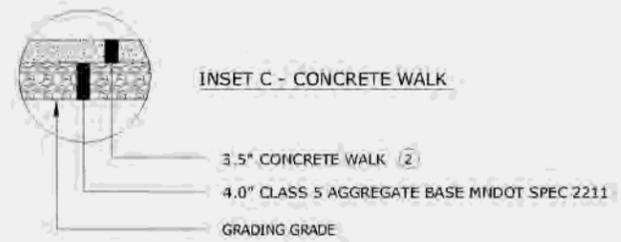
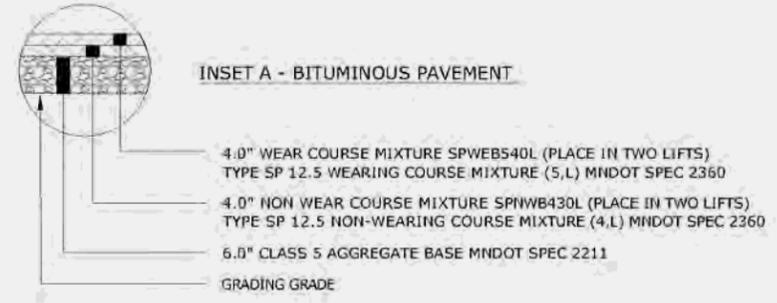
**6** TYPICAL SECTION  
 6TH AVENUE N  
 50+19.80 - 51+14.09  
 NOT TO SCALE



**7** TYPICAL SECTION  
 6TH AVENUE N  
 51+14.09 - 52+28.33  
 NOT TO SCALE



**8** TYPICAL SECTION  
 6TH AVENUE N  
 13+80.63 - 14+61.25  
 18+02.36 - 18+90.73  
 NOT TO SCALE



- GENERAL NOTES:**
- ALL CROSS SLOPES ARE IN FT/FT.
  - UNLESS OTHERWISE SPECIFIED, THE GRADING GRADE CROSS SLOPES WILL BE THE SAME AS THE PROPOSED DRIVING SURFACE.
  - DRIVEWAY APRONS INSTEAD OF CURBS AT LOCATIONS SHOWN IN PLANS.
- NOTES:**
- CROSS SLOPES VARY. SEE CROSS SECTIONS.
  - 6 INCH CONCRETE WALK REQUIRED ADJACENT DRIVEWAYS AND AT INTERSECTIONS. SEE CONSTRUCTION PLAN FOR LOCATIONS. SEE SHEETS 37-39.
  - BACKFILL WITH SUITABLE GRADING MATERIAL.
  - SEE CONSTRUCTION PLANS FOR DRIVEWAY LOCATIONS. DRIVEWAYS SHALL BE CONSTRUCTED PER MINNEAPOLIS STANDARD DETAILS.
  - CROSS SLOPE CAN VARY. SEE CROSS SLOPE CHART ON SHEET 29.
  - PEDESTRIAN ZONE. 1.5% SLOPE (1% MIN & 2% MAX)

Plot Date: 10/27/2016 9:45am  
 Program: AutoCAD 2016  
 Plotter: HP DesignJet T1100e  
 Plot Style: AutoCAD.ctb  
 Scale: 1/8" = 1'-0"  
 Sheet: 31 of 88

NO.	DATE	APPR.	REVISION

SURVEY	MPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
DRAWN	TJA	
DESIGNED	TJM	
CHECKED	CIVL	
APPROVED	ILPG	
PROJECT NO.	19362887	DATE: 8-4-15
		LIC. NO. 51774

Minneapolis  
City of Lakes

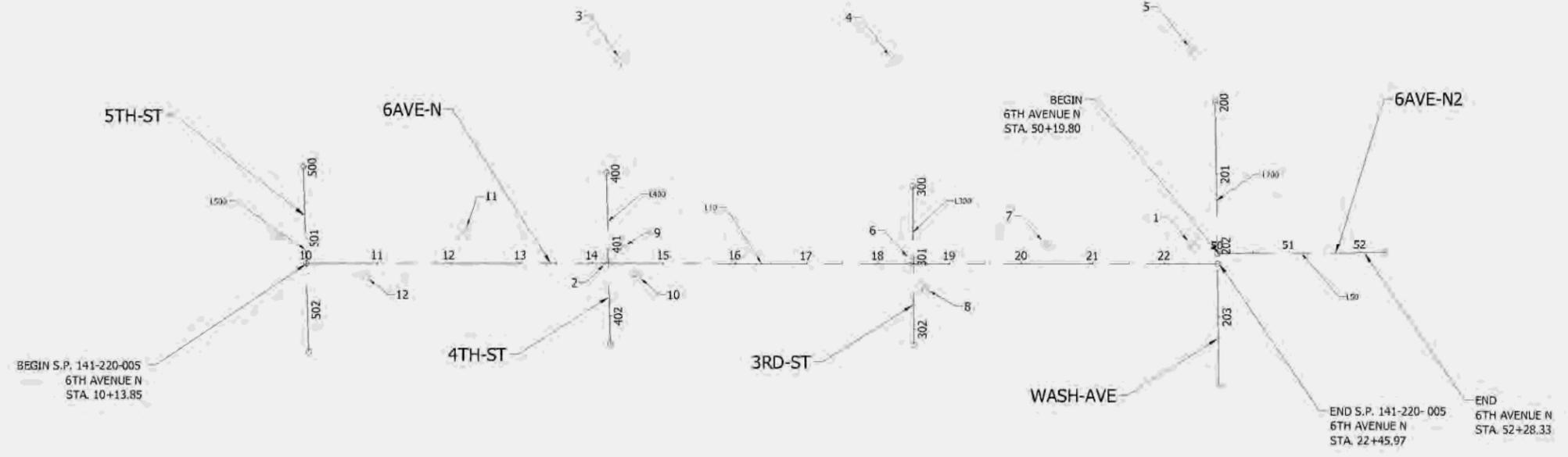
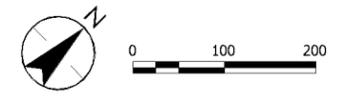
2335 Highway 36 W  
St. Paul, MN 55113  
www.stantec.com

TYPICAL SECTIONS

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE DESIGNER SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE PRODUCTION. THE DESIGNER SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE PRODUCTION. THE DESIGNER SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE PRODUCTION.

6TH AVENUE CONTROL POINTS				
1	171396.222	527771.551	826.866	GPSP 1 WASH
2	170801.505	527209.044	829.811	GPSP 2
3	171013.776	527026.545	828.944	GPSP 3
4	171282.567	527293.795	821.943	GPSP 4
5	171587.823	527580.451	823.786	GPSP 5 WASH
6	171101.992	527510.286	823.640	GPSP 6
7	171252.135	527627.315	825.016	GPSP 7 SMACK
8	171088.541	527547.525	823.096	GPSP 8 FALK
9	170829.916	527209.821	829.554	GPSP 9 STACKS
10	170816.898	527250.240	829.281	GPSP 10
11	170692.009	527038.148	823.736	GPSP 11 BEER
12	170547.899	526981.232	818.309	GPSP 12 SHAPCO



**STATION EQUATION**  
 6TH AVE N STA. 22+74.18 =  
 6TH AVE N2 STA 50+00

6AVE-N													
SEGMENT	BEGIN STATION	END STATION	PI STATION	DELTA	DEGREE	RADIUS	TANGENT	LENGTH	BEGIN (NORTH,EAST)	END (NORTH,EAST)	PI (NORTH, EAST)	BEGIN DIRECTION	END DIRECTION
L10	10+00	22+74.18							170501.98, 526913.02	171401.67, 527815.29		N45°04'55"E	

4TH-ST													
SEGMENT	BEGIN STATION	END STATION	PI STATION	DELTA	DEGREE	RADIUS	TANGENT	LENGTH	BEGIN (NORTH,EAST)	END (NORTH,EAST)	PI (NORTH, EAST)	BEGIN DIRECTION	END DIRECTION
L400	400+00	402+39.80							170888.70, 527120.31	170722.53, 527293.21		S46°08'10"E	

6AVE-N2													
SEGMENT	BEGIN STATION	END STATION	PI STATION	DELTA	DEGREE	RADIUS	TANGENT	LENGTH	BEGIN (NORTH,EAST)	END (NORTH,EAST)	PI (NORTH, EAST)	BEGIN DIRECTION	END DIRECTION
L50	50+00	52+35							171411.79, 527804.93	171579.03, 527970.03		N44°37'52"E	

3RD-ST													
SEGMENT	BEGIN STATION	END STATION	PI STATION	DELTA	DEGREE	RADIUS	TANGENT	LENGTH	BEGIN (NORTH,EAST)	END (NORTH,EAST)	PI (NORTH, EAST)	BEGIN DIRECTION	END DIRECTION
L300	300+00	302+21.99							171178.20, 527436.39	171022.25, 527594.37		S45°22'14"E	

5TH-ST													
SEGMENT	BEGIN STATION	END STATION	PI STATION	DELTA	DEGREE	RADIUS	TANGENT	LENGTH	BEGIN (NORTH,EAST)	END (NORTH,EAST)	PI (NORTH, EAST)	BEGIN DIRECTION	END DIRECTION
L500	500+00	502+59.12							170595.51, 526814.26	170417.34, 527002.40		S46°33'30"E	

WASH-AVE													
SEGMENT	BEGIN STATION	END STATION	PI STATION	DELTA	DEGREE	RADIUS	TANGENT	LENGTH	BEGIN (NORTH,EAST)	END (NORTH,EAST)	PI (NORTH, EAST)	BEGIN DIRECTION	END DIRECTION
L200	200+00	203+95.28							171559.93, 527653.22	171283.77, 527936.04		S45°41'00"E	

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 User: 10/27/2016 9:46am

NO.	DATE	APPR.	REVISION

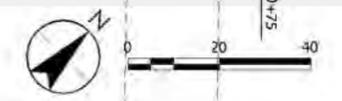
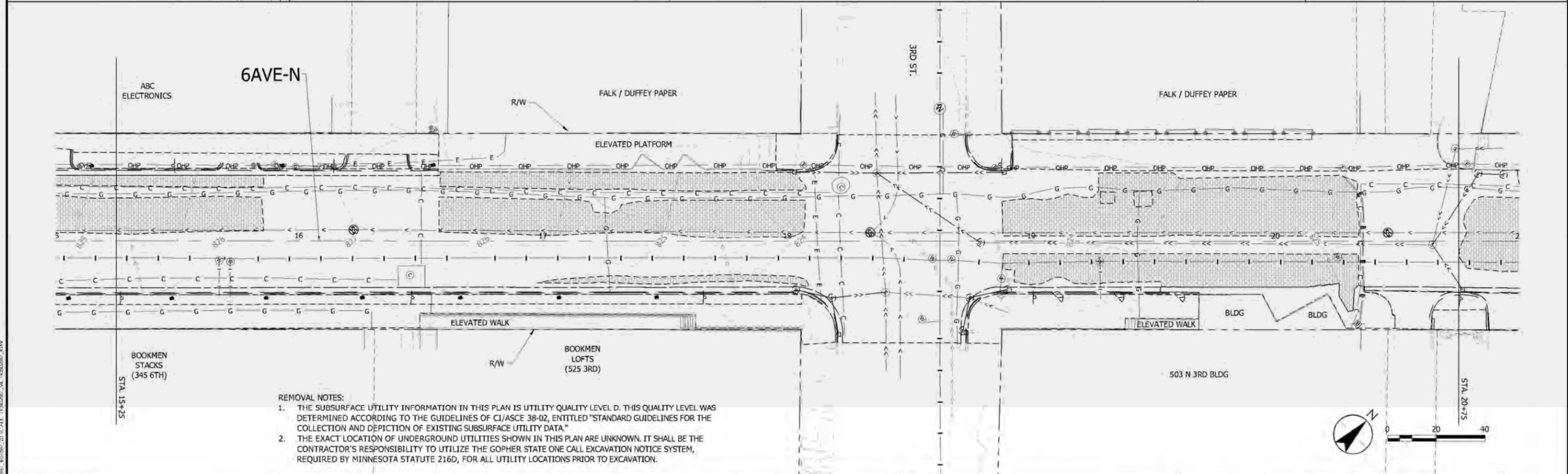
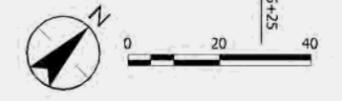
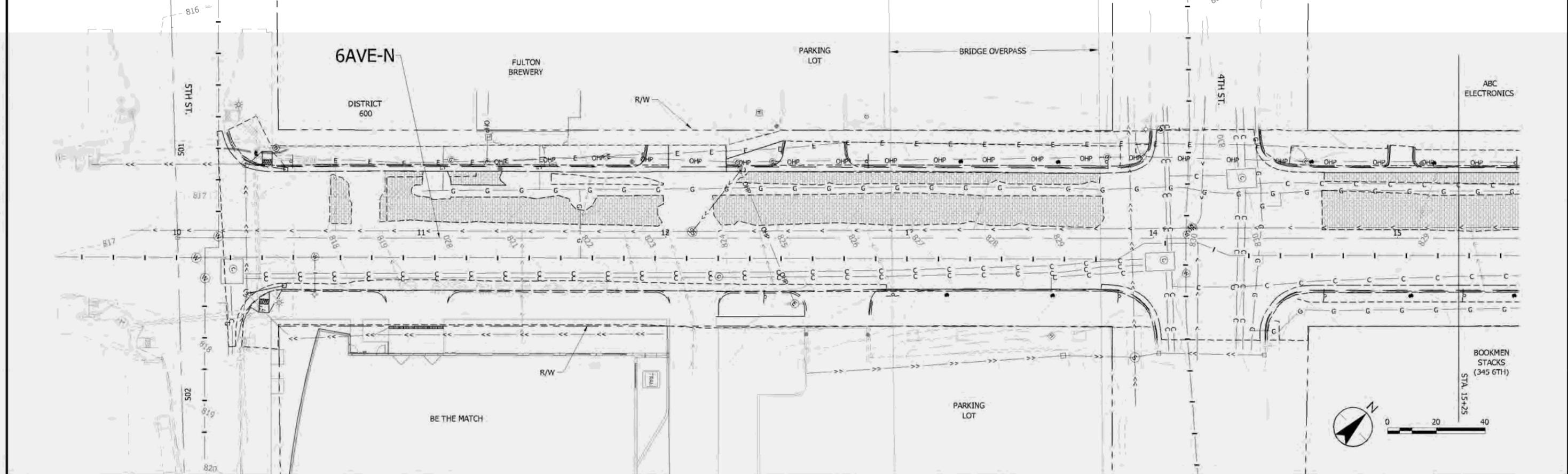
SURVEY	MPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: JEFFREY DANESON SIGNATURE: <i>[Signature]</i> DATE: 8/2/16 LIC. NO. 41964
DESIGNED	JDP	
CHECKED	CWL	
APPROVED	JLP	
PROJECT NO.	19260289	

2335 Highway 36 W  
St. Paul, MN 55113  
www.stantec.com

ALIGNMENT PLAN

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN.



- REMOVAL NOTES:**
1. THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."
  2. THE EXACT LOCATION OF UNDERGROUND UTILITIES SHOWN IN THIS PLAN ARE UNKNOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO UTILIZE THE GOPHER STATE ONE CALL EXCAVATION NOTICE SYSTEM, REQUIRED BY MINNESOTA STATUTE 216D, FOR ALL UTILITY LOCATIONS PRIOR TO EXCAVATION.

NO.	DATE	APPR.	REVISION

SURVEY	MPV	DESIGNED	TJM
DRAWN	TJM	CHECKED	CVL
APPROVED	EPG	DATE	8-4-15
PROJECT NO.	19360288	LIC. NO.	51774

MINNEAPOLIS City of Lakes

**Stantec**  
2335 Highway 56 W  
St. Paul, MN 55113  
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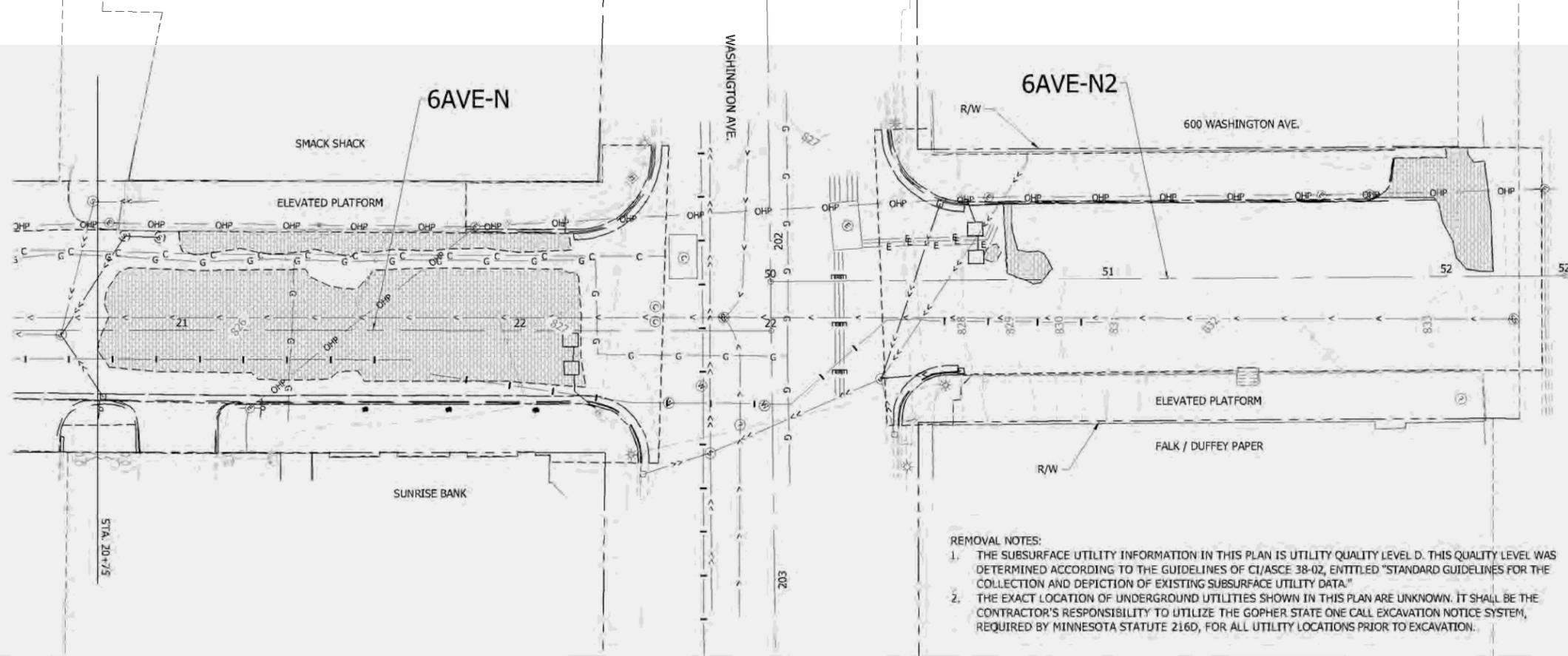
INPLACE TOPOGRAPHY AND INPLACE UTILITY PLAN

CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005

SHEET  
33  
88

Plot Date: 10/27/2015 9:45am  
Drawing Path: \\s01\proj\141220\SP141-220-005\Drawings\SP141-220-005.dwg  
User: jmorris  
Printer: 19360288.dwg

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN.



**REMOVAL NOTES:**

1. THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."
2. THE EXACT LOCATION OF UNDERGROUND UTILITIES SHOWN IN THIS PLAN ARE UNKNOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO UTILIZE THE GOPHER STATE ONE CALL EXCAVATION NOTICE SYSTEM, REQUIRED BY MINNESOTA STATUTE 216D, FOR ALL UTILITY LOCATIONS PRIOR TO EXCAVATION.



Proj. Date: 10/27/2016 9:49am  
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 User: T:\1352\1352.ctb  
 Plot Date: 10/27/2016 9:49am  
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 Plot Path: C:\Users\TJL\AppData\Local\Temp\1352.ctb

NO	DATE	APPR	REVISION

SURVEY:	W/PV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: TYLER J. MCLEETE SIGNATURE: <i>[Signature]</i> DATE: 8-4-15 LIC. NO.: SL724
DRAWN:	TJM	
DESIGNED:	TJM	
CHECKED:	CIVL	
APPROVED:	WPG	
PROJ. NO.:	13520298	

Minneapolis  
City of Lakes

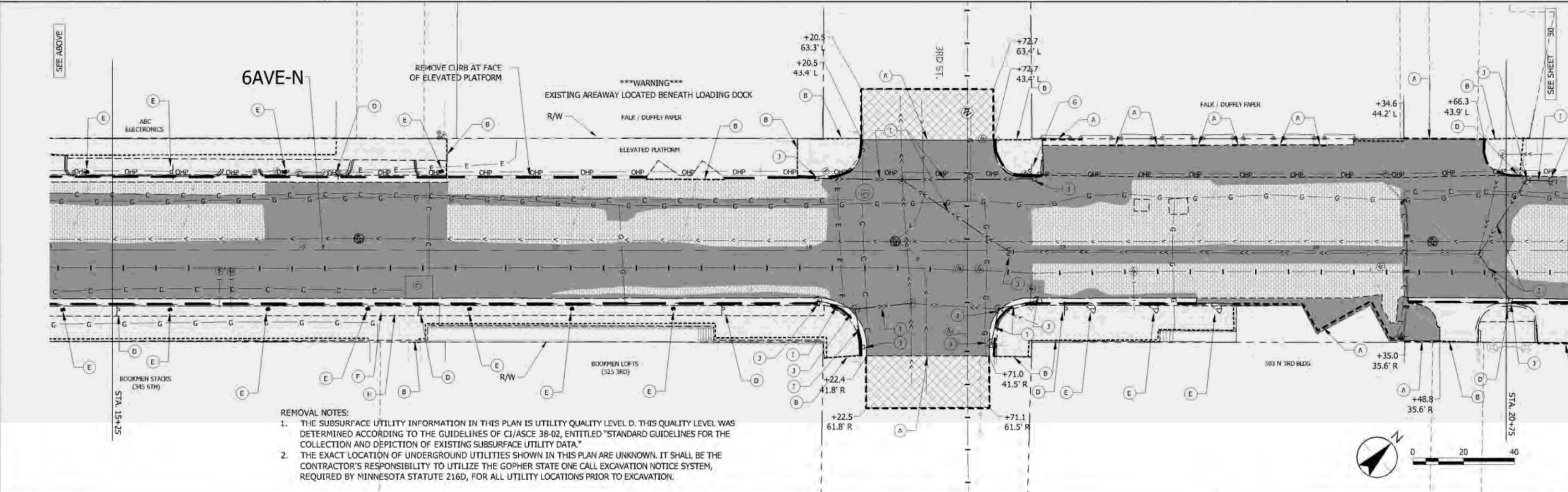
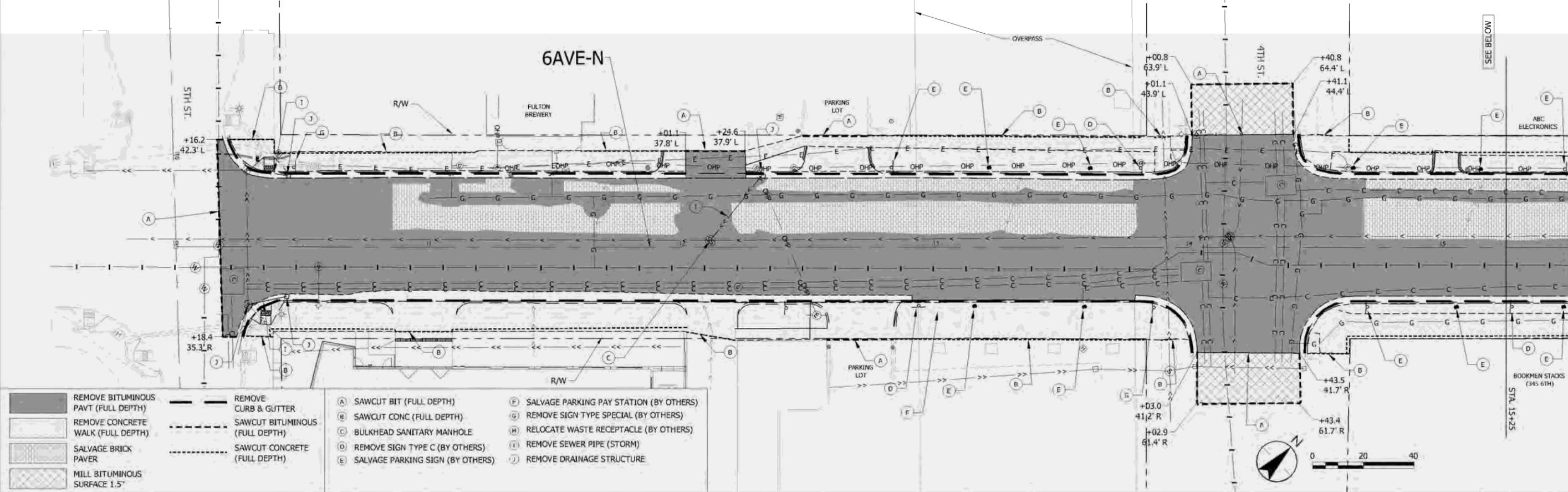
2335 Highway 36 W  
St. Paul, MN 55113  
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INPLACE TOPOGRAPHY AND INPLACE UTILITY PLAN

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET	34
TOTAL SHEETS	88

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN.



NO.	DATE	APPR.	REVISION

SURVEY	MPV	DESIGNED	TJM
DRAWN	TJM	CHECKED	CIVIL
APPROVED	EPG	DATE	8-4-15
PROJ. NO.	19362887	LIC. NO.	51774

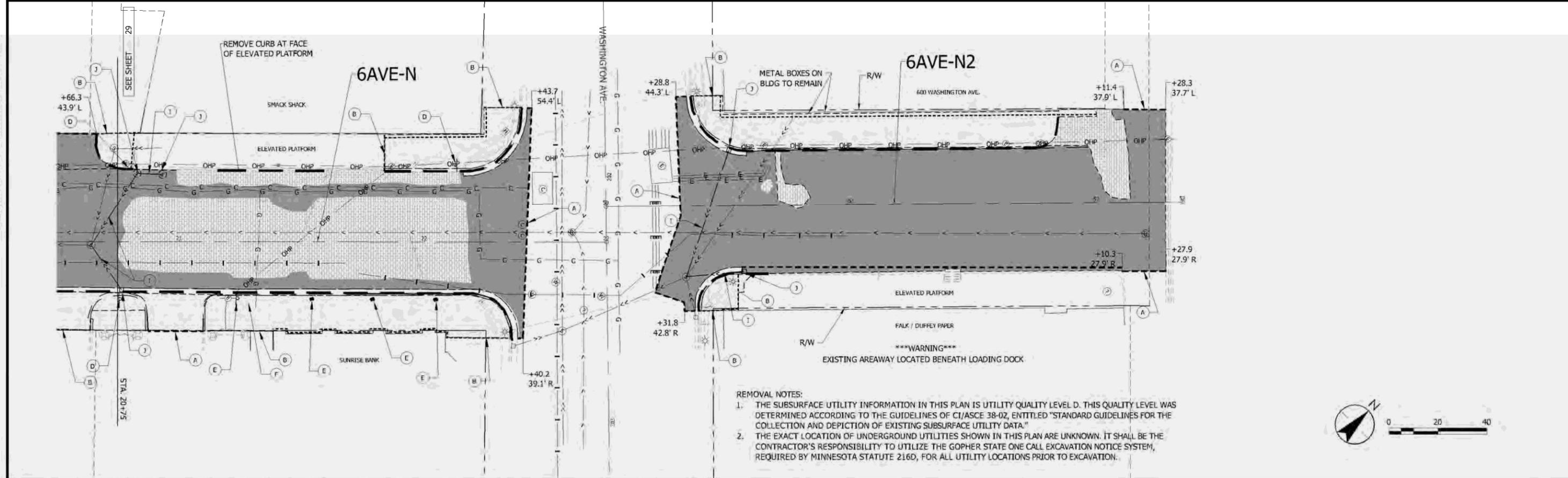
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: TYLER J. MOLETTE  
 SIGNATURE: *[Signature]*  
 DATE: 8-4-15 LIC. NO.: 51774



MINNEAPOLIS City of Lakes  
 REMOVAL PLAN

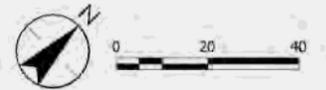
CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR THE ACCURACY OF THE SCALE. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR THE ACCURACY OF THE SCALE. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR THE ACCURACY OF THE SCALE.



**REMOVAL NOTES:**

1. THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."
2. THE EXACT LOCATION OF UNDERGROUND UTILITIES SHOWN IN THIS PLAN ARE UNKNOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO UTILIZE THE GOPHER STATE ONE CALL EXCAVATION NOTICE SYSTEM, REQUIRED BY MINNESOTA STATUTE 216D, FOR ALL UTILITY LOCATIONS PRIOR TO EXCAVATION.



	REMOVE BITUMINOUS PAVT (FULL DEPTH)		REMOVE CURB & GUTTER	(A)	SAWCUT BIT (FULL DEPTH)	(F)	SALVAGE PARKING PAY STATION (BY OTHERS)
	REMOVE CONCRETE WALK (FULL DEPTH)		SAWCUT BITUMINOUS (FULL DEPTH)	(B)	SAWCUT CONC (FULL DEPTH)	(G)	REMOVE SIGN TYPE SPECIAL (BY OTHERS)
	SALVAGE BRICK PAVER		SAWCUT CONCRETE (FULL DEPTH)	(C)	BULKHEAD SANITARY MANHOLE	(H)	RELOCATE WASTE RECEPTACLE (BY OTHERS)
	MILL BITUMINOUS SURFACE 1.5"			(D)	REMOVE SIGN TYPE C (BY OTHERS)	(I)	REMOVE SEWER PIPE (STORM)
				(E)	SALVAGE PARKING SIGN (BY OTHERS)	(J)	REMOVE DRAINAGE STRUCTURE

Proj. Date: 10/27/2016 9:52am  
 Drawing Name: V:\1352\1352.ctb  
 User: j...  
 Plot Date: 10/27/2016 9:52am  
 Plot Name: V:\1352\1352.ctb  
 Plot User: j...  
 Plot Date: 10/27/2016 9:52am  
 Plot Name: V:\1352\1352.ctb  
 Plot User: j...

NO	DATE	APPR	REVISION

SURVED:	WPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  PRINT NAME: TYLER J. MCLEETE SIGNATURE: DATE: 8-4-15 LIC. NO.: SL724
DRAWN:	TJM	
DESIGNED:	TJM	
CHECKED:	CIVL	
APPROVED:	WKG	
PROJ. NO.:	13520298	

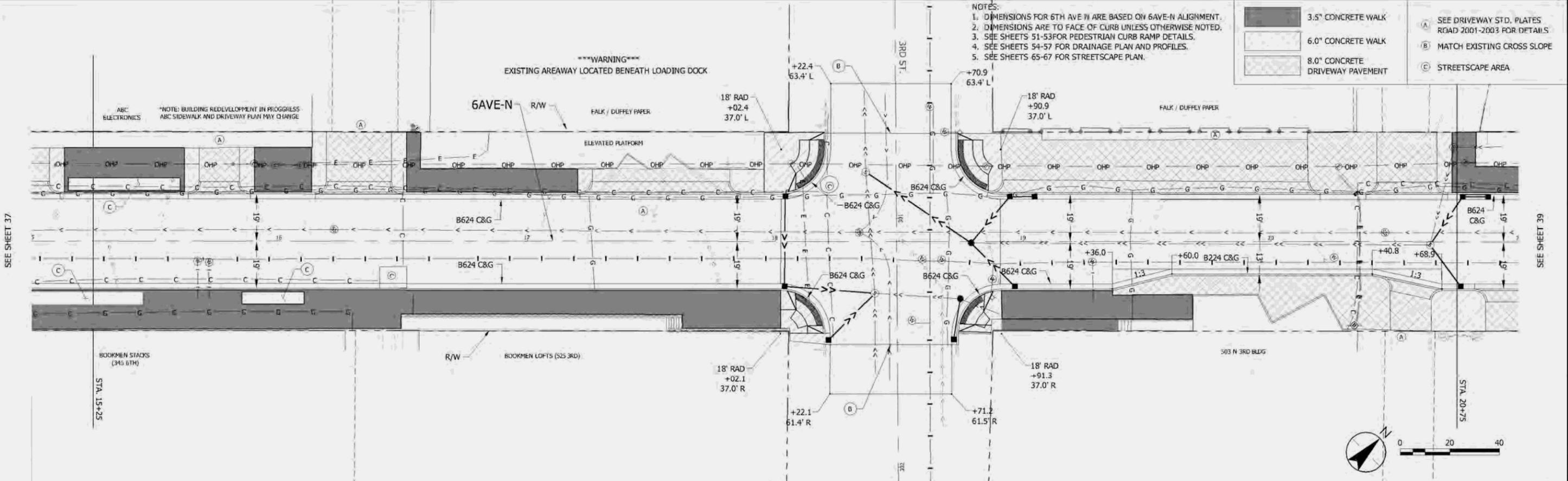
2335 Highway 36 W  
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REMOVAL PLAN

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

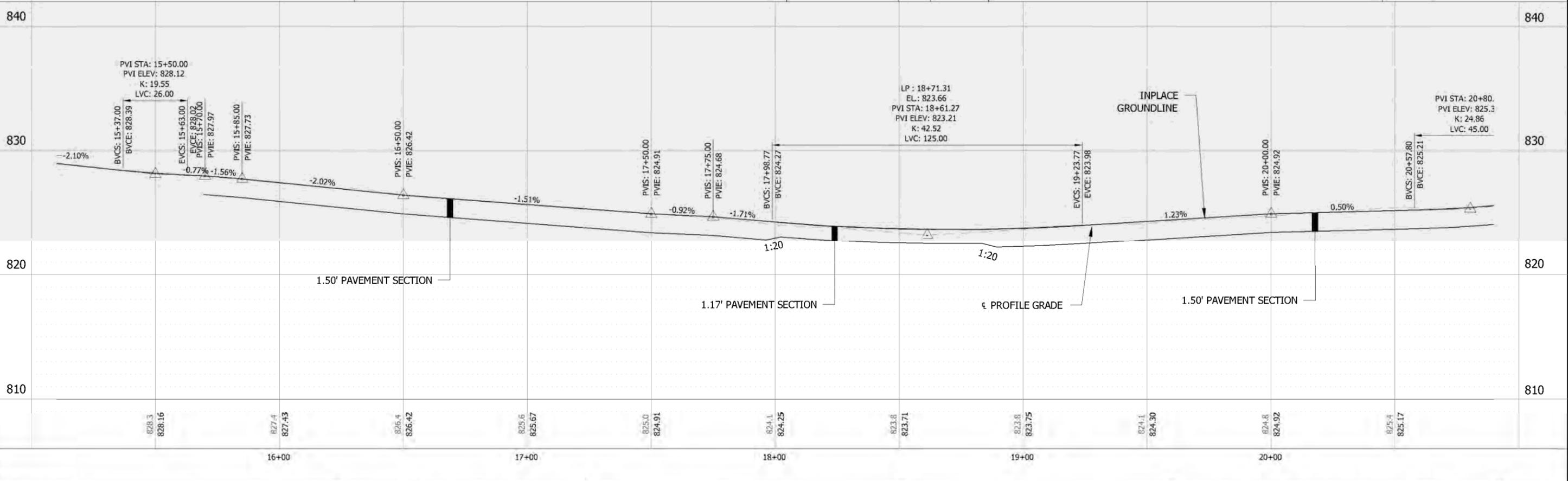


THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS OF THE EXISTING AND PROPOSED CURBS AND DRIVEWAYS. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS OF THE EXISTING AND PROPOSED CURBS AND DRIVEWAYS. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS OF THE EXISTING AND PROPOSED CURBS AND DRIVEWAYS.



- NOTES:**
1. DIMENSIONS FOR 6TH AVE N ARE BASED ON 6AVE-N ALIGNMENT.
  2. DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  3. SEE SHEETS 51-53 FOR PEDESTRIAN CURB RAMP DETAILS.
  4. SEE SHEETS 54-57 FOR DRAINAGE PLAN AND PROFILES.
  5. SEE SHEETS 65-67 FOR STREETScape PLAN.

	3.5" CONCRETE WALK	(A)	SEE DRIVEWAY STD. PLATES ROAD 2001-2003 FOR DETAILS.
	6.0" CONCRETE WALK	(B)	MATCH EXISTING CROSS SLOPE
	8.0" CONCRETE DRIVEWAY PAVEMENT	(C)	STREETScape AREA



NO.	DATE	APPR.	REVISION

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: JEFFREY D. JENSEN  
 SIGNATURE: *[Signature]*  
 DATE: 8/25/2011 LIC. NO. 41964

**Minneapolis**  
 City of Lakes

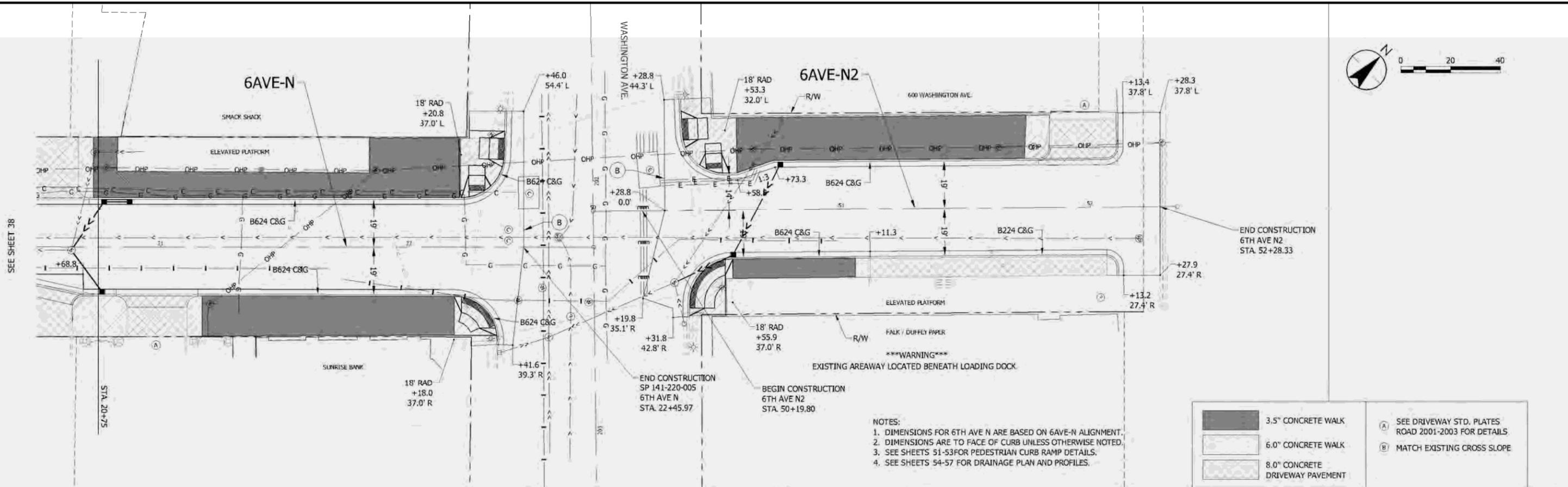
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 2335 Highway 36 W  
 St. Paul, MN 55113  
 www.stantec.com

CONSTRUCTION PLAN AND PROFILE

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET	38
OF	88

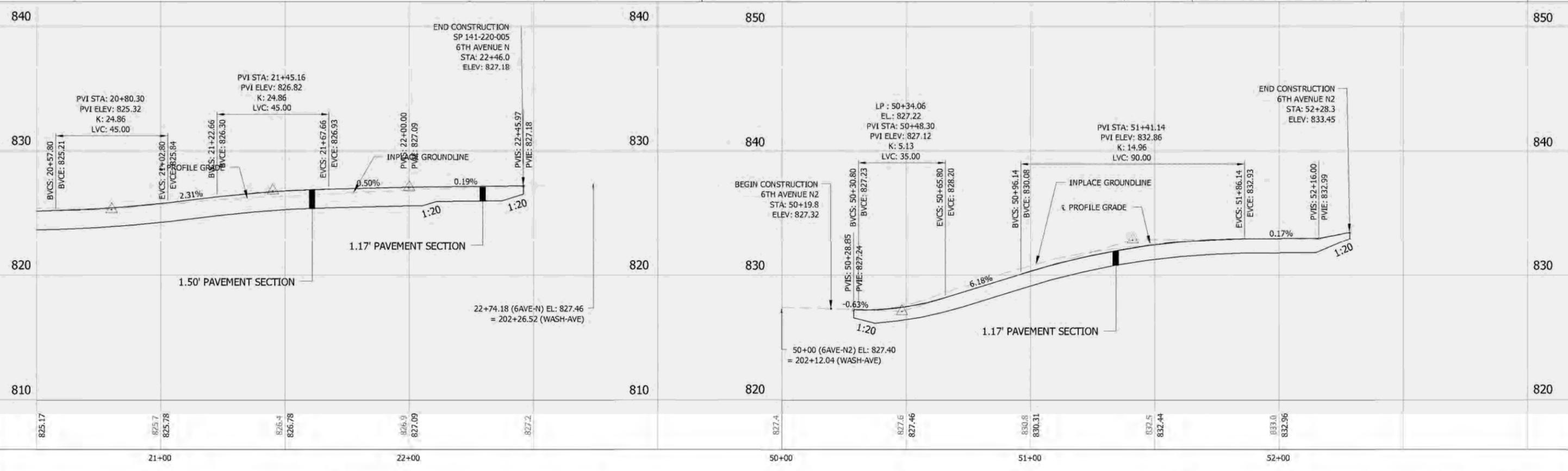
THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS NOT SCALE. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS NOT SCALE. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS NOT SCALE.



SEE SHEET 38

	3.5" CONCRETE WALK	(A)	SEE DRIVEWAY STD. PLATES ROAD 2001-2003 FOR DETAILS
	6.0" CONCRETE WALK	(B)	MATCH EXISTING CROSS SLOPE
	8.0" CONCRETE DRIVEWAY PAVEMENT		

- NOTES:
- DIMENSIONS FOR 6TH AVE N ARE BASED ON 6AVE-N ALIGNMENT.
  - DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - SEE SHEETS 51-53 FOR PEDESTRIAN CURB RAMP DETAILS.
  - SEE SHEETS 54-57 FOR DRAINAGE PLAN AND PROFILES.



Plot Date: 10/27/2015 10:00am  
 Project: 13352882 - 6TH AVENUE N STREET IMPROVEMENTS  
 Drawing: 13352882 - 6TH AVENUE N STREET IMPROVEMENTS - 20 SCALE: 1/8"=1'-0" (SCALE)  
 User: JG

NO	DATE	APPR	REVISION

SURVEY: MPV  
 DRAWN: JDP  
 DESIGNED: JDP  
 CHECKED: CIVL  
 APPROVED: EFG  
 PROJ. NO.: 13352882  
 I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: JEFFREY D. GIBSON  
 SIGNATURE: *[Signature]*  
 DATE: 8/15/15 LIC. NO.: 41964

**Minneapolis**  
 City of Lakes  
**Stantec**  
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CONSTRUCTION PLAN AND PROFILE

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE INFORMATION ON THIS SHEET IS FOR INFORMATION ONLY AND DOES NOT CONSTITUTE A CONTRACT. THE CONTRACT IS THE DRAWING AND SPECIFICATIONS AND ANY CHANGES TO THE PRODUCTION SHALL BE MADE BY THE CITY OF MINNEAPOLIS. THE CITY OF MINNEAPOLIS IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION ON THIS SHEET.

