



LAND USE APPLICATION SUMMARY

Property Location: 3200-54 West Lake Street
Project Name: Calhoun Village Shopping Center
Prepared By: Shanna Sether, Senior City Planner, (612) 673-2307
Applicant: Doran Companies
Project Contact: Tony Kuechele
Request: To allow for a new drive-through facility for a drugstore and site plan improvements to an existing shopping center.

Required Applications:

Site Plan Review	For a new drive-through facility.
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SITE DATA

Existing Zoning	C3S Community Shopping Center District SH Shoreland Overlay District
Lot Area	279,230 square feet / 6.41 acres
Ward(s)	7, adjacent to 13
Neighborhood(s)	Cedar-Isles-Dean Neighborhood Association; adjacent to West Calhoun Neighborhood Council
Designated Future Land Use	Mixed Use
Land Use Features	Commercial Corridor (Lake Street West) Major Retail Center (Calhoun and Excelsior)
Small Area Plan(s)	<i>Midtown Greenway Land Use and Development Plan (2007)</i>

Date Application Deemed Complete	June 2, 2016	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	August 1, 2016	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. Thompson Lumber Co. operated a lumber yard at the subject property until it was rezoned in 1987. The subject property was redeveloped as a shopping center with two structures. The site is designed with the two structures at the rear of the property with two parking lots, divided by a private street, running north-south on the property. The west building is two stories tall and has underground access to parking and loading. The east building is one story tall and has a small parking lot and loading dock at the rear. There is an access point to the Midtown Greenway located at the north end of the property. This access point is acknowledged in the *Midtown Greenway Land Use and Development Plan*.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The property is located between the Midtown Greenway to the north and West Lake Street to the south. The properties in the area are located in a designated Major Retail Center. There are several large retailers, office buildings and high-density residential development along West Lake Street. The properties to the north of the Midtown Greenway include a large residential tower, converted from a former grain elevator, a townhouse development and low-density residential.

PROJECT DESCRIPTION. The applicant is proposing to add a drive-through to the rear of the existing east building. Any use with a drive-through facility requires site plan review . In addition to the drive-through, the applicant is proposing a number of site plan and infrastructure improvements to bring the site more into compliance with the zoning code requirements.

The subject property is located within a future transit station area for the proposed West Lake Station in the South West Light Rail Transit Project (SWLRT). Ahead of the station area planning, the City of Minneapolis-Public Works conducted a West Lake Multimodal Transportation Study. The goal of the study was to identify opportunities to address non-motorized and motorized travel within the West Lake Station area with projects that can be implemented as a part of the construction of the Southwest LRT or as part of other capital initiatives. Potential longer-term improvements that would not occur before the LRT opening were also identified. The subject property and surrounding area is located within the study area of the multimodal study. Specifically, the study calls for a trail connection through the Calhoun Village parking lot along the east side of the drive aisle with raised crosswalks at aisle crossings within the parking lot. The long-range goal of the study is to allow for bicycle and pedestrian access from the Midtown Greenway to Lake Calhoun.

There is an existing access point to the Midtown Greenway at the north end of the property. The Midtown Greenway is owned by Hennepin County Regional Railroad Authority (HCRRA). The applicant is proposing to add dedicated bicycle and pedestrian connections from the north end of the site, which accesses the Midtown Greenway, through the site and tot West Lake Street and the public sidewalk, in the manner recommended in the multimodal study. Public Works and CPED staff has been working closely with the applicant to ensure that the infrastructure is provided in such a way that it is supported by the study. However, staff is concerned that marking the pavement or creating wayfinding signage through the property, prior to the connection being completed, would be confusing for bicyclists and pedestrians, because the trail would essentially terminate at West Lake Street.

RELATED APPROVALS.

Planning Case #	Application	Description	Action
BZZ-5442	Conditional use permit	CUP for a major sports and health	The CPC approved the application

		facility	1/19/2012
BZZ-5105	Conditional use permit	CUP for a major sports and health facility	The CPC approved the application 4/11/2011
BZZ-1985	Variance	Variance to increase the area for a wall sign	The BOA approved the application 10/21/2004
P-771	Rezoning	Petition to rezone from M1-I to B3S-I	The City Council approved the application 4/24/1987
C-1103	Conditional use permit	CUP to allow for a childcare center within an existing shopping center	The CPC approved the application 4/13/1988
C-1271	Conditional use permit	CUP to allow for a childcare center within an existing shopping center	The CPC approved the application 3/14/1990