840

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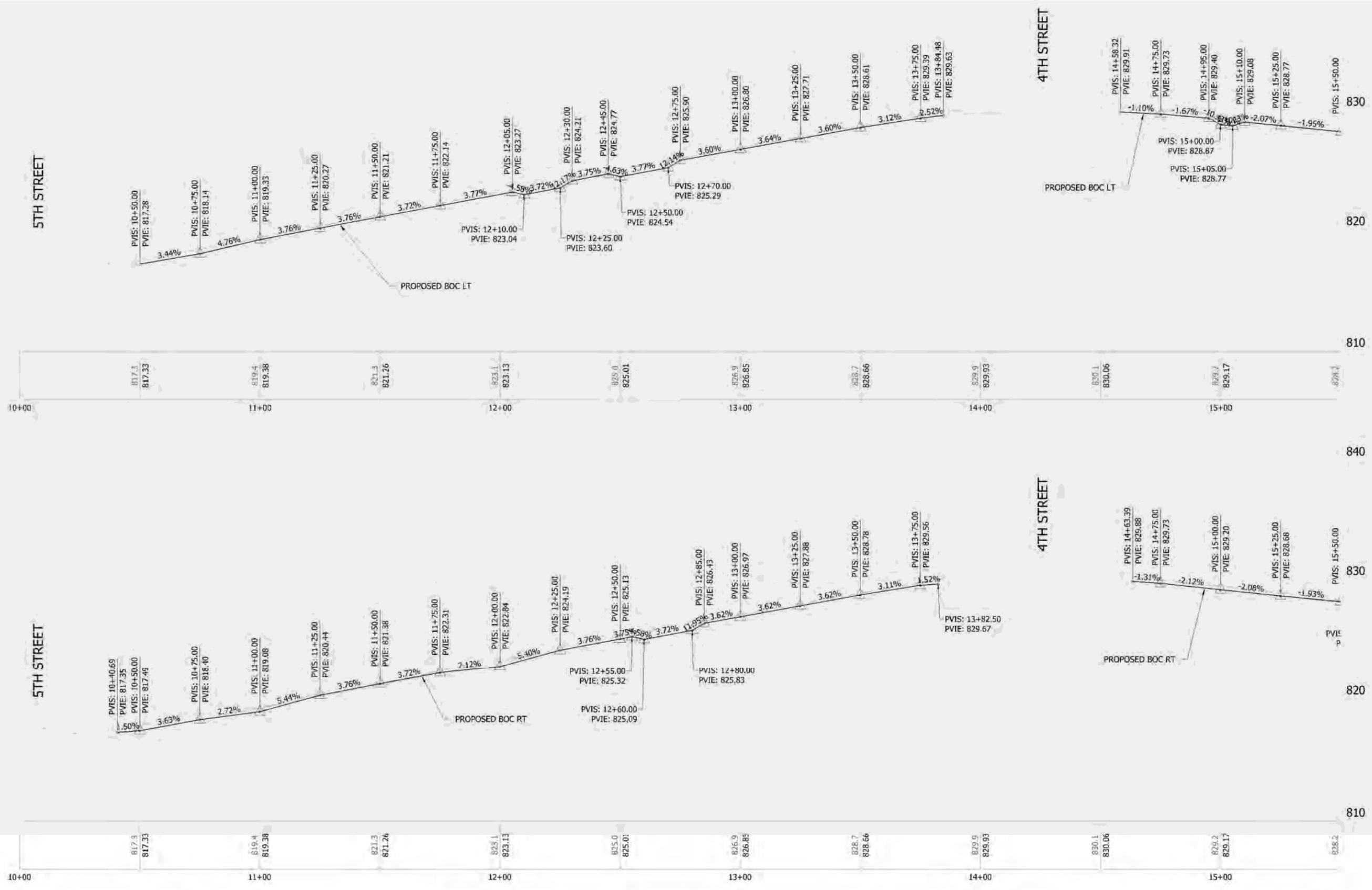
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Plot Date: 10/27/2015 10:10am  
 Drawing: 141-220-005-SP-08-Profile-01.dwg  
 Project: 141-220-005-SP-08-Profile-01.dwg  
 Scale: AS SHOWN

NO.	DATE	APPR.	REVISION

SURVEY	MPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  PRINT NAME: JEFFREY DANESON SIGNATURE: <i>[Signature]</i> DATE: 8/15 LIC. NO. 41964
DRAWN	JOP	
DESIGNED	JOP	
CHECKED	CWL	
APPROVED	LRG	
PROJ. NO.	1936288	

Minneapolis  
City of Lakes

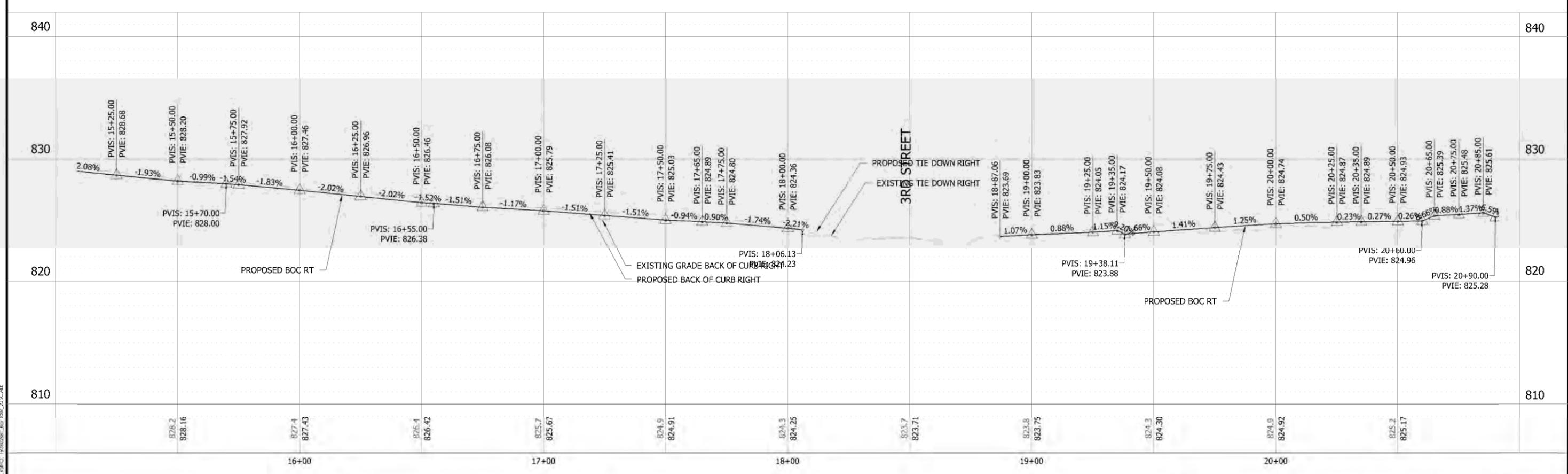
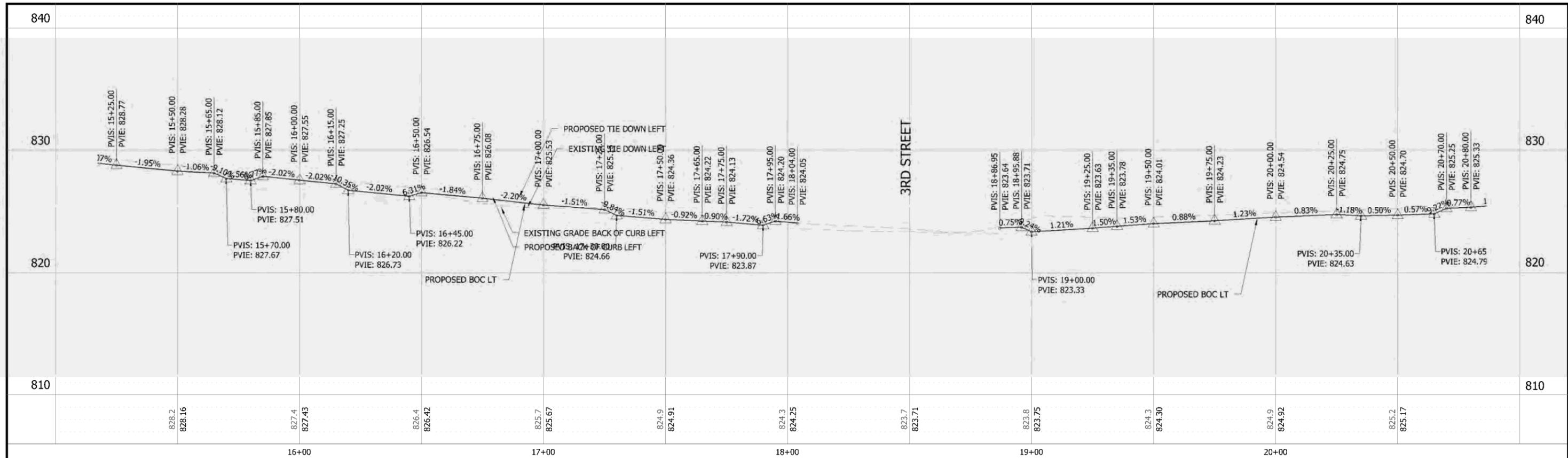
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PROFILES

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET	40
	88

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN.



Plot Date: 10/27/2015 10:10am  
 Drawing Path: C:\Users\jgarcia\Documents\19362288\19362288\_C068.dwg  
 Plot Path: C:\Users\jgarcia\Documents\19362288\19362288\_C068.dwg

NO.	DATE	APPR.	REVISION

SURVEY	MPV
DRAWN	TJM
DESIGNED	JOP
CHECKED	CIVL
APPROVED	EFG
PROJ. NO.	19362288

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: JEFFREY D. GARDNER  
 SIGNATURE: *[Signature]*  
 DATE: 8/15/15 LIC. NO.: 41964

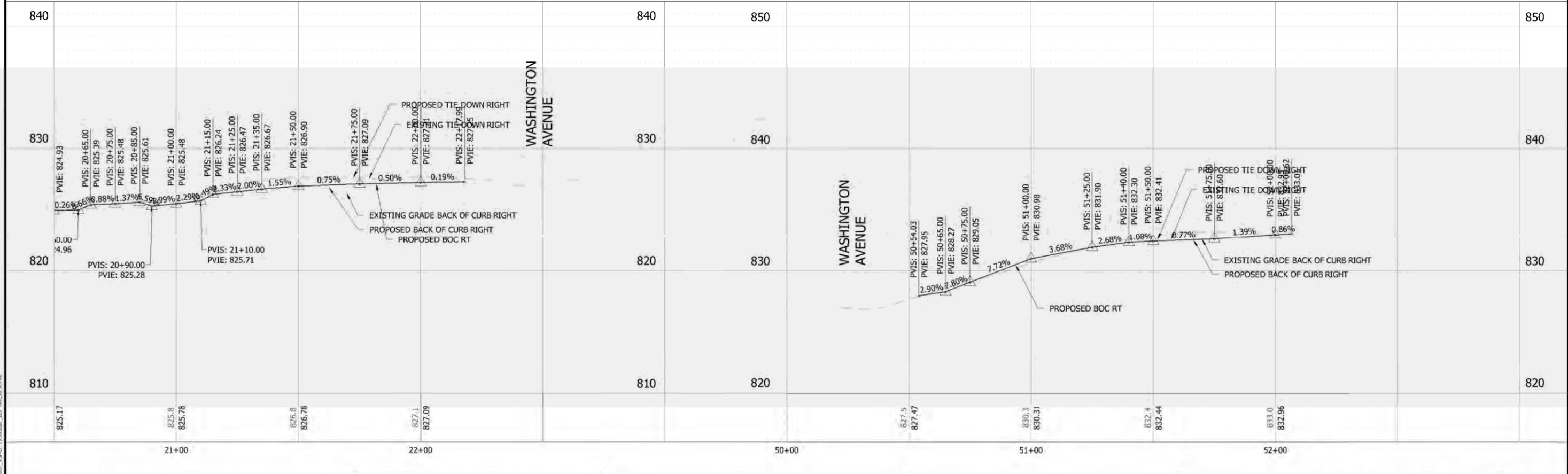
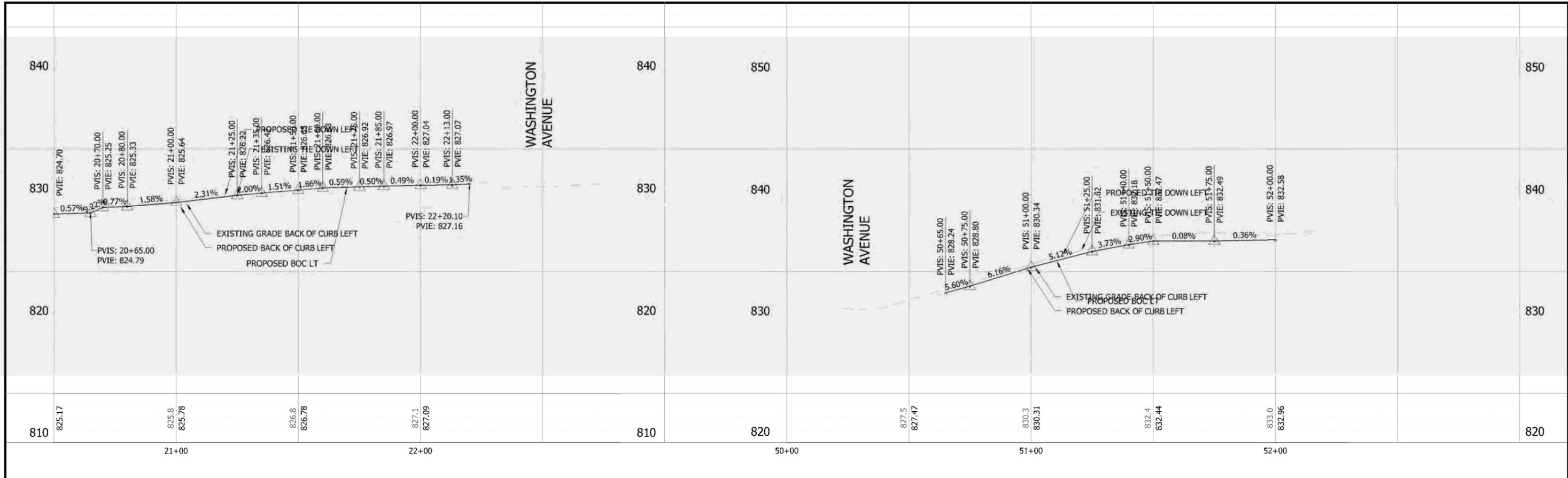


PROFILES  
 CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET	41
OF	88

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN.

Plot Date: 10/27/2015 10:10am  
 Drawing Path: C:\Users\jgarcia\Documents\19360288\_C68.dwg  
 User: jgarcia  
 Job: 19360288 - 6TH AVENUE N STREET IMPROVEMENTS - 25 SCALE



NO	DATE	APPR	REVISION

SURVEY	MPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: JEFFREY D. GARESON SIGNATURE: <i>[Signature]</i> DATE: 8/15/15 LIC. NO. 411964
DRAWN	TJM	
DESIGNED	JOP	
CHECKED	CIVL	
APPROVED	EPG	
PROJ. NO.	19360288	

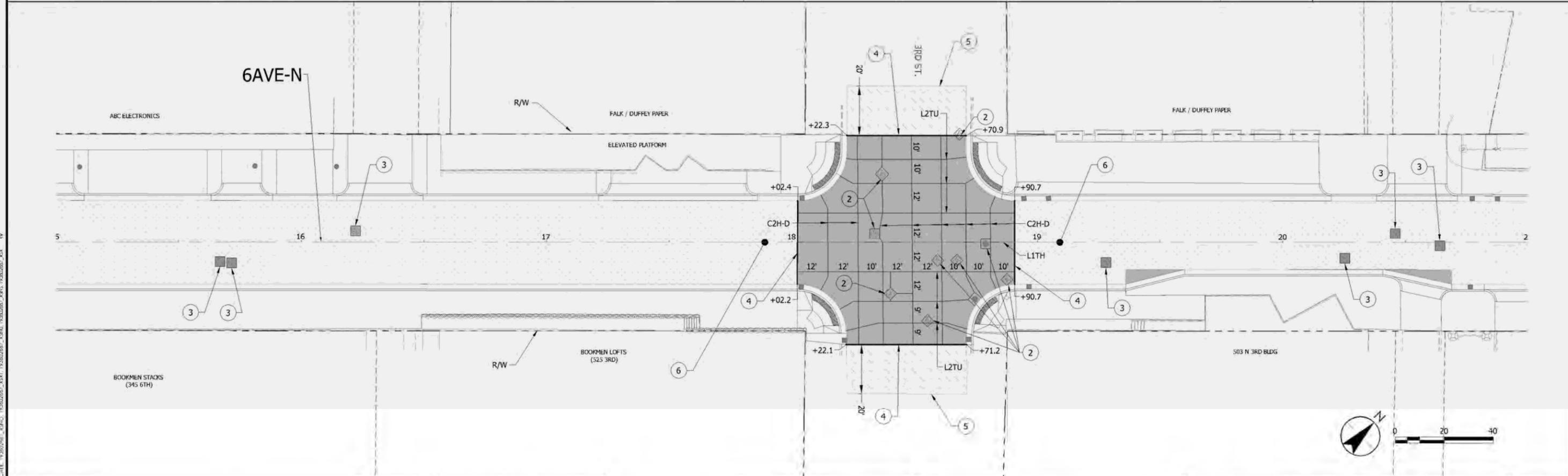
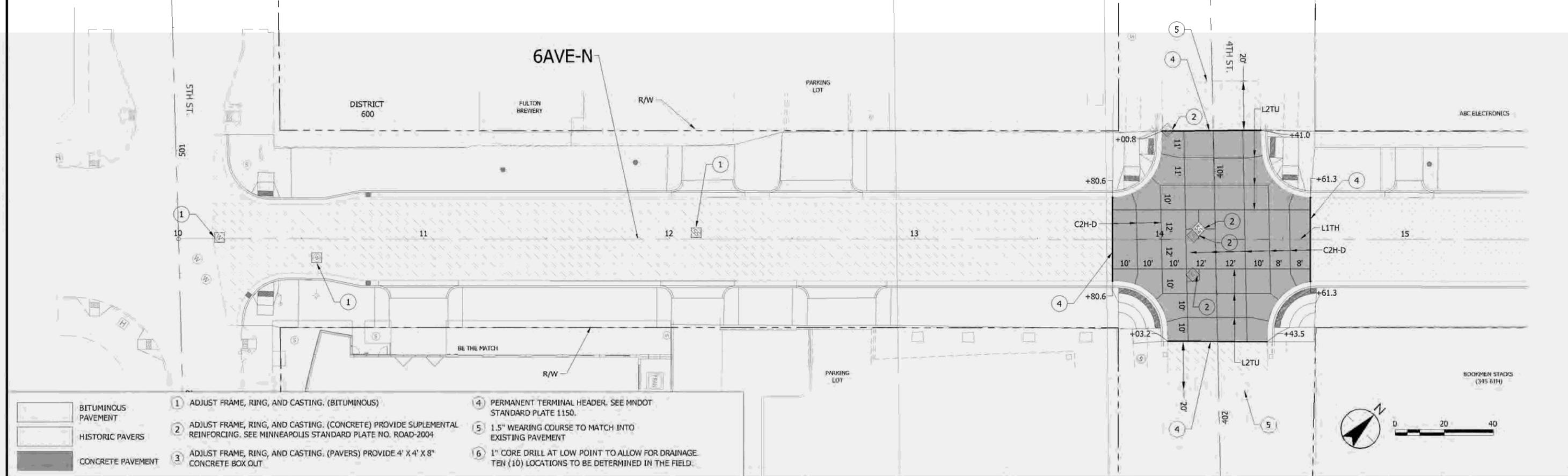
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City of Lakes

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PROFILES

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING.



NO.	DATE	APPR.	REVISION

SURVEY	MPV	DESIGNED	JDP	CHECKED	CIVL	APPROVED	LPB	PRJCT NO.	19362887
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.		PRINT NAME: JEFFREY DANESON		SIGNATURE: <i>[Signature]</i>		DATE: 8/15		LIC. NO.: 41964	




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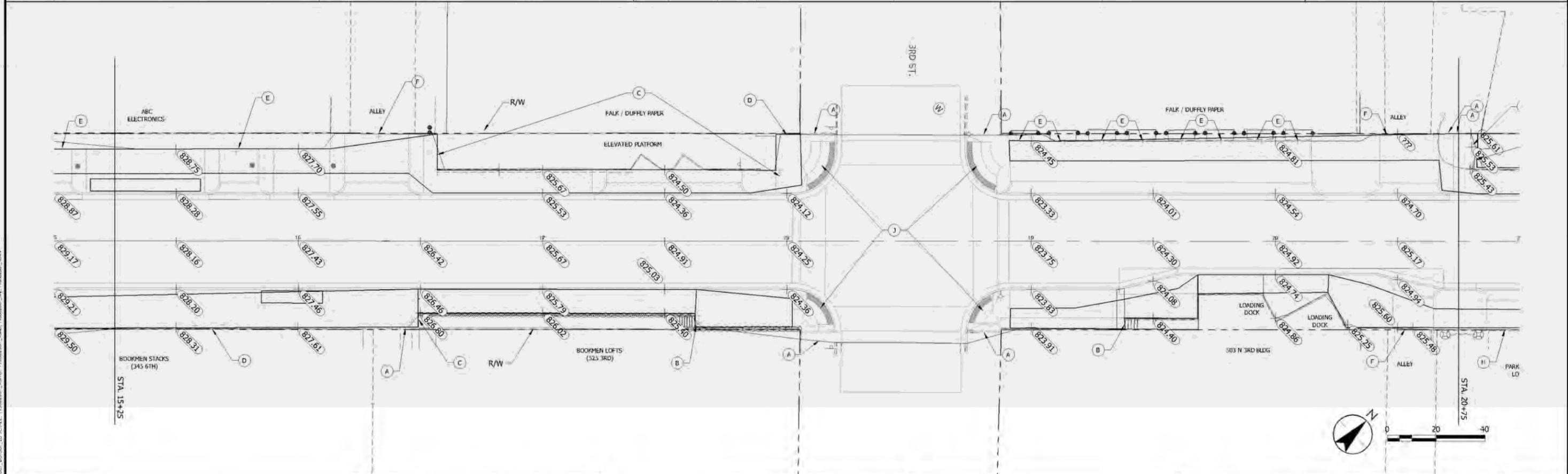
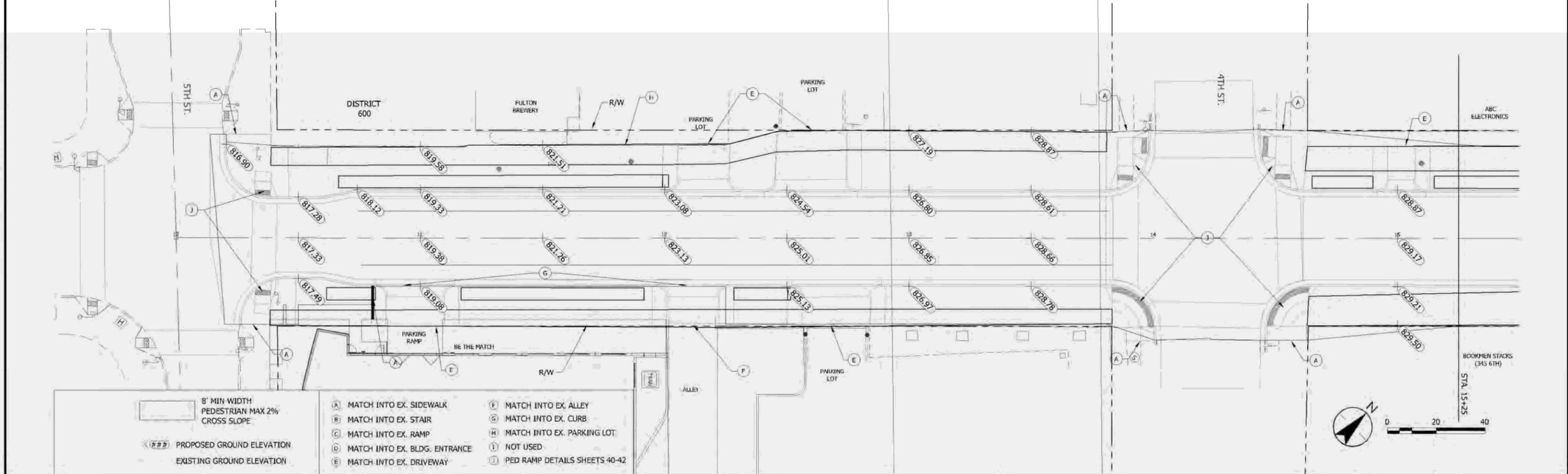
PAVING PLAN

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET  
 43  
 88



THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS OF THE SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS OF THE SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS OF THE SITE.



NO.	DATE	APPR.	REVISION

SURVEY	MPV	DESIGNED	TJM
CHECKED	CIVL	APPROVED	LPG
PROJ. NO.	19362887	DATE	8-4-15

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: TYLER J. MOLETTE  
SIGNATURE: [Signature]  
DATE: 8-4-15 LIC. NO.: 51774



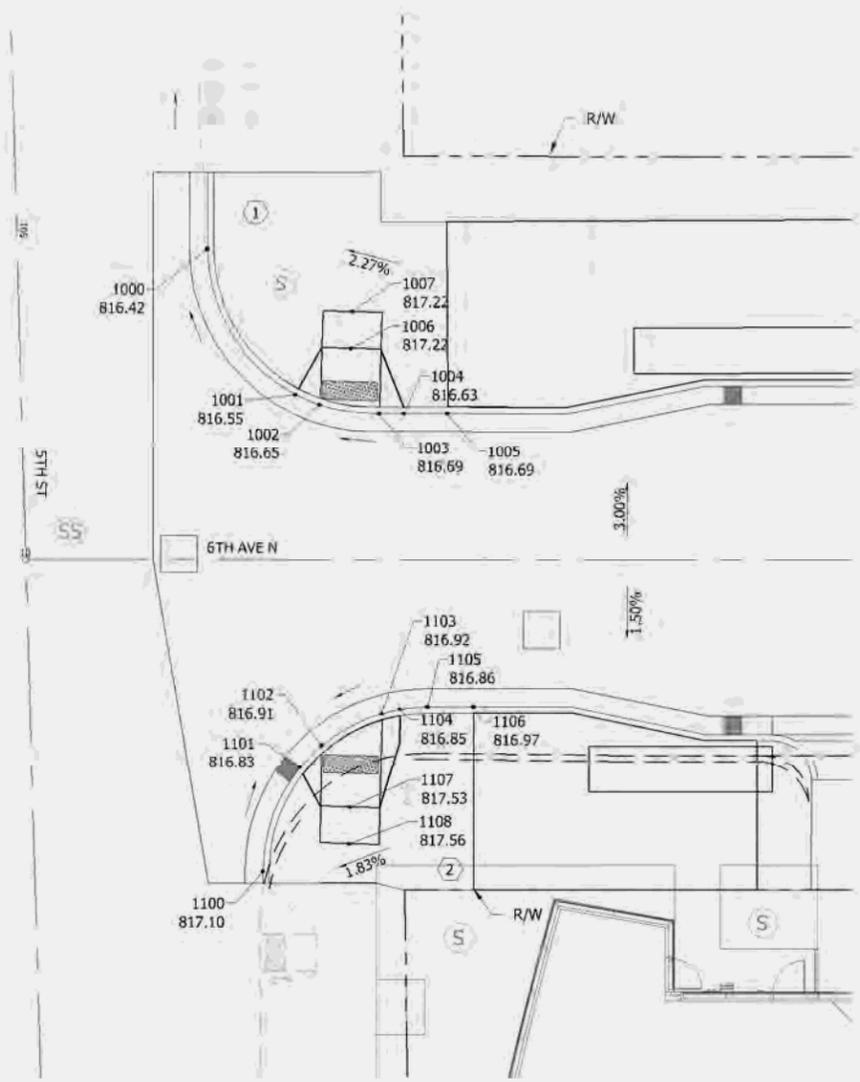
SPOT ELEVATION PLAN

CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005

SHEET 45 OF 88



THE INFORMATION HEREIN IS FOR INFORMATION ONLY AND IS NOT TO BE USED FOR CONSTRUCTION. THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.



POINT DATA				
POINT NUMBER	RAW DESCRIPTION	ELEVATION	NORTHING	EASTING
1000		816.422	170540.03	526903.09
1001	FULL HEIGHT CURB	816.554	170535.57	526921.12
1002	ZERO HEIGHT CURB	816.648	170536.63	526923.77
1003	ZERO HEIGHT CURB	816.693	170540.58	526929.07
1004	FULL HEIGHT CURB	816.634	170542.48	526930.98
1005		816.692	170545.85	526934.35
1006		817.220	170543.43	526921.90
1007		817.220	170546.33	526919.15
1100		817.104	170496.22	526955.37
1101	FULL HEIGHT CURB	816.828	170507.14	526950.20
1102	ZERO HEIGHT CURB	816.909	170510.48	526950.20
1103	ZERO HEIGHT CURB	816.924	170517.59	526952.36
1104	FULL HEIGHT CURB	816.854	170519.35	526953.46
1105		816.860	170521.66	526955.42
1106		816.968	170525.19	526958.96
1107		817.525	170507.90	526957.17
1108		817.565	170505.00	526959.93

# INDICATES DETAIL NUMBER SEE PEDESTRIAN CURB RAMP DETAILS SHEET 51 - 53.

NOTES:  
1. SPOT ELEVATIONS ARE LOCATED ALONG THE GUTTER FLOW LINE UNLESS OTHERWISE NOTED.

Plot Date: 10/27/2015 10:26am  
Drawing Path: \\snp\projects\15\15000001\15000001\_CAD\15000001\_CAD.dwg  
User: jason@stantec.com  
Scale: 1/8"=1'-0"  
Sheet: 47 of 88

NO.	DATE	APPR.	REVISION

SURVEY: JRPV  
 DRAWN: SWV  
 DESIGNED: GMY  
 CHECKED: GVL  
 APPROVED: JRPV  
 PROJ. NO.: 15000001

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: JEFFREY DANESON  
 SIGNATURE: *[Signature]*  
 DATE: 8/15/15 LIC. NO.: 41964

**Minneapolis**  
 City of Lakes

**Stantec**  
 2335 Highway 36 W  
 St. Paul, MN 55113  
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INTERSECTION DETAILS

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

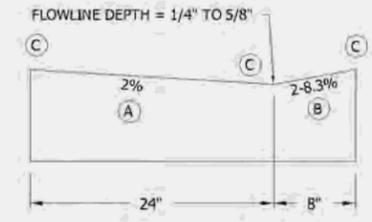
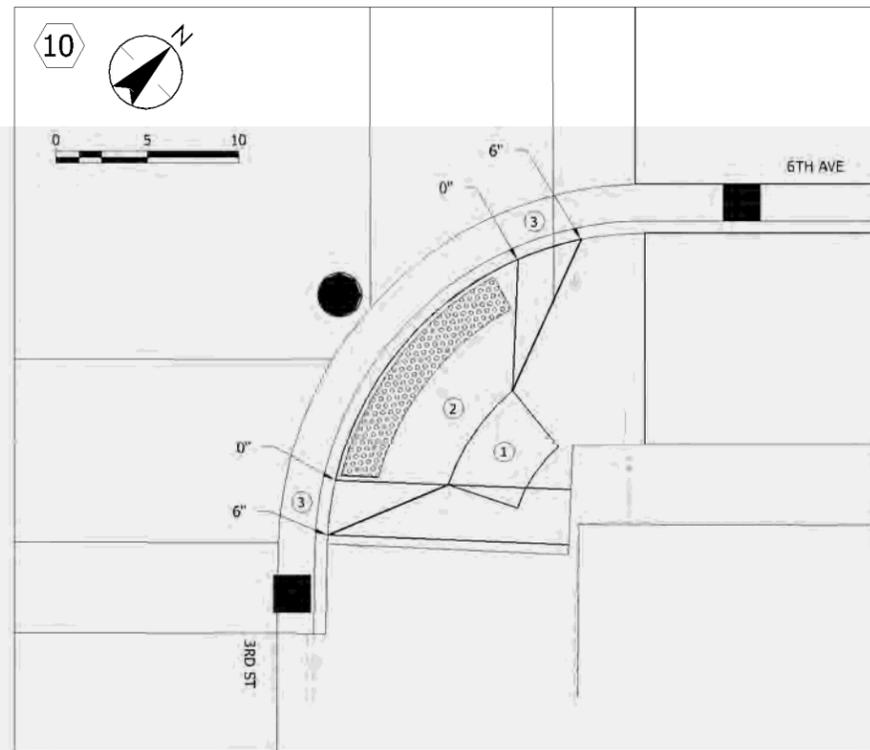
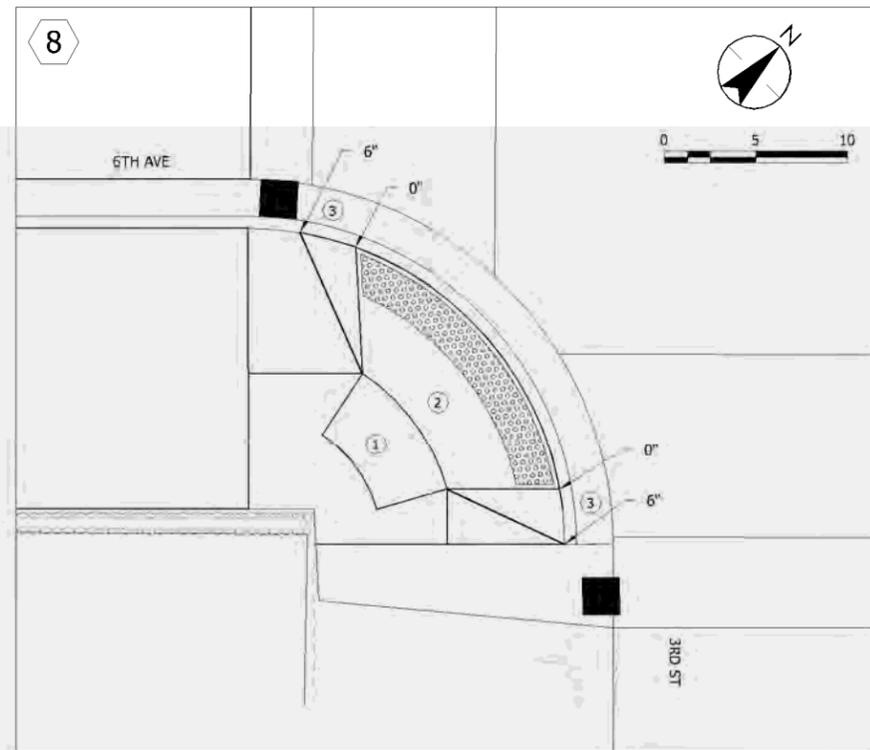
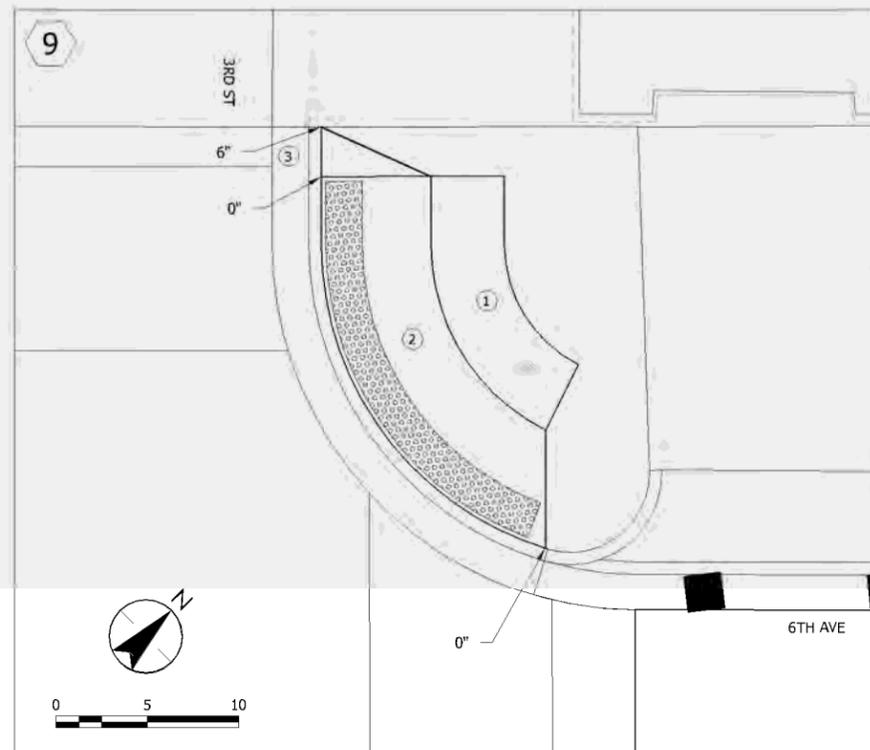
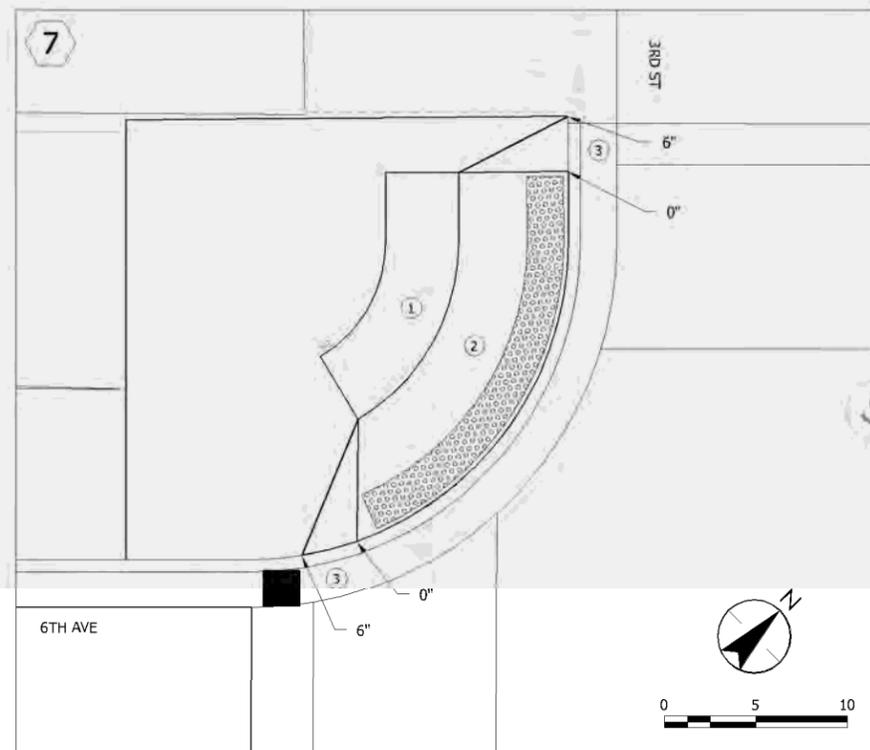








THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE DRAWING TO ALL DIMENSIONS AND DIMENSIONS ARE IN FEET AND INCHES. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE DRAWING TO ALL DIMENSIONS AND DIMENSIONS ARE IN FEET AND INCHES.



- NOTES:
- (A) THIS GUTTER SHOULD BE USED AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS NOT PERPENDICULAR TO THE GUTTER FLOW LINE.
  - (B) THE CURB SLOPE SHOULD MATCH THE SLOPE OF THE CONCRETE WALK BEHIND THE CURB.
  - (C) THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4 INCH.

**GUTTER SLOPE TAPER DETAIL**

- TRUNCATED DOMES
- CURB RAMP DETAIL. SEE SHEETS 47 - 50 FOR LOCATION

- NOTES:
- (1) LEVEL LANDING AREA, MINIMUM 4'X4' WITH 2.0% MAX SLOPE IN ANY DIRECTION
  - (2) RAMP AREA, LESS THAN OR EQUAL TO 8.3% SLOPE
  - (3) CURB HEIGHT TAPER SECTION - GUTTER SLOPE TAPER SECTION (SEE GUTTER SLOPE TAPER DETAIL ON SHEET 52.)
  - (4) MAX 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

Plan Date: 10/27/2015 10:28am  
 Drawing No: 141-220-005-SP-01  
 Project No: 141-220-005-SP-01  
 Revision: 01

NO.	DATE	APPR.	REVISION

SURVEY	WSP	DESIGNED	SMV	CHECKED	CWL	APPROVED	LLP	DATE	10/27/2015
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PROJECT NAME: JEFFREY D. JENSEN SIGNATURE: <i>Jeffrey D. Jensen</i> DATE: 10/27/2015 LIC. NO.: 41964									

Minneapolis City of Lakes  
 Stantec  
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PEDESTRIAN CURB RAMP DETAILS

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005



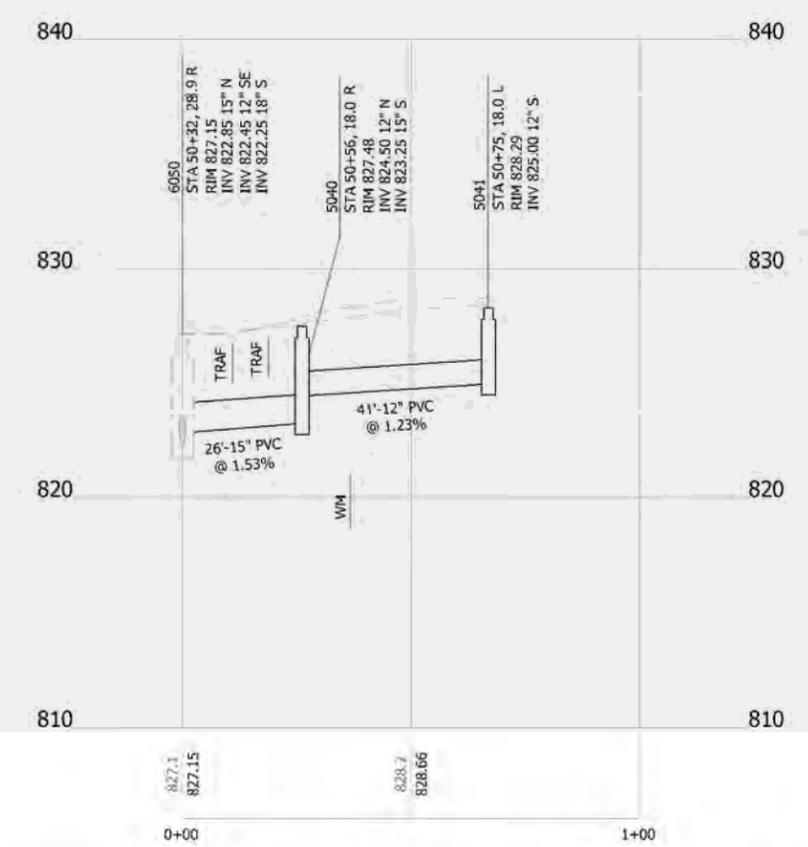
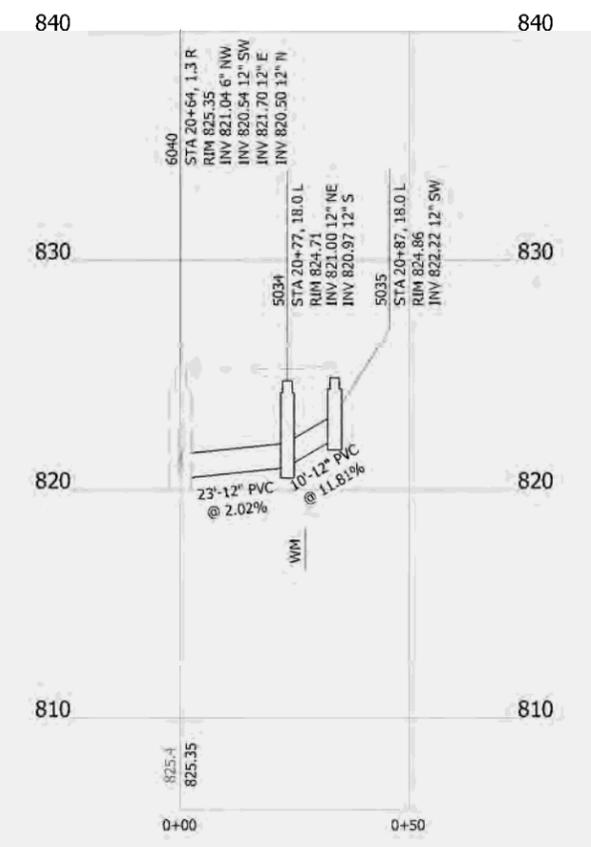
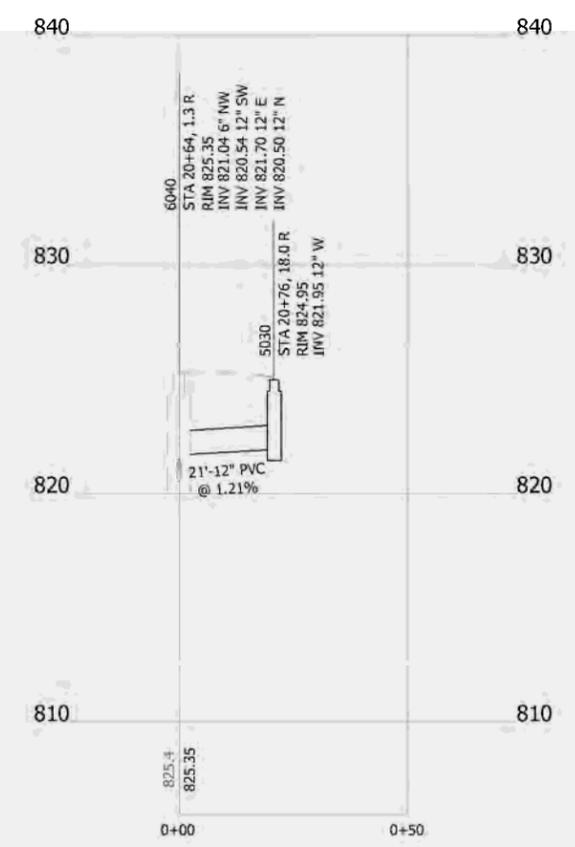
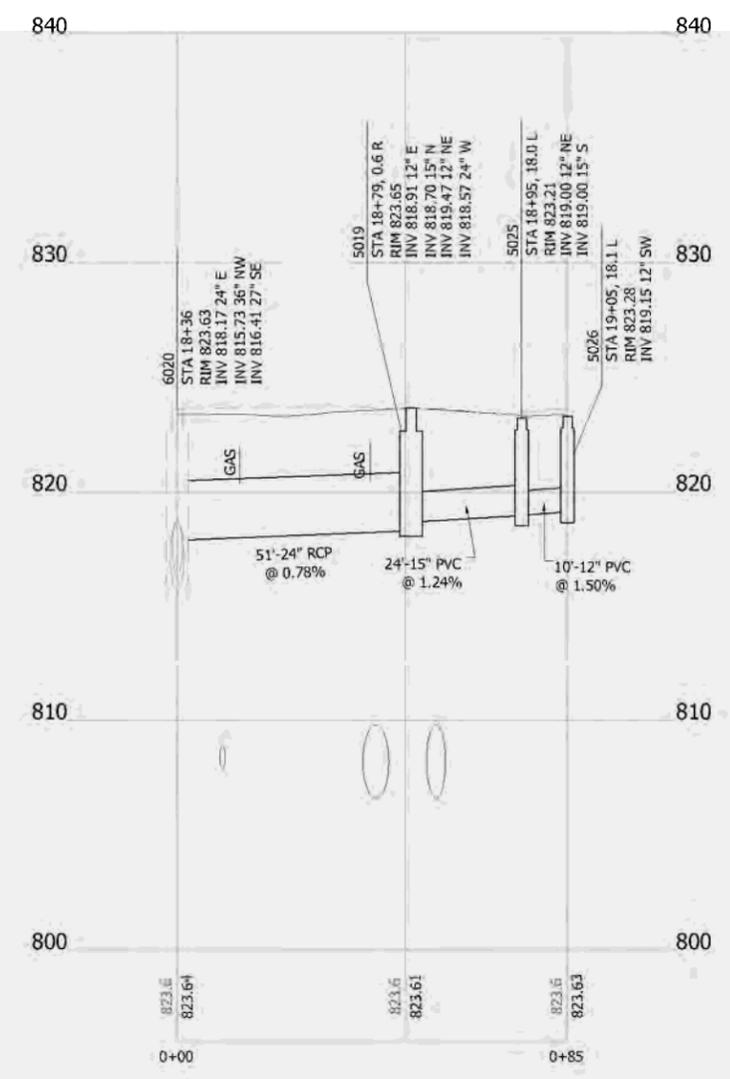
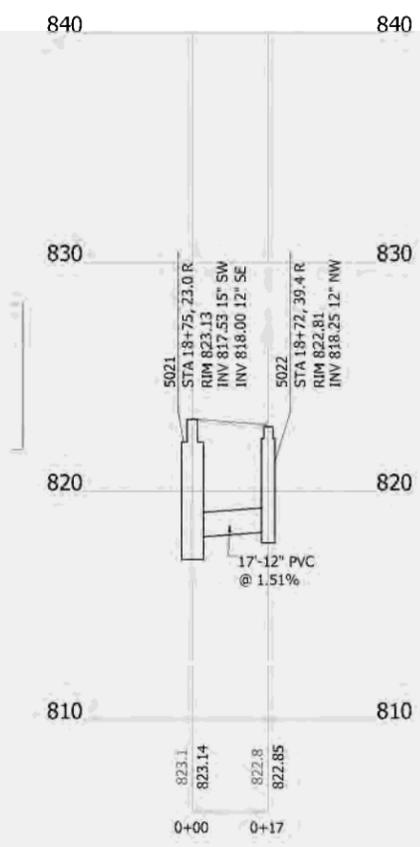






THE CONTRACTOR SHALL VERIFY THE REVISIONS FOR ACCURACY AND SCALE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF MINNEAPOLIS AND THE STATE OF MINNESOTA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF MINNEAPOLIS AND THE STATE OF MINNESOTA.