PUBLIC COMMENTS. Staff has received a resolution from the Cedar-Isles-Dean Neighborhood Association and a copy is attached to the staff report. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building placement – *Not applicable*

- The first floor of both buildings is located greater than eight feet from the front lot line abutting West Lake Street. The existing shopping center was approved for construction, in the present configuration, in 1987, and there are no proposed building additions.
- The existing placement of the building does not reinforce the street wall; however, the site is over 6 acres and the site has two expansive surface parking lots, which allow for views in and out of the property. There are no proposed buildings or additions; however, the applicant has provided a number of site improvements to improve pedestrian access and circulation through a combination of crosswalks, new stop signs, and elevated sidewalks.
- The applicant is proposing to improve the areas between the buildings and lot line by providing a new 9-foot landscaped yard. Burger Jones, at the southeast corner of the property has outdoor seating between the building and the front lot line.

- The majority of the on-site, accessory parking is located between the buildings and the front lot line. The existing shopping center was approved for construction, in the present configuration, in 1987.

Principal entrances – Meets requirements

- The building is oriented so that at least one principal entrance faces the front property line.
- All principal entrances are clearly defined and emphasized through the use of an arcade, canopy and signage.

Visual interest – Not applicable

- The building walls facing the two public parking lots provide architectural detail and contain windows in order to create visual interest.
- There are two existing buildings and the shape, varying height, windows and arcade divide the building into smaller identifiable sections.
- There are blank, uninterrupted walls exceeding 25 feet in length; however, this is an existing condition.

Exterior materials – Not applicable

- The existing building is a combination of stucco and concrete. There are no proposed changes to the exterior of the building and no proposed additions.

Windows – Meets requirements with Conditions of Approval

- For nonresidential uses, the zoning code requires that no less than 30 percent of the walls on the first floor are windows with clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher. No less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows. The proposed drug store will be replacing the existing display windows with windows that will allow views into and out of the building.
- The proposed tenant will be a drug store and based on the floor plans, all proposed shelving, mechanical equipment, and other similar fixtures allow views into and out of the building between four and seven feet above the adjacent grade.
- All windows are vertical in proportion and are evenly distributed along the building walls.

Ground floor active functions – Not applicable

- The ground floor facing West Lake Street contains nearly 100 percent active functions.
- Less than 70 percent of the ground floor facing the Midtown Greenway contains active functions; however, this is an existing condition. The proposed plan changes, including the proposed drive-through will increase the existing active function space.

Roof line – Not applicable

- The principal roof line is not proposed to change and is generally a flat roof. The majority of multiple-family residential and non-residential buildings in the area have flat roofs.

Parking garages – Not applicable

- There is an underground parking garage in the west building. No changes are proposed to the enclosed parking area.

ACCESS AND CIRCULATION

Pedestrian access – *Meets requirements with Conditions of Approval*

- There are clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities.
- The applicant will provide dedicated bicycle and pedestrian connections from the north end of the site, which accesses the Midtown Greenway, through the site and to West Lake Street and the public sidewalk. The applicant is showing a raised pedestrian and bicycle pathway where loading trucks and drive-through traffic at the north end of the site intersect. Additionally, there will be a three-way stop and marked crosswalks at the center of the property, which will calm traffic and increase visibility of pedestrians and bicyclists.

Transit access – *Not applicable*

- No transit shelters are proposed as part of this development.

Vehicular access – *Meets requirements with Conditions of Approval*

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian and bicycle traffic. The applicant will provide dedicated bicycle and pedestrian connections from the north end of the site, which accesses the Midtown Greenway, through the site and to West Lake Street and the public sidewalk.
- There are no public alleys adjacent to the site.
- The site has been designed so that service vehicle access does not conflict with pedestrian traffic. The applicant is proposing many site improvements specifically for pedestrians and bicyclists, including raised walkways, raised bike lanes, and new stops signs to calm traffic and increase driver awareness of bicycles and pedestrians.
- None of the existing truck loading areas are located next to residence or office residence districts.
- There is no maximum impervious surface requirement in the Community Shopping Center District. However, the applicant is proposing to add 4,000 square feet of landscape area. According to the materials submitted by the applicant, 86 percent of the site will be impervious, while 88 percent of the existing site is impervious.