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SURVEY	APPV
DRAWN	TJM
DESIGNED	TJM
CHECKED	CIVL
APPROVED	LFG
PROJ. NO.	19362867

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: TYLER J. MOLETTE  
 SIGNATURE: *[Signature]*  
 DATE: 8-4-15 LIC. NO.: 51774



DRAINAGE PROFILES

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

**STORM WATER POLLUTION PREVENTION PLAN (SWPPP) NARRATIVE**

**PROJECT DESCRIPTION/LOCATION**

SP 141-220-005 IS LOCATED ON 6TH AVE FROM 5TH ST N TO 216' NORTH OF WASHINGTON AVE IN THE CITY OF MINNEAPOLIS IN HENNEPIN COUNTY.

THE PLANNED SCOPE OF THE PROJECT INCLUDES:

GRADING, BITUMINOUS PAVEMENT, CONCRETE PAVEMENT, CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, AND STORM SEWER.

**SPECIAL AND IMPAIRED WATERS**

THESE SPECIAL AND IMPAIRED WATERS ARE LOCATED WITHIN ONE MILE (AERIAL RADIUS) OF THE PROJECT LIMITS AND RECEIVE RUNOFF FROM THE PROJECT SITE. DUE TO THE PROXIMITY OF THESE SPECIAL AND IMPAIRED WATERS. THE BMPS DESCRIBED IN APPENDIX A OF THE NPDES PERMIT WILL APPLY TO ALL AREAS OF THE SITE.

WATERBODY	IMPAIRMENT(S)
BASSETT CREEK	AQUATIC BIOTA (FISH BIOASSESSMENT, AQUATIC PLANT, BIOASSESSMENT AND AQUATIC MACROINVERTEBRATE BIOASSESSMENT)

**AREAS OF ENVIRONMENTAL SENSITIVITY (AES) AND INFESTED WATERS**

IN ADDITION TO THE LIST OF SPECIAL AND IMPAIRED WATERS THE CONTRACTOR SHALL BE AWARE THAT THERE ARE WETLANDS AND EXISTING STORMWATER FACILITIES WITHIN AND NEAR THE PROJECT BOUNDARY.

THE FOLLOWING WATER BODIES HAVE BEEN LISTED BY THE DNR AS BEING INFESTED BY INVASIVE SPECIES: LIST WATERBODIES HERE.  
MISSISSIPPI RIVER

**SOIL TYPES**

SOIL TYPES TYPICALLY FOUND ON THIS PROJECT ARE URBAN LAND - UDIPSAMMENTS

**PROJECT PERSONNEL AND TRAINING**

THIS SWPPP WAS PREPARED BY PERSONNEL THAT ARE CERTIFIED IN THE DESIGN OF CONSTRUCTION SWPPPS. COPIES OF THE CERTIFICATIONS ARE ON FILE WITH MNDOT AND ARE AVAILABLE UPON REQUEST.

PROVIDE A CERTIFIED EROSION CONTROL SUPERVISOR IN GOOD STANDING WHO IS KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES. THE EROSION CONTROL SUPERVISOR WILL WORK WITH THE PROJECT ENGINEER TO OVERSEE THE IMPLEMENTATION OF THE SWPPP AND THE INSTALLATION, INSPECTION, AND MAINTENANCE OF THE EROSION PREVENTION AND SEDIMENT CONTROL BMPS BEFORE, DURING AND AFTER CONSTRUCTION UNTIL THE NOTICE OF TERMINATION (NOT) HAS BEEN FILED WITH THE MPCA. PROVIDE PROOF OF CERTIFICATION AT THE PRECONSTRUCTION MEETING. WORK WILL NOT BE ALLOWED TO COMMENCE UNTIL PROOF OF CERTIFICATION HAS BEEN PROVIDED TO THE PROJECT ENGINEER.

PROVIDE AT LEAST ONE CERTIFIED INSTALLER FOR EACH CONTRACTOR OR SUBCONTRACTOR THAT INSTALLS THE PRODUCTS LISTED IN SPECIFICATION SECTION 2573.3.A.2. PROVIDE PROOF OF CERTIFICATION AT THE PRECONSTRUCTION MEETING. WORK WILL NOT BE ALLOWED TO COMMENCE UNTIL PROOF OF CERTIFICATION HAS BEEN PROVIDED TO THE PROJECT ENGINEER.

**CHAIN OF RESPONSIBILITY**

MINNEAPOLIS AND THE CONTRACTOR ARE COPERMITEES FOR THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) CONSTRUCTION PERMIT. THE CONTRACTOR IS RESPONSIBLE TO COMPLY WITH ALL ASPECTS OF THE NPDES CONSTRUCTION PERMIT AT ALL TIMES UNTIL THE NOTICE OF TERMINATION (NOT) HAS BEEN FILED WITH THE MPCA. THE CONTRACTOR WILL DEVELOP A CHAIN OF COMMAND WITH ALL OPERATORS ON THE SITE TO ENSURE THAT THE SWPPP WILL BE IMPLEMENTED AND STAY IN EFFECT UNTIL THE CONSTRUCTION PROJECT IS COMPLETE, THE ENTIRE SITE HAS UNDERGONE FINAL STABILIZATION, AND A NOTICE OF TERMINATION (NOT) HAS BEEN SUBMITTED TO THE MPCA.

**PROJECT CONTACTS**

THE PROJECT ENGINEER AND CONTRACTOR ARE RESPONSIBLE FOR IMPLEMENTATION OF THE SWPPP AND INSTALLATION, INSPECTION, AND MAINTENANCE OF THE EROSION PREVENTION AND SEDIMENT CONTROL BMPS BEFORE, DURING AND AFTER CONSTRUCTION UNTIL THE NOTICE OF TERMINATION HAS BEEN FILED. MNDOT METRO DISTRICT WATER RESOURCES STAFF ARE ALSO AVAILABLE FOR TECHNICAL ASSISTANCE.

ORGANIZATION	CONTACT NAME	PHONE
STANTEC	PHIL GRAVEL	651-604-4885
CITY OF MINNEAPOLIS	JEFF HANDELAND	612-673-2363
MINNESOTA POLLUTION CONTROL AGENCY (MPCA)	DAN SULLIVAN	651-757-2768
MINNESOTA DEPARTMENT OF NATURAL RESOURCES	PETER LEETE	651-366-3634
WATERSHED DISTRICT	LAURA JESTER	952-270-1990

MPCA DUTY OFFICER 24 HOUR EMERGENCY NOTIFICATION:  
651-649-5451 OR 800-422-0798

**LOCATION OF SWPPP REQUIREMENTS**

THE REQUIRED SWPPP ELEMENTS MAY BE LOCATED IN MANY PLACES WITHIN THE PLAN SET AS WELL AS IN THE SPECIAL PROVISIONS, MNDOT SPEC BOOK (2014 EDITION), OR ON FILE WITH MNDOT. THE NOTES AND TABLE BELOW ARE INTENDED TO BE A QUICK REFERENCE FOR THE CONTRACTOR AND PROJECT ENGINEER TO USE IN THE FIELD. THERE MAY BE ADDITIONAL REQUIRED SWPPP ELEMENTS INCLUDED ON THE PROJECT THAT ARE NOT LISTED ON THIS SHEET.

**LOCATION OF SWPPP REQUIREMENTS IN PROJECT PLAN**

DESCRIPTION	LOCATION
TEMPORARY EROSION CONTROL MEASURES	SHEETS NO. 62 - 64
PERMANENT EROSION CONTROL MEASURES	SHEETS NO. 65 - 66
DIRECTION OF FLOW	SHEETS NO. 63 - 64
FINAL STABILIZATION	SHEETS NO. 65 - 66
SOILS AND CONSTRUCTION NOTES	SHEETS NO. 6
DRAINAGE STRUCTURES	SHEETS NO. 56 - 57
DRAINAGE TABULATION	SHEETS NO. 58
STORM SEWER PROFILE SHEETS	SHEETS NO. 56 - 57
STORM SEWER TABULATION	SHEETS NO. 58
EROSION AND SEDIMENT CONTROL DETAILS	SHEETS NO. 62
EROSION CONTROL TABULATION	SHEETS NO. 14
TURF ESTABLISHMENT TABULATION	SHEETS NO. 14
SITE MAP	SHEETS NO. 5
STORMWATER TREATMENT CONSTRUCTION STAGING	STANTEC
STORMWATER CALCULATIONS AND MAPS	STANTEC
WATER RESOURCES NOTES	SHEET NO. 59 - 61

**SITE INSPECTION AND MAINTENANCE**

INSPECT THE ENTIRE CONSTRUCTION SITE A MINIMUM OF ONCE EVERY SEVEN DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS. INSPECT ALL TEMPORARY AND PERMANENT WATER QUALITY MANAGEMENT, EROSION PREVENTION AND SEDIMENT CONTROL BMPS UNTIL THE SITE HAS UNDERGONE FINAL STABILIZATION AND THE NOT HAS BEEN SUBMITTED. INSPECT SURFACE WATER INCLUDING DRAINAGE DITCHES FOR SIGNS OF EROSION AND SEDIMENT DEPOSITION. INSPECT CONSTRUCTION SITE VEHICLE EXIT LOCATIONS FOR EVIDENCE OF TRACKING ONTO PAVED SURFACES. INSPECT SURROUNDING PROPERTIES FOR EVIDENCE OF OFF SITE SEDIMENT ACCUMULATION. INSPECT INFILTRATION AREAS FOR SIGNS OF SEDIMENT DEPOSITION AND COMPACTION (TO ENSURE THAT EQUIPMENT IS NOT BEING DRIVEN ACROSS THE AREA).

RECORD ALL INSPECTIONS AND MAINTENANCE ACTIVITIES IN WRITING WITHIN 24 HOURS. SUBMIT INSPECTION REPORTS IN A FORMAT THAT IS ACCEPTABLE TO THE PROJECT ENGINEER. INCLUDE THE FOLLOWING IN THE RECORDS OF EACH INSPECTION AND MAINTENANCE ACTIVITY:

- A. DATE AND TIME OF INSPECTIONS
- B. NAME OF PERSONS CONDUCTING INSPECTIONS
- C. FINDINGS OF INSPECTIONS, INCLUDING RECOMMENDATIONS FOR CORRECTIVE ACTIONS
- D. CORRECTIVE ACTIONS TAKEN, INCLUDING DATES, TIMES, AND PARTY COMPLETING MAINTENANCE ACTIVITIES
- E. DATE AND AMOUNT OF ALL RAINFALL EVENTS GREATER THAN 0.5 INCH IN 24 HOURS
- F. DOCUMENTS AND CHANGES MADE TO THE SWPPP

REPLACE, REPAIR OR SUPPLEMENT ALL NONFUNCTIONAL BMPS BY THE END OF THE NEXT BUSINESS DAY FOLLOWING DISCOVERY UNLESS LISTED DIFFERENTLY BELOW:

- A. REPAIR, REPLACE, OR SUPPLEMENT PERIMETER CONTROL DEVICES WHEN IT BECOMES NONFUNCTIONAL OR SEDIMENT REACHES 1/2 THE HEIGHT OF THE DEVICE. COMPLETE REPAIRS BY THE END OF THE NEXT BUSINESS DAY FOLLOWING DISCOVERY.
- B. REPAIR OR REPLACE INLET PROTECTION DEVICES WHEN THEY BECOME NONFUNCTIONAL OR SEDIMENT REACHES 1/2 THE HEIGHT AND/OR DEPTH OF THE DEVICE.
- C. DRAIN AND REMOVE SEDIMENT FROM TEMPORARY AND PERMANENT SEDIMENT BASINS ONCE THE SEDIMENT HAS REACHED 1/2 THE STORAGE VOLUME. COMPLETE WORK WITHIN 72 HOURS OF DISCOVERY.
- D. REMOVE ALL DELTAS AND SEDIMENT DEPOSITED IN SURFACE WATERS INCLUDING DRAINAGE WAYS, CATCH BASINS, AND OTHER DRAINAGE SYSTEMS. RESTABILIZE ANY AREAS THAT ARE DISTURBED BY SEDIMENT REMOVAL OPERATIONS. SEDIMENT REMOVAL AND STABILIZATION MUST BE COMPLETED WITHIN 7 DAYS OF DISCOVERY. PREPARE AND SUBMIT A SITE MANAGEMENT PLAN FOR WORKING IN SURFACE WATERS. CONTACT ALL APPROPRIATE AUTHORITIES PRIOR TO WORKING IN SURFACE WATERS.
- E. REMOVE TRACKED SEDIMENT FROM PAVED SURFACES BOTH ON AND OFF SITE WITHIN 24 HOURS OF DISCOVERY. STREET SWEEPING MAY HAVE TO OCCUR MORE OFTEN TO MINIMIZE OFF SITE IMPACTS. LIGHTLY WET THE PAVEMENT PRIOR TO SWEEPING.
- F. MAINTAIN ALL BMPS UNTIL WORK HAS BEEN COMPLETED, SITE HAS GONE UNDER FINAL STABILIZATION, AND THE NOTICE OF TERMINATION (NOT) HAS BEEN SUBMITTED TO THE MPCA.

**ENVIRONMENTAL REVIEW**

THERE ARE NO STORMWATER MITIGATION MEASURES REQUIRED AS A RESULT OF AN ENVIRONMENTAL, ARCHEOLOGICAL OR AGENCY REVIEW. ALL MITIGATION MEASURES HAVE BEEN ADDRESSED IN THIS PLAN SET OR THE SPECIAL PROVISIONS.

THIS PROJECT IS NOT LOCATED IN A WELL HEAD PROTECTION AREA.

THIS PROJECT IS NOT LOCATED IN A DRINKING WATER SUPPLY MANAGEMENT AREA (DWSMA). THE DWSMA VULNERABILITY IS CLASSIFIED AS XXXX.

**LAND FEATURE CHANGES**

TOTAL DISTURBED AREA	2.31 ACRES
TOTAL EXISTING IMPERVIOUS SURFACE AREA	2.31 ACRES
TOTAL PROPOSED IMPERVIOUS SURFACE AREA	2.28 ACRES
TOTAL PROPOSED NET CHANGE IN IMPERVIOUS SURFACE AREA	(0.02) ACRES

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND LOCATIONS. SCALE THE DRAWING TO ALL DIMENSIONS AND LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS AND LOCATIONS. SCALE THE DRAWING TO ALL DIMENSIONS AND LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS AND LOCATIONS. SCALE THE DRAWING TO ALL DIMENSIONS AND LOCATIONS.

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NO.	DATE	APPR.	REVISION

SURVEY	REV	DATE	BY
DESIGNED			
CHECKED			
APPROVED			
PROJECT NO.	19560288	DATE	8-4-15

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: TYLER J. MOLETTE  
SIGNATURE:   
DATE: 8-4-15 LIC. NO.: 51774




2335 Highway 56 W  
St. Paul, MN 55113  
www.stantec.com

SWPPP NARRATIVE
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**CITY OF MINNEAPOLIS, MN**  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005

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THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING.

**STORM WATER POLLUTION PREVENTION PLAN (SWPPP) NARRATIVE (CONTINUED)**

**STABILIZATION TIME FRAMES**

AREA	TIME FRAME	NOTES
LAST 200 LINEAL FEET OF DRAINAGE DITCH OR SWALE	WITHIN 24 HOURS OF CONNECTION TO SURFACE WATER OR PROPERTY EDGE	1, 2, 3
REMAINING PORTIONS OF DRAINAGE DITCH OR SWALE	7 DAYS	1, 3
PIPE AND CULVERT OUTLETS	24 HOURS	
EXPOSED SOILS AND STOCKPILES	7 DAYS	1

- INITIATE STABILIZATION IMMEDIATELY WHEN CONSTRUCTION HAS TEMPORARILY OR PERMANENTLY CEASED ON ANY PORTION OF THE SITE. COMPLETE STABILIZATION WITHIN THE TIME FRAME LISTED. IN MANY INSTANCES THIS WILL REQUIRE STABILIZATION TO OCCUR MORE THAN ONCE DURING THE COURSE OF THE PROJECT. TEMPORARY SOIL STOCKPILES WITHOUT SIGNIFICANT CLAY OR SILT AND STOCKPILED AND CONSTRUCTED ROAD BASE ARE EXEMPT FROM THE STABILIZATION REQUIREMENT.
- STABILIZE WETTED PERIMETER OF DITCH (I.E. WHERE THE DITCH GETS WET).
- APPLICATION OF MULCH, HYDROMULCH, TACKIFIER AND POLYACRYLAMIDE ARE NOT ACCEPTABLE STABILIZATION METHODS IN THESE AREAS.
- STABILIZE ALL AREAS OF THE SITE PRIOR TO THE ONSET OF WINTER. ANY WORK STILL BEING PERFORMED WILL BE SNOW MULCHED, SEEDED, AND BLANKETED WITHIN THE TIME FRAMES IN THE NPDES PERMIT.
- TOPSOIL BERMS MUST BE STABILIZED IN ORDER TO BE CONSIDERED PERIMETER CONTROL BMPS. USE RAPID STABILIZATION METHOD 2, 3, OR 4 AS DIRECTED BY THE ENGINEER. THE SEED MIX USED IN THE RAPID STABILIZATION MAY BE SUBSTITUTED AS FOLLOWS:
  - SINGLE YEAR CONSTRUCTION BETWEEN MAY 1 - AUGUST 1, SEED WITH SEED MIXTURE 21-111
  - SINGLE YEAR CONSTRUCTION BETWEEN AUGUST 1 AND OCTOBER 31, SEED WITH SEED MIXTURE 21-112
  - MULTI YEAR CONSTRUCTION 22-111
- KEEP DITCHES AND EXPOSED SOILS IN AN EVEN ROUGH GRADED CONDITION IN ORDER TO BE ABLE TO APPLY EROSION CONTROL MULCHES, HYDROMULCHES AND BLANKETS.

**GENERAL SWPPP NOTES FOR CONSTRUCTION ACTIVITY**

- AMEND THE SWPPP AND DOCUMENT ANY AND ALL CHANGES TO THE SWPPP AND ASSOCIATED PLAN SHEETS IN A TIMELY MANNER. STORE THE SWPPP AND ALL AMENDMENTS ON SITE AT ALL TIMES.
- PREPARE AND SUBMIT A SITE MANAGEMENT PLAN FOR THE ENGINEER'S ACCEPTANCE FOR CONCRETE MANAGEMENT, CONCRETE SLURRY APPLICATION AREAS, WORK IN AND NEAR AREAS OF ENVIRONMENTAL SENSITIVITY, AREAS IDENTIFIED IN THE PLANS AS "SITE MANAGEMENT PLAN AREA", ANY WORK THAT WILL REQUIRE DEWATERING, AND AS REQUESTED BY THE ENGINEER. SUBMIT ALL SITE MANAGEMENT PLANS TO THE ENGINEER IN WRITING. ALLOW A MINIMUM OF 7 DAYS FOR MINNEAPOLIS TO REVIEW AND ACCEPT SITE MANAGEMENT PLAN SUBMITTALS. WORK WILL NOT BE ALLOWED TO COMMENCE IF A SITE MANAGEMENT PLAN IS REQUIRED UNTIL ACCEPTANCE HAS BEEN GRANTED BY THE ENGINEER. THERE WILL BE NO EXTRA TIME ADDED TO THE CONTRACT DUE TO THE UNTIMELY SUBMITTAL.
- IT IS THE DESIGNER'S INTENT THAT THE CONTRACTOR BUILD PONDS AND INSTALL EROSION CONTROL BMPS BEFORE PUTTING THEM INTO ACTIVE SERVICE TO THE MAXIMUM EXTENT PRACTICABLE.
- BURNING OF ANY MATERIAL IS NOT ALLOWED WITHIN PROJECT BOUNDARY.
- DO NOT DISTURB AREAS OUTSIDE OF THE CONSTRUCTION LIMITS. DELINEATE AREAS NOT TO BE DISTURBED PRIOR TO STARTING GROUND DISTURBING ACTIVITIES. IF IT BECOMES NECESSARY TO DISTURB AREAS OUTSIDE OF THE CONSTRUCTION LIMITS OBTAIN WRITTEN PERMISSION FROM THE PROJECT ENGINEER PRIOR TO PROCEEDING. PRESERVE ALL NATURAL BUFFERS SHOWN ON THE PLANS.
- ROUTE STORMWATER AROUND UNSTABILIZED AREAS OF THE SITE WHENEVER FEASIBLE. PROVIDE EROSION CONTROL AND VELOCITY DISSIPATION DEVICES AS NEEDED TO KEEP CHANNELS FROM ERODING AND TO PREVENT NUISANCE CONDITIONS AT THE OUTLET.
- DIRECT DISCHARGES FROM BMPS TO VEGETATED AREAS WHENEVER FEASIBLE. PROVIDE VELOCITY DISSIPATION DEVICES AS NEEDED TO PREVENT EROSION.
- THE EROSION PREVENTION AND SEDIMENT CONTROL BMPS SHALL BE PLACED AS NECESSARY TO MINIMIZE EROSION FROM DISTURBED SURFACES AND TO CAPTURE SEDIMENT ON SITE. ALL EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO COMMENCEMENT OF ANY REMOVAL WORK AND/OR GROUND DISTURBING ACTIVITIES COMMENCE.
- ESTABLISH SEDIMENT CONTROL DEVICES ON ALL DOWN GRADIENT PERIMETERS AND UPGRADIENT OF ANY BUFFER ZONES BEFORE ANY UP GRADIENT LAND DISTURBING ACTIVITIES BEGIN. MAINTAIN SEDIMENT CONTROL DEVICES UNTIL CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- LOCATE PERIMETER CONTROL ON THE CONTOUR TO CAPTURE OVERLAND, LOW- VELOCITY SHEET FLOWS DOWN GRADIENT OF ALL EXPOSED SOILS AND PRIOR TO DISCHARGING TO SURFACE WATERS. PLACE J-HOOKS AT A MAXIMUM OF 100 FOOT INTERVALS.
- PROVIDE PERIMETER CONTROL AROUND ALL STOCKPILES. PLACE BMP A MINIMUM 5 FEET FROM THE TOE OF SLOPE WHERE FEASIBLE. DO NOT PLACE STOCKPILES IN NATURAL BUFFER AREAS, SURFACE WATERS OR STORMWATER CONVEYANCES.
- FLOATING SILT CURTAIN IS ALLOWED AS PERIMETER CONTROL FOR IN WATER WORK ONLY. INSTALL THE FLOATING SILT CURTAIN AS CLOSE TO SHORE AS POSSIBLE. PLACE PERIMETER CONTROL BMP ON LAND IMMEDIATELY AFTER THE IN WATER WORK IS COMPLETED.
- DITCH CHECKS WILL BE PLACED AS INDICATED ON THE PLANS DURING ALL PHASES OF CONSTRUCTION.

- PROTECT STORM SEWER INLETS AT ALL TIMES WITH THE APPROPRIATE INLET PROTECTION FOR EACH SPECIFIC PHASE OF CONSTRUCTION. PROVIDE INLET PROTECTION DEVICES WITH EMERGENCY OVERFLOW CAPABILITIES. SILT FENCE PLACED IN THE INLET GRATE IS NOT AN ACCEPTABLE INLET PROTECTION BMP FOR GRADING OPERATIONS. SILT FENCE PLACED IN THE GRATE IS ONLY ALLOWED FOR SHORT INTERVALS DURING MILLING OR PAVING OPERATIONS. INLET PROTECTION DEVICES MAY NEED TO BE PLACED MULTIPLE TIMES IN THE SAME LOCATION OVER THE LIFE OF THE CONTRACT. INLET PROTECTION DEVICES WILL BE PAID FOR ONCE PER INLET REGARDLESS OF THE NUMBER OF TIMES THE BMP IS PLACED. KEEP ALL STORM SEWER INLET PROTECTION DEVICES IN GOOD FUNCTIONAL CONDITION AT ALL TIMES. REPLACE INLET PROTECTION DEVICE WITH A SUITABLE ALTERNATIVE IF THE PROJECT ENGINEER DEEMS AN INLET PROTECTION DEVICE TO BE NONFUNCTIONAL, IN POOR CONDITION, INEFFECTIVE, OR NOT APPROPRIATE FOR THE CURRENT CONSTRUCTION ACTIVITIES. THERE WILL BE NO COST TO MINNEAPOLIS FOR REPLACEMENT OF INLET PROTECTION DEVICES.
- PLACE CONSTRUCTION EXITS, AS NECESSARY, TO PREVENT TRACKING OF SEDIMENT ONTO PAVED SURFACES BOTH ON AND OFF THE PROJECT SITE. PROVIDE CONSTRUCTION EXITS OF SUFFICIENT SIZE TO PREVENT TRACK OUT. MAINTAIN CONSTRUCTION EXITS WHEN EVIDENCE OF TRACKING IS DISCOVERED. REGULAR STREET SWEEPING IS NOT AN ACCEPTABLE ALTERNATIVE TO PROPER CONSTRUCTION EXIT INSTALLATION AND MAINTENANCE.
- DISCHARGE TURBID OR SEDIMENT LADEN WATER TO TEMPORARY SEDIMENT BASINS WHENEVER FEASIBLE. IN THE EVENT THAT IT IS NOT FEASIBLE TO DISCHARGE THE SEDIMENT LADEN WATER TO A TEMPORARY SEDIMENT BASIN, THE WATER MUST BE TREATED SO THAT IT DOES NOT CAUSE A NUISANCE CONDITION IN THE RECEIVING WATERS OR TO DOWNSTREAM LANDOWNERS. CLEAN OUT ALL PERMANENT STORMWATER BASINS REGARDLESS OF WHETHER USED AS TEMPORARY SEDIMENT BASINS OR TEMPORARY SEDIMENT TRAPS TO THE DESIGN CAPACITY AFTER ALL UPGRADIENT LAND DISTURBING ACTIVITY IS COMPLETED.
- PROVIDE SCOUR PROTECTION AT ANY OUTFALL OF DEWATERING ACTIVITIES.
- PROVIDE STABILIZATION IN ANY TRENCHES CUT FOR DEWATERING OR SITE DRAINING PURPOSES.

**POLLUTION PREVENTION**

- PROVIDE A SPILL KIT AT EACH WORK LOCATION ON THE SITE.
- STORE ALL BUILDING MATERIALS THAT HAVE THE POTENTIAL TO LEACH POLLUTANTS, PESTICIDES, HERBICIDES, INSECTICIDES, FERTILIZERS, TREATMENT CHEMICALS, AND LANDSCAPE MATERIALS UNDER COVER AND WITH SECONDARY CONTAINMENT.
- PROVIDE A SECURE STORAGE AREA WITH RESTRICTED ACCESS FOR ALL HAZARDOUS MATERIALS AND TOXIC WASTE. RETURN ALL HAZARDOUS MATERIALS AND TOXIC WASTE TO THE DESIGNATED STORAGE AREA AT THE END OF THE BUSINESS DAY UNLESS INFEASIBLE. STORE ALL HAZARDOUS MATERIALS AND TOXIC WASTE (INCLUDING BUT NOT LIMITED TO OIL, DIESEL FUEL, GASOLINE, HYDRAULIC FLUIDS, PAINT, PETROLEUM BASED PRODUCTS, WOOD PRESERVATIVES, ADDITIVES, CURING COMPOUNDS, AND ACIDS) IN SEALED CONTAINERS WITH SECONDARY CONTAINMENT. CLEAN UP SPILLS IMMEDIATELY.
- STORE, COLLECT AND DISPOSE OF ALL SOLID WASTE.
- POSITION ALL PORTABLE TOILETS SO THAT THEY ARE SECURE AND CANNOT BE TIPPED OR KNOCKED OVER. PROPERLY DISPOSE OF ALL SANITARY WASTE.
- FUEL AND MAINTAIN VEHICLES IN A DESIGNATED CONTAINED AREA WHENEVER FEASIBLE. USE DRIP PANS OR ABSORBENT MATERIALS TO PREVENT SPILLS OR LEAKED CHEMICALS FROM DISCHARGING TO SURFACE WATER OR STORMWATER CONVEYANCES. PROVIDE A SPILL KIT AT EACH LOCATION THAT VEHICLES AND EQUIPMENT ARE FUELED OR MAINTAINED AT.
- LIMIT VEHICLE AND EQUIPMENT WASHING TO A DEFINED AREA OF THE SITE. CONTAIN RUNOFF FROM THE WASHING AREA TO A TEMPORARY SEDIMENT BASIN OR OTHER EFFECTIVE CONTROL. PROPERLY DISPOSE OF ALL WASTE GENERATED BY VEHICLE AND EQUIPMENT WASHING. ENGINE DEGREASING IS NOT ALLOWED ON THE SITE.
- PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OF CONCRETE, STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS. LIQUID AND SOLID WASHOUT WASTES MUST NOT CONTACT THE GROUND. DESIGN THE CONTAINMENT SO THAT IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR CONTAINMENT AREA.
- CREATE AND FOLLOW A WRITTEN DISPOSAL PLAN FOR ALL WASTE MATERIALS. INCLUDE IN THE PLAN HOW THE MATERIAL WILL BE DISPOSED OF AND THE LOCATION OF THE DISPOSAL SITE. SUBMIT PLAN TO THE ENGINEER.
- USE METHODS AND OPERATIONAL PROCEDURES THAT PREVENT DISCHARGE OR PLACEMENT OF BITUMINOUS GRINDINGS, CUTTINGS, MILLINGS, AND OTHER BITUMINOUS WASTES FROM AREAS OF EXISTING OR FUTURE VEGETATED SOILS AND FROM ALL WATER CONVEYANCE SYSTEMS, INCLUDING INLETS, DITCHES AND CURB FLOW LINES.
- USE METHODS AND OPERATIONAL PROCEDURES THAT PREVENT CONCRETE DUST, PARTICLES, CONCRETE WASH OUT, AND OTHER CONCRETE WASTES FROM LEAVING MNDOT RIGHT OF WAY, DEPOSITING IN EXISTING OR FUTURE VEGETATED AREAS, AND FROM ENTERING STORMWATER CONVEYANCE SYSTEMS, INCLUDING INLETS, DITCHES AND CURB FLOW LINES. USE METHODS AND OPERATIONAL PROCEDURES THAT PREVENT SAW CUT SLURRY AND PLANING WASTE FROM LEAVING MNDOT RIGHT OF WAY AND FROM ENTERING STORMWATER CONVEYANCE SYSTEMS INCLUDING DITCHES AND CULVERTS.

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SURVEY	RPV
DRAWN	TSM
DESIGNED	GAJ
CHECKED	CWL
APPROVED	LFG
PROJ. NO.	19060289

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: TYLER J MOLETTE  
 SIGNATURE:   
 DATE: 8-4-15 LIC. NO. 51774



SWPPP NARRATIVE

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET	60
TOTAL	88

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE COPYRIGHTS TO ALL DESIGNS AND DRAWINGS ARE THE PROPERTY OF STANTEC. RE PRODUCTION OR USE FOR ANY PURPOSE OTHER THAN THAT AUTHORIZED BY STANTEC IS PROHIBITED.

## STORM WATER POLLUTION PREVENTION PLAN (SWPPP) NARRATIVE (CONTINUED)

### WATER RESOURCES NOTES

THESE NOTES ALONG WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) NARRATIVE ARE INTENDED TO GIVE INFORMATION ON CRITICAL DRAINAGE FEATURES, NATURAL RESOURCES AND CONTRACTOR OPERATIONS THAT MAY IMPACT DRAINAGE AND NATURAL RESOURCES.

1. THE SIZE AND ELEVATION OF CULVERTS, STORM SEWER PIPES, CATCH BASINS, PONDS, INFILTRATION/FILTRATION BASINS, PERMEABLE DITCH BLOCKS AND OVERFLOW DEVICES HAVE BEEN SPECIFICALLY DESIGNED TO CONFORM TO MNDOT DESIGN STANDARDS, MINNESOTA POLLUTION CONTROL AGENCY (MPCA) AND WATERSHED DISTRICT PERMIT REQUIREMENTS. THE DESIGN COMPUTATIONS ARE ON FILE WITH MINNEAPOLIS METRO WATER RESOURCES. CHANGING THESE ITEMS OR THE DIRECTION OF FLOW FROM WHAT IS SHOWN ON THE PLANS MAY CAUSE PROBLEMS OFF THE PROJECT AND COULD MEAN THE PROJECT IS OUT OF COMPLIANCE WITH APPROVED DRAINAGE PERMITS. ANY CHANGES TO THE SIZE, ELEVATION OR DIRECTION OF FLOW OF THE DRAINAGE SYSTEM MUST BE APPROVED BY THE METRO WATER RESOURCES DESIGNER.

2. SUBSOIL ALL DISTURBED GREEN SPACES EXCEPT AS LISTED IN 2574.3A.2.

3. PERFORM POST INSTALLATION MANDREL TESTING OF ALL PLASTIC PIPE.

4. ANY SUBSURFACE DRAINAGE TILES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED, REPLACED OR REROUTED, AND CONNECTED TO THE EXISTING TILE OR DRAINAGE SYSTEM TO ENSURE THAT EXISTING UPLAND DRAINAGE IS PERPETUATED. THIS SHOULD BE DONE TO THE APPROVAL AND SATISFACTION OF THE ENGINEER.

5. THE FOLLOWING WATER RELATED PERMITS APPLY TO THIS PROJECT:

AGENCY	TYPE OF PERMIT
MINNESOTA POLLUTION CONTROL AGENCY (MPCA)	NPDES CONSTRUCTION PERMIT

REVIEW ALL PERMITS FOR ANY SPECIAL CONDITIONS THAT WILL EFFECT CONSTRUCTION OF THE PROJECT.

TEMPORARY DEWATERING ACTIVITIES MAY BE REQUIRED FOR ROADWAY CONSTRUCTION AND UTILITY WORK. THEREFORE IT IS POSSIBLE THAT A PERMIT FOR THE TEMPORARY APPROPRIATION OF WATERS OF THE STATE, NON-IRRIGATION FROM MNDNR WILL BE REQUIRED FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING THIS PERMIT PRIOR TO COMMENCING DEWATERING ACTIVITIES. ALL TEMPORARY DEWATERING SHALL BE DISCHARGED TO AN APPROVED LOCATION FOR TREATMENT PRIOR TO DISCHARGE TO THE RECEIVING WATER. SUBMIT A SITE MANAGEMENT PLAN TO THE ENGINEER FOR APPROVAL PRIOR TO COMMENCING WORK.

### LANDSCAPE NOTES

- FILTER LOGS SHALL BE PLACED, AS NEEDED, TO TRAP SEDIMENT ON THE LOWER EDGE OF BEDS OR TREE HOLES. FILTER LOGS WILL BE LEFT TO PHOTO DEGRADE.
- TILLING FOR BEDS OR TREE HOLES MUST BE PLANTED AND MULCHED WITH WOOD CHIP WITHIN 7 DAYS OR STRAW MULCHED UNTIL PLANTING OPERATIONS CAN BE COMPLETED.
- ANY POND CORNERS OPENED DUE TO TILLING FOR SHRUB BEDS OR TREE HOLES MUST BE PLANTED AND MULCHED WITH WOOD CHIP WITHIN 24 HOURS OR STRAW MULCHED UNTIL PLANTING OPERATIONS CAN BE COMPLETED.

Plot Date: 10/27/2015 10:40am  
 Program: 10/27/2015 10:40am  
 User: 10/27/2015 10:40am  
 Path: C:\Users\jmollete\Documents\Projects\150620057\_C200.dwg

NO.	DATE	APPR.	REVISION

SURVEY	MPV
DRAWN	TJM
DESIGNED	GAJ
CHECKED	CWL
APPROVED	LEP
PROJ. NO.	150620057

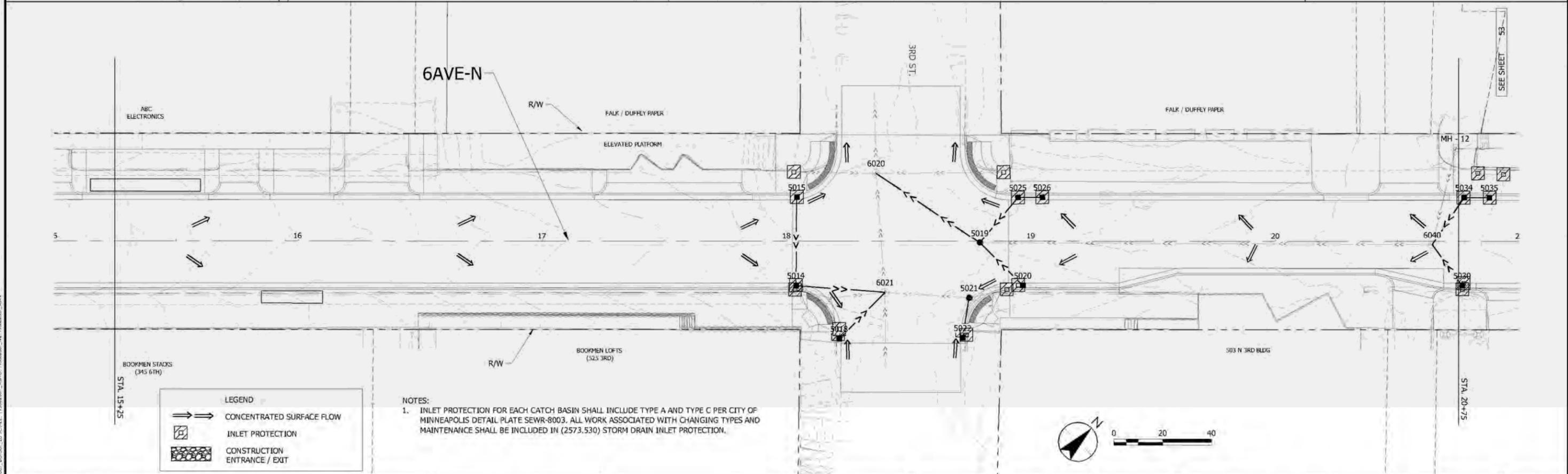
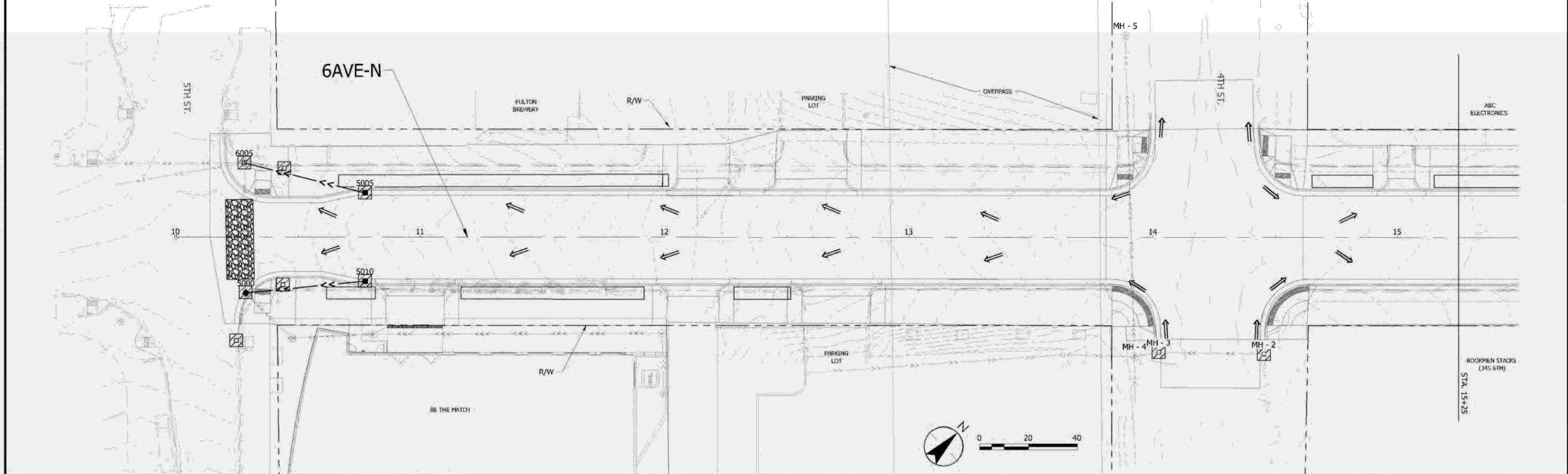
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: TYLER J. MOLETTE  
 SIGNATURE:   
 DATE: 8-4-15 LIC. NO.: 51774



SWPPP NARRATIVE

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING.



LEGEND	
	CONCENTRATED SURFACE FLOW
	INLET PROTECTION
	CONSTRUCTION ENTRANCE / EXIT

NOTES:  
 1. INLET PROTECTION FOR EACH CATCH BASIN SHALL INCLUDE TYPE A AND TYPE C PER CITY OF MINNEAPOLIS DETAIL PLATE SEWR-8003. ALL WORK ASSOCIATED WITH CHANGING TYPES AND MAINTENANCE SHALL BE INCLUDED IN (2573.530) STORM DRAIN INLET PROTECTION.

NO.	DATE	APPR.	REVISION

SURVEY	MPV	DATE	12/15/2011
DRAWN	JDP	DATE	12/15/2011
DESIGNED	JDP	DATE	12/15/2011
CHECKED	CIVL	DATE	12/15/2011
APPROVED	LPG	DATE	12/15/2011
PROJECT NO.	19360288	DATE	12/15/2011

MINNEAPOLIS City of Lakes

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TEMPORARY EROSION CONTROL PLAN

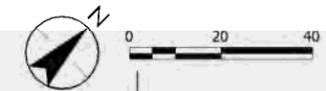
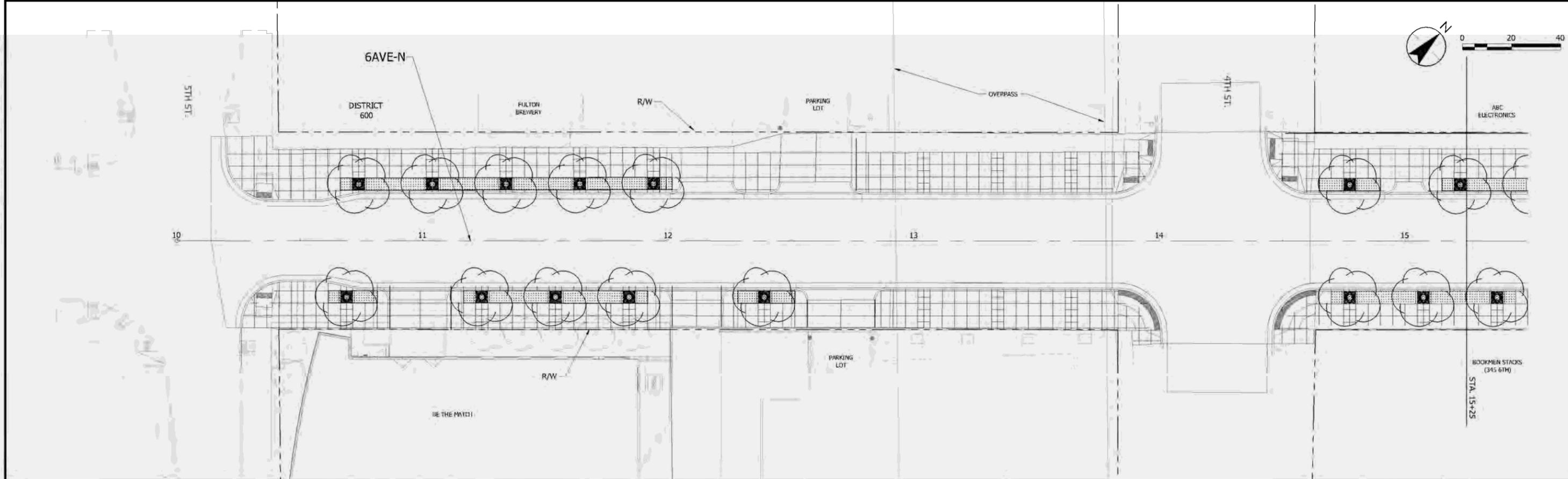
CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET
63
88

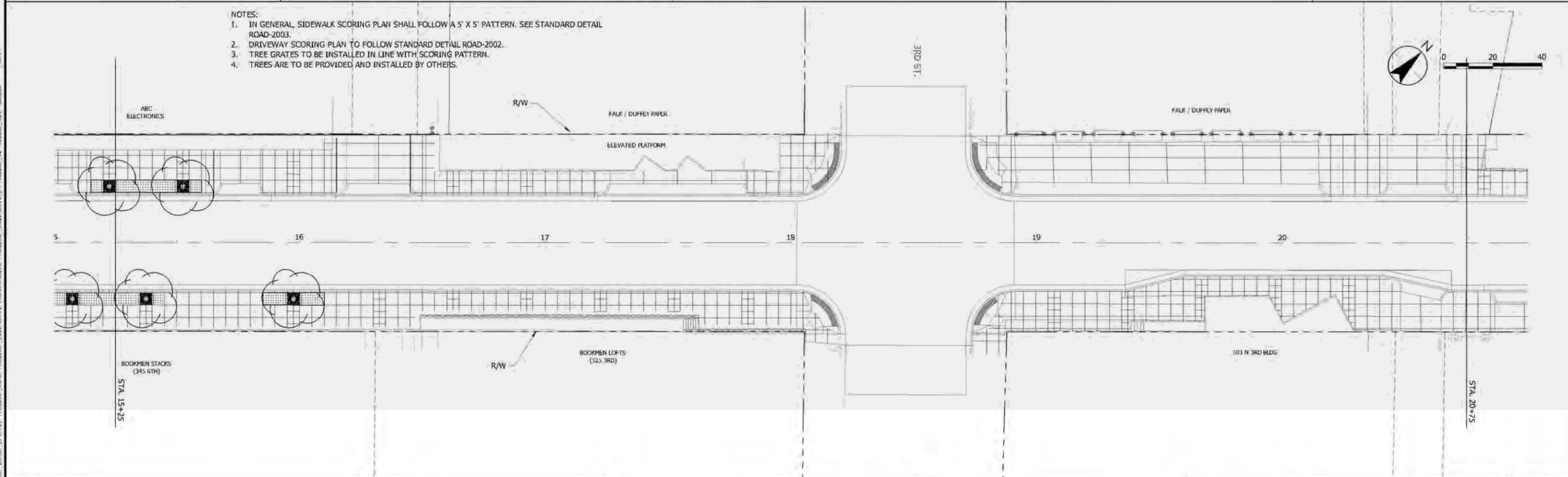
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THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE DRAWING TO ALL DIMENSIONS AND DIMENSIONS ARE PROPORTIONAL TO THE PRODUCTION. DO NOT USE ANY DIMENSIONS OTHER THAN THOSE AUTHORIZED BY THE CONTRACTOR.



- NOTES:
1. IN GENERAL, SIDEWALK SCORING PLAN SHALL FOLLOW A 5' X 5' PATTERN. SEE STANDARD DETAIL ROAD-2003.
  2. DRIVEWAY SCORING PLAN TO FOLLOW STANDARD DETAIL ROAD-2002.
  3. TREE GRATES TO BE INSTALLED IN LINE WITH SCORING PATTERN.
  4. TREES ARE TO BE PROVIDED AND INSTALLED BY OTHERS.



NO.	DATE	APPR.	REVISION

SURVEY	MPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: TYLER J. MOLETTE SIGNATURE: <i>[Signature]</i> DATE: 8-4-18 LIC. NO.: 51774
DRAWN	TJM	
DESIGNED	TJM	
CHECKED	CIVL	
APPROVED	LPG	
PROJ. NO.	19362887	

Minneapolis City of Lakes

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STREETScape PLAN

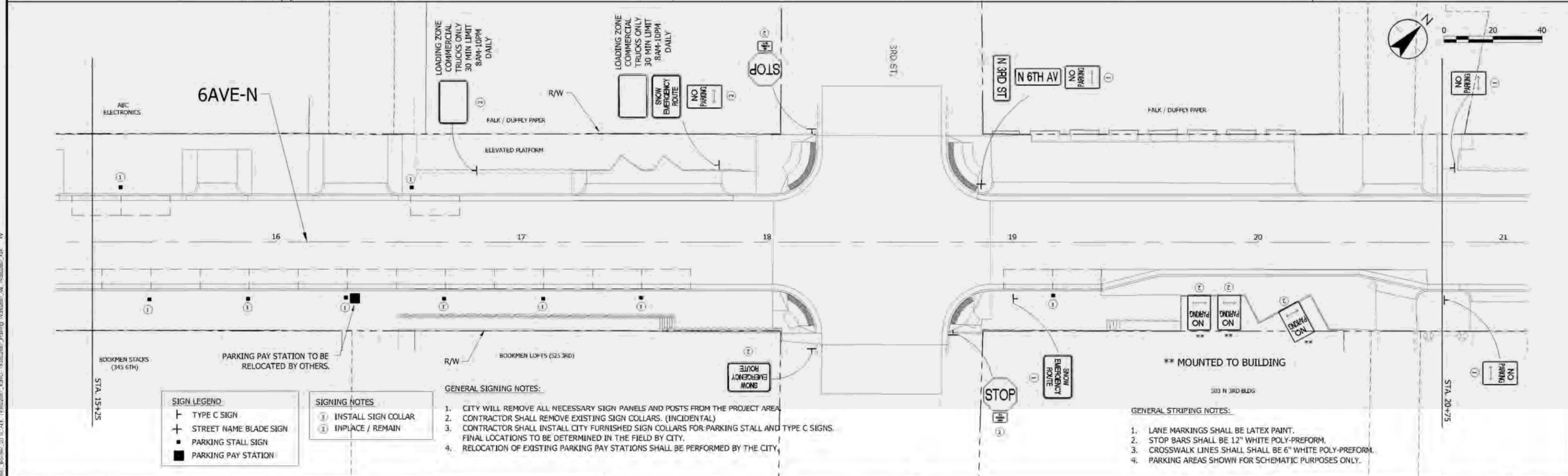
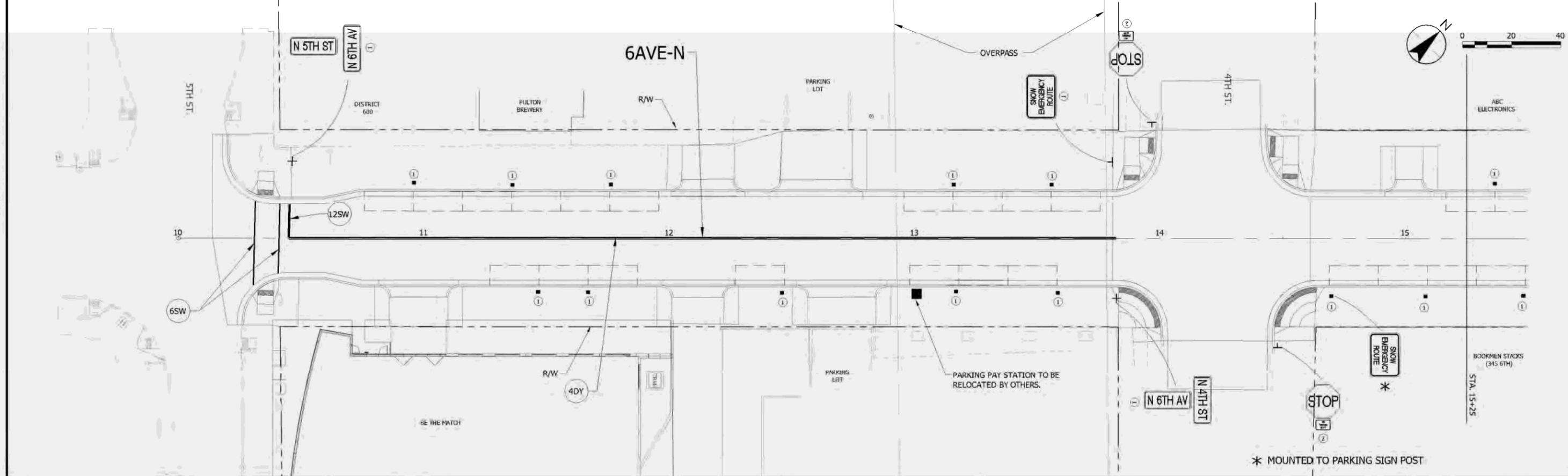
CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005

SHEET  
65  
88





THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DIMENSIONS SHOWN ON THIS PLAN.



**SIGN LEGEND**

+	TYPE C SIGN
+	STREET NAME BLADE SIGN
■	PARKING STALL SIGN
■	PARKING PAY STATION

**SIGNING NOTES**

①	INSTALL SIGN COLLAR
②	INPLACE / REMAIN

- GENERAL SIGNING NOTES:**
1. CITY WILL REMOVE ALL NECESSARY SIGN PANELS AND POSTS FROM THE PROJECT AREA.
  2. CONTRACTOR SHALL REMOVE EXISTING SIGN COLLARS. (INCIDENTAL)
  3. CONTRACTOR SHALL INSTALL CITY FURNISHED SIGN COLLARS FOR PARKING STALL AND TYPE C SIGNS. FINAL LOCATIONS TO BE DETERMINED IN THE FIELD BY CITY.
  4. RELOCATION OF EXISTING PARKING PAY STATIONS SHALL BE PERFORMED BY THE CITY.

- GENERAL STRIPING NOTES:**
1. LANE MARKINGS SHALL BE LATEX PAINT.
  2. STOP BARS SHALL BE 12" WHITE POLY-PREFORM.
  3. CROSSWALK LINES SHALL BE 6" WHITE POLY-PREFORM.
  4. PARKING AREAS SHOWN FOR SCHEMATIC PURPOSES ONLY.

NO.	DATE	APPR.	REVISION

SURVEY	MPV
DRAWN	JDP
DESIGNED	JDP
CHECKED	CWL
APPROVED	LFG
PROJ. NO.	19562881

MINNEAPOLIS CITY OF LAKES

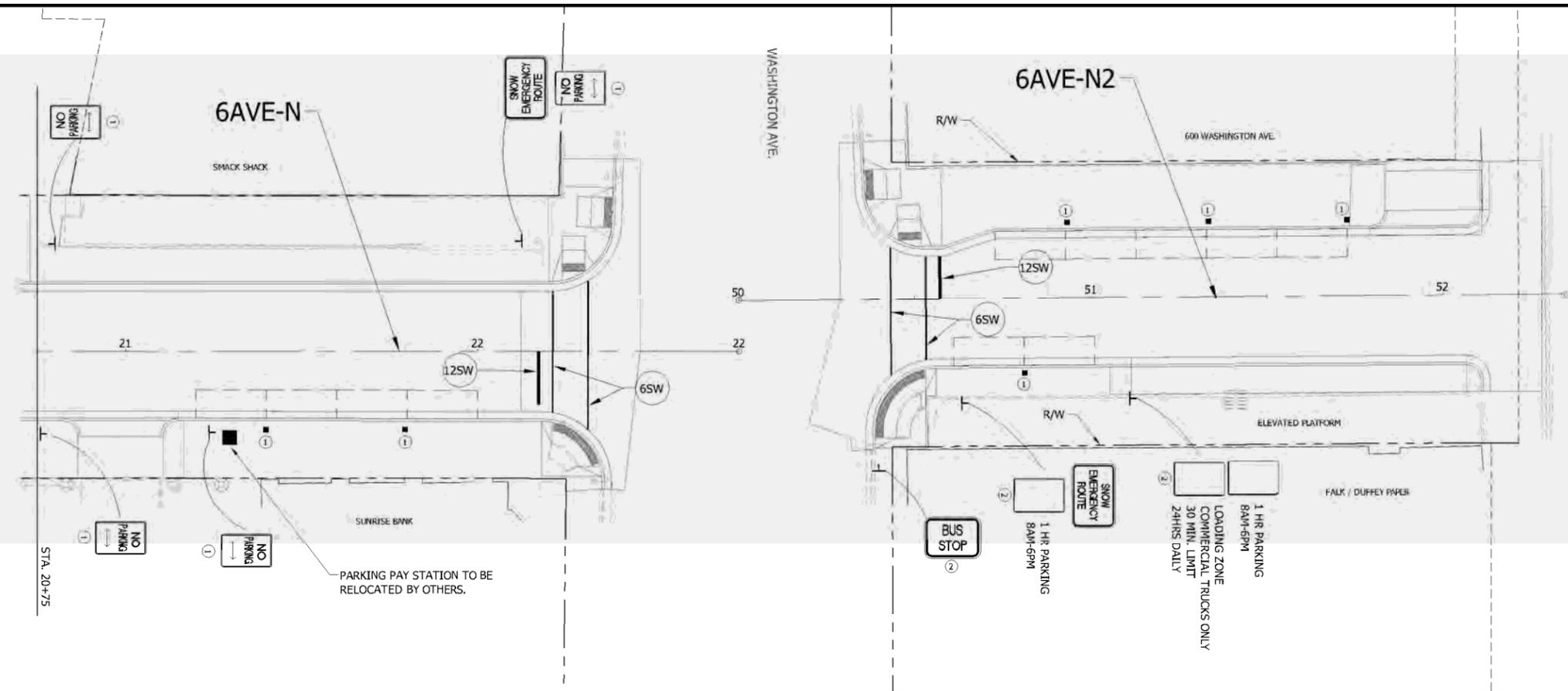
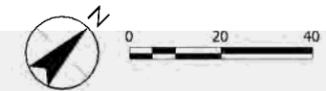
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SIGNING AND PAVEMENT MARKING PLAN

CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
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**SIGN LEGEND**

├	TYPE C SIGN
+	STREET NAME BLADE SIGN
■	PARKING STALL SIGN
■	PARKING PAY STATION

**SIGNING NOTES**

①	INSTALL SIGN COLLAR
②	INPLACE / REMAIN

- GENERAL SIGNING NOTES:**
1. CITY WILL REMOVE ALL NECESSARY SIGN PANELS AND POSTS FROM THE PROJECT AREA.
  2. CONTRACTOR SHALL REMOVE EXISTING SIGN COLLARS. (INCIDENTAL)
  3. CONTRACTOR SHALL INSTALL CITY FURNISHED SIGN COLLARS FOR PARKING STALL AND TYPE C SIGNS. FINAL LOCATIONS TO BE DETERMINED IN THE FIELD BY CITY.
  4. RELOCATION OF EXISTING PARKING PAY STATIONS SHALL BE PERFORMED BY THE CITY.

- GENERAL STRIPING NOTES:**
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  2. STOP BARS SHALL BE 12" WHITE POLY-PREFORM.
  3. CROSSWALK LINES SHALL BE 6" WHITE POLY-PREFORM.
  4. PARKING AREAS SHOWN FOR SCHEMATIC PURPOSES ONLY.

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 Plot Scale: 1:1  
 Plot User: jay  
 Plot Title: 10250000\_6TH AVENUE N\_10250000.dwg  
 Plot Sheet: 1 of 1

NO.	DATE	APPR.	REVISION

SURVEY	JEFF
DRAWN	JOP
DESIGNED	JOP
CHECKED	CWL
APPROVED	JLP
PROJECT NO.	10250000

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: JEFFREY DANESON  
 SIGNATURE: *[Signature]*  
 DATE: 8/15/15 LIC. NO.: 41964

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SIGNING AND PAVEMENT MARKING PLAN

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS, DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS, DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS, DO NOT SCALE THE DRAWING.

## NOTES

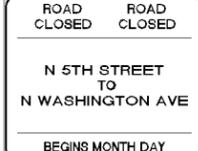
### GENERAL INFORMATION:

1. THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN THE DEVICES IN THIS DETOUR PLAN UNLESS OTHERWISE NOTED.
2. FIELD CONDITIONS MAY REQUIRE MODIFICATIONS OF THIS LAYOUT AS DEEMED NECESSARY BY THE ENGINEER.
3. ALL DISTANCES ARE APPROXIMATE.
4. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ANY WORK AREAS NEAR TRAFFIC IN ACCORDANCE WITH THE MN MUTCD.
5. IF THE CONTRACTOR DECIDES TO PERFORM THE CONSTRUCTION WORK IN A MANNER OTHER THAN SHOWN IN THIS TRAFFIC CONTROL/DETOUR PLAN, THE CONTRACTOR SHALL PROVIDE COMPLETE REVISED TRAFFIC CONTROL/DETOUR PLANS TO BE APPROVED BY THE ENGINEER.
6. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS".

### SIGNING:

1. ALL TRAFFIC CONTROL DEVICES, INCLUDING OVERHEAD SIGNS ON ROADS OPEN TO TRAFFIC THAT ARE NOT CONSISTENT WITH TRAFFIC OPERATION SHALL BE COVERED, REMOVED OR REVISED AS DIRECTED BY THE ENGINEER.
2. WHEN SIGNS ARE INSTALLED, THEY SHALL BE MOUNTED ON POSTS DRIVEN INTO THE GROUND AT THE PROPER HEIGHT AND LATERAL OFFSET AS DETAILED IN THE MNMUTCD. IF THIS IS NOT POSSIBLE THEY WILL BE MOUNTED ON PORTABLE SUPPORTS AS APPROVED BY THE ENGINEER. WHEN THE SIGNS ARE REMOVED THE SIGN POSTS SHALL ALSO BE REMOVED AS SOON AS POSSIBLE.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY EXTRA SIGNING NEEDED TO FACILITATE TRAFFIC SWITCHES OR FOR TRANSITIONING TRAFFIC FROM ONE STAGE TO ANOTHER.
4. ALL SIGNS SHALL MEET MINIMUM RETRO-REFLECTIVITY REQUIREMENTS AS OUTLINED IN THE MNMUTCD.
5. LONGITUDINAL DROPOFFS SHALL BE SIGNED AS SHOWN IN THE "TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS" FIELD MANUAL UNLESS OTHERWISE SPECIFIED IN THESE PLANS.
6. THE CONTRACTOR SHALL COORDINATE THE INSTALLATION OF THE FINAL SIGNS TO ASSURE THAT THE FINAL SIGNS ARE INSTALLED AS NEEDED, OR PROVIDE TEMPORARY SIGNING AT THEIR EXPENSE UNTIL THE FINAL SIGNING IS INSTALLED.

## SIGN LEGEND

	W20-1 (48X48)	BLACK ON ORANGE		TYPE A FLASHER
	W20-3 (48X48)	BLACK ON ORANGE		TYPE II BARRICADE
	R11-2 (48X30)	BLACK ON WHITE		TYPE III BARRICADE
	G20-X1 54X48 (INSTALL 7 DAYS PRIOR TO CLOSURE)	BLACK ON ORANGE		TRAFFIC SIGN
	R11-3A (60X30)	BLACK ON WHITE		TYPE II BARRICADE
	G20-2A (48X24)	BLACK ON ORANGE		TYPE III BARRICADE
	G20-X2 54X48 (INSTALL 7 DAYS PRIOR TO CLOSURE)	BLACK ON ORANGE		

Proj. Date: 10/27/2016 - 10:56am  
 Drawn: J. J. [unreadable]  
 Checked: [unreadable]  
 Approved: [unreadable]

NO	DATE	APPR	REVISION

SURVEY	WJV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  FIRST NAME: TYLER J. MCLEETE  SIGNATURE:   DATE: 8-6-15 LC NO: SL724
DRAWN	TJM	
DESIGNED	TJM	
CHECKED	CIVL	
APPROVED	WJV	
PROJ. NO.	173502987	





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TRAFFIC CONTROL & DETOUR PLAN

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

SHEET  
 70  
 OF  
 88

THESE INSTRUCTIONS SHALL BE READ IN CONJUNCTION WITH ALL DIMENSIONS AND NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND SCALE THE DRAWING.

**LEGEND**

 WORK AREA

\* PLACE MIN. 7 DAYS PRIOR TO CLOSURE

**A**

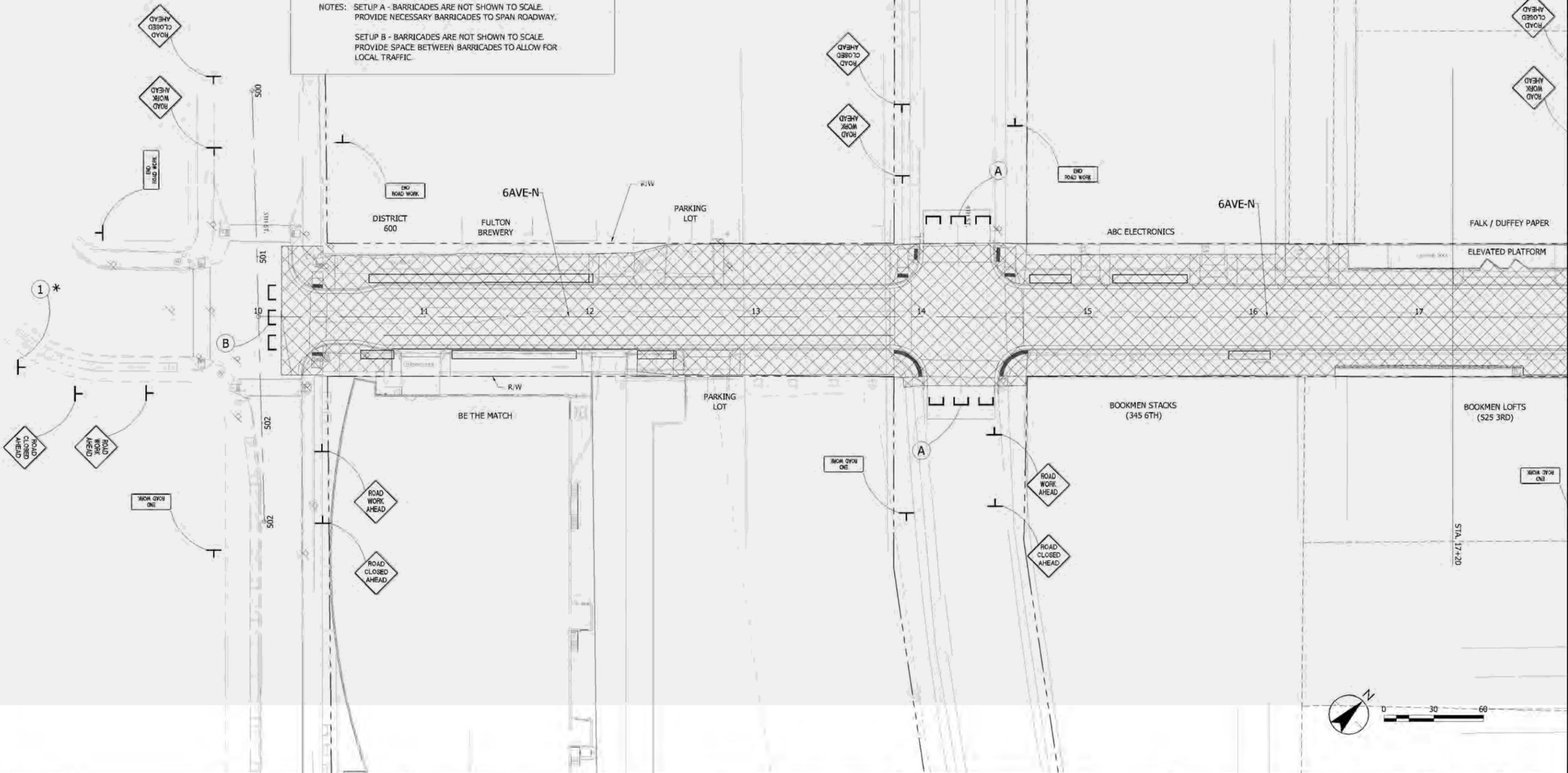


**B**



NOTES: SETUP A - BARRICADES ARE NOT SHOWN TO SCALE. PROVIDE NECESSARY BARRICADES TO SPAN ROADWAY.

SETUP B - BARRICADES ARE NOT SHOWN TO SCALE. PROVIDE SPACE BETWEEN BARRICADES TO ALLOW FOR LOCAL TRAFFIC.



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 User: 13302882\_001\_001  
 Scale: 1:1000  
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 Plot Path: C:\Users\tyler.j.mollete\AppData\Local\Temp\13302882\_001\_001\13302882\_001\_001.dwg

NO.	DATE	APPR.	REVISION

SURVEY	MPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: TYLER J. MOLETTE SIGNATURE:  DATE: 8-4-15 LIC. NO.: 51774
DRAWN	TJM	
DESIGNED	TJM	
CHECKED	CIVL	
APPROVED	EFG	
PROJ. NO.	193602882	

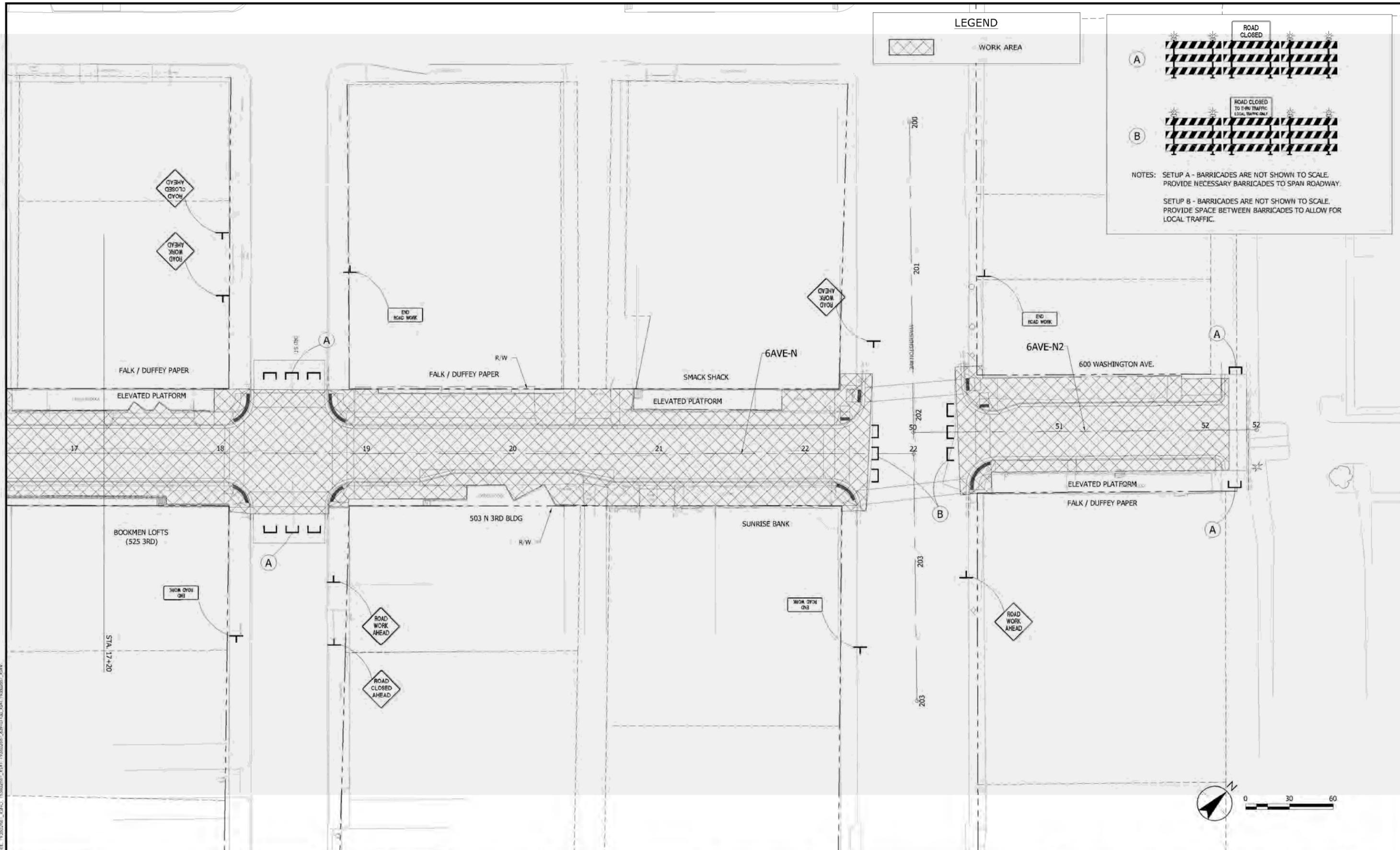
**Minneapolis**  
City of Lakes

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St. Paul, MN 55113  
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TRAFFIC CONTROL & DETOUR PLAN

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. SCALE THE DRAWING TO MATCH THE FIELD CONDITIONS. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. SCALE THE DRAWING TO MATCH THE FIELD CONDITIONS. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. SCALE THE DRAWING TO MATCH THE FIELD CONDITIONS.



**LEGEND**

WORK AREA

**A** ROAD CLOSED

**B** ROAD CLOSED TO THRU TRAFFIC LOCAL TRAFFIC ONLY

NOTES: SETUP A - BARRICADES ARE NOT SHOWN TO SCALE. PROVIDE NECESSARY BARRICADES TO SPAN ROADWAY.

SETUP B - BARRICADES ARE NOT SHOWN TO SCALE. PROVIDE SPACE BETWEEN BARRICADES TO ALLOW FOR LOCAL TRAFFIC.



NO.	DATE	APPR.	REVISION

SURVEY	MPV	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
DRAWN	TJM	
DESIGNED	TJM	
CHECKED	CIVL	
APPROVED	EPG	
PROJ. NO.	19360288	DATE: 8-4-18 LIC. NO. 51774

**Minneapolis**  
City of Lakes

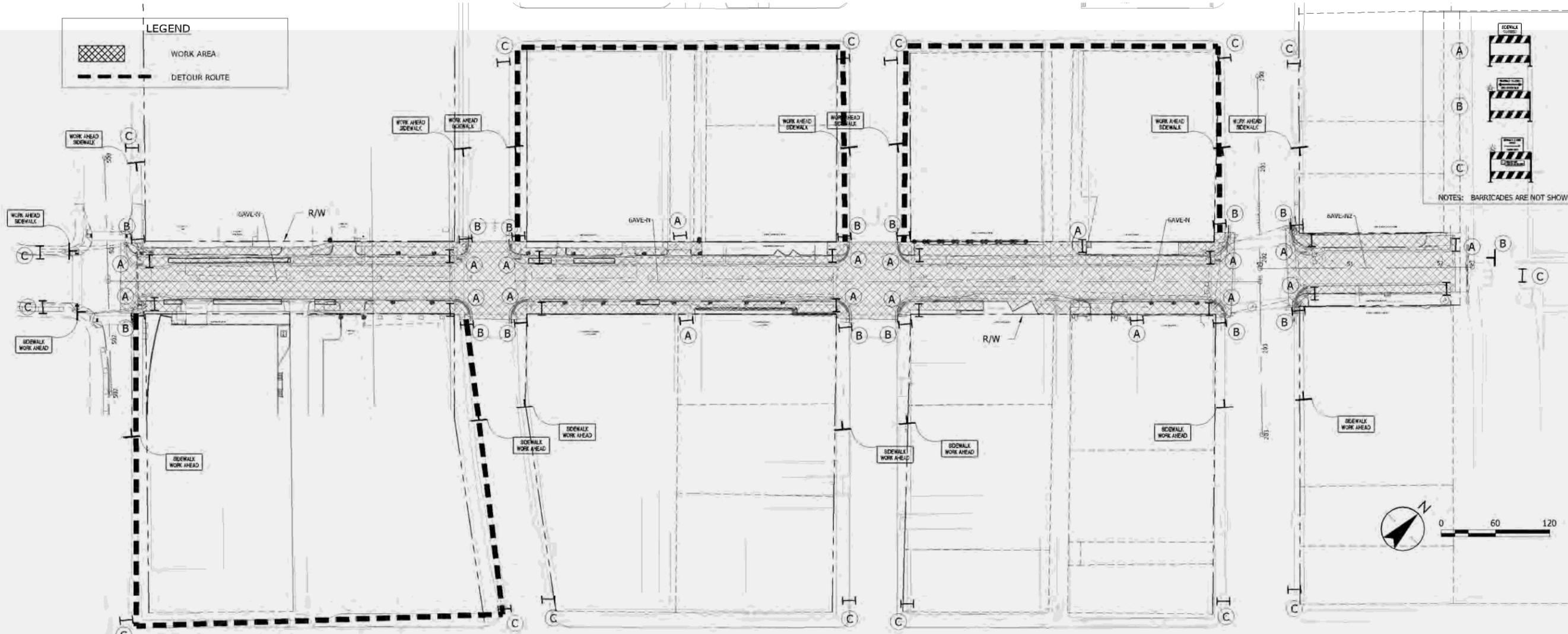
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TRAFFIC CONTROL & DETOUR PLAN

CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005

SHEET  
72  
OF  
88

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND SCALE THE DRAWING.



NOTES: BARRICADES ARE NOT SHOWN TO SCALE

**NOTES**

**GENERAL INFORMATION:**

1. PEDESTRIANS MUST BE ABLE TO CROSS THROUGH THE PROJECT ON PERPENDICULAR STREETS AT ALL TIMES OUTSIDE NORMAL WORKING HOURS.
2. DETOUR ROUTES SHOWN WILL BE UTILIZED IF PEDESTRIAN ACCESS CANNOT BE MAINTAINED ON A MINIMUM OF ONE SIDE OF N. 6TH AVE.
3. THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN THE DEVICES UNLESS OTHERWISE NOTED.
4. FIELD CONDITIONS MAY REQUIRE MODIFICATIONS OF THIS LAYOUT AS DEEMED NECESSARY BY THE ENGINEER.
5. ALL DISTANCES ARE APPROXIMATE.
6. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ANY WORK AREAS NEAR TRAFFIC IN ACCORDANCE WITH THE MN MUTCD.
7. IF THE CONTRACTOR DECIDES TO PERFORM THE CONSTRUCTION WORK IN A MANNER OTHER THAN SHOWN IN THIS TRAFFIC CONTROL/DETOUR PLAN, THE CONTRACTOR SHALL PROVIDE COMPLETE REVISED TRAFFIC CONTROL/DETOUR PLANS TO BE APPROVED BY THE ENGINEER.
8. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS".

**SIGNING:**

1. SIGNS SHALL BE INSTALLED AS DETAILED IN THE MNMUTCD.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY EXTRA SIGNING NEEDED TO FACILITATE TRAFFIC SWITCHES OR FOR TRANSITIONING TRAFFIC FROM ONE STAGE TO ANOTHER.
3. ALL SIGNS SHALL MEET MINIMUM RETRO-REFLECTIVITY REQUIREMENTS AS OUTLINED IN THE MNMUTCD.
4. TEMPORARY TRAFFIC CONTROL DEVICES AND PEDESTRIAN DEVICES SHALL BE SIGNED AS SHOWN IN THE "TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS" FIELD MANUAL UNLESS OTHERWISE SPECIFIED IN THESE PLANS.

- TYPE A FLASHER
- TYPE II BARRICADE
- TYPE III BARRICADE
- TRAFFIC SIGN
- TYPE II BARRICADE
- TYPE III BARRICADE

- SIDEWALK CLOSED R9-9 (30X18) BLACK ON WHITE
- SIDEWALK WORK AHEAD W20-1a (36X18) BLACK ON ORANGE
- SIDEWALK CLOSED USE OTHER SIDE R9-10 (24X12) BLACK ON WHITE
- D9-6 (12X12) WHITE ON BLUE
- SIDEWALK CLOSED AHEAD CROSS HERE R9-11 (48X36) BLACK ON WHITE
- ENDS WAY SIGN CONTACT 612-XXX-XXXX G20-X (30X12) BLACK ON ORANGE

NO.	DATE	APPR.	REVISION

SURVEY	MPV	DATE	8-4-15
DRAWN	TJM	DATE	8-4-15
DESIGNED	TJM	DATE	8-4-15
CHECKED	CIVL	DATE	8-4-15
APPROVED	LEP	DATE	8-4-15
PROJ. NO.	19362887	DATE	8-4-15

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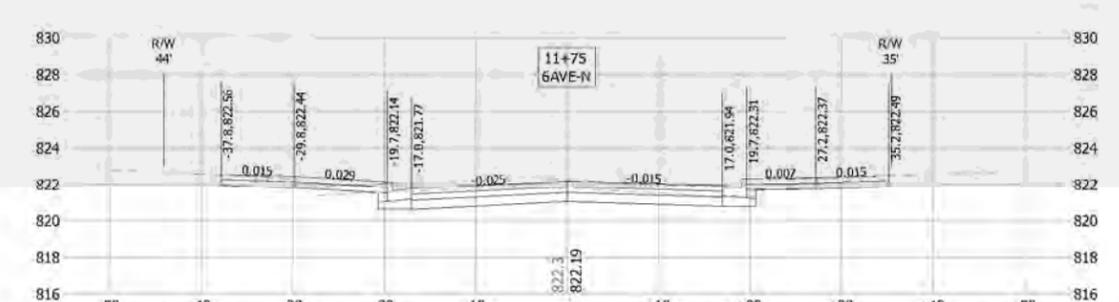
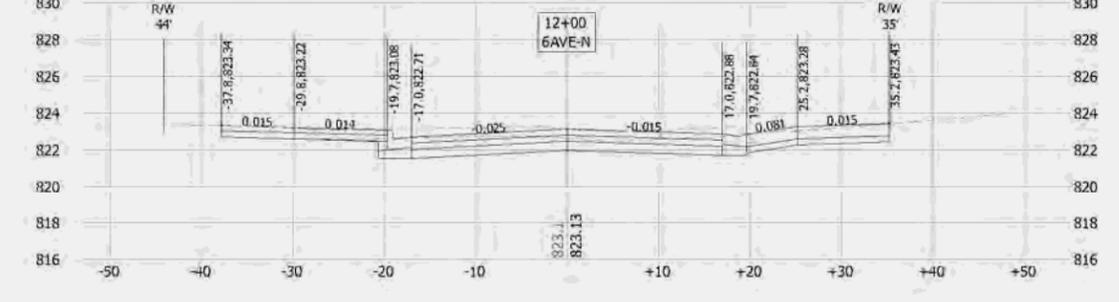
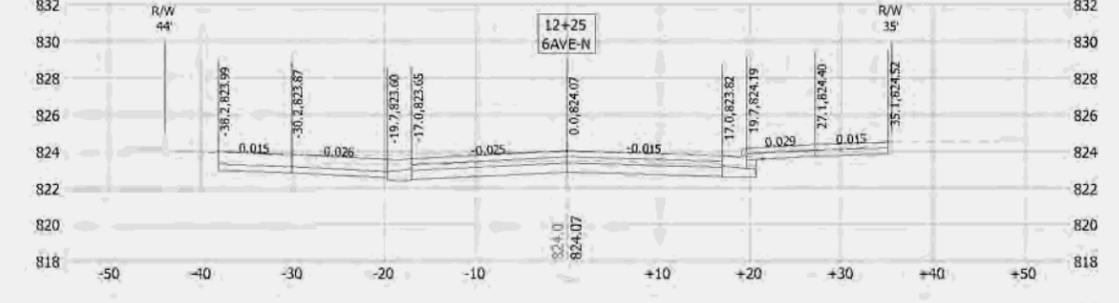
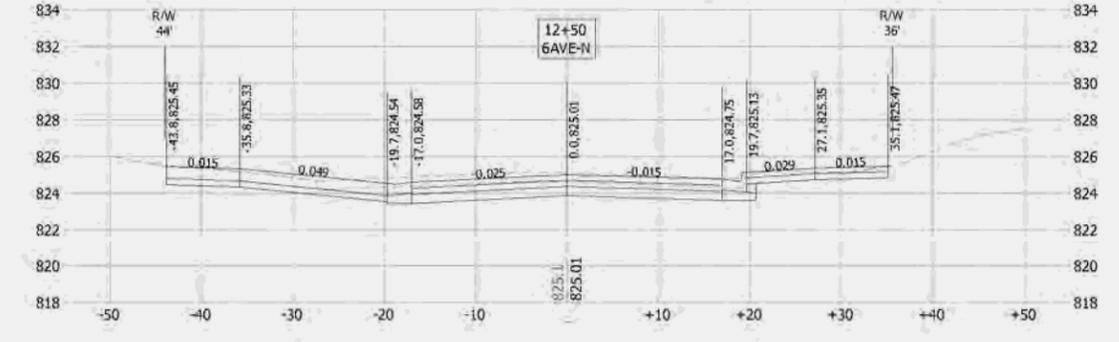
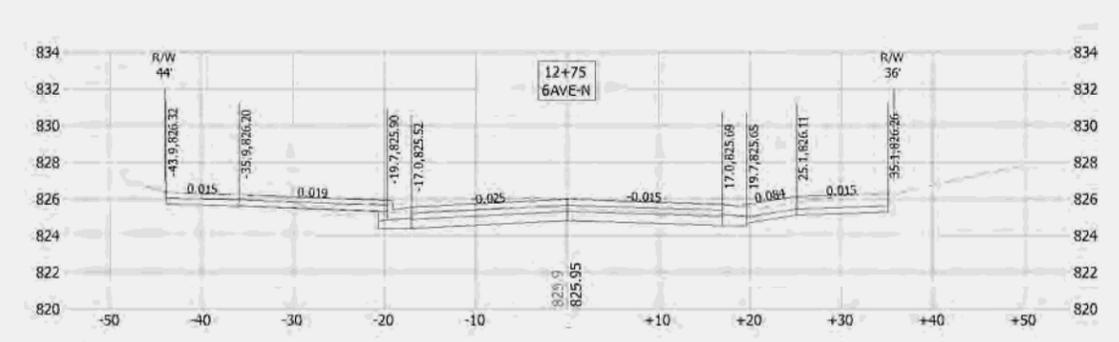
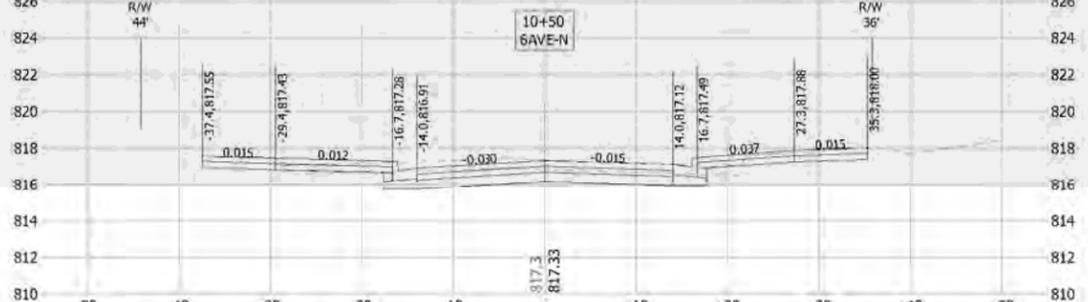
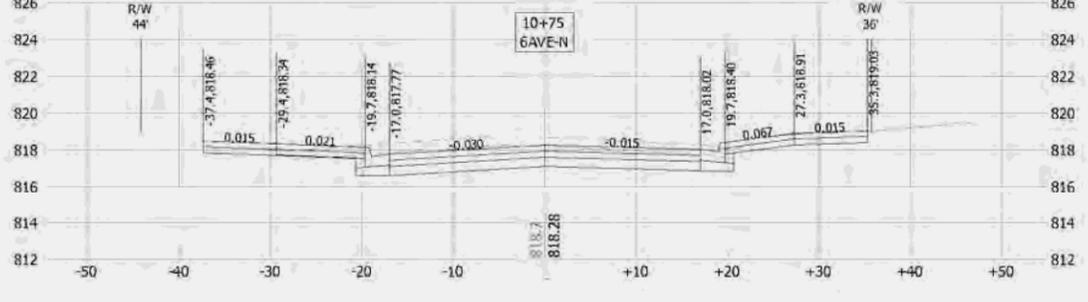
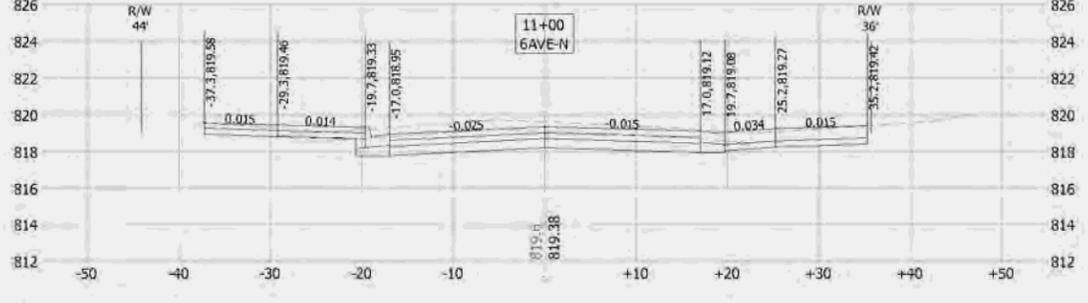
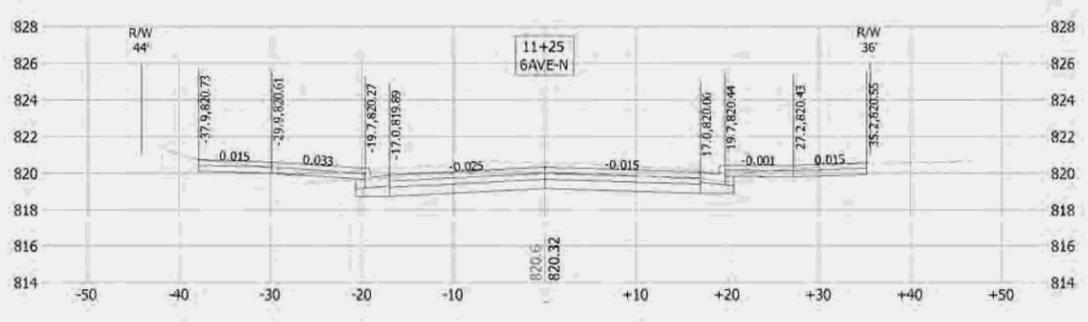
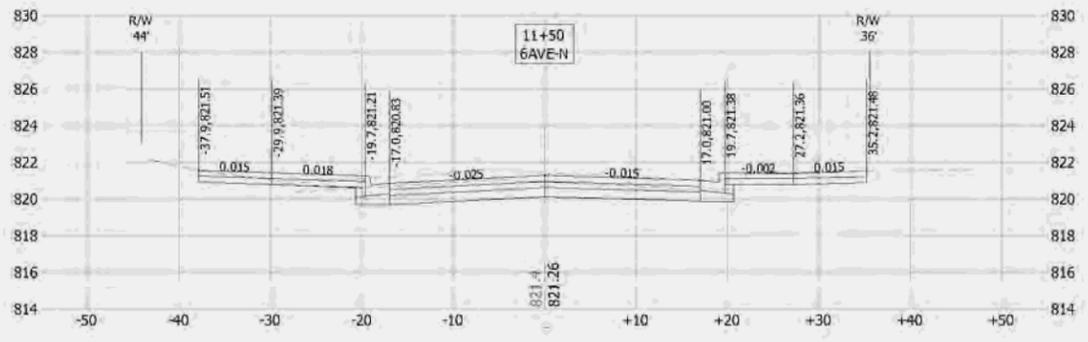
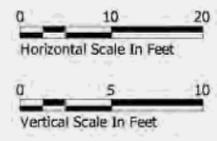
TEMPORARY PEDESTRIAN ACCESS ROUTE

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

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Plot Date: 10/27/2015 11:17am  
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 User: jgibson  
 Scale: 1"=20'