LANDSCAPING AND SCREENING

General landscaping and screening – *Meets requirements*

- The overall composition and location of landscaped areas complement the scale of development and its surroundings.
- At least 20 percent of the site not occupied by the building is landscaped. The applicant is proposing approximately 38,479 square feet of landscaping on site, or approximately 20.3 percent of the site not occupied by buildings (see Table I).
- The applicant is proposing at least one canopy tree per 500 square feet of the required landscaped area, including all required landscaped yards. The tree requirement for the site is 76 and the applicant is proposing a total of 78 trees.
- The applicant is proposing at least one shrub per 100 square feet of the required landscaped area, including all required landscaped yards. The shrub requirement for the site is 379 and the applicant is proposing 386 shrubs.
- The remainder of the required landscaped area is covered with turf grass, native grasses, perennial flowering plants, vines, shrubs and other trees.

Table I. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	279,230 sq. ft.
Building Footprint	--	89,932 sq. ft.
Remaining Lot Area	--	189,298 sq. ft.
Landscaping Required	37,860 sq. ft.	38,479 sq. ft.
Canopy Trees (1:500 sq. ft.)	76 trees	79 trees
Shrubs (1:100 sq. ft.)	379 shrubs	385 shrubs

Parking and loading landscaping and screening – Requires alternative compliance

- The parking area facing West Lake Street contains an on-site landscaped yard of at least nine feet in width. The applicant is proposing to add a nine-foot wide landscaped yard adjacent to the south property line; however, the applicant is not proposing to add any shrubs on the west half of the west parking area. This area is where West Lake Street approach elevates as the roadway crosses over the Midtown Greenway and there is a wall at the edge of the right-of-way and the required shrubs would not be visible from the public sidewalk. Therefore, staff is recommending alternative compliance.
- The applicant is proposing a three-foot screen consisting of a decorative fence and hedge that is at least 60 percent opaque.
- At least one tree will be provided for each 25 linear feet, or fraction thereof, of parking or loading area frontage. The proposed plan meets this requirement.
- The parking area abutting the residence district to the north does not contain an on-site landscaped yard of at least nine feet in width. The parking area at the rear of the site has a five-foot setback, which was required at the time the property was developed as a shopping center. The applicant has provided additional landscaped area and materials to conform to the minimum requirements elsewhere on the site. In addition, the adjacent property to the north is owned by HCRRA and the area is naturally landscaped. Staff is recommending that the planning commission grant alternative compliance to providing a nine-foot landscaped yard.
- The zoning code requires a six-foot screen that is at least 95 percent opaque for the parking lot facing the residential district to the north. As previously mentioned, the HCRRA owns the property directly to the north and there is a dense stand of trees that runs along the shared property line and there is a fence between the HCRRA property and the Midtown Greenway. Staff is recommending that the planning commission grant alternative compliance.
- The corners of the parking lot that are unavailable for parking or vehicular circulation are landscaped as specified for a required landscaped yard.
- With the newly proposed landscaped yards and islands, all of the parking spaces are located within 50 feet of the center of an on-site deciduous tree, except the existing parking stalls at the northwest corner of the property. These stalls are not proposed to change and they are located up to the property line, without sufficient space to plant trees on the property.
- The proposed tree islands are at least seven feet wide in any direction.
- Information included in the landscape plan indicates that the plant materials, and installation and maintenance of the plant materials, would comply with sections 530.200 and 530.210 of the zoning code.
- All other areas not occupied by buildings, parking and loading facilities, or driveways would be covered with turf grass, native grasses, perennials, wood mulch, shrubs, and trees.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – Meets requirements

- The parking lot is defined by a six-inch by six-inch continuous concrete curb. The applicant is working with Public Works on ways to ensure best management practices of stormwater management. The Preliminary Development Review report was not available at the time of writing the CPC staff report. Staff will forward the PDR report to the city planning commission at the public hearing.

Site context – Not applicable

- There are no important elements of the city, such as parks, greenways, significant buildings, and water bodies, near the site are obstructed by the existing building.
- This existing building has minimal shadowing effects on public spaces and adjacent properties.
- This building was designed to minimize the generation of wind currents at ground level.

Crime prevention through environmental design – Meets requirements with Conditions of Approval

- The site plan employs best practices to increase natural surveillance and visibility, to control and guide movement on the site, and to distinguish between public and non-public spaces.
- The proposed site, landscaping, and existing buildings promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.
- The project provides lighting on site, at all building entrances, and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.
- The landscaping, sidewalks, lighting, fencing, and building features are located to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- The entrances, exits, signs, fencing, landscaping, and lighting are located to distinguish between public and private areas, to control access, and to guide people coming to and going from the site.

Historic preservation – Not applicable

- This site is neither historically designated nor is it located in a historic district.