SEE PLAN SHEETS FOR LOCATION OF EXISTING AND PROPOSED UTILITIES  
 SEE PLAN SHEETS FOR INPLACE ROW LOCATIONS



NO.	DATE	APPR.	REVISION

SURVEY	MPV
DRAWN	JOP
DESIGNED	JOP
CHECKED	CIVL
APPROVED	EPG
PROJECT NO.	19362881

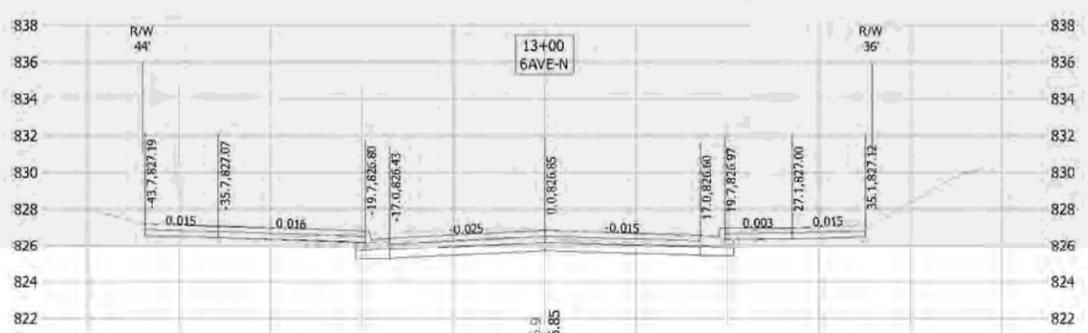
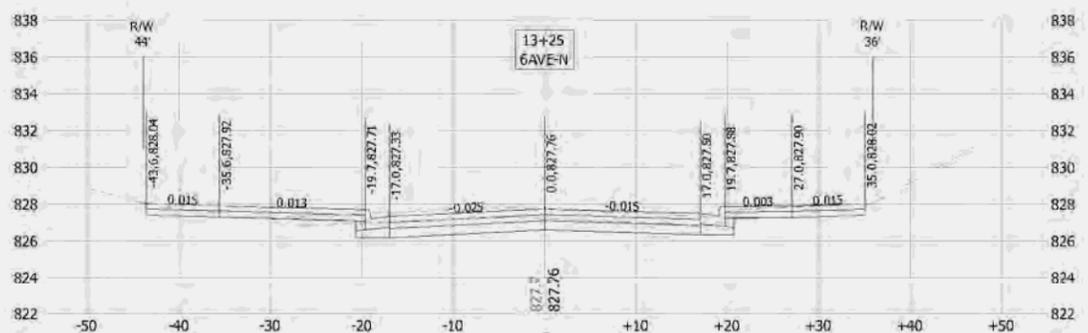
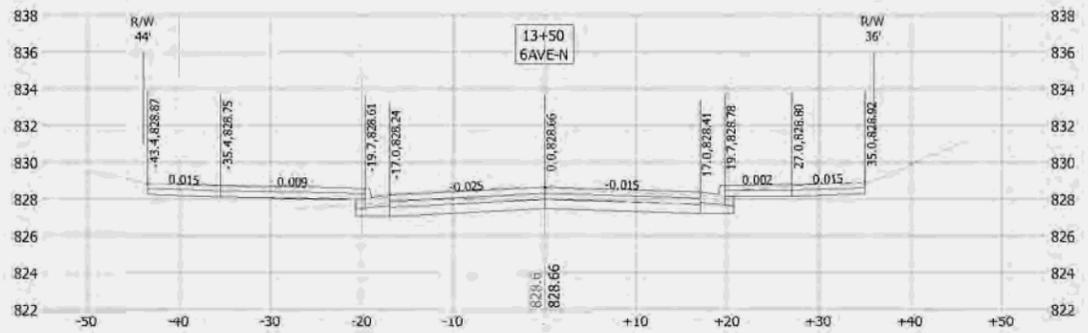
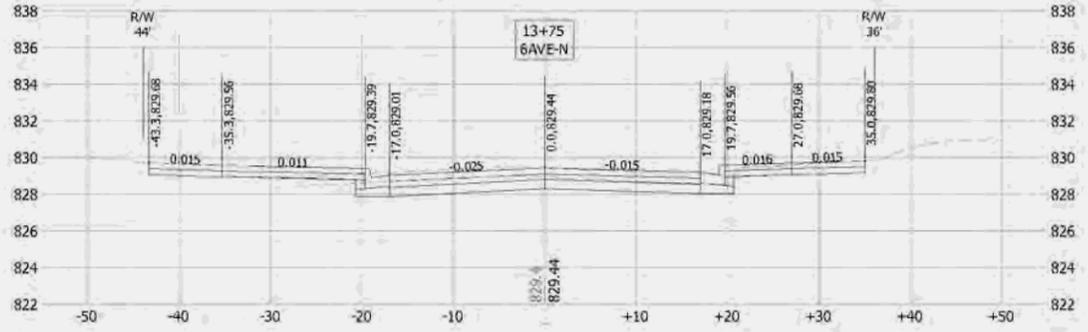
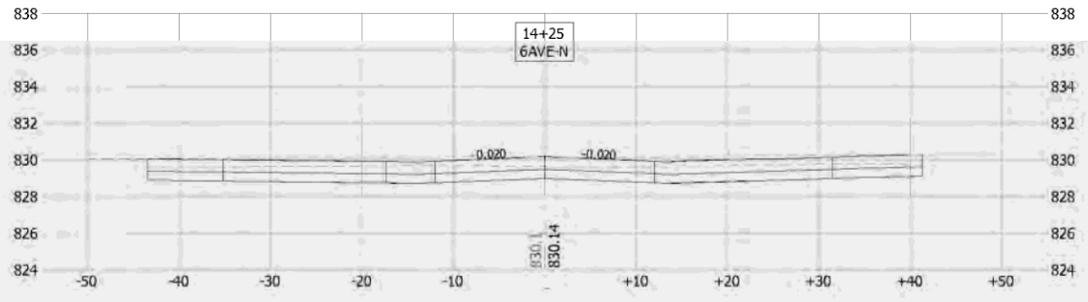
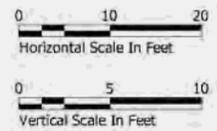
CROSS SECTIONS  
 5TH ST TO 4TH ST

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

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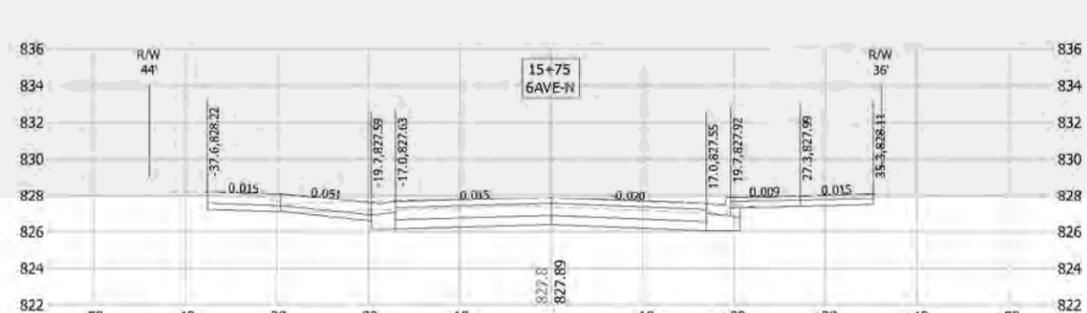
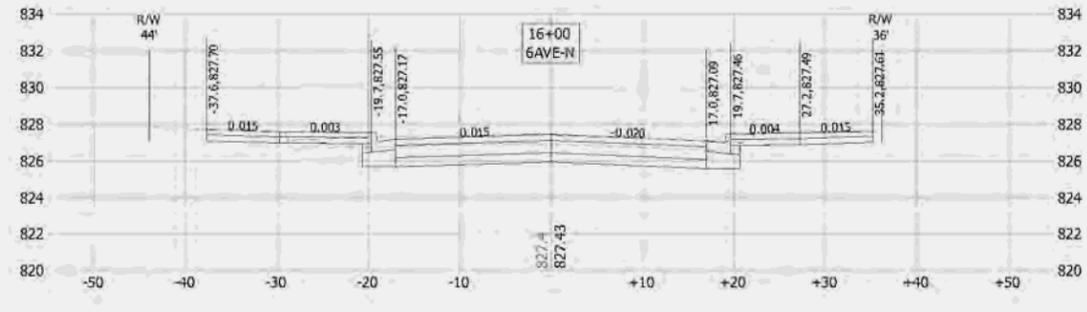
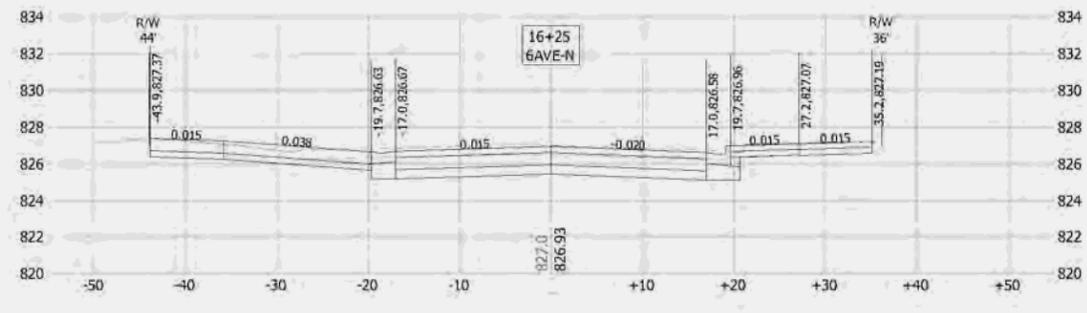
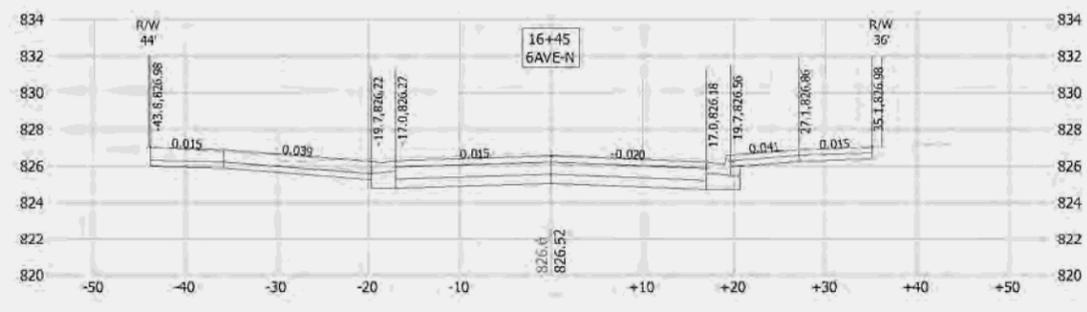
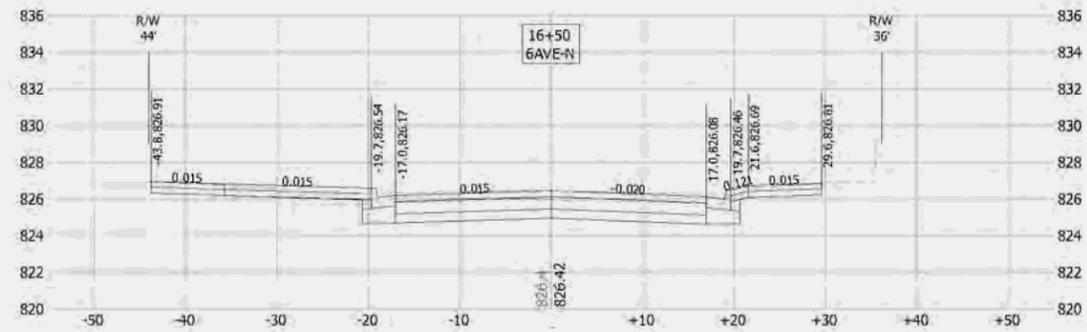
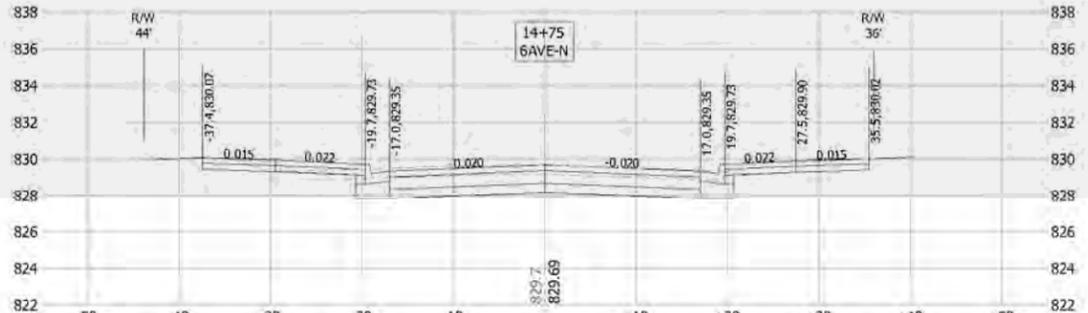
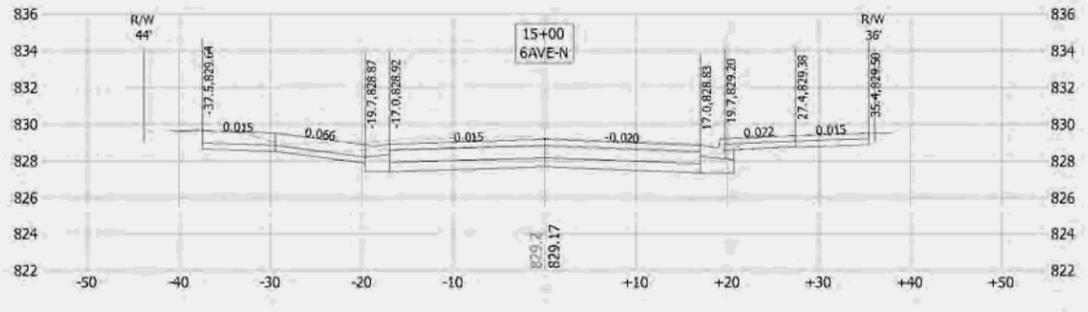
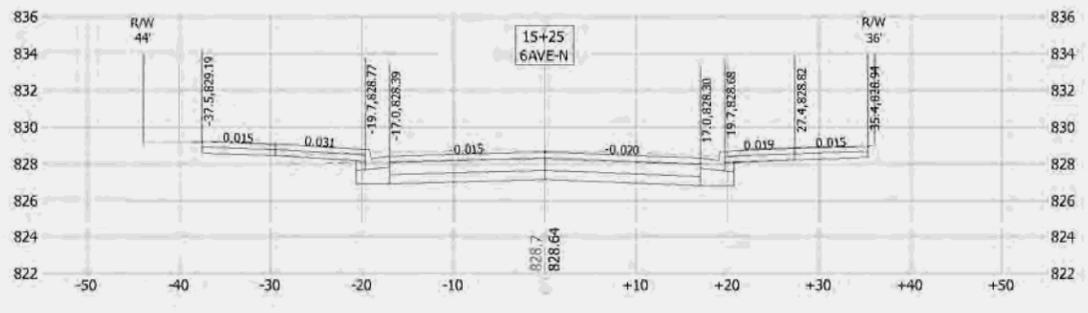
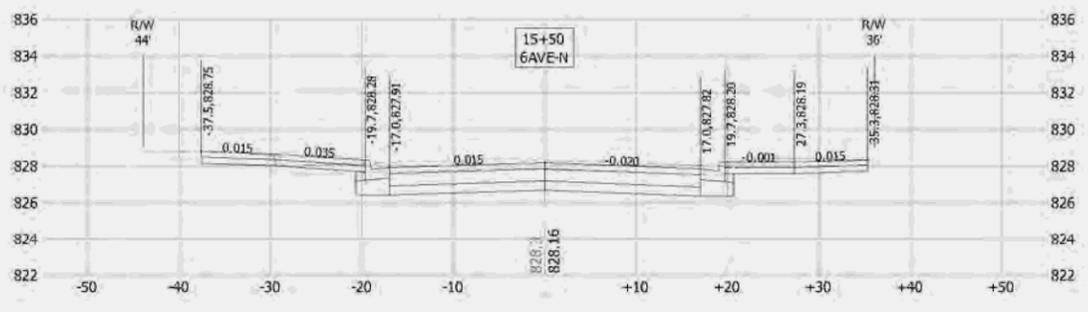
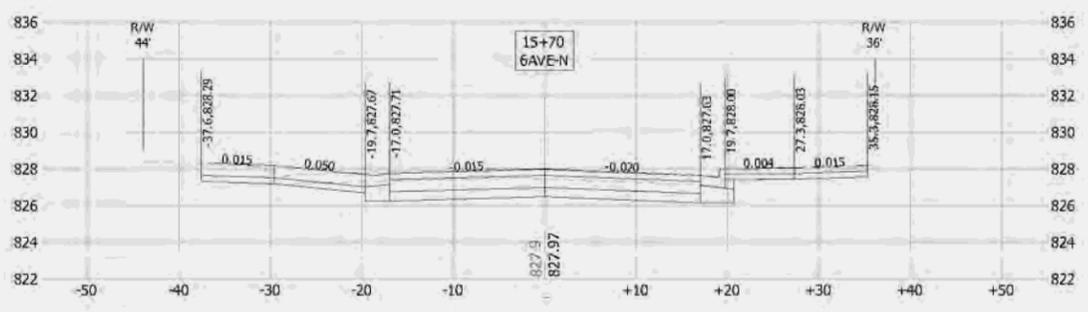
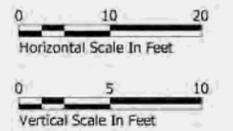
SURVEY	MPV
DRAWN	JOP
DESIGNED	JOP
CHECKED	CWL
APPROVED	LFG
PROJECT NO.	141-220-005

CROSS SECTIONS  
 5TH ST TO 4TH ST

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

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SEE PLAN SHEETS FOR LOCATION OF EXISTING AND PROPOSED UTILITIES  
SEE PLAN SHEETS FOR INPLACE ROW LOCATIONS



NO.	DATE	APPR.	REVISION

SURVEY	MPV
DESIGNED	JOP
CHECKED	CIVL
APPROVED	EPG
PROJECT NO.	19362881



CROSS SECTIONS  
4TH ST TO 3RD ST

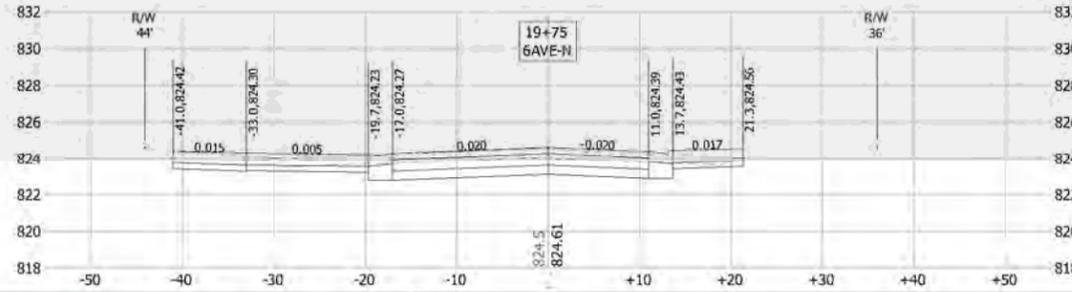
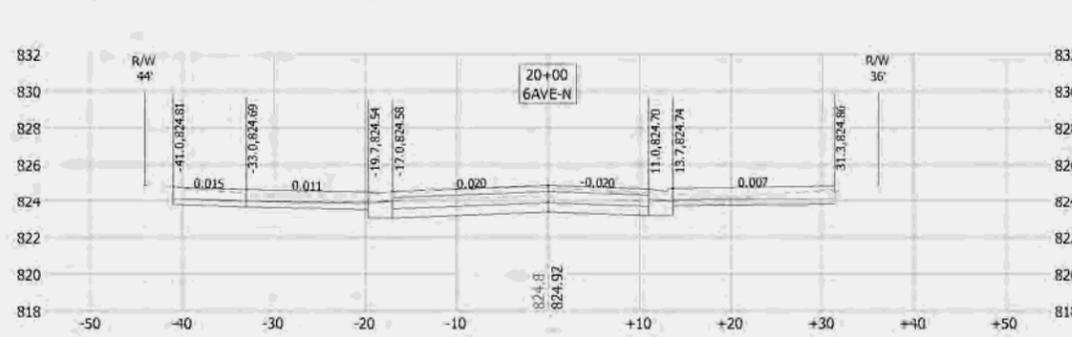
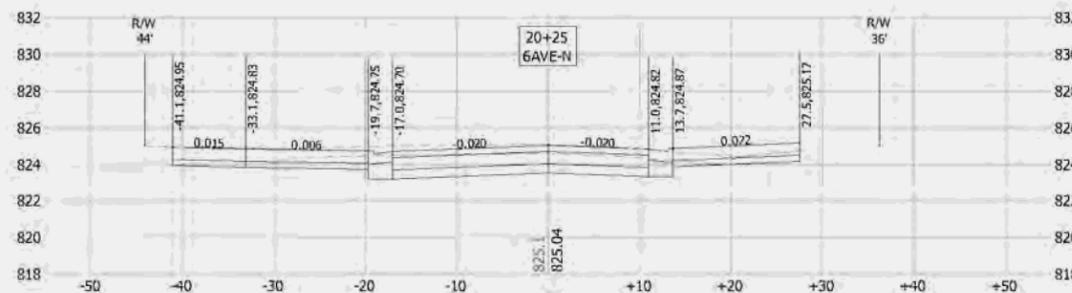
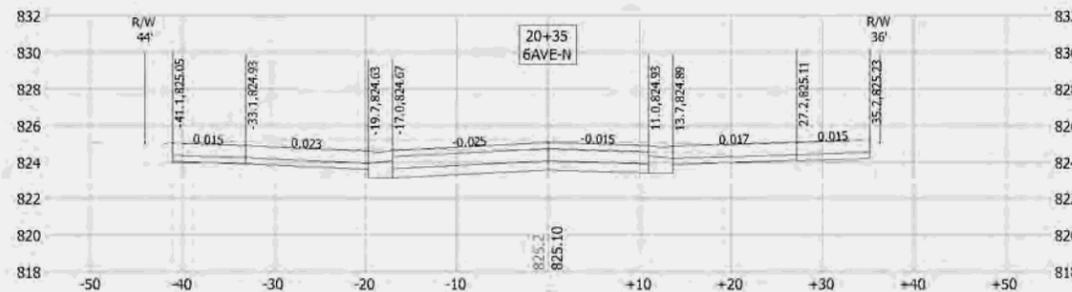
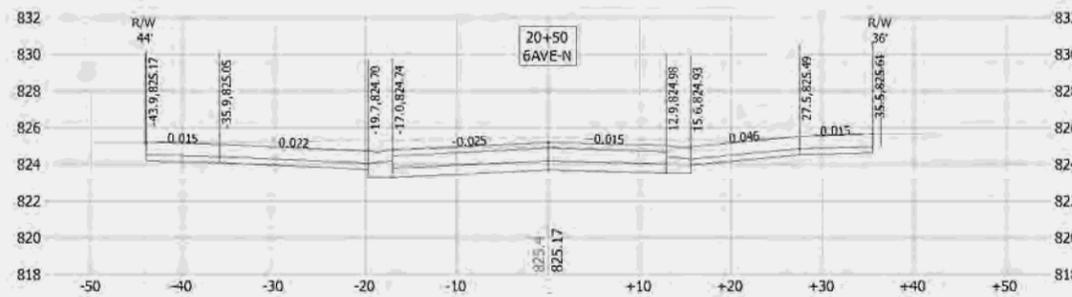
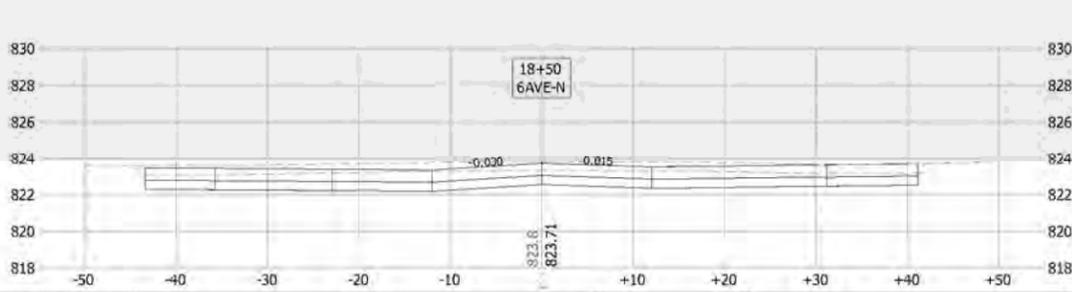
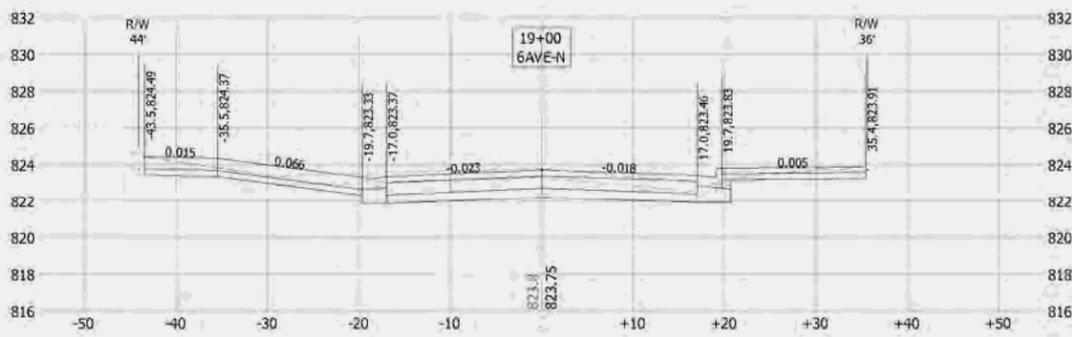
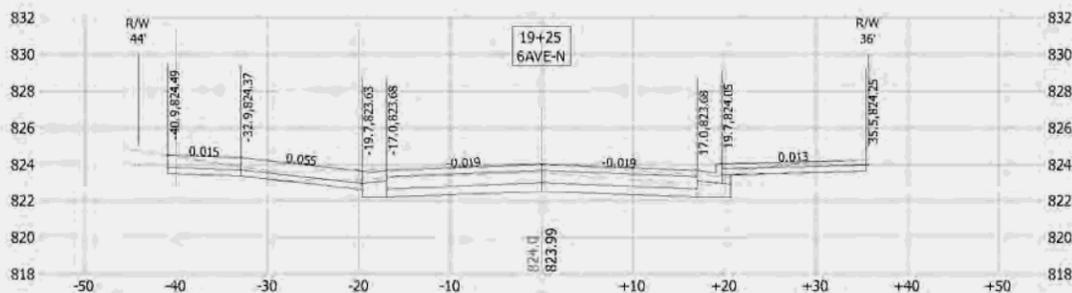
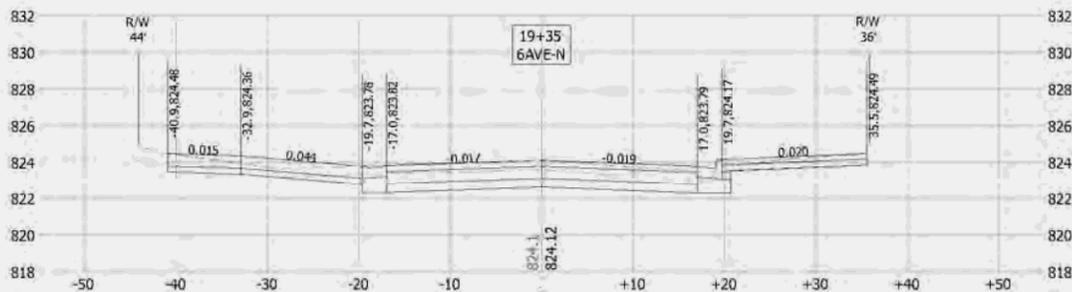
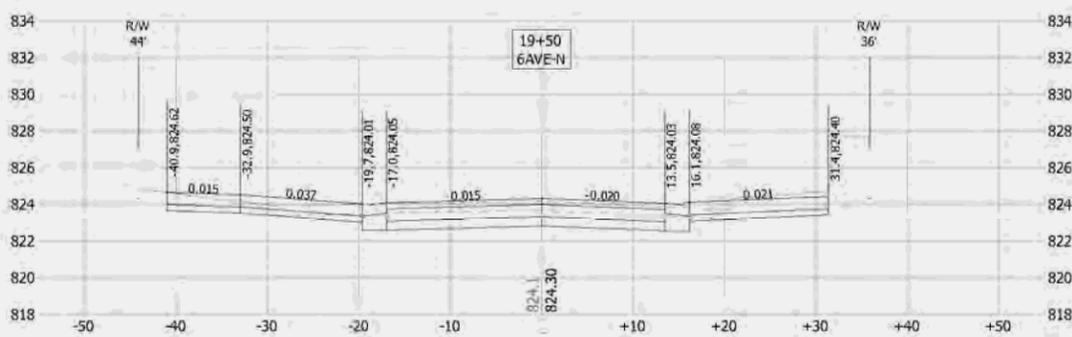
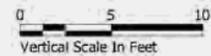
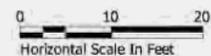
CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005



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SEE PLAN SHEETS FOR LOCATION OF EXISTING AND PROPOSED UTILITIES

SEE PLAN SHEETS FOR INPLACE ROW LOCATIONS



NO	DATE	APPR	REVISION

SURVEY	MPV
DRAWN	JOP
DESIGNED	JOP
CHECKED	CIVL
APPROVED	LPG
PROJ. NO.	1936288

Minneapolis City of Lakes

Stantec  
2335 Highway 36 W  
St. Paul, MN 55113  
www.stantec.com

CROSS SECTIONS  
3RD ST TO WASHINGTON AVE

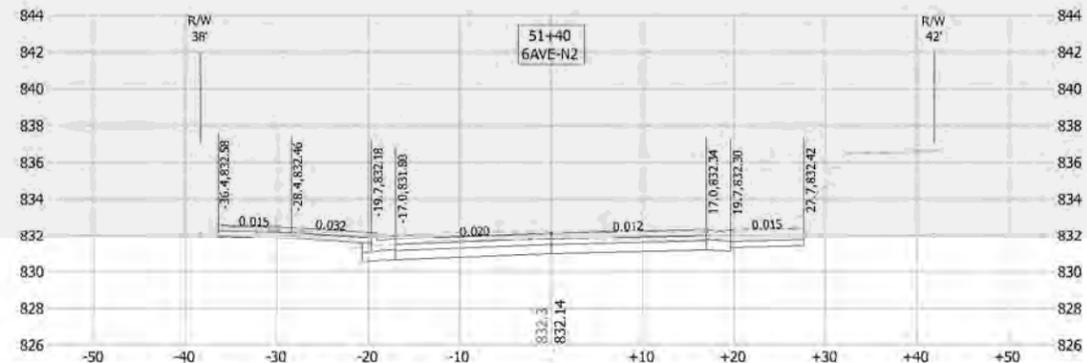
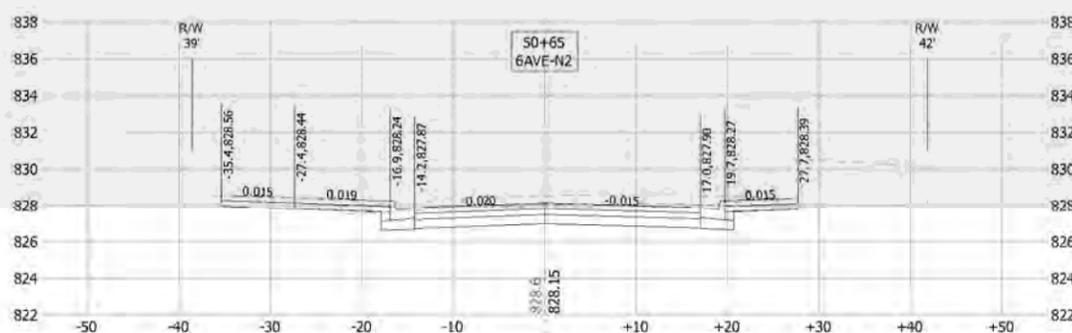
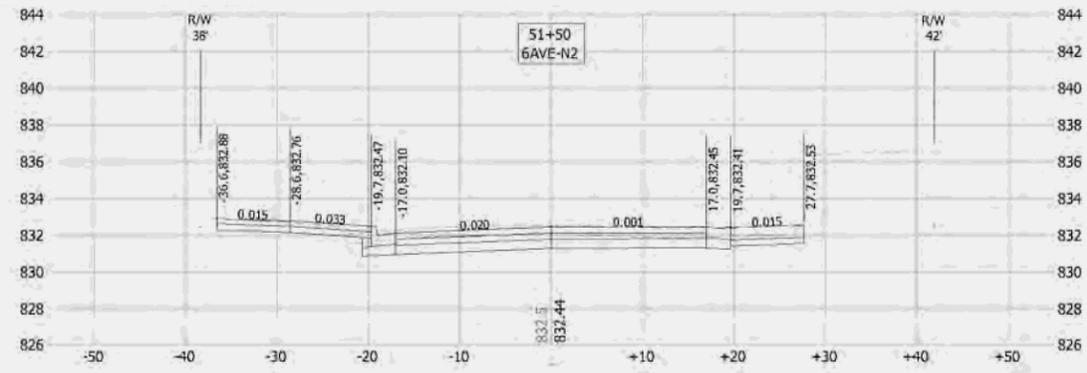
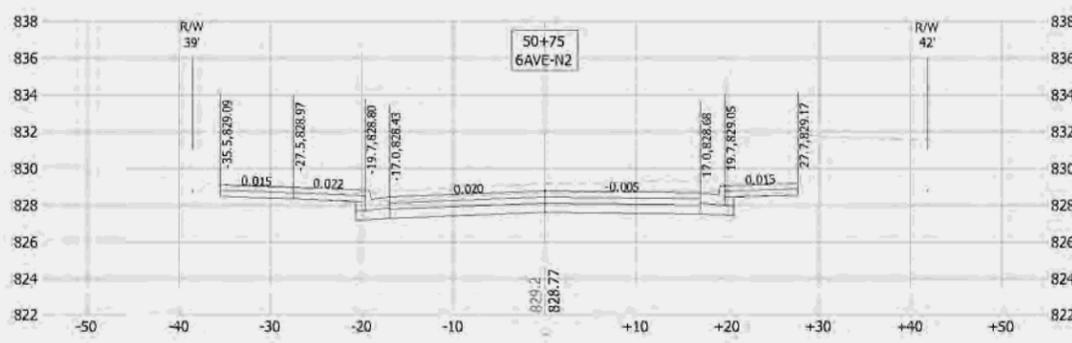
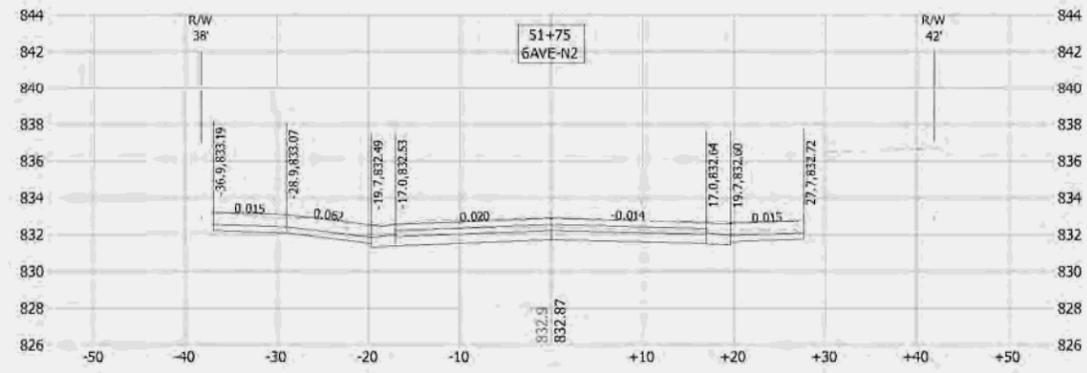
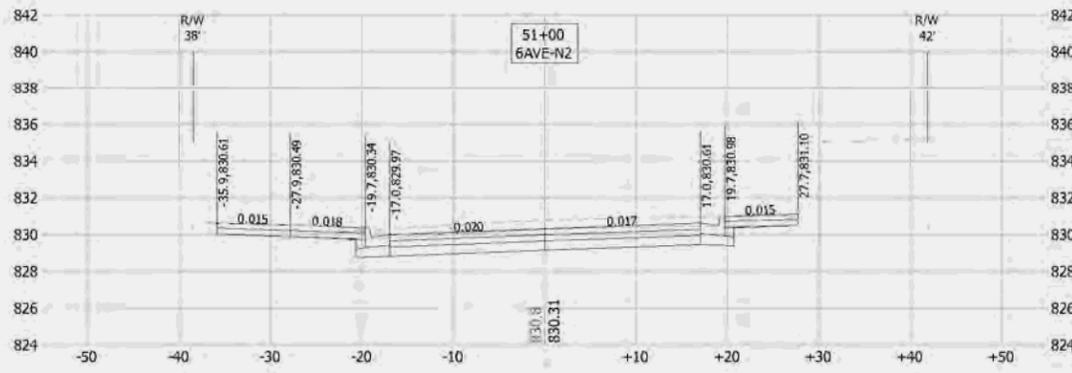
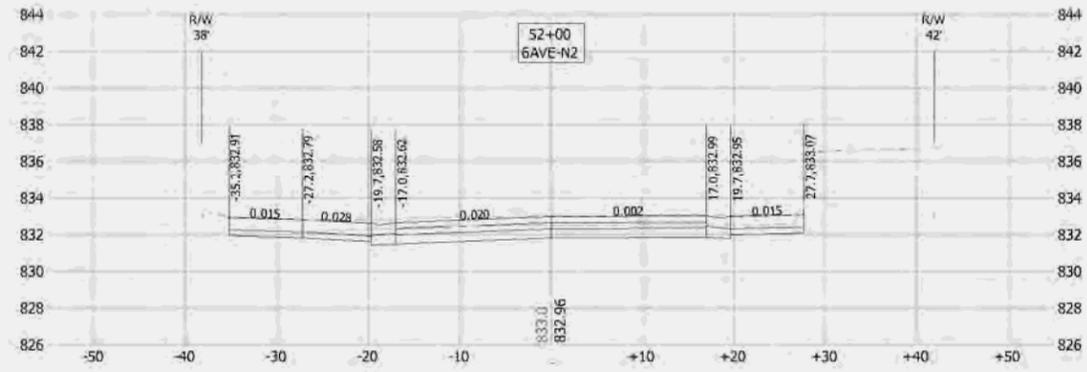
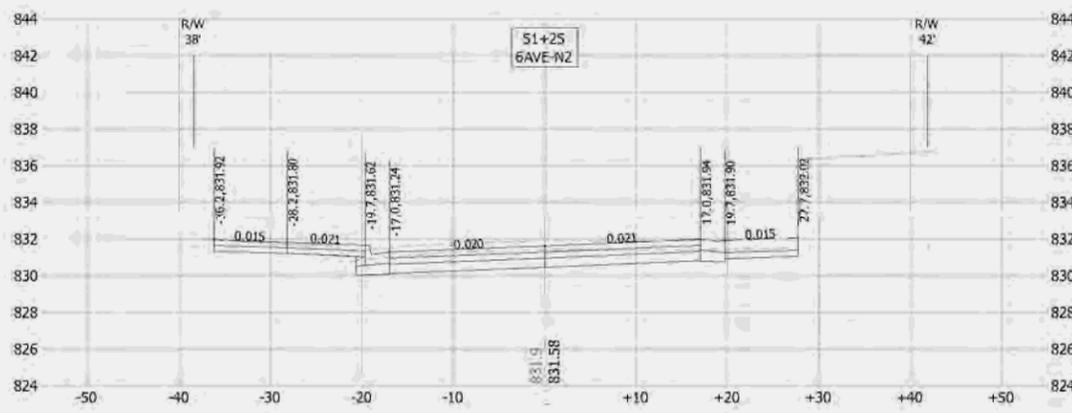
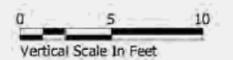
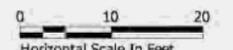
CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005



THE ESTIMATED YEARLY PERCENTAGE INCREASE IN COST OF CONSTRUCTION SHALL BE BASED ON THE ESTIMATED YEARLY PERCENTAGE INCREASE IN COST OF CONSTRUCTION AS REPORTED IN THE ENR SURVEY OF CONSTRUCTION COSTS FOR THE YEAR 2013. THE ESTIMATED YEARLY PERCENTAGE INCREASE IN COST OF CONSTRUCTION SHALL BE BASED ON THE ESTIMATED YEARLY PERCENTAGE INCREASE IN COST OF CONSTRUCTION AS REPORTED IN THE ENR SURVEY OF CONSTRUCTION COSTS FOR THE YEAR 2013.

SEE PLAN SHEETS FOR LOCATION OF EXISTING AND PROPOSED UTILITIES

SEE PLAN SHEETS FOR INPLACE ROW LOCATIONS



NO.	DATE	APPR.	REVISION

SURVEY	MPV
DRAWN	JDP
DESIGNED	JDP
CHECKED	CIVL
APPROVED	EPG
PROJ. NO.	1936288

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CROSS SECTIONS  
NORTH OF WASHINGTON AVE

CITY OF MINNEAPOLIS, MN  
6TH AVENUE N STREET IMPROVEMENTS  
SP 141-220-005

Attachment D - Agency Coordination Letters and Environmental Documentation

STATE OF MINNESOTA  
DEPARTMENT OF TRANSPORTATION  
and  
THE CITY OF MINNEAPOLIS

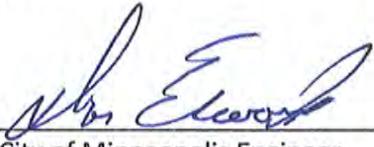
PROJECT MEMORANDUM  
AND DESIGN EXCEPTION REQUEST  
AND SECTION 4(F) EVALUATION  
FOR

SP 141-220-005  
Minn Proj. No. TA 2716(051)

ROUTE NAME 6<sup>th</sup> Avenue (MSAS 220)  
FROM: 5<sup>th</sup> Street North  
TO: 200 feet North of Washington Avenue

**PROPOSED IMPROVEMENT:** Reconstruction of 0.27 miles of 6<sup>th</sup> Avenue, including restoration of historic pavers. Improvements also include utilities and sidewalk.

Recommended:

  
\_\_\_\_\_  
City of Minneapolis Engineer

9/8/15  
\_\_\_\_\_  
Date

Reviewed and Recommended:

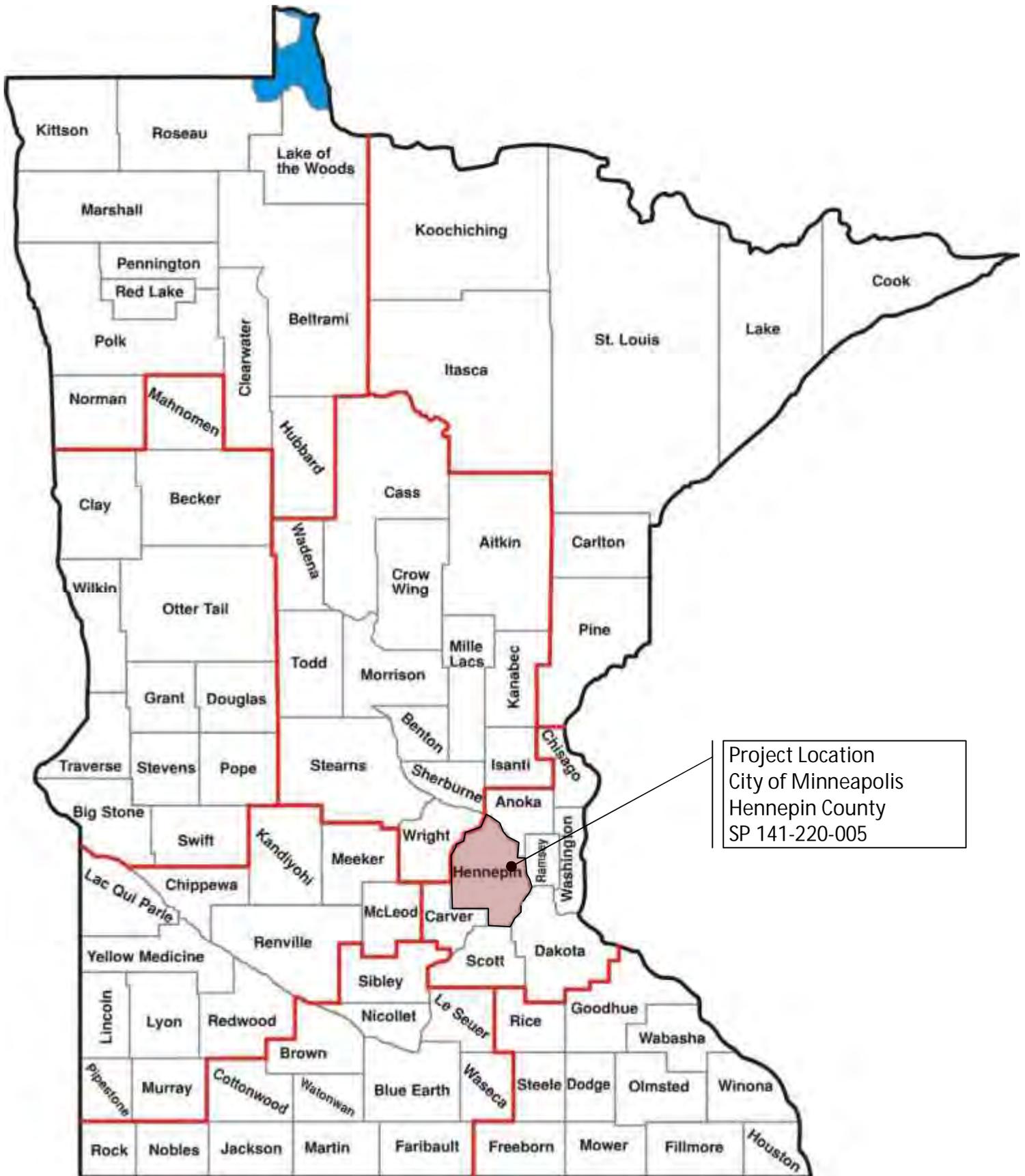
*fx*   
\_\_\_\_\_  
District State Aid Engineer

9-11-2015  
\_\_\_\_\_  
Date

Approved:

\_\_\_\_\_  
State Aid Engineer  
State Aid For Local Transportation

\_\_\_\_\_  
Date



Project Location  
 City of Minneapolis  
 Hennepin County  
 SP 141-220-005

## STATE MAP



**WAREHOUSE HISTORIC DISTRICT**  
**PROJECT LOCATION MAP**      **FIGURE: 1**  
**CITY OF MINNEAPOLIS**  
**6TH AVENUE N STREET IMPROVEMENTS**  
 DATE: AUGUST 2015      PROJ. NO.: 193802887

## I. REPORT PURPOSE

This Project Memorandum (PM) documents the need for the proposed improvement, environmental impacts and mitigation, and schedule, funding and design information.

This documentation was prepared to demonstrate that the project does not have a significant environmental effect and is excluded from the requirement to prepare an EA or EIS in accordance with 23 CFR 771.115.

## II. PROJECT CONTACTS

### Project Manager

Name	Lee Mann, P.E.
Title	Project Manager
Company/Agency	Stantec
Address	2335 West Highway 36 St. Paul, MN 55113
Telephone	651-604-4850
Fax	651-636-1311
E-mail	Lee.mann@stantec.com

### Preparer

Name	Theresa Maahs, P.E.
Title	Senior Engineer
Company/Agency	Stantec
Address	2355 West Highway 36 St. Paul, Minnesota 55113
Telephone	651-604-4786
E-mail	theresa.maahs@stantec.com

### Project Owner

Name	Jeff Handeland, P.E.
Title	Project Manager
Company/Agency	City of Minneapolis
Address	350 South Fifth Street, Room 203 Minneapolis, MN 55415-1385
Telephone	612-673-2363
E-mail	<a href="mailto:jeff.handeland@minneapolismn.gov">jeff.handeland@minneapolismn.gov</a>

### III. HIGHWAY SECTION DESCRIPTION

#### Highway Section Termini:

From: North 7<sup>th</sup> Street

To: A point 200 feet north of North Washington Avenue

Length: 2580 feet (0.49 miles)

See additional 'existing condition' elements description in the Design Study on page 17.

#### Unusual Traffic or Road / Facility Use:

Due to the light industrial businesses lining the corridor, 6th Avenue (MSAS 220) experiences a high level of heavy truck traffic. Trucks currently access loading docks located on both sides of the roadway. Due to the limited distance between the existing buildings, access to the loading docks is difficult for large trucks. The trucks will back up to the loading docks and remain parked in the drive lanes of 6th Avenue while being loaded or unloaded.

6th Avenue is also frequented by pedestrians and bicycle traffic. Residential condominiums are located along the corridor and on adjacent streets. Residents of these properties frequently walk or bike to the nearby employment centers and commercial/retail centers in Minneapolis.

#### Horizontal/Vertical Alignment:

Horizontal: The existing horizontal alignment of 6<sup>th</sup> Avenue North is straight with no curves. The centerline of the roadway shifts approximately 10 feet as it crosses Washington Avenue North.

Vertical: The vertical alignment is generally flat with steeper grades near Washington Avenue N. Grades south of 4<sup>th</sup> Street vary between 3-4%, while grades north of 4<sup>th</sup> Street to Washington Avenue are relatively flat (around 1%). North of Washington Avenue, the road profile increases again to around 5-6%.

#### Adjacent Land Use:

The land use adjacent to 6<sup>th</sup> Avenue has changed from mainly industrial/commercial to a mix of residential, commercial and light industrial. Located directly adjacent to the corridor are multi-family housing buildings, commercial properties (including a bank, a brewery and two restaurants), private businesses and light industrial facilities. The south end of the roadway study segment is adjacent to the Target Field and the Metro Transit light rail line. North of the project is a mixed use area consisting of multi-family housing units and commercial properties.

Bridge Crossing(s):

Bridge number: N/A.

The I-94 access ramp crosses over 6<sup>th</sup> Avenue just south of 4<sup>th</sup> Street, but will not be affected by this project. No other bridges are located within the project limits.

Railroad Crossing Location (s):

No railroad facilities are located within the project area. The Metro Transit Light Rail line crosses above 6<sup>th</sup> Avenue approximately 250 feet south of the project corridor. The rail line, including rail platforms are elevated and grade separated.

Airport Proximity:

The project is not located within five (5) miles of a public airport and is not within an airport influence area.

Traffic Signals:

Existing traffic signals are located at Washington Avenue and 5th Avenue. Improvements to the signals, such as pedestrian push buttons, will be made to accommodate the ADA compliant pedestrian ramps. No other changes to this signal are planned as part of this project and no new signals are planned.

IV. PROJECT PURPOSE AND NEED

Purpose/Objectives:

The fundamental objective of this project is to preserve the historic paver brick street surface, while providing transportation links and improving walkability through the North Loop neighborhood.

Need/Deficiencies:

Structural Deficiencies - Roadway

Aside from the on-going maintenance and patching, this roadway was last reconstructed in 1926. The current street pavement surface is in poor condition and is currently a mix of bituminous patches and failing paver blocks. The road base under the driving surface is failing, leading to significant cracking and an uneven driving surface. The settling pavement provides areas for storm water to collect within the roadway. The roadway driving surface requires frequent maintenance and repairs.

Geometric Deficiencies – Roadway

The roadway geometry is not clearly defined. Currently the 49 foot wide roadway contains two through lanes (one in each direction) and on-street parking along each side. The angled parking limits the circulation of pedestrians and vehicles through a corridor already impaired by trucks accessing the loading docks.

### Deficiencies – Pedestrian Facilities

Pedestrian and ADA compliant facilities along the corridor are lacking. The sidewalk adjacent to the existing buildings does not have a consistent route for pedestrians to follow. Most intersections do not have ADA compliant ramps or facilities. Several corners have depressed curb that is cracked and uneven in lieu of pedestrian ramps.

Transit in the form of multiple bus lines and light rail facilities serve this area, introducing a large pedestrian component to the corridor. However, due to the industrial history of the corridor, concrete loading docks line both sides of 6th Avenue. The loading docks are active and frequently used. Due to the loading docks and stairs, the sidewalk is non-continuous adjacent to some of the existing buildings. Pedestrians enter the street to pass by the building delivery areas along 6th Avenue. Due to the poor pavement condition, the Minneapolis Public Works Department has included 6<sup>th</sup> Avenue in Minneapolis's Capital Improvement Program.

## V. ALTERNATIVES

### No Build Alternative

The "No Build" alternative does not address the on-going pavement failure or lack of pedestrian facilities on 6<sup>th</sup> Avenue as discussed above in section IV, Project Purpose and Need. With only minor maintenance, the quality of operation within the corridor and the structure of the roadway will continue to disintegrate.

### Preferred Alternative

The proposed project includes the reconstruction of 6th Avenue North from 200-feet north of Washington Avenue to 5th Street North. The project will preserve the historic vitrified clay paver brick surface and loading docks within the historic district. The project will salvage and reinstall the existing clay pavers in the segment between 4th Street N. and Washington Avenue (800 feet). The project also includes construction of ADA compliant sidewalks on both sides of 6th Avenue and a subsurface infiltration storm water system.

### Other Reasonable Location or Design Alternatives

#### Location Alternatives:

No location alternatives were considered for this project. Reconstructing the road on a new location would present additional social, environmental and economic impacts not justified by the project.

Design/Construction Alternatives:

6<sup>th</sup> Avenue is located in the City of Minneapolis's Historic Warehouse District. Therefore, the proposed roadway surface (pavers) and urban (curb and gutter) section for 6<sup>th</sup> Avenue will be designed to match into the adjacent existing roadway sections and to match the recommendations set in the City of Minneapolis Heritage Streets Plan.

VI. PROJECT COST, FUNDING & SCHEDULE

Estimate of Cost:

Roadway Costs:	\$ 2,624,400
Bridge/Culvert Bridge Costs:	\$ 0
Total:	\$ 2,624,400

Anticipated Funding:

Type and amount of Federal and matching funds:

Federal:	\$ 1,120,000	Transportation Alternative Program (TAP)
State Aid:	\$ 1,329,400	MSAS
Other State:	\$ 0	
Local:	\$ 175,000	

The project is in the 2015-2018 State Transportation Improvement Program (STIP).

Federal fiscal year 2016, Sequence # 1697

Estimated cost shown in STIP: \$ 2,799,104

Federal funding shown in STIP: \$ 1,120,000

Anticipated Schedule

Public Meeting #1	October, 2014
Public Meeting #2	January, 2015
Project Memorandum	October, 2015
Right-of-Way Acquisition	N/A
Plans, Specifications & Estimate	October, 2015
Bid Opening	December, 2015

Future Stages Or Improvements

No further phases of construction are planned for this location.

## VI. SOCIAL, ECONOMIC AND ENVIRONMENTAL (SEE) IMPACTS

### Section 4(f) Of The Transportation Act Of 1966

The proposed project impacts the 6th Avenue Corridor, part of the Historic Warehouse District, which is listed on the National Register of Historic Places (NHRP).

Since the project utilizes federal transportation funds and will involve the use of a historic site that has been identified as a Section 4(f) resource, the Section 4(f) statute applies.

For historic properties, a de minimis impact is one that results in a Section 106 determination of "no adverse effect" or "no historic properties affected." The 106 Process has been completed for impacts to 6<sup>th</sup> Avenue and a determination of "No Effect" has been provided by MnDOT CRU and concurred upon by SHPO.

Opportunities for public involvement were provided through the 106 process. SHPO in conjunction with MnDOT CRU worked closely with the City of Minneapolis and the Minneapolis Heritage Preservation Commission (HPC), including the officials with jurisdiction over the Section 4(f) property throughout the 106 process for impacts to the Historic Warehouse District.

Attached is the Section 4(f) Temporary Use impact finding correspondence.

### Section 6(f) Of The Land And Water Conservation Fund Act Of 1965

The project will not impact Section 6(f) lands or properties.

### Section 106 Of The National Historic Preservation Act Of 1966

The proposed project impacts 6<sup>th</sup> Avenue North, located in the Historic Warehouse District within the City of Minneapolis, which is listed on the National Register of Historic Places. The project will have no adverse effects on this NRHP listed property.

See attached letters from the CRU, with letters of concurrence from the SHPO.

### Endangered Species Act Of 1973

The project will have no effect on Federally-listed T&E species or critical habitat.

See attached letter from Mn/DOT's Office of Environmental Stewardship (OES) for Federally listed species.

## Right-Of-Way

The project will require approximately:

0 acres of permanent right of way acquisition, and

0 acres of temporary easements, and

0 parcels secured by permit or agreement

The project will not require residential or business relocations.

## Hazardous Materials

A Limited Phase I Environmental Site Assessment (ESA) was completed in May 2015 for the 6th Avenue project corridor. The ESA was performed to determine the presence of potentially contaminated properties (defined as properties where soil and/or groundwater is impacted with pollutants, contaminants or hazardous wastes). The study determined 33 sites of potential concern, which were further studied and then ranked as having a high, medium or low potential for contamination.

There were 8 sites identified within or near the corridor that have a LOW potential for contamination. These sites include properties where known or inferred small or very small quantities of chemicals are used or stored and where there is no obvious evidence of "poor housekeeping" practices. These sites generally do not require further investigation.

The ESA found 14 MEDIUM sites within or near the corridor. MEDIUM sites include those properties with a documented release that has been addressed to the satisfaction of the appropriate regulatory agency, sites with a history of land use that indicates potential environmental concern, sites with underground storage tanks and/or sites with "poor housekeeping" practices. For this project, the medium sites consist primarily of petroleum related properties including closed LUST sites, UST/AST sites, and historic filling station properties.

Eleven sites within the corridor were identified as having a HIGH potential for soil and/or groundwater contamination. Sites with HIGH potential for contamination include those within the excavation area or with documented subsurface contamination, sites with on-going investigation and cleanup, properties with large amounts of chemicals and/or sites with indications that a release has occurred. If needed, a Phase II ESA will be completed to further investigate the HIGH potential sites.

A contingency plan will be developed to address unexpected conditions, wastes or contaminated media that may be encountered during construction. Potential for impacts from contaminated properties has been considered, but because the project

will not require bridge or building demolition or relocation, and the project is located completely within the existing right of way, there is little potential for encountering contaminated materials.

Any potentially contaminated materials encountered during construction will be handled and treated in accordance with applicable state and federal regulations.

#### Farmland Protection Policy Act Of 1981

The project will not involve the acquisition of farmland.

#### Air Quality

The project will not significantly impact air quality.

#### Highway Traffic Noise

The project is not a Type 1 project. Procedures for the abatement of highway traffic noise do not apply in accordance with 23 CFR 772.

#### Construction Noise

Noise will be generated during the construction of this project, which is expected to last four months. Night construction activities are not anticipated. Construction work will be performed during daytime hours in an attempt to reduce the effects of noise on local residents. Adherence to applicable State standards and County noise ordinances will be followed.

#### Floodplain Management

The project will not encroach into a floodplain.

#### Wetland Protection

The project will not impact or encroach into a wetland.

#### Section 404 Of The Clean Water Act

The project will not involve placement of fill into waters of the U.S. (defined in 33CFR 328).

## Water Pollution / MPCA--NPDES

This project will not increase the existing impervious surface area and associated highway runoff, and it will not change the existing drainage system. This project does not introduce highway runoff to sensitive water bodies. Therefore, no impacts on water quality are anticipated.

The construction activities will disturb 1 or more acre of land area (including clearing, grading, & excavation). A Phase II NPDES permit is required.

The permit will be submitted to Mn/DOT State Aid prior to project authorization, and a Stormwater Pollution Prevention Plan (SWPPP) will be included in the construction plan package.

## Controversial Issues

The project is not anticipated to be controversial.

## Environmental Justice

The purpose of Executive Order 12898 is to identify, address, and avoid disproportionately high and adverse human health or environmental effects on minority and low income populations. Based on a field review of the project area, and discussions with City officials, it has been determined that there are no minority or low income populations within the project area. Therefore, there are no Environmental Justice concerns on this project.

## State Environmental Review (MEQB)

The project does not meet the mandatory EAW threshold and does not have potential for significant environmental effects.

## Federal Action Determination Statement

Based on the environmental study in accordance with 23 CFR 771.117, it is determined that the proposed improvement is a Class II Action (categorical exclusion) anticipated to have no foreseeable change on the quality of the human environment.

VII. AGENCY COORDINATION (Not covered in the “SEE” impact section above)

City of Minneapolis Heritage Preservation Commission (HPC)

The HPC has been involved in the project through the 106 process in conjunction with the Minnesota State Historical Preservation Office (SHPO) and MnDOT’s Cultural Resources Unit (CRU). The HPC will provide a ‘certificate of appropriateness’ letter as the design of the project nears completion.

DNR Natural Heritage and Nongame Research Program

The DNR was contacted and a review of the Natural Heritage database was requested to determine the existence of State T&E within the project area. The DNR Division of Ecological and Water Resources provided a letter that states that the DNR does not need to review the proposed 6th Avenue North Reconstruction project. Given that there will not be any disturbances; the DNR does not believe it would result in a significant impact to threatened or endangered species or critical habitats.

See attached letter from the MnDNR Division of Ecological and Water Resources in Appendix B.

Permits Required			
Agency	REQ'D	Status / Date Received	Attached
MPCA--NPDES	Y	Not yet submitted.	N
Watershed District	N		N
Wetland Conservation Act / BSWR	N		N
Railroad	N		N

## VIII. PUBLIC INVOLVEMENT

### Public Information Meeting #1

Who was invited & how: *Letters were sent to nearby property and business owners. Attendees included residents, business owners and members of the North Loop Neighborhood Association.*

Purpose of the Meeting:

- *Present high level overview of the proposed project*
- *Answer questions about the project regarding Warehouse District/Historic Area design guidance, assessments; receive comments and input.*

The following concerns were raised at the meeting and were addressed on an individual basis:

- *Traffic needs to be slowed down. Install 4-way stop signs or signals at all intersections for pedestrian safety.*
- *Will there be bike lanes? Should have signage to direct bike traffic from Washington Avenue's new bike lanes through the appropriate neighborhoods.*
- *Moving utilities underground, lighting adjacent to homes.*
- *Overall support and excitement for the project and the preserving of the historic character.*

### Public Information Meeting #2

Who was invited & how: *Letters were sent to nearby property and business owners. Attendees included residents, business owners and members of the North Loop Neighborhood Association.*

Meeting purpose: *To present the project layout information and, receive feedback from residents and property owners regarding the project relative to the design guidance contained in the Heritage Streets Plan.*

*No significant concerns were raised at the meeting. Comments were supportive of the project. A copy of the meeting materials for Public Information Meeting #1 and #2 are attached.*

### Lighting – Results of Lighting Installation Petition

Per Minneapolis' Street Lighting Policy, street lighting is proposed to be installed on street reconstruction projects. The cost of the lighting is assessed to properties in the

assessment influence area. Property owners/taxpayers that are impacted by the street lighting assessment may option out through a petition process. If the property owners/taxpayers representing 51% of the property in the impacted area do not support the project, the lighting project does not move forward.

Property owners/taxpayers impacted by the proposed lighting assessment were sent a petition form (see example attached). The petitions that were returned to the City resulted in the property owners/taxpayers representing more than 51% of the property in the impacted area not supporting the lighting project. Therefore, a lighting assessment will not be levied. However, the City was able to secure funding for the lighting in an amount not to exceed \$300,000 through the Minnesota Ballpark Authority for the purpose of adding pedestrian lighting on 6th Avenue North. Materials will be paid for through Ballpark Capital Projects Funds and the lights will be installed by City of Minneapolis forces, who will coordinate the installation with the contractor for this project. A copy of the funding resolution is attached.

## IX. DESIGN STUDY

The project will be designed in accordance with the FHWA-Mn/DOT Stewardship Plan. For this project, the following design standards are applicable:

State Aid Geometric Design Standards:

8820.9936 Urban; New or Reconstruction Projects.

Minnesota Manual on Uniform Traffic Control Devices (MMUTCD)

Americans with Disabilities Act (ADA)

Public Right of Way Access Guidance (PROWAG)

The project will be constructed in accordance with the current edition of the Minnesota Department of Transportation's "Standard Specifications for Construction", including all Supplemental Specifications.

Geometric Design Elements  
 6<sup>th</sup> Avenue Segment Termini:  
 From: 5<sup>th</sup> Street North  
 To: 200 feet North of Washington Avenue

Design Element	Existing Condition	Proposed Design	Required
Roadway Type	Urban	Urban	
Project or segment length, ft	1415	1415	
Functional Class	Urban Collector	Urban Collector	
ADT (Year)	5,402 (2016)	5,942 (2036)	
Heavy Commercial, %	5	5	
Speed, mph	(30 statutory) Regulatory Speed	30 Design Speed	30
# Thru Lanes each direction	1	1	
Lane width, ft	12	11	11
Surfacing type	Paver	Paver/Concrete Intersections	Paved
Structural Design Strength, ton	NA	10	9
Shoulder Width, ft	NA	NA	NA
Surfacing type	Paver	Paver/Concrete Intersections	Paved
Recovery Area From Edge of Traffic Lane, ft	NA	NA	NA
Inslope, rise:run	NA	NA	NA
Approach Sideslopes	NA	NA	NA
Curb & Gutter type	B624	B624, B224 (Bump outs)	
Curb Reaction, ft	8	8	2
Clearance from Face Curb, ft	2	2	NA
Parking Lane, ft	8	8	8
Storm Sewer, Y / N	Y	Y	
Utilities, Y / N	Y	Y	
Sidewalk Width, ft	Varies	8 Min.	

Design Element	Existing Condition	Proposed Design	Required
Distance from edge of traveled way to sidewalk, ft	8	8	
Curb Ramps with detectable warning, Y/ N	N	Y	
Design Exception Required	NA	Y	

X. DESIGN EXCEPTION REQUEST

The purpose of this section is to request an exception for a design element that does not meet the criteria set forth in the standards. A design exception is hereby requested with the following justification and considerations.

Design Standard the Exception is from:

State Aid Geometric Design Standards:

8820.9936 Urban; New or Reconstruction Projects.

Design Element Involved: Two sag vertical curves and one crest vertical curve

6th Avenue							
Station Range	Curve Type	Deficiency	A	L	Design Speed Met	Design Exception Required	Variance Required
10+30 - 10+60	SAG	Vertical Curve (Min 90' L for 30mph)	5.35	25	14.7	Y	Y
21+40 - 21+55	CREST	Vertical Curve (Min 90' L for 30mph)	1.81	45	15	Y	Y

6th Avenue - North							
Station Range	Curve Type	Deficiency	A	L	Design Speed Met	Design Exception Required	Variance Required
50+31 - 50+66	SAG	Vertical Curve (Min 90' L for 30mph)	6.82	45	17.5	Y	N

Required Standard: The sag and crest vertical curves in as shown in table above are required to meet 30 mph design speeds.

Proposed "in lieu of" Design: ie: In lieu of 90 foot curves meeting the 30 mph design speed, the proposed curves range from 25-45 feet and meet speeds between 14.7 – 17.5 mph. The table on above lists the required design and the proposed design for each curve that does not meet standards.

Location: The locations of the curves are shown on the attached profile sheet.

Economics : Impacts to the buildings would incur significant cost to adjust adjacent windows and doorways.

Environmental Impacts/Encroachments: Adjusting the profile to meet the required 30 mph sag vertical curves at these locations would result in impacts to adjacent buildings that would render the project unfeasible. The corridor is fully developed and contains historic loading docks, ADA compliant sidewalks and a concrete boulevard. Adjustments to the profile directly impact the buildings and the loading docks. Grades are constrained due to existing tie in elevations, buildings, structures, and ADA requirements.

Design betterments: Cross slope adjustments were made to accommodate the required vertical curves within the profile. Adjustments were also made within the concrete boulevard between the sidewalk and the back of curb.

Conclusion: The three curves listed in the table above do not meet the minimum 30 mph vertical curve length and/or maximum difference in grade for a PVI without a vertical curve. The constrained corridor contains historic loading docks, historic buildings and an ADA compliant walkway. Adjustment to the profile to meet minimum standards would create social, environmental and economic impacts that would render the project unfeasible.

Variance: Due to the use of State Aid funds for construction, this project also requires a design variance for the two curves that are located on the State Aid system (6<sup>th</sup> Avenue south of Washington Avenue). A variance to MnDOT State Aid Rule 8820.9936, Chapter 8820, effective 2011 was granted by the Minnesota Commissioner of Transportation. The variance allows a vertical profile between station 10+30 and 10+60 that meets a design speed of 14.7 MPH instead of 30 MPH as required by rule 8820.9936; and with a vertical profile between station 21+40 and 21+55 that meets a design speed of 15 MPH instead of 30 MPH as required by rule 8820.9936. The City resolution requesting the variance from State Aid rules is attached.

#### XI. TRAFFIC DURING CONSTRUCTION

6th Avenue will be closed to through-traffic during construction. Local traffic will be provided access throughout the construction, which is anticipated to last approximately 4 months. Traffic to loading docks will be maintained throughout construction as needed.

Construction will be staged on one side of the roadway at a time, so pedestrians will be provided a route on one side of the street, opposite of the construction. Temporary Pedestrian Access Routes (TPARs) will be provided for pedestrians in the evening after construction has ended for the day. Segments of 6<sup>th</sup> Avenue without pedestrian routes will be detoured. Traffic control will follow the Minnesota Manual on Uniform Traffic Control Devices (MMUTCD).

# ATTACHMENTS

## Attachment A - Maps/Drawings

- Roadway Typical Section
- Proposed Project Layout
- Project Profiles

## Attachment B – Agency Coordination

- Mn/DOT's Cultural Resources Unit (CRU) letter for Historic/Archaeological determination.
- Concurrence letter from the SHPO on 30% plans – May 15, 2015
- CRU letter to SHPO regarding 60% plan review – June 1, 2015
- Concurrence letter from the SHPO on 60% plans - July 1, 2015
- Section 4(f) Temporary Use Letter
- Mn/DOT's Office of Environmental Services (OES) letter for Federally listed species determination
- MnDNR Letter

## Attachment C – Public Meeting Information

- Public Meeting 1 Materials
- Public Meeting 2 Materials

## Attachment D –Street Lighting Documentation

- Example Petition
- Resolution to authorize Minnesota Ballpark Authority funding

## Attachment E - Design Variance Resolution

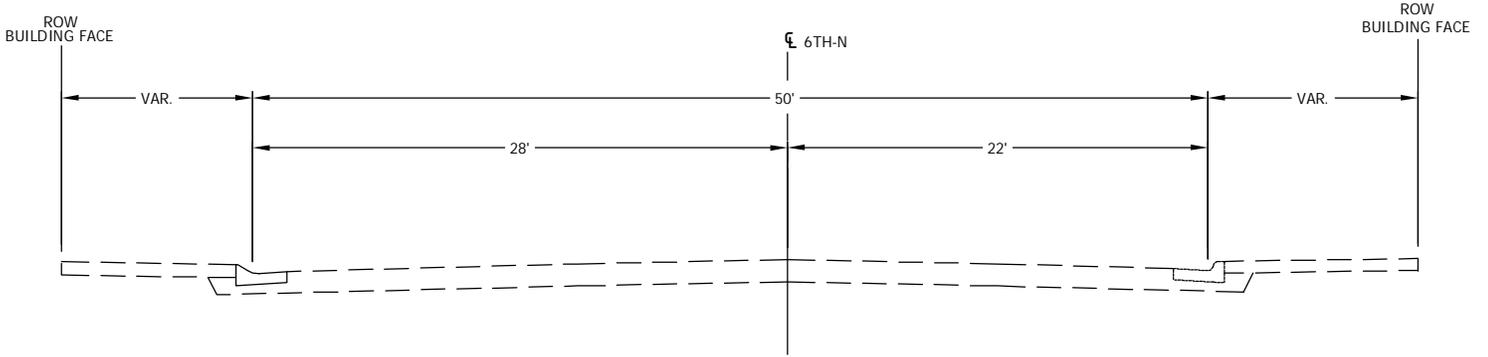
- Approved Design Variance Resolution

# Attachment A

## Maps/Drawings

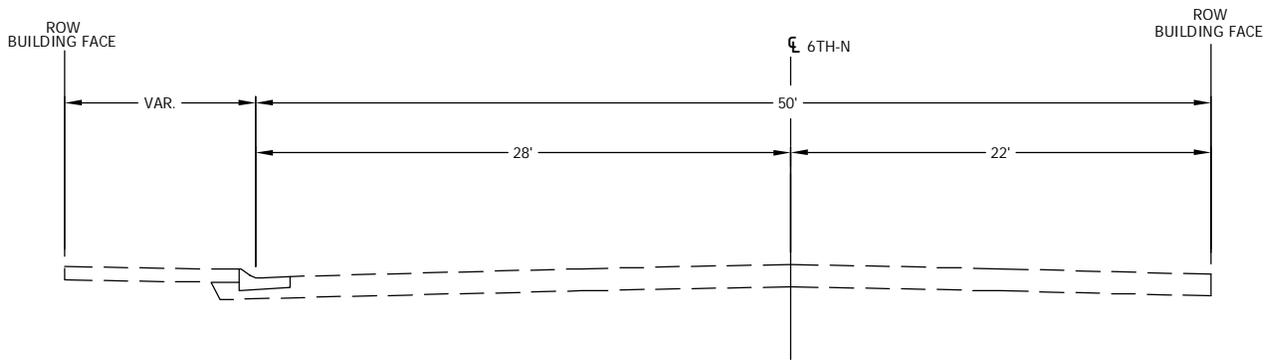
- Roadway Typical Section
- Proposed Project Layout
- Project Profiles





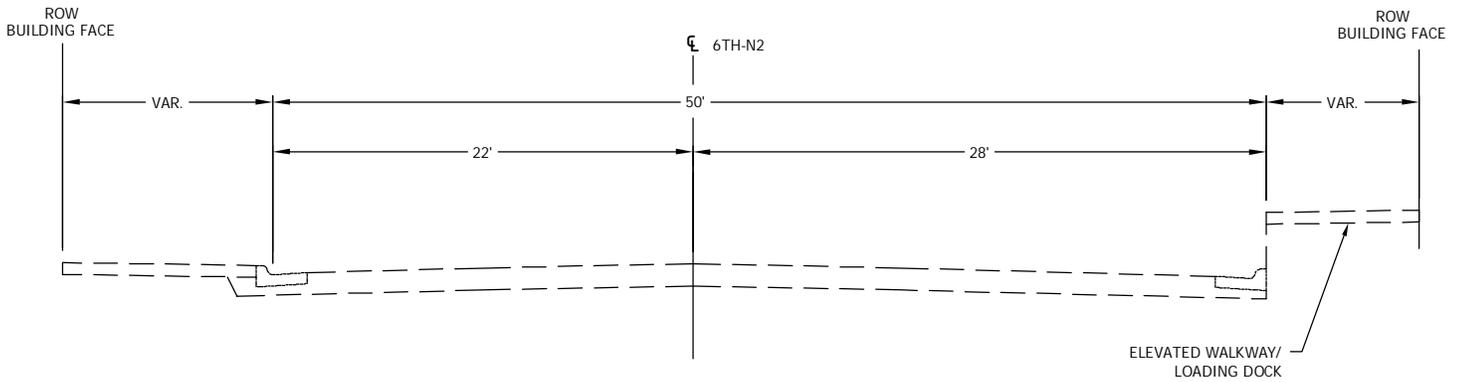
**3** EXISTING TYPICAL SECTION NOT TO SCALE  
 6TH AVENUE N

18+92 - 19+69



**4** EXISTING TYPICAL SECTION NOT TO SCALE  
 6TH AVENUE N

19+69 - 20+80

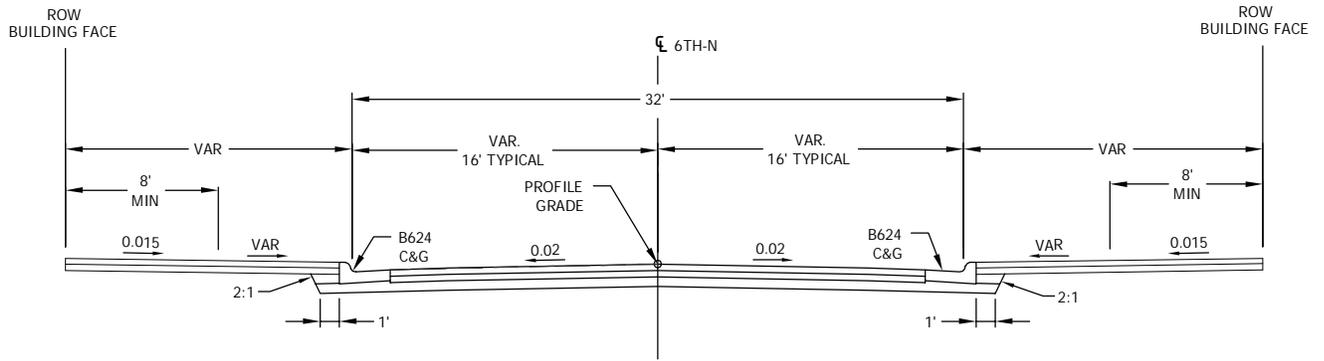


**5** EXISTING TYPICAL SECTION NOT TO SCALE  
 6TH AVENUE N

50+20 - 52+28

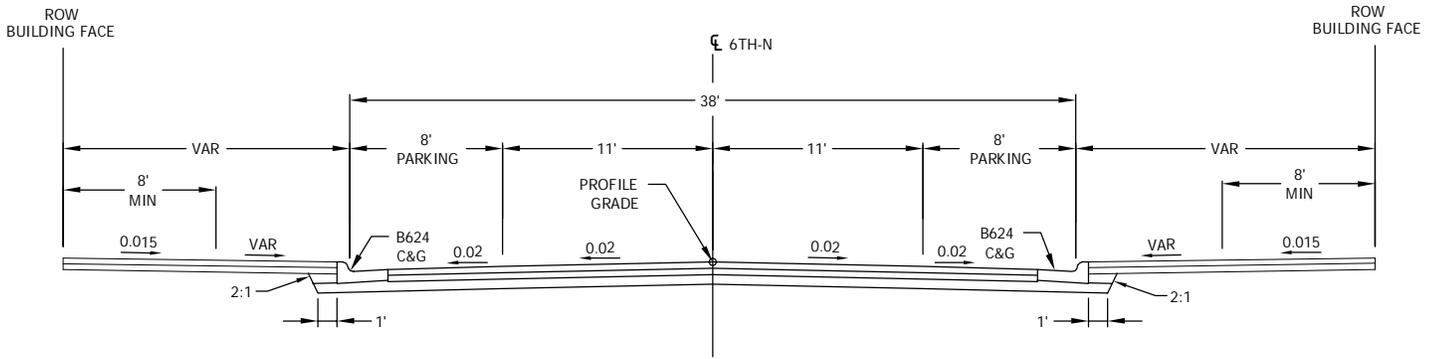
## EXISTING TYPICAL SECTIONS

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 Scale: 1"=20' SCALE  
 Drawing: 141-220-005-01



1 TYPICAL SECTION NOT TO SCALE  
6TH AVENUE N

10+14 - 10+75



2 TYPICAL SECTION NOT TO SCALE  
6TH AVENUE N

10+75 - 13+81

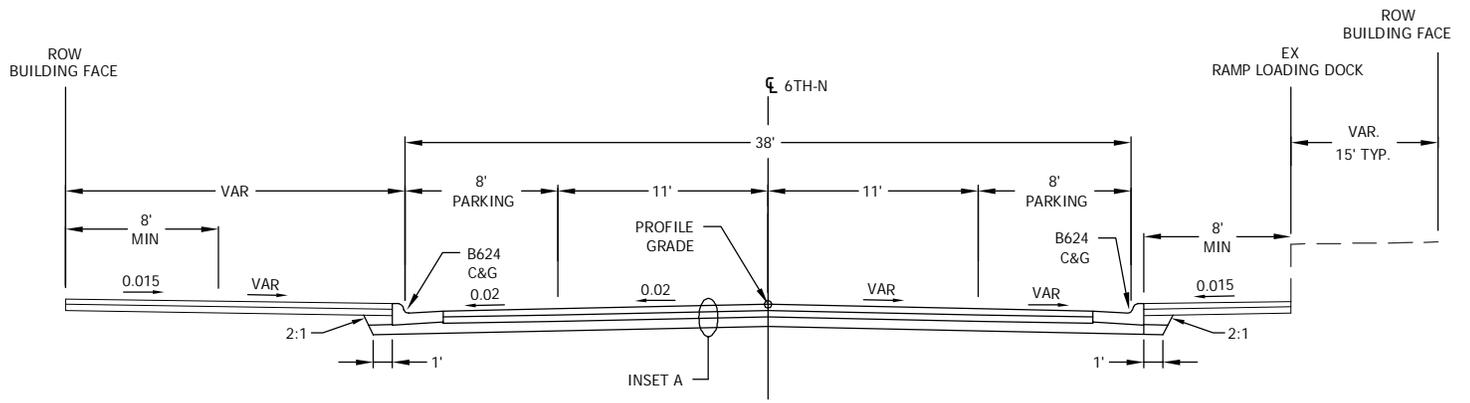
## PROPOSED TYPICAL SECTIONS

CITY OF MINNEAPOLIS  
6TH AVENUE N STREET IMPROVEMENTS

FIGURE: 4

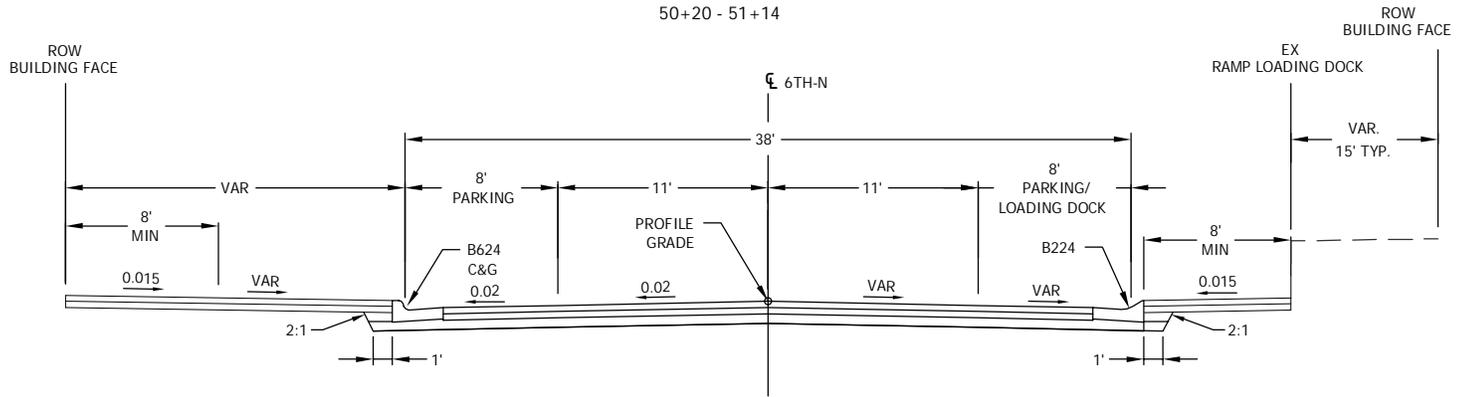


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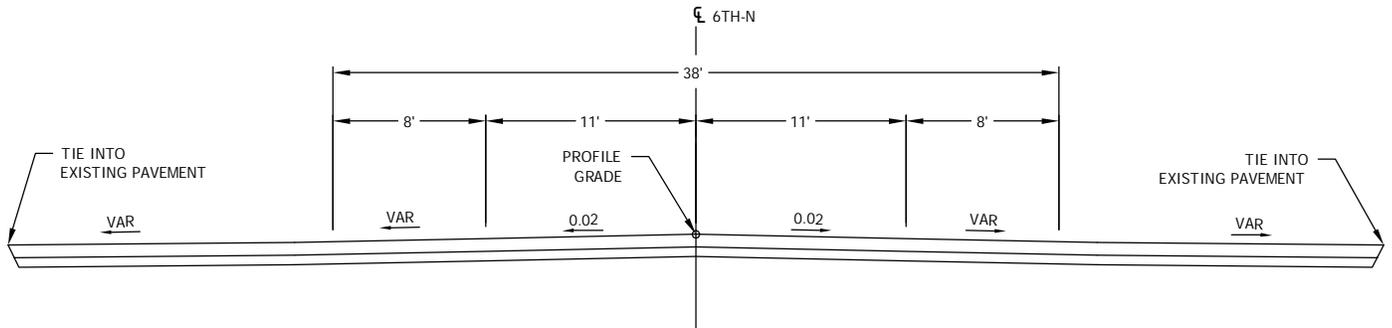
6 TYPICAL SECTION NOT TO SCALE  
6TH AVENUE N

50+20 - 51+14



7 TYPICAL SECTION NOT TO SCALE  
6TH AVENUE N

51+14 - 52+28



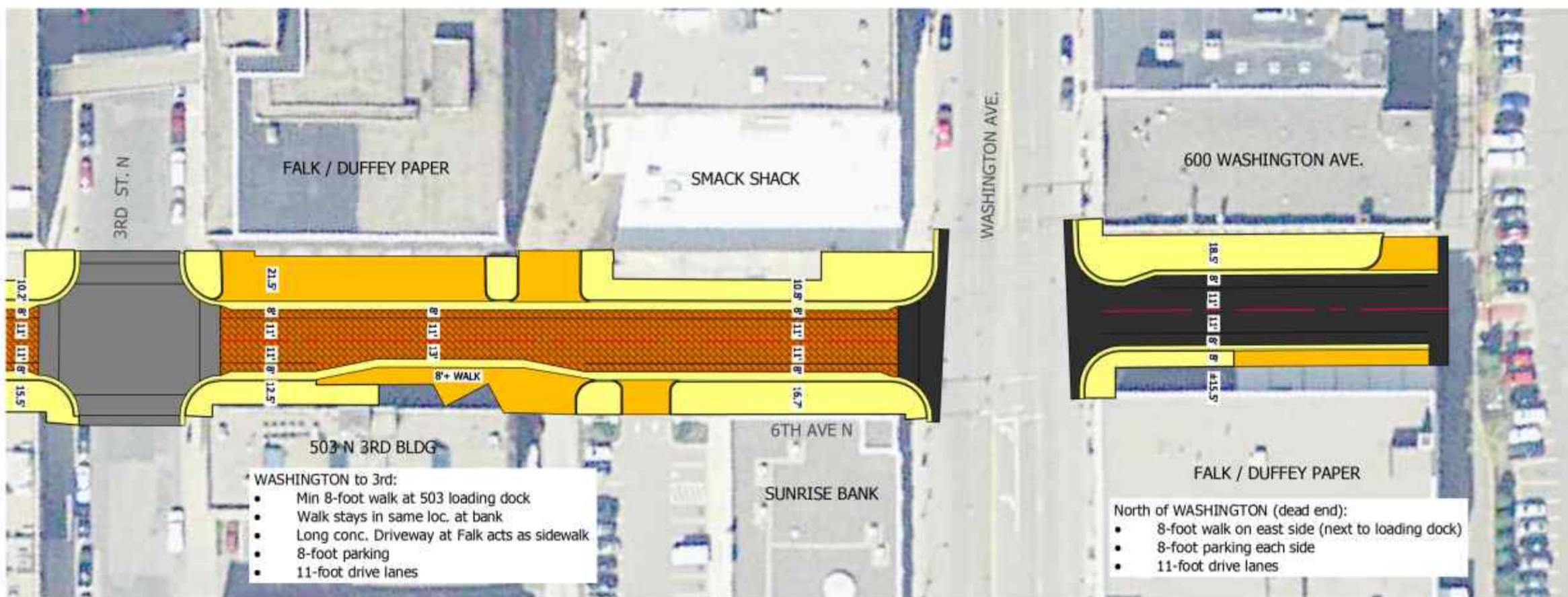
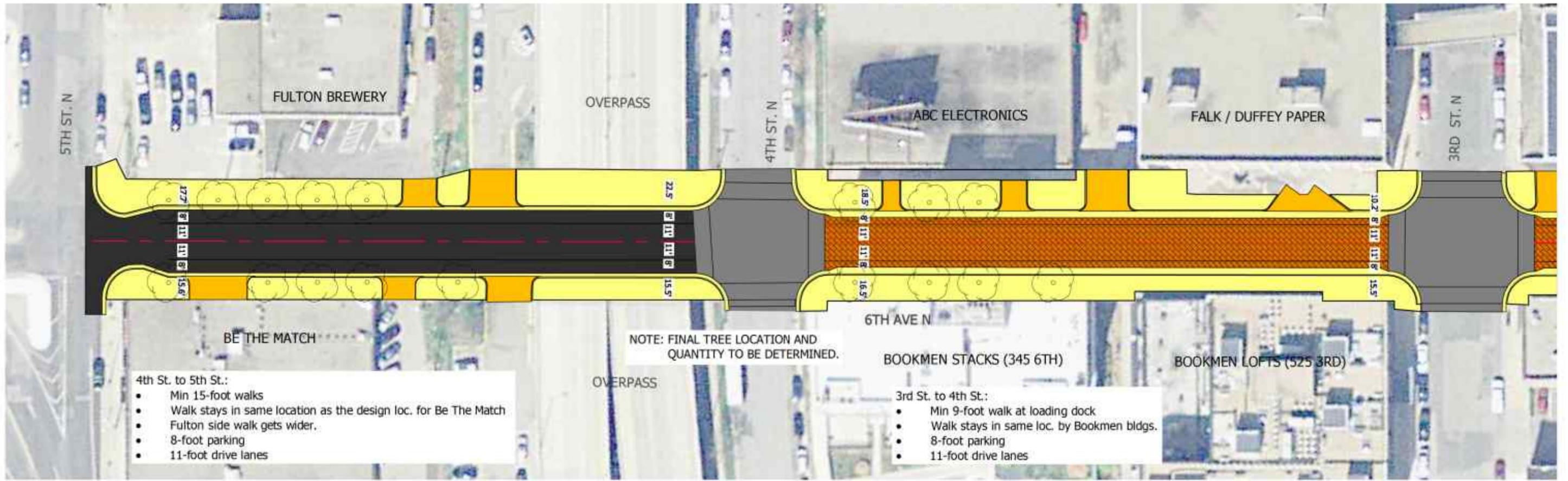
8 TYPICAL SECTION NOT TO SCALE  
6TH AVENUE N

13+81 - 14+61  
18+02 - 18+91

## PROPOSED TYPICAL SECTIONS

CITY OF MINNEAPOLIS  
6TH AVENUE N STREET IMPROVEMENTS

FIGURE: 6

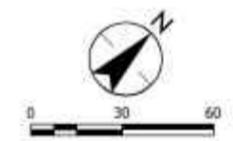


# 6th Avenue N 30% Layout Plan

February 18, 2015

LEGEND

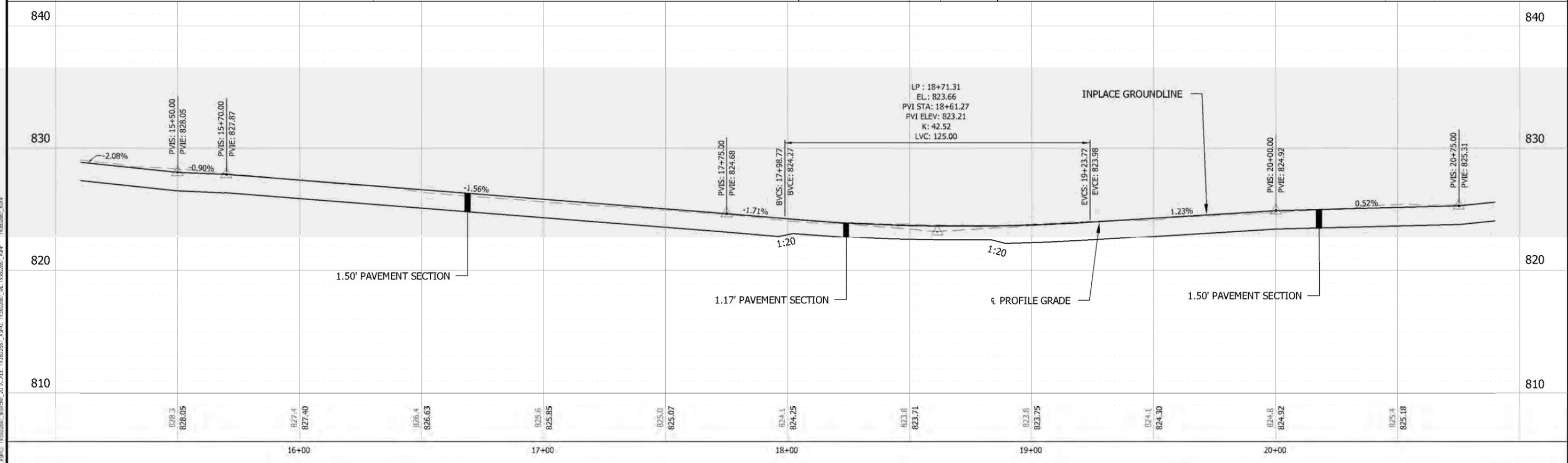
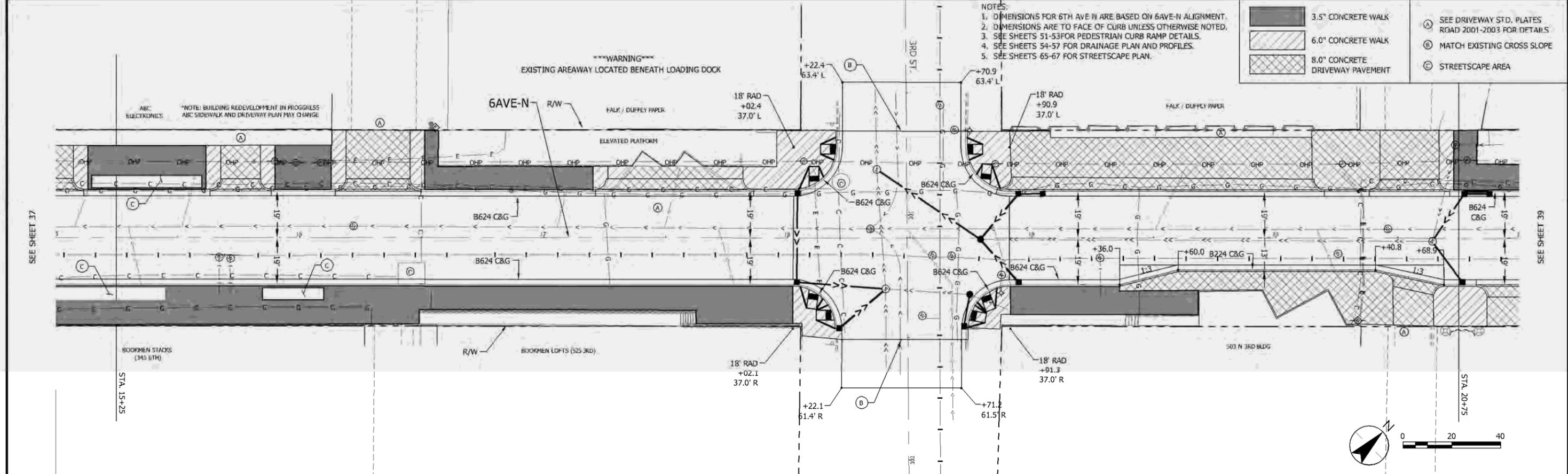
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	HISTORIC PAVERS
	CONCRETE PAVEMENT
	CONCRETE DRIVEWAY
	CONCRETE SIDEWALK



File Date: 02/12/2015  
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 User: JEFFREY\_BROWN  
 Plot Date: 02/18/2015 10:00:00 AM  
 Plot Name: 6th Ave 30% Layout.dwg



THE ENGINEER SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND ELEVATIONS SHOWN ON THIS PLAN AND PROFILE. THE ENGINEER SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND ELEVATIONS SHOWN ON THIS PLAN AND PROFILE. THE ENGINEER SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND ELEVATIONS SHOWN ON THIS PLAN AND PROFILE.