2. Conformance with all applicable regulations of the zoning ordinance.

The existing use is conditional in the Community Shopping Center District.

Off-street Parking and Loading – Not applicable

- The off-street vehicle parking requirement for a shopping center is as determined by the zoning administrator based on the principal uses in the shopping center. Staff has assigned a general parking requirement of one space per 500 square feet of gross floor area; in excess of 4,000 square feet and a maximum parking requirement of one space per 200 square feet of gross floor area (see Table 2).
- The minimum bicycle parking requirement is 3 spaces or 1 space per 1,000 square feet of gross floor area (GFA), whichever is greater. The minimum requirement is 105 bicycle parking spaces; however, this requirement was established after the proposed shopping center was constructed and the existing property has nonconforming rights (see Table 3). There are 32 existing bicycle parking spaces and the applicant is proposing to add 10 bicycle parking spaces near the connection to the Midtown Greenway at the north end of the property.

- The off-street loading requirement for a shopping center is as approved by the conditional use permit (see 4). There are two loading docks, one for each of the buildings and both accessed at the rear of the property. There are no proposed changes to the gross floor area.

Table 2. Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
Shopping Center	201	--	201	522	335
	--	--	201	522	335

Table 3. Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Reductions	Short-Term	Long-Term	Proposed
Shopping Center	105	(105)	Not less than 50%	--	42
	105	(105)	--	--	42

Table 4. Loading Requirements (Chapter 541)

Use	Loading Requirement	Minimum Requirement	Proposed
Shopping Center	As approved by C.U.P.	2 small loading spaces	2 large loading spaces

Building Bulk and Height – Not applicable

- The existing buildings were constructed in 1987 and there are no proposed additions that would increase the GFA (see 5).

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	279,230 sq. ft. / 6.41 acres
Gross Floor Area	--	104,555 sq. ft.
Floor Area Ratio (Minimum)	--	.37
Floor Area Ratio (Maximum)	2.7	
Building Height (Maximum)	4 stories or 56 feet, whichever is less	1 – 2 stories

Lot Requirements – Not applicable

- There are no minimum lot requirements for a shopping center.

Yard Requirements – Not applicable

- The subject property is adjacent to residential zoning to the north and a permitted residential use to the east (see6). There are no changes to the existing building and no new obstructions in the required yards.

Table 6. Minimum Yard Requirements

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (South)	0 ft.	--	0 ft.	9 ft. to parking area
Interior Side (East)	15 ft.	--	15 ft.	5 ft.
Interior Side (West)	0 ft.	--	0 ft.	0 ft. to parking area
Rear (North)	5 ft.	--	5 ft.	0 ft. to existing building and parking area

Signs – Meets requirements with Conditions of Approval

- All signs are subject to Chapter 543, On-Premise Signs. The applicant will be required to submit a separate sign permit application for any signage that is proposed.
- The applicant has provided a sign inventory and is proposing to replace the existing Barnes and Noble sign with the drug store sign. The proposed wall sign is smaller than the existing sign.

Screening of Mechanical Equipment – Meets requirements

- All mechanical equipment is enclosed within the building or is screened from the public street by the building itself.

Refuse Screening – Meets requirements

- All refuse and recycling storage containers are located within the building.

Lighting – Meets requirements

- The existing site has lighting in the parking lot and on the building. No changes to the lighting plan are being proposed.

Fences – Not applicable

- Fences must comply with the requirements in Chapter 535. The applicant is proposing to continue with the existing screening on the property. There is an existing four-foot, open and decorative metal fence between the east parking lot and the public sidewalk. This fence will be replaced with the same height and material after the landscaping improvements.

Specific Development Standards – Meets requirements

- The applicant’s proposal meets the specific development standards for a shopping in Chapter 536.

SH Overlay District Standards – Meets requirements

- The proposal is in compliance with the SH Shoreland Overlay District standards.

3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.

The Minneapolis Plan for Sustainable Growth identifies the site as Mixed Use on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- I.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.
- I.1.6 Develop small area plans for designated land use features, particularly Activity Centers, Growth Centers, and Major Retail Centers, in consultation with neighborhood associations, residents, and other stakeholders.
- I.1.7 Invest in targeted place-making strategies to build upon and enhance existing community assets and encourage private sector development.

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- I.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- I.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.
- I.3.3 Encourage above-ground structured parking facilities to incorporate development that provides active uses on the ground floor.