NO	DATE	APPR	REVISION

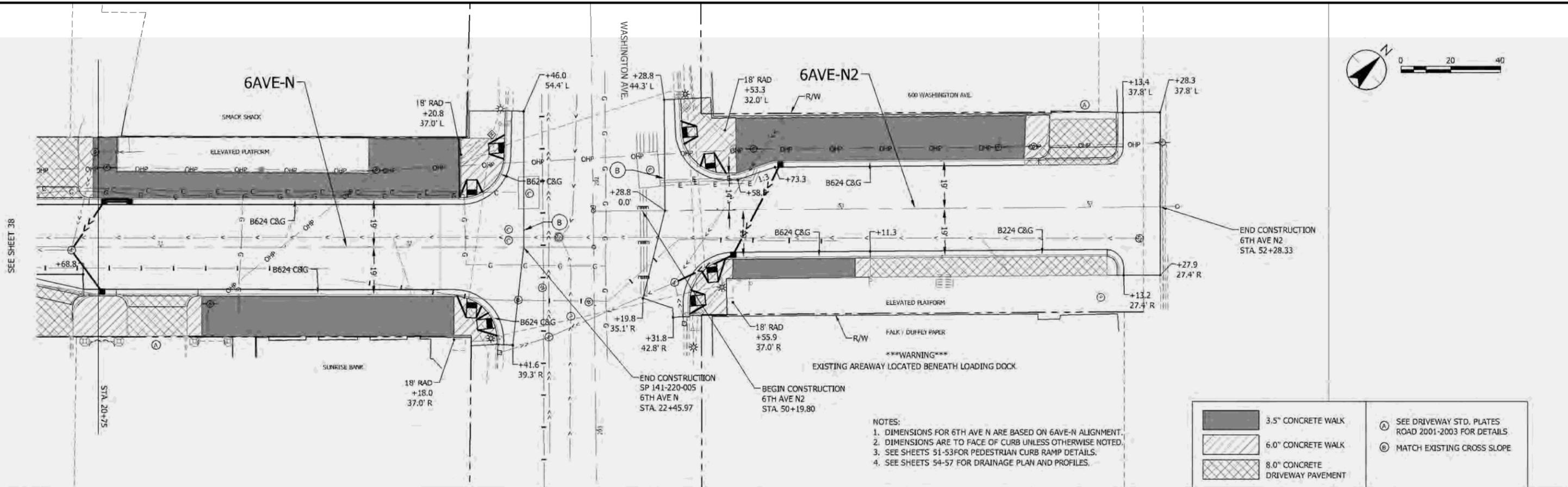
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PROJECT NAME: 6TH AVENUE N STREET IMPROVEMENTS  
 SIGNATURE: *Jeffrey D. Peterson*  
 DATE: 02/15/2018 LIC. NO. 41964

2335 Highway 36 W  
 St. Paul, MN 55113  
 www.stantec.com

CONSTRUCTION PLAN AND PROFILE

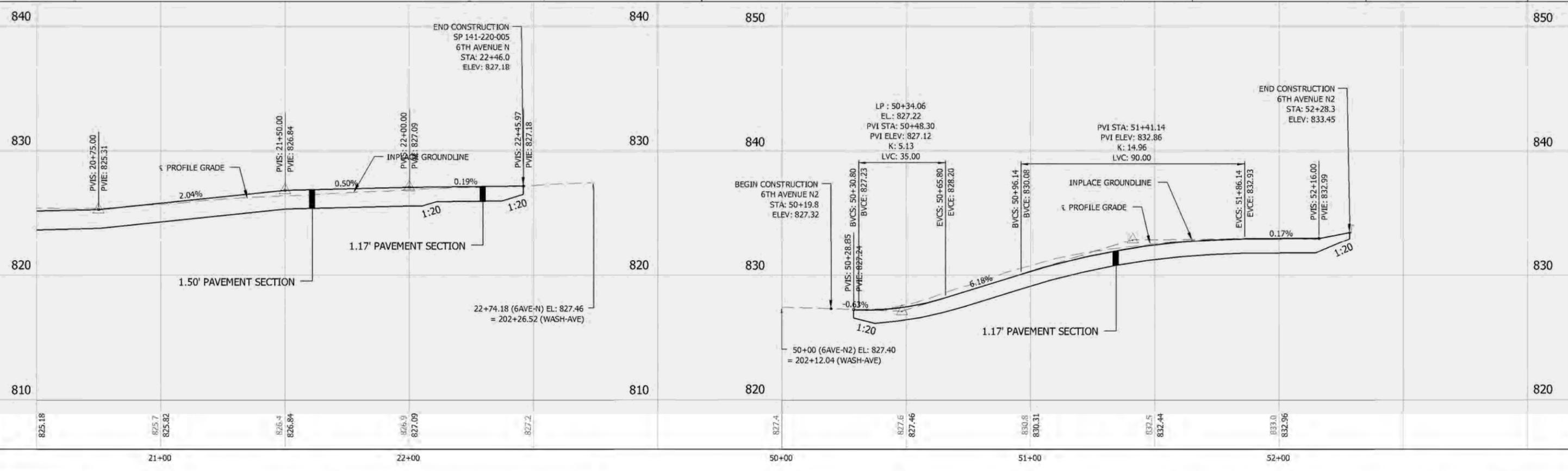
CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

THE ELECTRICAL SYMBOLS AND NOTATION ARE SUBJECT TO ALL APPLICABLE CODES AND REGULATIONS. THE CONTRACTOR SHALL VERIFY THE ACCURACY OF ALL DIMENSIONS AND LOCATIONS BEFORE PRODUCTION. SCALE: AS SHOWN. ANY DISCREPANCY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.



- NOTES:
1. DIMENSIONS FOR 6TH AVE N ARE BASED ON 6AVE-N ALIGNMENT.
  2. DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  3. SEE SHEETS 51-53 FOR PEDESTRIAN CURB RAMP DETAILS.
  4. SEE SHEETS 54-57 FOR DRAINAGE PLAN AND PROFILES.

	3.5" CONCRETE WALK		SEE DRIVEWAY STD. PLATES ROAD 2001-2003 FOR DETAILS
	6.0" CONCRETE WALK		MATCH EXISTING CROSS SLOPE
	8.0" CONCRETE DRIVEWAY PAVEMENT		



Plot Date: 09/14/2016 4:02pm  
 Plot Path: \\s01\proj\19382981\Drawings\19382981\_000.dwg  
 Plot Scale: 1"=40'-0" (AS SHOWN)  
 Plot Orientation: Landscape  
 Plot Title: 6TH AVE N STREET IMPROVEMENTS

NO.	DATE	APPR.	REVISION

SURVEY: MPV  
 DRAWN: JDP  
 DESIGNED: JDP  
 CHECKED: CIVL  
 APPROVED: LFG  
 PROJ. NO.: 19382981  
 DATE: 08/15/16  
 LIC. NO.: 41964

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
 PRINT NAME: JEFFREY DANIELSON  
 SIGNATURE: *[Signature]*  
 DATE: 08/15/16 LIC. NO.: 41964

**Minneapolis**  
 City of Lakes

**Stantec**  
 2335 Highway 36 W  
 St. Paul, MN 55113  
 www.stantec.com

CONSTRUCTION PLAN AND PROFILE

CITY OF MINNEAPOLIS, MN  
 6TH AVENUE N STREET IMPROVEMENTS  
 SP 141-220-005

# Attachment B

## Agency Coordination

- Mn/DOT's Cultural Resources Unit (CRU) letter for Historic/Archaeological determination.
- Concurrence letter from the SHPO on 30% Plans – May 15, 2015
- CRU letter to SHPO on 60% Plan Review – June 1, 2015
- Concurrence letter from the SHPO on 60% Plans - July 1, 2015
- Section 4(f) Temporary Use Letter
- Mn/DOT's Office of Environmental Services (OES) letter for Federally listed species determination
- MnDNR Letter



**Minnesota Department of Transportation**

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**Office of Environmental Stewardship**

Mail Stop 620  
395 John Ireland Boulevard  
St. Paul, MN 55155-1899

Office Tel: (651) 366-4291

Fax: (651) 366-3603

March 24, 2015

Sarah Beimers, Manager  
Government Programs & Compliance  
State Historic Preservation Office  
Minnesota Historical Society  
345 Kellogg Blvd. W.  
St. Paul, MN 55101

Re: S.P. 141-220-005 (6<sup>th</sup> Avenue North Reconstruction, Minneapolis, Hennepin County)

Dear Ms. Beimers,

We have reviewed the above-referenced undertaking pursuant to our FHWA-delegated responsibilities for compliance with Section 106 of the National Historic Preservation Act, as amended (36 CFR 800), and as per the terms of the applicable Programmatic Agreements between the FHWA and the Minnesota State Historic Preservation Office (SHPO).

This project will reconstruct 6th Avenue North from 200 feet north of Washington Avenue to 5th Street North. The project will salvage and reinstall the clay pavers in the segment of 6th Avenue North between 4th Street N. and Washington Avenue. It will also construct ADA-compliant sidewalks on both sides of 6th Avenue and a subsurface infiltration stormwater system. Additionally, city street lighting will be updated or added through this corridor as needed and some sidewalk trees planted. It is anticipated that the project will not require right of way or temporary easements. This reconstruction was first proposed as part of the Warehouse District Heritage Street Plan (2011, which the Minneapolis Heritage Preservation Commission and the State Historic Preservation Office participated) which provides an approach to preserve and rehabilitate the historic infrastructure, improve pedestrian accessibility and increase sustainable practices through better stormwater management.

The area of potential effects (APE) for the project consists of the proposed construction limits. Liz Abel, MnDOT CRU historic archeologist, reviewed the project APE, and found that since all work will occur within previously disturbed ground for the reconstruction of 6th Avenue North and the existing sidewalks, it is unlikely that the APE contains intact, significant archaeological resources.

The APE for architectural history is limited to the first tier of properties along 6th Avenue North. The APE takes into account physical changes, atmospheric changes, changes to traffic patterns, and visual effects to historic properties.

The project APE is partially located within the NRHP-listed Minneapolis Warehouse Historic District (HE-MPC-0441). The district also has local designation by the City of Minneapolis, and has design guidelines for the district. The historic district is listed under Criterion A as an area of early commercial growth during the development of Minneapolis and for its role as Minneapolis became a major distribution and jobbing center in the upper Midwest. It is also listed under Criterion C as an intact concentration of commercial buildings designed by important architects during the late nineteenth and early

twentieth centuries.

The overall project is considered rehabilitation since many alterations have occurred in the district and to the streetscape since the period of significance. While all the Secretary of Interior's Standards for Rehabilitation apply, these two are most relevant to the proposed project.

*2) The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.*

*5. Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize the preserved historic property shall be preserved.*

Historic pavers located along 6<sup>th</sup> Avenue will be salvaged and used within the thru-lane/ parking portions street. Pavers will be removed from the block between 5<sup>th</sup> St. N and 4<sup>th</sup> St. N., which is outside the historic district boundary, and if salvageable used in the drive/park lanes between 4<sup>th</sup> St. N and Washington Avenue. In addition no work will occur on the existing loading docks which are a character-defining feature.

The proposed street lighting within the district is a typical lighting fixture currently used throughout the historic district (see attached specification sheets for lights). Sidewalks will be widened in some areas in order to meet ADA compliance, but the overall street grid will remain symmetrical. While these items constitute changes to the district, they are needed safety improvements to keep the area vital and useable as a retail/commercial/residential area. The sidewalk trees will not be planted within the district.

The project will preserve the historic vitrified clay paver brick surface and loading docks, which are important historic fabric to the district, and will only introduce modern safety features (lighting and ADA compliant sidewalks and crossing) that will not negatively impact the historic district's integrity of location, design, setting, materials, workmanship, feeling and setting.

Therefore the finding of this office is that there will be **No Adverse Effects** to historic properties by the project as currently proposed. If the project scope changes, we will provide your office with the revised information to conduct an additional review.

Sincerely,



Renée Hutter Barnes, Historian  
Cultural Resources Unit

#### Attachments

cc: Theresa Maahs, Stantec  
MnDOT CRU Project File

May 15, 2015

Ms. Renée Hutter Barnes  
Cultural Resources Unit  
MN Dept of Transportation, MS 620  
395 John Ireland Blvd  
St Paul, MN 55155

RE: SP 141-220-005, 6<sup>th</sup> Avenue North Reconstruction  
Minneapolis, Hennepin County  
SHPO Number: 2015-1581

Dear Ms Hutter Barnes:

Thank you for the opportunity comment on the above project. Information received in our office regarding this undertaking has been reviewed pursuant to the responsibilities given the State Historic Preservation Officer by the National Historic Preservation Act of 1966 and implementing federal regulations at 36 CFR 800, and per the terms of the 2005 Programmatic Agreement between the Federal Highway Administration, the Minnesota Department of Transportation and the Minnesota State Historic Preservation Office.

Your correspondence dated 26 March 2015 indicates that the proposed 6<sup>th</sup> Avenue North Reconstruction project is located within the Minneapolis Warehouse Historic District which is listed in the National Register of Historic Places (NRHP) and, as such, this is the only identified historic property within the area of potential effect (APE) for this undertaking. We agree with this determination.

In order for our office to concur with a determination that the project will not adversely affect the historic property, it must be designed in conformance with the Secretary of the Interior's *Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings* (Standards), in particular the Standards and associated guidelines pertaining to new construction in historic districts.

Because this undertaking is located within the City of Minneapolis which has a Heritage Preservation Commission (HPC), pursuant to Stipulation 3.a.iii of the PA your agency has invited the HPC to participate in this Section 106 review as a consulting party. This particular undertaking is located within a NRHP-listed historic district which has also been locally designated by the City of Minneapolis and therefore any proposed projects in this district are subject to review by the HPC.

We have completed our review of the submitted project materials, including:

- Initial Section 106 project review correspondence dated March 24, 2014 including preliminary design plans for the streetscape project (Stantec, 2/6/2015) and spec sheets for proposed light standards;

- Additional project information and photographs of current conditions on 6<sup>th</sup> Avenue North as included in your 4/23/2015 e-mail to our office; and
- Email dated 5/12/2015 from Beth Elliott, City of Minneapolis, to our office regarding the HPC's role in decision-making and review of this project.

It is our understanding that, in addition to the Standards, several other locally-developed planning and historic preservation guidance documents have been utilized during design development for the proposed 6<sup>th</sup> Avenue North streetscape improvements. These documents include the *Minneapolis Warehouse Historic District Guidelines*, the *North Loop Small Area Plan*, and the *Warehouse District Heritage Street Plan*. We have also been made aware of the fact that the HPC has participated in design review of this project through the 60% design development stage and will ultimately review a Certificate of Appropriateness for the project later this year upon completion of final design.

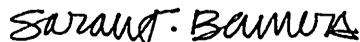
At this point, we will concur with your determination that the project, as currently proposed, has been designed in conformance with the Standards and therefore will have **no adverse effect** on the historic property provided that the following conditions are met:

- the final design for the streetscape project will be consistent with the design we have reviewed at this time; and
- the Minneapolis HPC approves the Certificate of Appropriateness for the federal project.

Upon completion, we request that your agency submit documentation that that these conditions have been met. If your agency determines that these conditions cannot be met, please contact our office to re-initiate consultation on this project.

Please contact me (651) 259-3456 or [sarah.beimers@mnhs.org](mailto:sarah.beimers@mnhs.org) if you have any questions regarding our review of this project.

Sincerely,



Sarah J. Beimers, Manager  
Government Programs & Compliance

cc: Beth Elliott, City of Minneapolis, CPED  
Laura Faucher, Chair, Minneapolis HPC



**Minnesota Department of Transportation**

---

**Office of Environmental Stewardship**

Mail Stop 620  
395 John Ireland Boulevard  
St. Paul, MN 55155-1899

Office Tel: (651) 366-4291

Fax: (651) 366-3603

June 1, 2015

Sarah Beimers, Manager  
Government Programs & Compliance  
State Historic Preservation Office  
Minnesota Historical Society  
345 Kellogg Blvd. W.  
St. Paul, MN 55101

Re: S.P. 141-220-005 (6<sup>th</sup> Avenue North Reconstruction 60% plans, Minneapolis, Hennepin County)

Dear Ms. Beimers,

We have reviewed the above-referenced undertaking pursuant to our FHWA-delegated responsibilities for compliance with Section 106 of the National Historic Preservation Act, as amended (36 CFR 800), and as per the terms of the applicable Programmatic Agreements between the FHWA and the Minnesota State Historic Preservation Office (SHPO).

This project will reconstruct 6th Avenue North from 200 feet north of Washington Avenue to 5th Street North. The project will salvage and reinstall the clay pavers in the segment of 6<sup>th</sup> Avenue North between 4th Street N. and Washington Avenue. It will also construct ADA-compliant sidewalks on both sides of 6th Avenue and a subsurface infiltration stormwater system. Additionally, city street lighting will be updated or added through this corridor as needed and some sidewalk trees planted. It is anticipated that the project will not require right of way or temporary easements. This reconstruction was first proposed as part of the Warehouse District Heritage Street Plan (2011, which the Minneapolis Heritage Preservation Commission and the State Historic Preservation Office participated) which provides an approach to preserve and rehabilitate the historic infrastructure, improve pedestrian accessibility and increase sustainable practices through better stormwater management.

We have previously submitted 30% plans, now we are submitting 60% plans for your review and records. There are no design changes between the 30% and 60%. Additionally the conditions set by your office in the May 15, 2015 letter will be met, but not till later this year, when the 90% plans have been reviewed by the Minneapolis HPC. The Minneapolis HPC are continually involved in this process and agreed to be a consulting party. Until then MnDOT CRU will continue to monitor the design and if any substantial changes that could adversely affect the district are proposed, we will contact SHPO as soon as possible.

Therefore the continued finding of this office is that there will be **No Adverse Effects** to historic properties by the project as currently proposed and as long as the previous conditions are met. If the project scope changes, we will provide your office with the revised information to conduct an additional review. If you have any additional comments please submit them within the 30 day review period, if there are none within that time we will assume the 60% plans continue to meet the Standards and District Guidelines. We will submit the 90% and the Minneapolis HPC decision on the certificate of appropriateness as we receive the information.

Sincerely,

A handwritten signature in black ink that reads "Renée Hutter Barnes". The signature is written in a cursive, flowing style.

Renée Hutter Barnes, Historian  
Cultural Resources Unit

#### Attachments

cc: Theresa Maahs, Stantec  
Beth Elliott, City of Minneapolis, CPED  
Laura Faucher, Chair, Minneapolis HPC  
MnDOT CRU Project File

MINNESOTA HISTORIC PRESERVATION OFFICE

July 1, 2015

Ms. Renée Hutter Barnes  
Cultural Resources Unit  
MN Dept of Transportation, MS 620  
395 John Ireland Blvd  
St Paul, MN 55155

RE: SP 141-220-005, 6<sup>th</sup> Avenue North Reconstruction  
Minneapolis, Hennepin County  
SHPO Number: 2015-1581

Dear Ms Hutter Barnes:

Thank you for continuing consultation on the above project. Information received in our office on 3 June 2015 regarding this undertaking has been reviewed pursuant to the responsibilities given the State Historic Preservation Officer by the National Historic Preservation Act of 1966 and implementing federal regulations at 36 CFR 800, and per the terms of the 2005 Programmatic Agreement between the Federal Highway Administration, the Minnesota Department of Transportation and the Minnesota State Historic Preservation Office.

On May 15, 2015 we provided concurrence with your agency's "no adverse effect" determination provided that the following conditions were met:

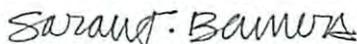
- the final design for the streetscape project would be consistent with the design we had reviewed at that time; and
- the Minneapolis HPC approves the Certificate of Appropriateness for the federal project.

We have completed our review of the 60% design development plans for this project and your accompanying correspondence in which you indicate that no changes to the design have occurred since our earlier review at the 30% stage. Therefore, our earlier Section 106 determination remains valid.

Also, you have indicated that the Minneapolis HPC will complete its review later this year. We request that your agency submit documentation of the HPC approval and a statement that the above conditions have been met. If your agency determines that these conditions cannot be met, please contact our office to re-initiate consultation on this project.

Please contact me (651) 259-3456 or [sarah.beimers@mnhs.org](mailto:sarah.beimers@mnhs.org) if you have any questions regarding our review of this project.

Sincerely,



Sarah J. Beimers, Manager  
Government Programs & Compliance

cc: Beth Elliott, City of Minneapolis, CPED  
Laura Faucher, Chair, Minneapolis HPC



**Minneapolis**  
City of Lakes

**Department of  
Public Works**  
Steven A. Kotke, P.E.  
City Engineer  
Director

350 South 5th Street - Room 203  
Minneapolis MN 55415

Office 612 673-3000  
Fax 612 673-3565  
TTY 612 673-2157

Date: August 4, 2015

Ms. Sarah Beimers  
Manager of Government Programs and Compliance  
Minnesota State Historic Preservation Office  
345 W. Kellogg Boulevard  
St. Paul, MN 55102

AUG 07 2015

**RE: 6<sup>th</sup> Avenue North Reconstruction  
SP 141-220-005  
Temporary occupancy in the Historic Warehouse District**

Ms. Beimers:

The City of Minneapolis, in cooperation with MnDOT, is proposing to reconstruct approximately 0.27 miles of 6<sup>th</sup> Avenue from 5th St N to the dead end north of Washington Ave N. Currently, this segment of 6<sup>th</sup> Avenue has several areas of broken or non-existent curb, and the driving surface provides a mixture of street pavers and asphalt patches. The current condition of the roadway requires frequent maintenance. Although sidewalk currently exists along the majority of the corridor, it varies in width and slope, and does not consistently meet ADA standards.

This project proposes to fully reconstruct the roadway and sidewalk through the corridor, while keeping the historic loading docks intact. The attached typical sections (Figures 2-6) compare the existing corridor and the proposed corridor. The project will involve complete removal of the roadway section including historic pavers, replacement of the existing storm sewer, reconstruction of the roadway, restoration and reinstallation of the historic pavers and reconstruction of the sidewalk as an ADA-compliant pedestrian walkway. The corridor will remain a two lane roadway with parking on both sides.

This project falls within a historically designated area (Historic District) and design of the street follows guidance contained in the Warehouse District Heritage Streets Plan. The project has also been designed in coordination with the City of Minneapolis's Heritage Preservation Commission (HPC). The City has been awarded federal funding to reconstruct the 6<sup>th</sup> Avenue roadway.

A portion of 6<sup>th</sup> Avenue is located within the Historic District: from a point midblock between 3<sup>rd</sup> Street and 4<sup>th</sup> Street through to the dead end north of Washington Ave N. Figure 1 illustrates the limits of the proposed project in relation to the Historic District. The segment of 6<sup>th</sup> Avenue within the historic district is considered to be a property subject to Section 4(f) regulations (23 CFR 774).

As per the 23 CFR 774.13(d), a temporary occupancy does not constitute a Section 4(f) use when all of the conditions listed below are satisfied:

- **Duration must be temporary, less than the time needed for construction of the project, and there should be no change in ownership of the land;**



*SP 141-220-005 is anticipated to be constructed under a single contract with a total estimated duration of one construction season (approximately 6 months). The work within the historic district is anticipated to be substantially completed within that six-month period.*

*The City of Minneapolis will retain ownership of the land on which the 6<sup>th</sup> Avenue corridor is located, from existing Right of Way to existing Right of Way throughout the corridor. The City Public Works Department will also retain jurisdiction over 6<sup>th</sup> Avenue as a City street, including the roadway and sidewalk. No change in ownership will occur.*

- **Scope of the work must be minor, both the nature and the magnitude of the changes to the Section 4(f) property are minimal;**

*The scope of work within the Section 4(f) property will include the complete removal and replacement of the roadway section including historic pavers, storm sewer and sidewalk. This project will not require right of way or temporary easements. This reconstruction was first proposed as part of the Warehouse District Heritage Street Plan (2011), which provides an approach to preserve and rehabilitate the historic infrastructure, improve pedestrian accessibility and increase sustainable practices through better stormwater management. Both the HPC and the State Historic Preservation Office (SHPO) participated in the development of the Plan. The design guidelines that will be followed in the design of this project are set forth in the Warehouse District Heritage Street Plan. Within the Heritage Street Plan, 6<sup>th</sup> Avenue is considered a 'Freight Street'. The Minneapolis Warehouse District Heritage Street design guidelines list the following requirements for 'Freight Streets', which are met by this project:*

*1.11. Loading docks and canopies dating from the period of significance shall be preserved and retained.*

*1.14. On freight streets, Street Design: The main aspects to be considered when improving freight streets shall include the preservation of historic loading docks and canopies to reflect their service function and proper management of vehicular and service access to the property.*

*1.22. Original historic street paving materials shall be maintained and preserved.*

*1.24. Loading docks shall be preserved. Their heights or widths shall not be altered.*

*1.25. The visual corridors created by the public and private roadways, bridges, alleys, and former rail corridors or other infrastructure are significant and shall be preserved.*

*1.26. New or replacement street furnishings such as street lights and street furniture shall be compatible with the character of the historic district in terms of location, design, materials, color, and scale.*

*This work is necessary to preserve the historic paver brick street surface of 6<sup>th</sup> Avenue, while providing ADA accessible connections through the North Loop neighborhood. The existing sidewalk along the corridor currently blends in and out of large concrete areas in front of the existing buildings and varies in cross slope and width. Along with reconstructing the roadway, the proposed project includes reconstructing these variable areas of concrete. The proposed project will include a Pedestrian Access Route (PAR) that will provide a consistent minimum 6 foot "corridor" that has a maximum cross slope of 1.5%. The project was designed to meet the requirements in the design guidelines and subsequently, will result in minimal change to the corridor. No change to the loading docks will occur with this project.*

- **There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;**

*There are no anticipated permanent adverse physical impacts associated with this work. The SHPO and MnDOT's CRU have provided letters of 'no adverse effect' for the proposed project as a result of the Section 106 process for Historic Properties.*

- **The land being used must be fully restored, the property must be returned to a condition which is at least as good as that which existed prior to the project;**

*The roadway and sidewalk areas will be fully restored to its prior use upon completion of the project. Repairing, replacing and stabilizing the pavers with a new base and curb lines will restore the paver surface in an improved condition. Improving the sidewalk to include an ADA accessible PAR will provide a safer route for pedestrians.*

- **There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.**

Please review the attached figures and indicate your concurrence with the work proposed, and that the above conditions are met, by signing below. Please forward the signed original back to me for our records. I will forward this information to the Mn/DOT State Aid Office for concurrence by the Federal Highway Administration.

If you have questions regarding this matter, feel free to contact me at your earliest convenience at (651) 604-4850. Thank you.

Sincerely,



Lee Mann, P.E.  
City of Minneapolis Project Manager

I concur that the proposed work constitutes a Temporary Occupancy of 6<sup>th</sup> Avenue North within the historic district as noted.



Ms. Sarah Beimers  
Minnesota State Historic Preservation Office

8/18/2015  
Date

**Attachments:**

1. Figure 1 – Project Location Map
2. Figures 2-6 – Existing and Proposed Typical Sections
3. Figures 7a-b – 6<sup>th</sup> Avenue Reconstruction Plan View

cc: Renee Hutter Barnes, MnDOT Cultural Resources Unit

Maahs-Henderson, Theresa

---

From: Alcott, Jason (DOT) <jason.alcott@state.mn.us>  
Sent: Monday, September 15, 2014 2:01 PM  
To: Maahs-Henderson, Theresa  
Cc: Reihl, Gary (DOT); Moynihan, Debra (DOT)  
Subject: S.P. 141-220-005- ESA (Section 7) - Determination of No Effect/No Jeopardy

No Effect Determination/No Jeopardy:  
S.P. 141-220-005, 6<sup>th</sup> Avenue North  
Roadway Reconstruction  
City of Minneapolis, Hennepin County

Federally-Listed Species/Designated Critical Habitat in the Action Area

Section 7 of Endangered Species Act of 1973, as amended (Act), requires each Federal agency to review any action that it funds, authorizes or carries out to determine whether it may affect threatened, endangered, proposed species or listed critical habitat. Federal agencies (or their designated representatives) must consult with the U.S. Fish and Wildlife Service (Service) if any such effects may occur as a result of their actions. Consultation with the Service is not necessary if the proposed action will not directly or indirectly affect listed species or critical habitat. If a federal agency finds that an action will have no effect on listed species or critical habitat, it should maintain a written record of that finding that includes the supporting rationale. According to the official County Distribution of Minnesota's Federally-Listed Threatened, Endangered, Proposed, and Candidate Species list (revised in July 2014), maintained by the Service, the project county is within the distribution range of the following:

Hennepin	<a href="#">Northern long-eared bat</a> <i>Myotis septentrionalis</i>	Proposed as Endangered	Hibernates in caves and mines - swarming in surrounding wooded areas in autumn. Roosts and forages in upland forests during spring and summer.
	<a href="#">Higgins eye pearl mussel</a> <i>(Lampsilis higgins)</i>	Endangered	Mississippi River
	<a href="#">Snuffbox</a> <i>(Epioblasma triquetra)</i>	Endangered	Mississippi River

**Based on the nature and location of the activities, a determination of no effect has been made.**

Proposed Federal Species in the Action Area

Section 7(a)(4) requires Federal agencies to confer with the Services on any agency action that is likely to jeopardize the continued existence of any species proposed for listing or result in the adverse modification of critical habitat proposed to be designated. A conference may involve informal discussions between the Services, the action agency, and the applicant. Following informal conference, the Services issue a conference report containing recommendations for reducing adverse effects. These recommendations are discretionary, because an agency is not prohibited from jeopardizing the continued existence of a proposed species or from adversely modifying proposed critical habitat. However, as soon as a listing action is finalized, the prohibition against jeopardy or adverse modification applies, regardless of the stage of the action.

According to the official County Distribution of Minnesota's Federally-Listed Threatened, Endangered, Proposed, and Candidate Species list (revised in July 2014), maintained by the Service, the project county is within the distribution range of the Northern long-eared bat (*Myotis septentrionalis*) which is currently proposed for listing as an endangered species (Federal Register October 2, 2013). The Service is currently working on developing consultation guidance for lead federal agencies to use in making determinations of effect for this species. Until this guidance is distributed and the species officially listed, the lead federal agency must assess the potential for jeopardy. **According to discussions with the Service, projects that are minor in scope and with limited potential for impacts (as is the case for this project) are not likely to jeopardize the continued existence of this species. Please note: if the project has not been completed by the time the listing becomes official, further coordination and possible consultation with the Service may be necessary.**

Jason Alcott  
Minnesota Department of Transportation  
Office of Environmental Stewardship  
395 John Ireland Boulevard  
St. Paul, MN 55155  
Phone: 651-366-3605



# Minnesota Department of Natural Resources

Division of Ecological and Water Resources, Box 25

500 Lafayette Road

St. Paul, Minnesota 55155-4025

Phone: (651) 259-5109 E-mail: [lisa.joyal@state.mn.us](mailto:lisa.joyal@state.mn.us)

October 6, 2014

[Transmitted via Electronic Mail]

Theresa Maahs  
Stantec  
2335 West Highway 36  
St. Paul, MN 55113

Dear Ms. Maahs,

The DNR Division of Ecological and Water Resources has determined that we do not need to review the proposed 6<sup>th</sup> Avenue North Reconstruction project. Given that there will not be any disturbances, we do not believe it would result in a significant impact to threatened or endangered species or critical habitats, and as such would not trigger the preparation of an Environmental Impact Statement under the National Environmental Policy Act. We further believe that such activities would be in compliance with Minnesota's Endangered Species Statute (*Minnesota Statutes*, section 84.0895) and associated Rules (*Minnesota Rules*, part 6212.1800 to 6212.2300 and 6134).

Please contact me if you have any questions or concerns about this decision. Thank you for your interest in preserving Minnesota's rare natural resources.

Sincerely,

A handwritten signature in black ink that reads "Samantha Bump". The signature is written in a cursive, flowing style.

Samantha Bump  
Natural Heritage Review Specialist

# Attachment C

## Public Meeting Information

- Public Meeting 1 Materials
- Public Meeting 2 Materials



**Department of  
Public Works**

Steven A. Kotke, P.E.  
City Engineer  
Director

350 South 5th Street - Room 203  
Minneapolis MN 55415

Office 612 673-3000  
Fax 612 673-3565  
TTY 612 673-2157

September 19, 2014

**RE: 6<sup>th</sup> Avenue North Reconstruction Project**

Dear Property Owner/Tax Payer,

You are invited to attend an informational meeting regarding the proposed reconstruction of 6<sup>th</sup> Avenue North from 5<sup>th</sup> St North to just north of Washington Avenue (location map attached). The City of Minneapolis has identified this corridor for roadway and sidewalk improvements scheduled to begin construction in 2016. This project falls within a historically designated area and is subject to recommendations included in the City's *Warehouse District Heritage Streets Plan*. The purpose of the meeting is to introduce the project to the neighborhood, and to solicit input from residents and property owners regarding the project to supplement the design guidance contained in the *Heritage Streets Plan*. This is the first project meeting for the 6<sup>th</sup> Avenue Reconstruction Project. The City plans to conduct follow-up meetings with property owners, neighborhood groups as well as additional general informational meetings over the fall and winter, to assist with the design process and incorporate input from the community prior to seeking City Council action in early 2015.

The meeting is scheduled for **October 6<sup>th</sup> at 5:00 – 6:00 PM**. A brief presentation will be given at 5:00 PM. The meeting will be held at the Fulton Brewery taproom (the bar will not be in service during this meeting).

**Fulton Brewery**  
414 6<sup>th</sup> Avenue North  
Minneapolis MN 55401

If you are unable to attend or need this material in an alternative format please call Lee Mann at 651-604-4850 or email [lee.mann@stantec.com](mailto:lee.mann@stantec.com) to make arrangements for receiving the information presented at the meeting. Deaf and hard-of-hearing persons may use a relay service to call 311 agents at (612) 673-3000. TTY users may call (612) 673-2157 or (612) 673-2626.

Attention: If you have any questions regarding this material please call 311 Hmong - Ceeb toom. Yog koj xav tau kev pab txhais cov xov no rau koj dawb, hu (612) 673-2800;  
Spanish - Atención. Si desea recibir asistencia gratuita para traducir esta información, llame al teléfono (612) 673-2700;  
Somali - Ogow. Haddii aad dooneyso in lagaa kaalmeeyo tarjamadda macluumaadkani oo lacag la' aan wac (612) 673-3500

We look forward to meeting you and hearing your input on this project.

Sincerely,

Jeff Handeland  
City Street Design Supervisor









**Department of  
Public Works**

Steven A. Kotke, P.E.  
City Engineer  
Director

350 South 5th Street - Room 203  
Minneapolis MN 55415

Office 612 673-3000  
Fax 612 673-3565  
TTY 612 673-2157

January 5, 2015

**RE: 6<sup>th</sup> Avenue North Reconstruction Project**

Dear Property Owner/Tax Payer,

You are invited to attend an informational meeting regarding the proposed reconstruction of 6<sup>th</sup> Avenue North from 5<sup>th</sup> St North to just north of Washington Avenue (location map attached). The City of Minneapolis has identified this corridor for roadway and sidewalk improvements scheduled to begin construction in 2016. This project falls within a historically designated area and is subject to recommendations included in the City's *Warehouse District Heritage Streets Plan*. The purpose of the meeting is to present the project layout information and, to receive feedback from residents and property owners regarding the project relative to the design guidance contained in the *Heritage Streets Plan*. This is the second project meeting for the 6<sup>th</sup> Avenue Reconstruction Project.

The meeting is scheduled for **January 21<sup>st</sup>, 2015 at 5:00 – 6:00 PM**. A brief presentation will be given at 5:00 PM. The meeting will be held at the Metro Transit Offices - FTH Chambers:

Metro Transit – FTH Chambers  
560 Sixth Avenue North  
Minneapolis, MN 55411

If you are unable to attend or need this material in an alternative format please call Lee Mann at 651-604-4850 or email [lee.mann@stantec.com](mailto:lee.mann@stantec.com) to make arrangements for receiving the information presented at the meeting. Project information is also available on the City's website for the project at <http://www.ci.minneapolis.mn.us/cip/all/WCMS1P-130827>. Deaf and hard-of-hearing persons may use a relay service to call 311 agents at (612) 673-3000. TTY users may call (612) 673-2157 or (612) 673-2626.

Attention: If you have any questions regarding this material please call 311 Hmong - Ceeb toom. Yog koj xav tau kev pab txhais cov xov no rau koj dawb, hu (612) 673-2800; Spanish - Atención. Si desea recibir asistencia gratuita para traducir esta información, llame al teléfono (612) 673-2700; Somali - Ogow. Haddii aad dooneyso in lagaa kaalmeeyo tarjamadda macluumaadkani oo lacag la' aan wac (612) 673-3500

We look forward to seeing you and hearing your input on this project.

Sincerely,

Jeff Handeland  
City Street Design Supervisor





# Attachment D

## Street Lighting Documentation

- Example Petition
- Resolution to authorize Minnesota Ballpark Authority funding



**REVISED**  
**PETITION FOR STREET LIGHTING INSTALLATION**

6th Avenue North will be reconstructed from 5th Street North to the dead end north of Washington Avenue. Per the City of Minneapolis Street Lighting Policy, street lighting will be installed with the reconstruction project. There is an additional assessment for the street lighting installation. Property owners/taxpayers that are impacted by the street lighting assessment may option out through this petition

The total estimated cost for the street lighting installation is \$276,822 This would result in an assessment rate of \$1.81 per square foot of influence area. Should the project cost less than this estimate, the assessment rate will be reduced accordingly. The estimated street lighting assessment for your property at

**600 3RD ST N** is **\$2,107.29**

Assessments can be paid over a 20 year period following city assessment rules. If you do not support the installation of street lighting and the associated street lighting installation assessment, it is required that you sign and return this petition indicating you do not support the installation of street lights and associated street light installation assessment. Petitions not completed and returned, or returned indicating support for the project, will be counted in favor of the project, and assessment going forward. Petitions must be returned by April 2nd, 2015 to Lee Mann at 309 2nd Ave S, Room 300, Minneapolis, MN 55401.

This project will move forward unless owners/taxpayers representing 51% of the property in the impacted area do not support the project.

PID: **2202924130041**

Property Address: **600 3RD ST N**

Property Owner/Taxpayer Name: **B N & SANTA FE RR CO**

**I do not support the street lighting project moving forward and the additional assessment.**

**I support the street lighting project moving forward and the additional assessment.**

---

Signature of Property Owner/Taxpayer

Date

For additional information, contact the project manager for the project, Lee Mann, Stantec Consulting Services, at [lee.mann@stantec.com](mailto:lee.mann@stantec.com) or 651.604.4850. For general project information, see the project website at: <http://www.ci.minneapolis.mn.us/cip/all/WCMS1P-130827>

## **10-08-15 Proposed Resolution 15-MBA-116**

### **Purpose:**

To authorize the expenditure of Minnesota Ballpark Authority funding from the Ballpark Capital Projects Fund for pedestrian lighting along 6<sup>th</sup> Avenue North; to authorize MBA staff to finalize the scope, design and cost details with the City of Minneapolis; and to authorize the Chair and Executive Director to enter into any necessary inter-agency agreements to achieve these improvements.

### **Background:**

In April 2008, the Minnesota Ballpark Authority adopted Resolution 08-MBA-05R1 which created the Ballpark District Enhancements and Public Art Incentive fund.

The fund was set up to allow the MBA to work with other government agencies, private developers, and other interested parties to encourage the coordinated development of amenities that would serve both the long-term interests of the ballpark and this emerging area of the City. Priorities for this fund include public art, streetscape and way-finding enhancements.

As summarized in the attached document, over the past several years the Minnesota Ballpark Authority has worked with the Minnesota Twins, Target Corporation, Hennepin County, the Minnesota Department of Transportation, the City of Minneapolis, and other partners to complete several public realm improvement projects around both Target Field and Target Field Station.

In 2016, the City of Minneapolis will be investing \$4 million in the reconstruction of 6th Avenue North from 5th Street through Washington Avenue. While this is a key connection from the North Loop to Target Field and Target Field Station, the existing project scope does not include pedestrian lighting.

Consistent with the MBA's previous efforts, there is an opportunity to further enhance the pedestrian environment and the connections between the North Loop and Target Field. MBA staff is requesting authorization to work with the City of Minneapolis to enter into a cost participation agreement in an amount not-to-exceed \$300,000 to add pedestrian lighting to the 6<sup>th</sup> Avenue North project.

### **Action Requested:**

**BE IT RESOLVED** that the Minnesota Ballpark Authority authorizes the expenditure of funds from the Ballpark Capital Projects Fund in an amount not-to-exceed \$300,000, for the purpose of adding pedestrian lighting on 6<sup>th</sup> Avenue North, as part of the street reconstruction project being led by the City of Minneapolis.

**BE IT FURTHER RESOLVED** that MBA staff is authorized to finalize the scope, design and cost details with the City, and the Chair and Executive Director are authorized to enter into any necessary inter-agency agreements to achieve these improvements.

# Attachment E

## Design Variance Resolution

- Approved Design Variance Resolution

CITY OF MINNEAPOLIS  
CERTIFICATION

STATE OF MINNESOTA )  
COUNTY OF HENNEPIN ) SS  
CITY OF MINNEAPOLIS )

I, Casey Joe Carl, City Clerk of the City of Minneapolis, in the County of Hennepin, and State of Minnesota, certify that I have examined the attached copy of **RESOLUTION 2015R-431 Directing the City Engineer to proceed with a variance request from State Aid Rules per Administrative Variance to State Aid Operation Rules, Chapter 8820 effective 2011, including amendments adopted through February 4, 2013, to the Minnesota Commissioner of Transportation for the reconstruction of 6th Ave N between 5th St N and Washington Ave N**, adopted by the Minneapolis City Council at a meeting held on **October 23, 2015**, and have carefully compared the same with the original on file in this office, and that the attached copy is a true, correct and complete copy of the original.



IN WITNESS WHEREOF, I have signed and  
affixed the City seal on **November 2, 2015**.

  
Casey Joe Carl, City Clerk

2015R- 431  
**RESOLUTION  
of the  
CITY OF  
MINNEAPOLIS**

By Reich

**Directing the City Engineer to proceed with a variance request from State Aid Rules per Administrative Variance to State Aid Operations Rules, Chapter 8820 effective 2011, including amendments adopted through February 4, 2013, to the Minnesota Commissioner of Transportation for the reconstruction of 6th Ave N between 5th St N and Washington Ave N.**

Whereas, the City of Minneapolis wishes to reconstruct 6th Ave N between 5th St N and Washington Ave N; and

Whereas, Minnesota Department of Transportation State Aid Rule 8820.9936 (Design Standards, Urban; New or Reconstruction Projects) per Administrative Variance to State Aid Operation Rules, Chapter 8820, effective October 1, 2011, require "New or reconstruction projects for urban roadways must meet or exceed the minimum dimensions indicated in the following design chart"; and

Whereas, the design chart has design speed indicated for Collectors or Locals with Average Daily Traffic (ADT) less than 10,000 vehicles; and

Whereas, mixed development and the urban streetscape limit the ability to accommodate a vertical profile meeting a minimum design speed of 30 miles per hour (MPH) on 6th Ave N between 5th St N and Washington Ave N; and

Whereas, State Aid Rules provide that a political subdivision may request a variance from the Rules;

Now, Therefore, Be It Resolved by The City Council of The City of Minneapolis:

That the City Engineer is hereby directed to submit to the Commissioner of Transportation a formal request for variance from Minnesota Department of Transportation (MnDOT) State Aid Rule 8820.9936 per Administrative Variance to State Aid Operations Rules, Chapter 8820, effective 2011, including Amendments adopted through February 4, 2013, for Collectors or Locals with ADT less than 10,000 vehicles to permit the reconstruction of 6th Ave N between 5th St N and Washington Ave N with a vertical profile between Station 10+30 and 10+60 that meets a design speed of 14.7 MPH instead of 30 MPH as required by Rule 8820.9936; and with a vertical profile between Station 21+40 and 21+55 that meets a design speed of 15 MPH instead of 30 MPH as required by Rule 8820.9936.

Be It Further Resolved, that the City Council of the City of Minneapolis hereby indemnifies, saves, and hold harmless the State of Minnesota and its agents and employees of and from claims, demands, actions, or causes of action arising out of or by reason of the reconstruction of 6th Ave N between 5th St N and Washington Ave N in accordance with Minnesota Rules 8820.9936 and further agrees to defend at their sole cost and expense any action or proceeding commenced for the purpose of asserting any claim arising as a result of the granting this variance.

Certified as an official action of the City Council: 

RECORD OF COUNCIL VOTE (X INDICATES VOTE)													
COUNCIL MEMBER	AYE	NAY	ABSTAIN	ABSENT	VOTE TO OVERRIDE	VOTE TO SUSTAIN	COUNCIL MEMBER	AYE	NAY	ABSTAIN	ABSENT	VOTE TO OVERRIDE	VOTE TO SUSTAIN
Reich	X						Glidden	X					
Gordon	X						Cano	X					
Frey	X						Bender	X					
B Johnson	X						Quincy	X					
Yang	X						A Johnson	X					
Warsame	X						Palmisano	X					
Goodman	X												

ADOPTED OCT 23 2015  
DATE

ATTEST   
CITY CLERK

APPROVED  NOT APPROVED  VETOED

  
MAYOR HODGES

RECEIVED OCT 28 2015  
DATE

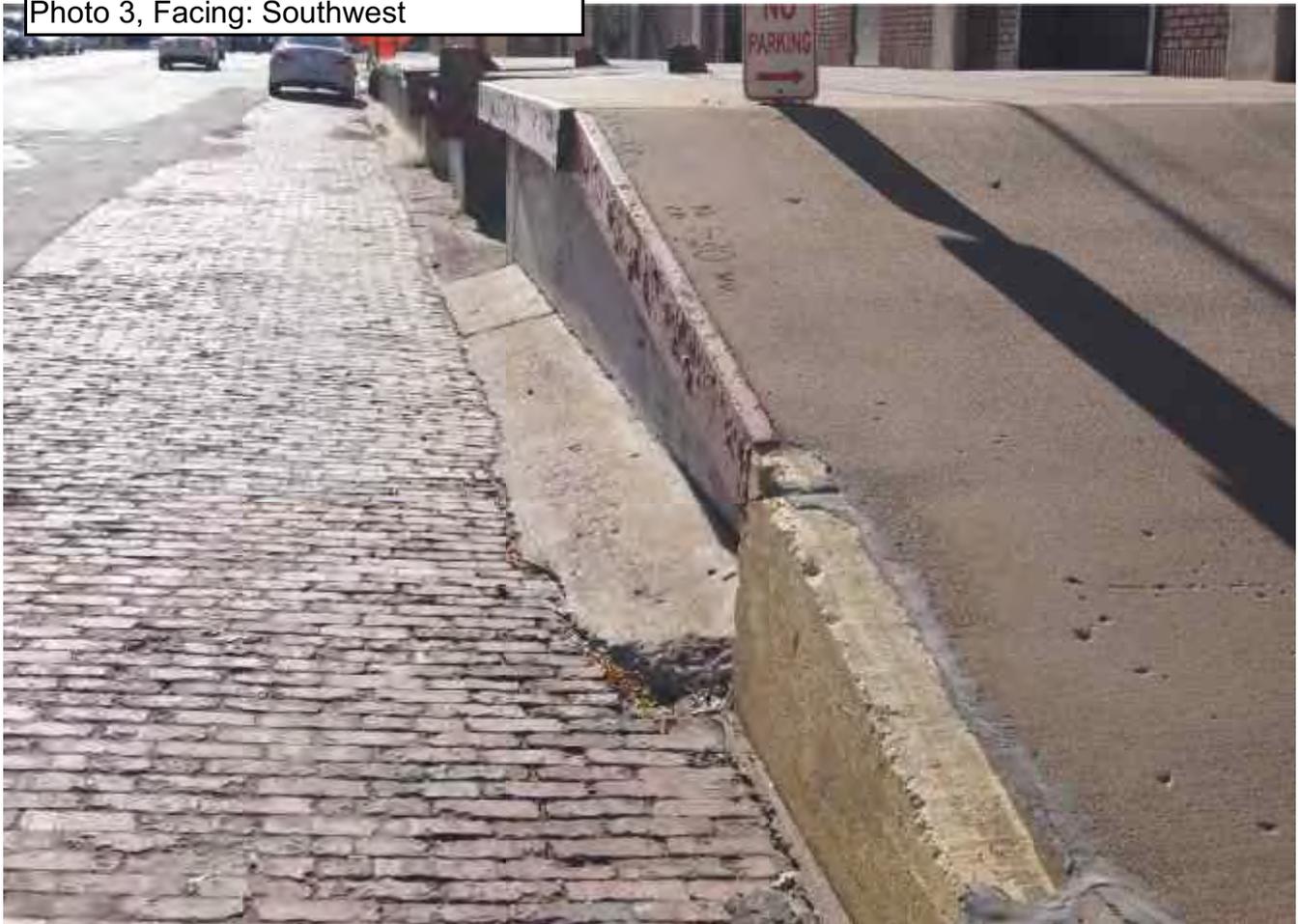
On: 6th Ave (west side), south of 3rd St.  
Photo: 1, Facing: Southwest



On: 6th Ave (west side), south of 3rd St.  
Photo 2, Facing: Southwest



On: 6th Ave (west side), south of 3rd St.  
Photo 3, Facing: Southwest



On: 6th Ave sidewalk (west side), south of 3rd St.  
Photo 4, Facing: Southwest



On: 6th Avenue at N 3rd Street  
Photo 5, Facing: west



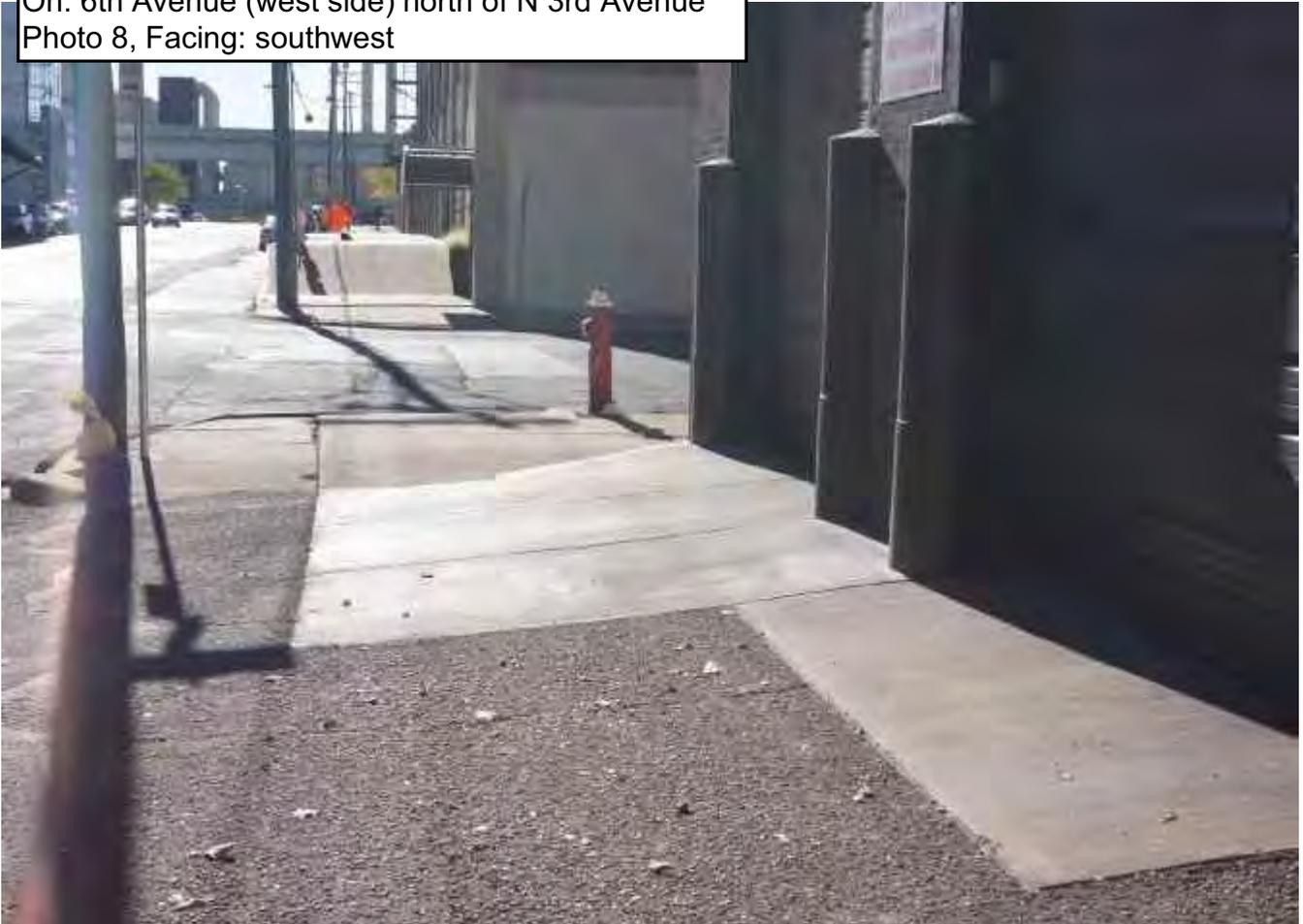
On: N 3rd Street (north side) at 6th Avenue  
Photo 6, Facing: northwest



On: N 3rd Street (south side) at 6th Avenue  
Photo 7, Facing: northwest



On: 6th Avenue (west side) north of N 3rd Avenue  
Photo 8, Facing: southwest



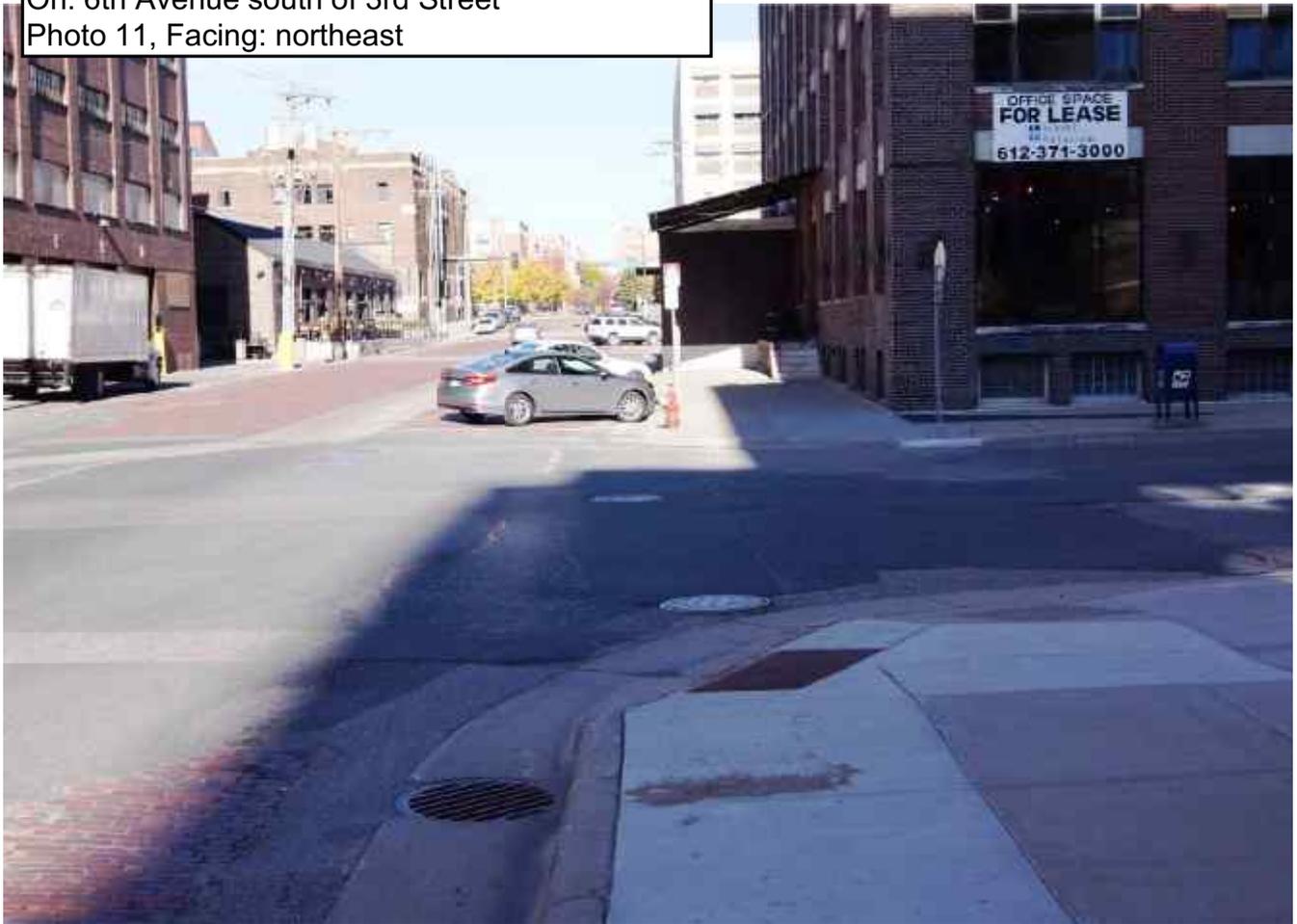
On: 6th Avenue (west side), south of Washington Ave  
Photo 9, Facing: southwest



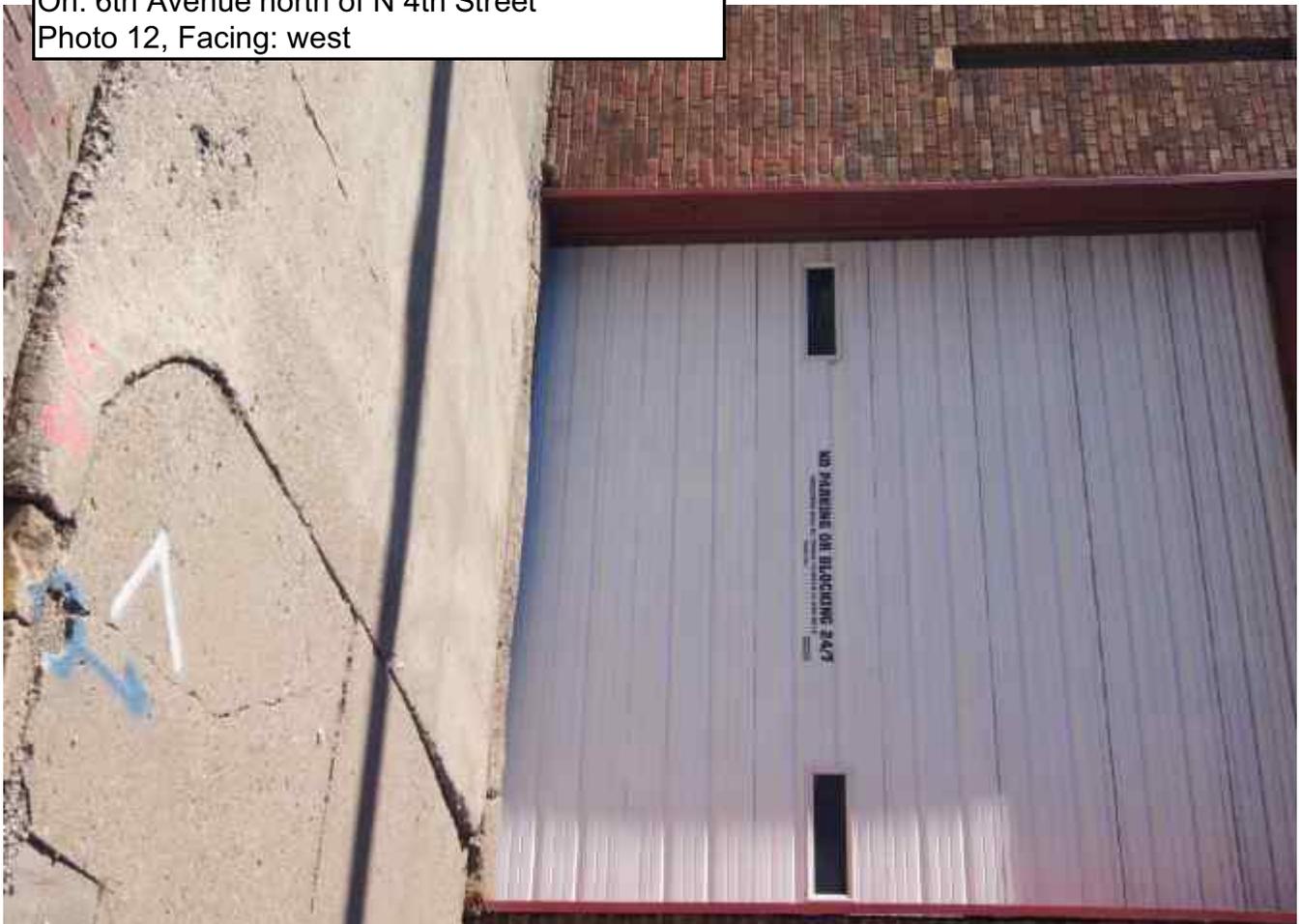
On: 6th Avenue at alley south of Washington Ave  
Photo 10, Facing: northeast



On: 6th Avenue south of 3rd Street  
Photo 11, Facing: northeast



On: 6th Avenue north of N 4th Street  
Photo 12, Facing: west



On: 6th Avenue (west side) at 3rd Street  
Photo 13, Facing: southwest



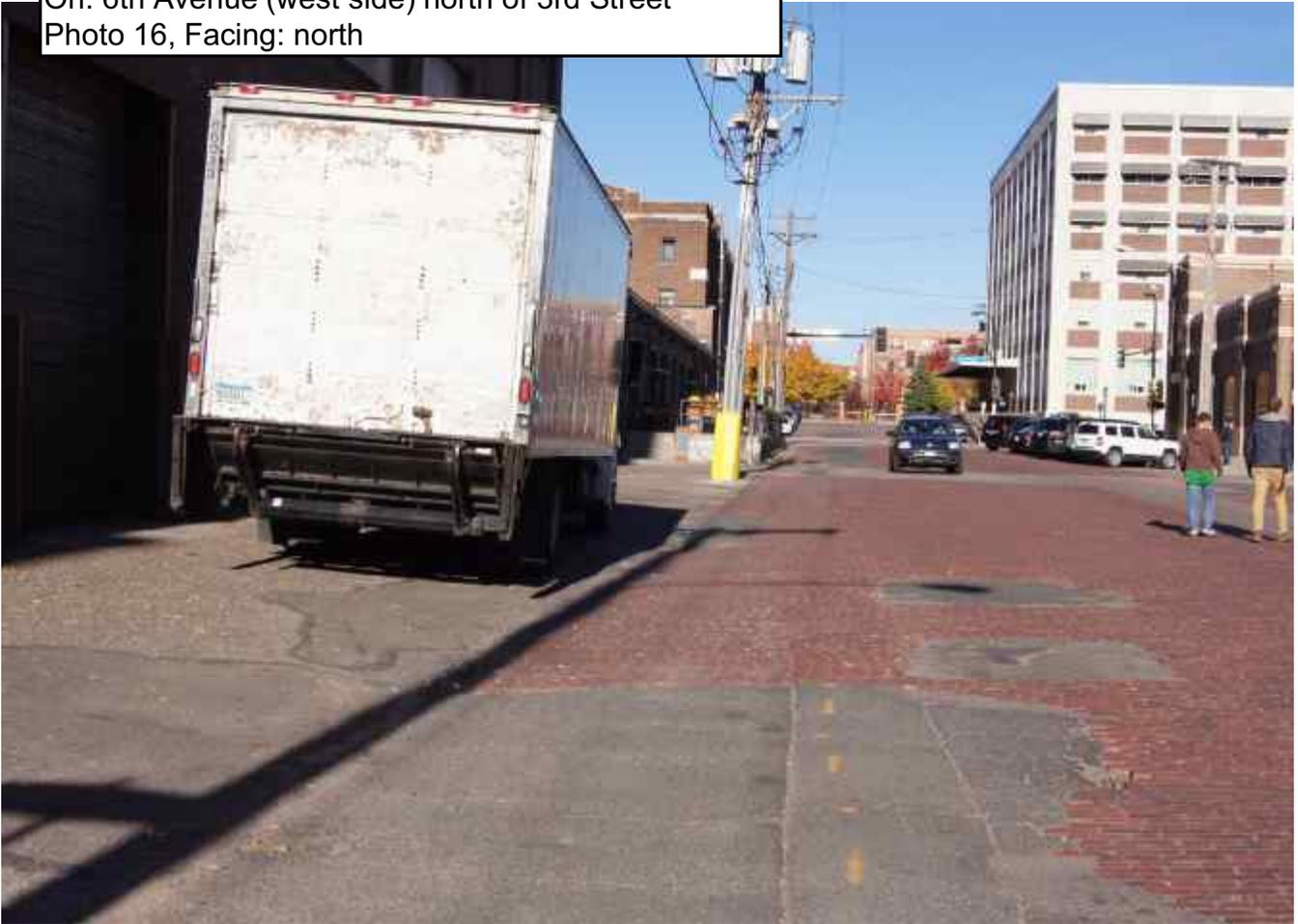
On: 6th Avenue (west side) at 3rd Street  
Photo 14, Facing: north



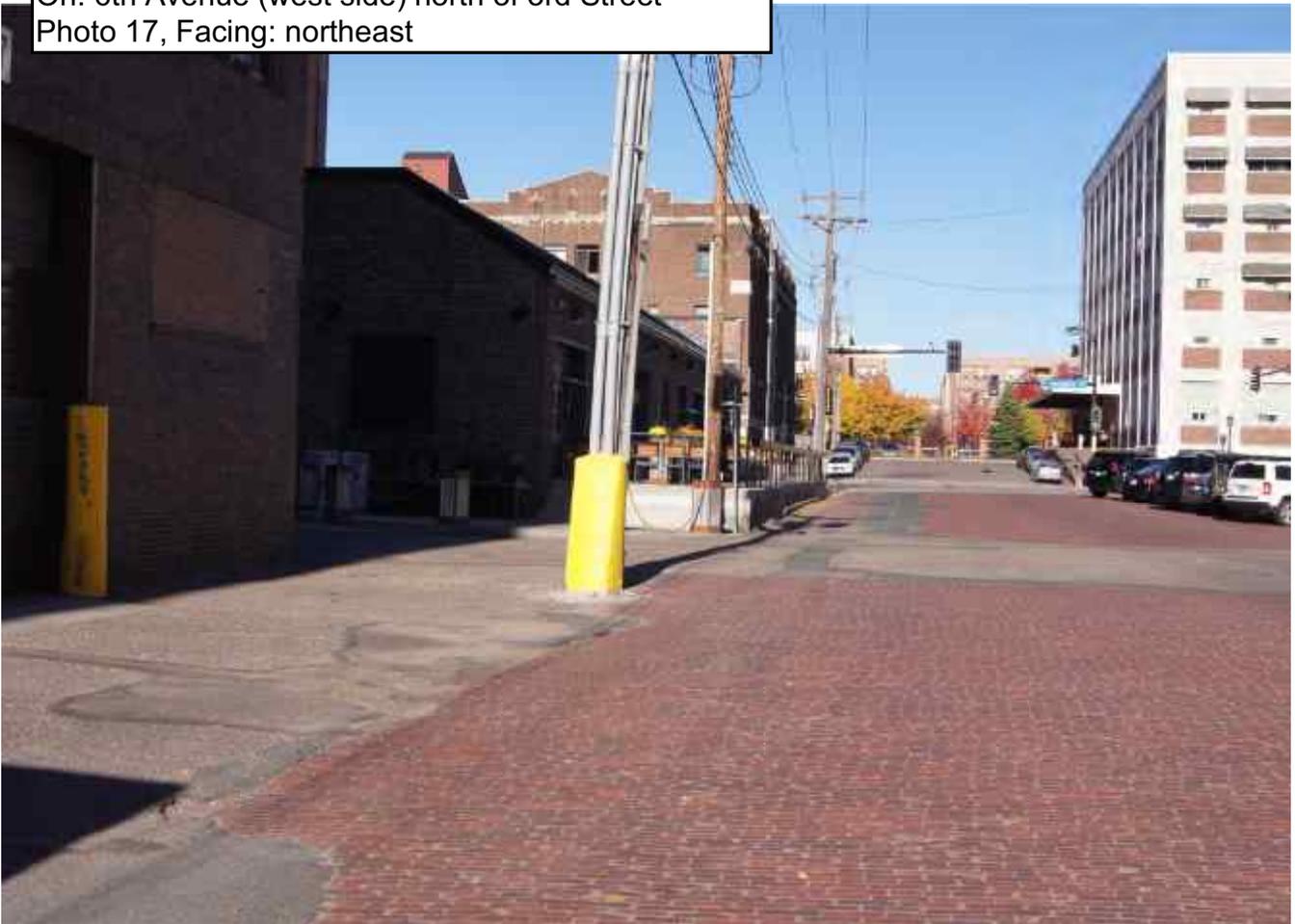
On: 6th Avenue (west side) north of 3rd Street  
Photo 15, Facing: north



On: 6th Avenue (west side) north of 3rd Street  
Photo 16, Facing: north



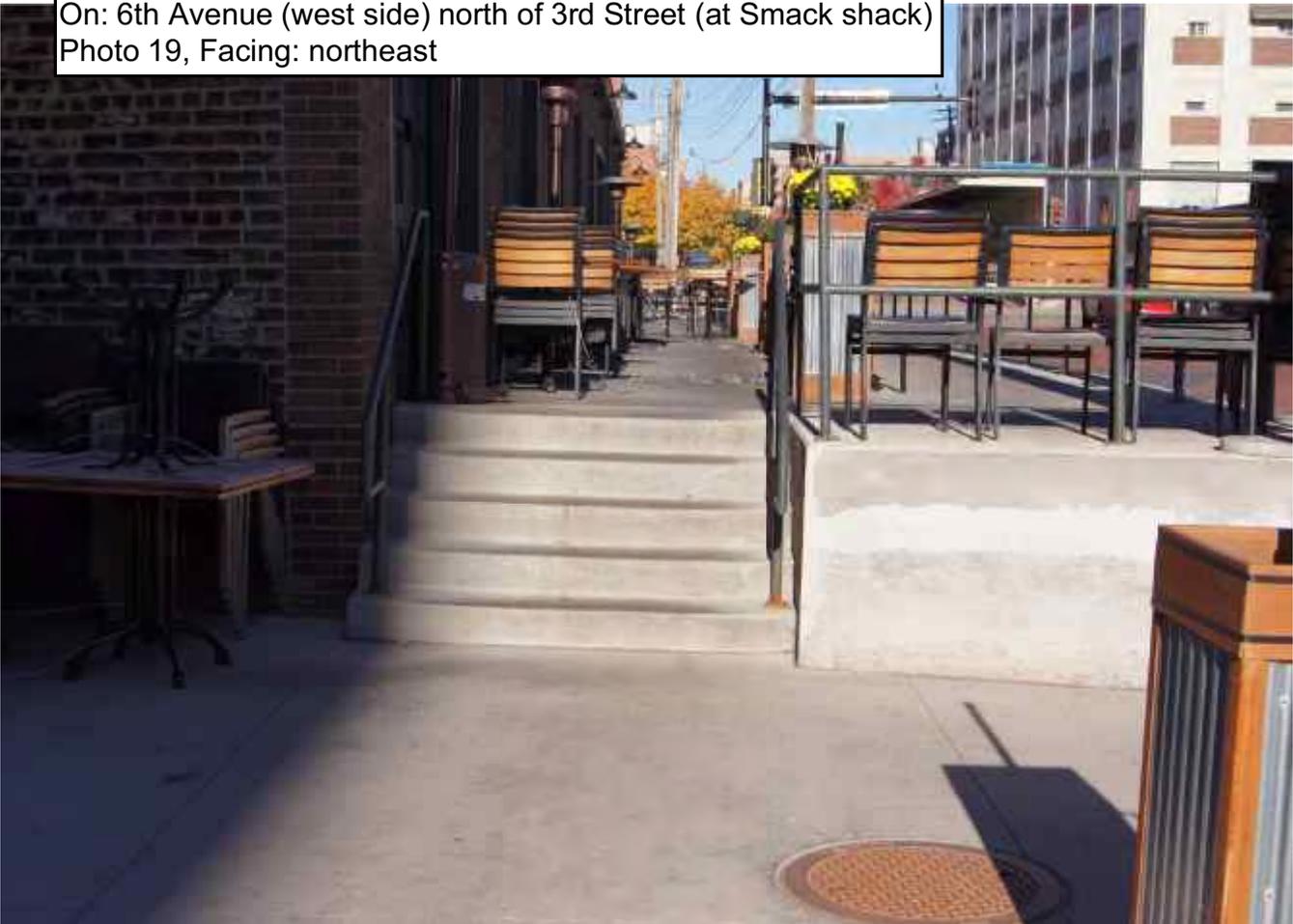
On: 6th Avenue (west side) north of 3rd Street  
Photo 17, Facing: northeast



On: 6th Avenue (west side) north of 3rd Street  
Photo 18, Facing: northeast



On: 6th Avenue (west side) north of 3rd Street (at Smack shack)  
Photo 19, Facing: northeast



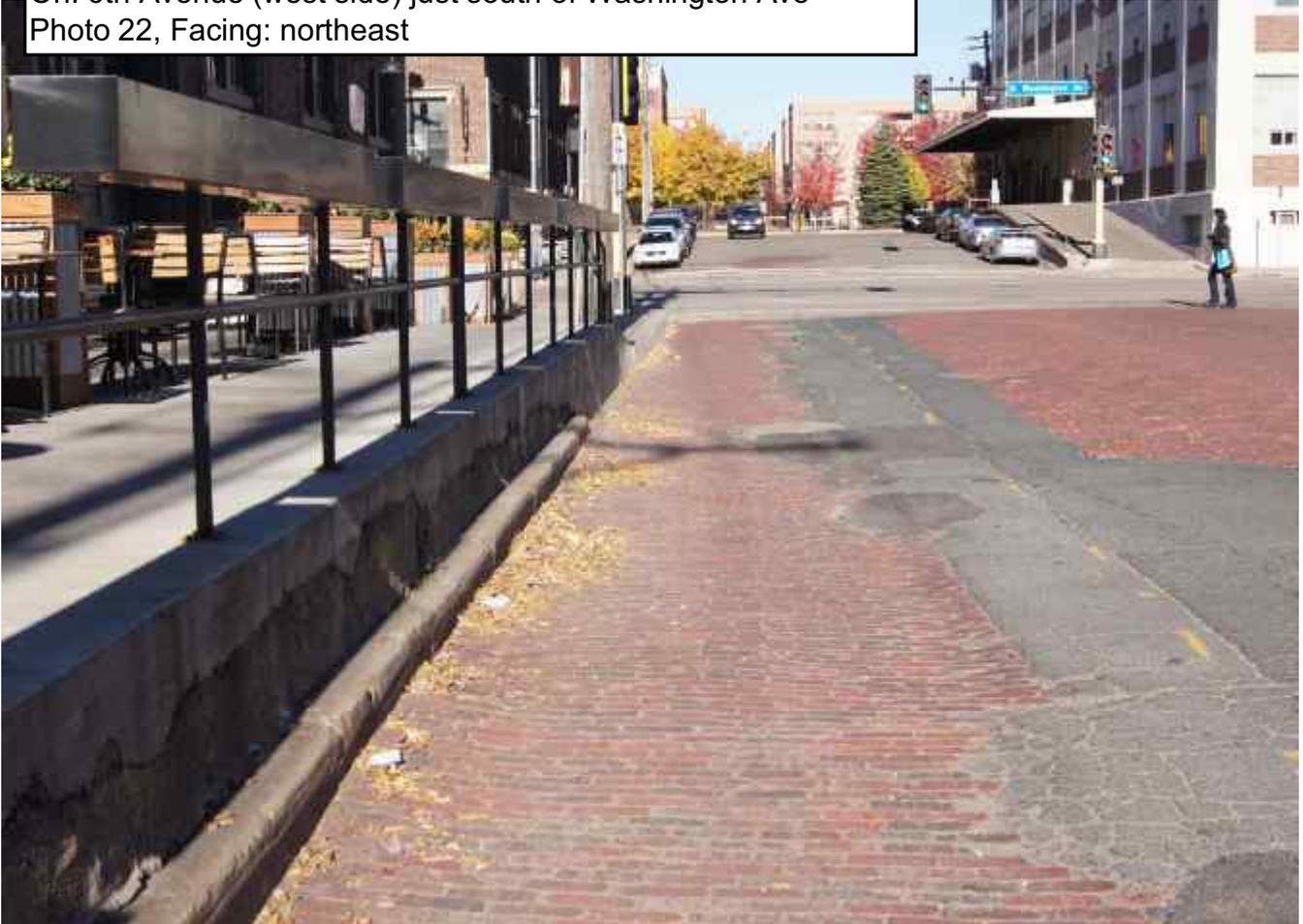
On: 6th Avenue north of 3rd Street (at Smack shack)  
Photo 20, Facing: northwest



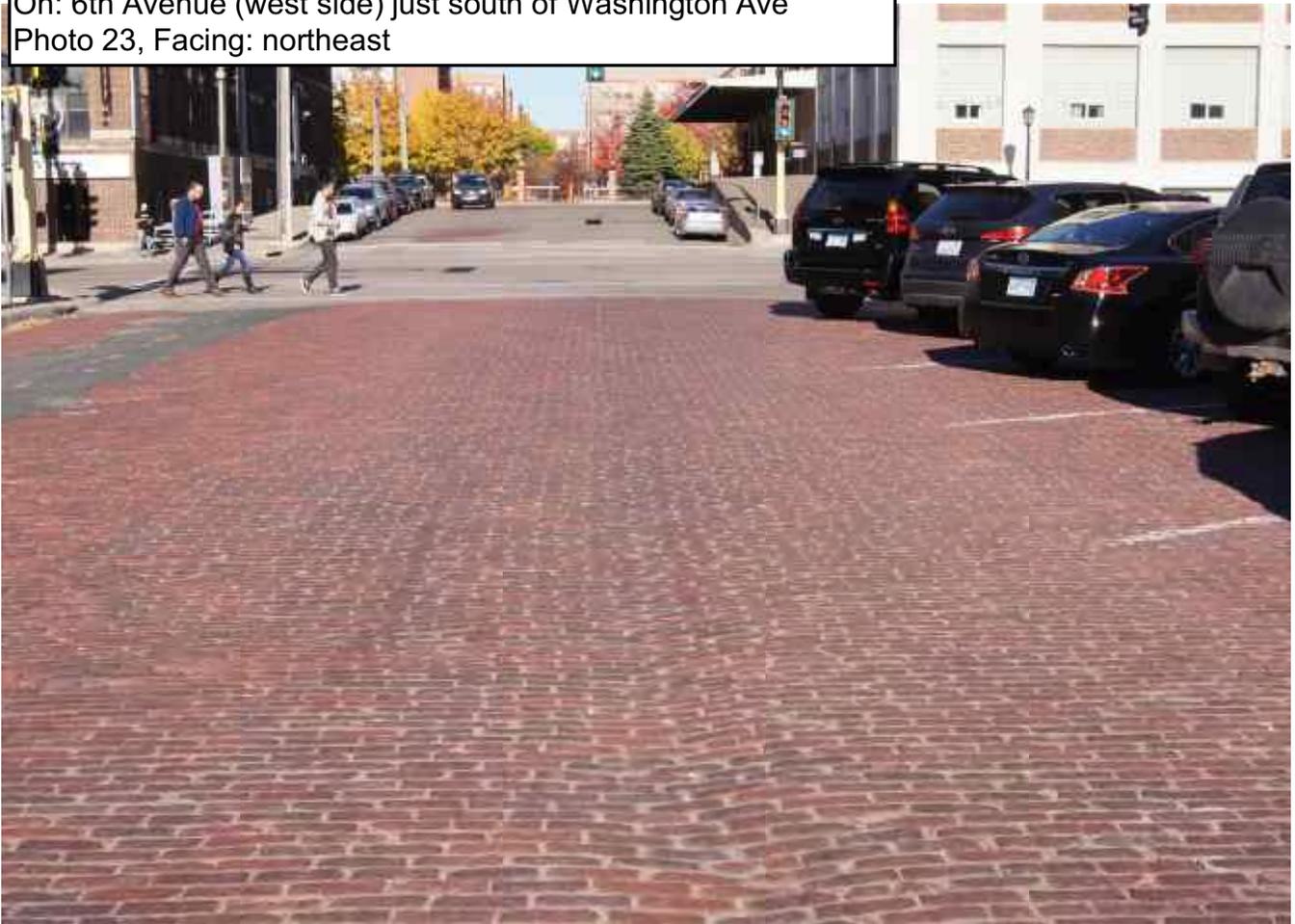
On: 6th Avenue just north of alley  
Photo 21, Facing: west



On: 6th Avenue (west side) just south of Washington Ave  
Photo 22, Facing: northeast



On: 6th Avenue (west side) just south of Washington Ave  
Photo 23, Facing: northeast



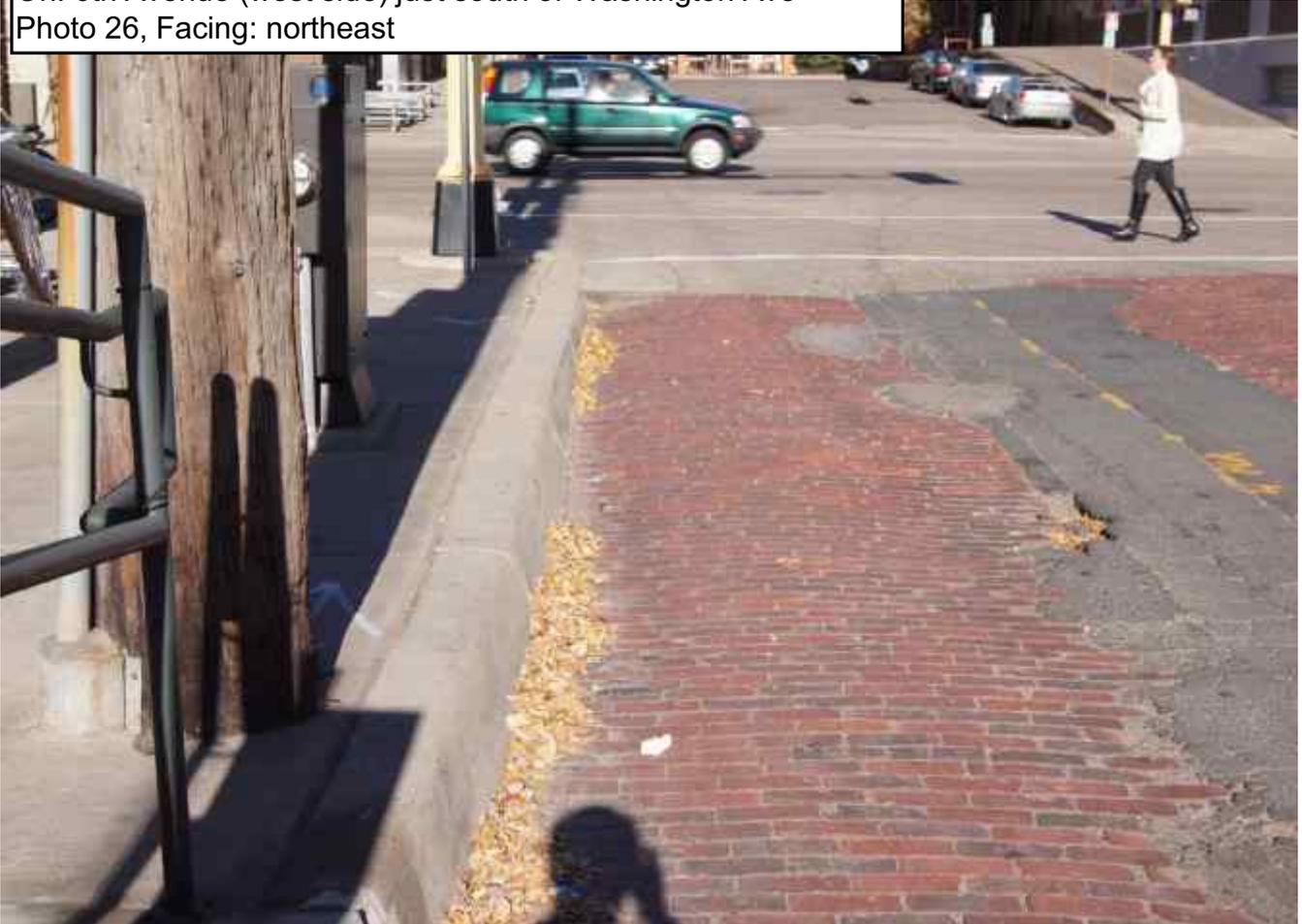
On: 6th Avenue (east side) just south of Washington Ave  
Photo 24, Facing: southeast



On: 6th Avenue (west side) just south of Washington Ave  
Photo 25, Facing: northeast



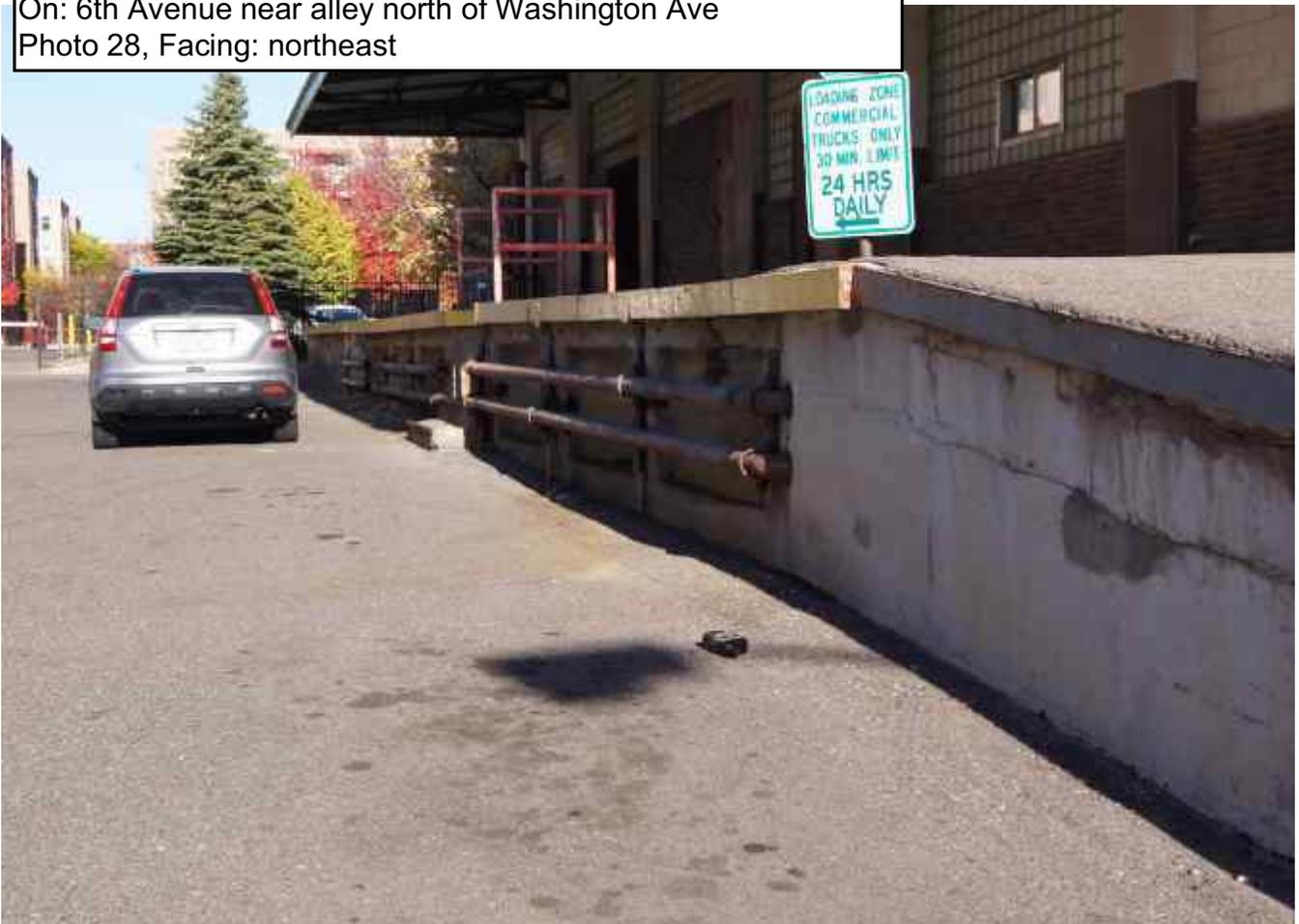
On: 6th Avenue (west side) just south of Washington Ave  
Photo 26, Facing: northeast



On: 6th Avenue near alley north of Washington Ave  
Photo 27, Facing: southwest



On: 6th Avenue near alley north of Washington Ave  
Photo 28, Facing: northeast



On: 6th Avenue just south of Washington Ave  
Photo 29, Facing: southwest



On: 6th Avenue just south of Washington Ave  
Photo 30, Facing: southwest



On: 6th Avenue (east side) south of Washington Ave  
Photo 31, Facing: northeast



On: 6th Avenue (east side) south of alley, north of 3rd St.  
Photo 32, Facing: northeast



## **Dvorak, Hilary A.**

---

**From:** Handeland, Jeff S.  
**Sent:** Tuesday, November 03, 2015 9:21 AM  
**To:** Frey, Jacob; Yang, Blong; Yang, Ger; Ritchie, Heidi; David R. Frank (david0frank@gmail.com)  
**Cc:** Kotke, Steven A.; Hamilton, Heidi J; Elwood, Don R.; Dvorak, Hilary A.; Mann, Lee; Elliott, Beth M.; Miller, Paul D.  
**Subject:** 6th Ave N Heritage Preservation Application

Dear Council Members Frey and Yang and Neighborhood Board Chair Frank,

As you know, the City of Minneapolis is proposing to reconstruct approximately 0.27 miles of 6th Avenue North from 5th Street N to the dead end north of Washington Ave N. A portion of the 6th Avenue project is located within the National Register of Historic Places (NRHP)-listed Minneapolis Warehouse Historic District: from a point midblock between 3rd Street and 4th Street through to the dead end north of Washington Ave N.

This project proposes to fully reconstruct the roadway and sidewalk through the corridor, while keeping the historic loading docks intact. The project will involve complete removal of the roadway section including historic pavers, replacement of the existing storm sewer, reconstruction of the roadway, restoration and reinstallation of the historic pavers and reconstruction of the sidewalks as ADA-compliant pedestrian walkways. The corridor will remain a two lane roadway with parking on both sides.

Public Works held public meetings during the preliminary design phase in October 2014 and January 2015. City Council approved the project layout in March 2015. The project is currently in the detailed design phase and construction is expected to begin in spring 2016.

The City is in the process of preparing a heritage preservation application for a Certificate of Appropriateness (COA). The COA requires approval by the Minneapolis Heritage Preservation Commission. The project utilizes federal funds authorized by the Federal Highway Department; therefore, the project is required to meet all National Environmental Policy Act (NEPA) regulations. NEPA requires the review and avoidance of potential impacts to the social and natural environment for proposed transportation projects. The project has been reviewed by the Minnesota Department of Transportation Cultural Resources Unit (CRU) and the Minnesota State Historic Preservation Office (SHPO) and was determined to have no anticipated permanent adverse physical impacts.

A public hearing will be held on December 1<sup>st</sup>, 2015 at the Minneapolis City Council Chambers in City Hall to present the heritage preservation application for this project. Hilary Dvorak will be sending notices for the heritage preservation public hearing.

Feel free to contact me with any questions about the project, and feel free to contact Hilary with any questions about the heritage preservation public hearing process or notification.

Sincerely,

Jeff

Jeff Handeland | Principal Professional Engineer | City of Minneapolis – Public Works | Office: 612-673-2363 | [jeff.handeland@minneapolismn.gov](mailto:jeff.handeland@minneapolismn.gov)



**NORTH  
LOOP**  
NEIGHBORHOOD

November 2, 2015

The North Loop Neighborhood Association supports the 6<sup>th</sup> Avenue North reconstruction project. We believe it is a great step forward for the City and for our neighborhood to bring our infrastructure into the 21<sup>st</sup> century, in a way, which is compatible with this historic area.

The neighborhood has several comments on the project we would like to bring to your attention, and we request your consideration of each of these as part of the project.

The overhead power lines should be buried. This will significantly increase the attractiveness and the property values in the area, and the right time to do it is while the street is under construction.

More trees should be planted, especially on the blocks which currently do not have trees shown in the plans we have seen. Trees are a significant priority for the neighborhood, and we believe room for trees should be included in the project.

A stop sign should be installed for 6<sup>th</sup> Avenue North traffic at 4<sup>th</sup> Street North. This intersection is a very challenging one to cross for walkers and bikers, and the neighborhood believes this is more of a priority than north / south auto traffic.

Thanks for your consideration. Please let me know if I can answer any questions.