Land Use Policy 1.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

- I.4.1 Support a variety of commercial districts and corridors of varying size, intensity of development, mix of uses, and market served.
- I.4.2 Promote standards that help make commercial districts and corridors desirable, viable, and distinctly urban, including: diversity of activity, safety for pedestrians, access to desirable goods and amenities, attractive streetscape elements, density and variety of uses to encourage walking, and architectural elements to add interest at the pedestrian level.
- I.4.4 Continue to encourage principles of traditional urban design including site layout that screens off-street parking and loading, buildings that reinforce the street wall, principal entrances that face the public sidewalks, and windows that provide “eyes on the street”.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street’s character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- 1.10.1 Support a mix of uses – such as retail sales, office, institutional, high-density residential and clean low-impact light industrial – where compatible with the existing and desired character.
- 1.10.4 Encourage a height of at least two stories for new buildings along Commercial Corridors, in keeping with neighborhood character.

Land Use Policy 1.16: Support a limited number of Major Retail Centers, while promoting their compatibility with the surrounding area and their accessibility to transit, bicycle and foot traffic

- 1.16.1 Encourage the development of mixed residential, office, institutional and, where appropriate, small-scale retail sales and services to serve as transitions between Major Retail Centers and neighboring residential areas.
- 1.16.2 Incorporate principles of traditional urban design in new and phased development, including buildings that reinforce the street wall, have windows that provide “eyes on the street”, and principal entrances that face the public sidewalks.
- 1.16.3 Encourage and implement buffering to lessen potential conflicts between uses in Major Retail Centers and surrounding areas.
- 1.16.4 Ensure the provision of high quality transit, bicycle and pedestrian access to Major Retail Centers.
- 1.16.5 Support district parking strategies in Major Retail Centers, including shared parking facilities, uniform signage for parking facilities, and other strategies.

Transportation Policy 2.3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant, and accessible.

- 2.3.1 Ensure that there are safe and accessible pedestrian routes to major destinations, including transit corridors, from nearby residential areas.
- 2.3.2 Identify and encourage the development of pedestrian routes within Activity Centers, Growth Centers, and other commercial areas that have superior pedestrian facilities.
- 2.3.3 Develop and implement guidelines for streets and sidewalks to ensure safe, attractive, and accessible pedestrian facilities.
- 2.3.6 Provide creative solutions to increasing and improving pedestrian connectivity across barriers such as freeways, creeks and the river, and commercial areas, such as shopping centers.

Transportation Policy 2.8: Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city’s business community.

- 2.8.1 Implement off-street parking regulations which provide a certain number of parking spaces for nearby uses, while still maintaining an environment that encourages bicycle, pedestrian, and transit travel.
- 2.8.2 Design and implement incentives for shared parking and on-site car sharing programs, as well as carpooling and vanpooling.

- 2.8.3 Maximize the efficient use of off-street parking by developing district parking strategies in high density mixed-use areas such as Activity Centers and Growth Centers.
- 2.8.4 Consider eliminating minimum parking requirements for certain small-scale uses as well as parking requirements in areas served by off-street parking facilities that are available to the general public.
- 2.8.5 Continue to prohibit new commercial surface parking lots and to restrict the size of accessory surface parking lots in Downtown.
- 2.8.6 Encourage management of on-street parking in commercial areas primarily for short-term use by adjoining land uses.
- 2.8.7 Promote transit, walking, and biking as safe and comfortable transportation alternatives through reduced parking requirements, encouragement of employee transit incentive programs, and improved facilities.
- 2.8.8 Encourage employers to offer economic incentives that support transit use, such as providing employee transportation allowances as alternatives to free parking.
- 2.8.9 Ensure that parking facilities do not under-price their parking fees as compared to transit fares except to support carpooling and vanpooling as primary commuting modes.
- 2.8.10 Continue to implement discounted packages for carpooling and vanpooling in City-owned or controlled parking facilities, and in leading by example, encourage private parking facilities to do likewise.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

- 10.18.1 Require that parking lots meet or exceed the landscaping and screening requirements of the zoning code, especially along transit corridors, adjacent to residential areas, and areas of transition between land uses.
- 10.18.4 Provide walkways within parking lots in order to guide pedestrians through the site.
- 10.18.7 Minimize the width of ingress and egress lanes along the public right of way in order to provide safe pedestrian access across large driveways.

Staff has identified numerous policies and implementation steps that affirm the necessity and significance in providing dedicated bicycle and pedestrian connections through the existing shopping center. The applicant is proposing to add raised walkways, raised bike lanes, and new stop signs to calm traffic and increase driver awareness of bicycles and pedestrians. The proposed drive-through will be located at the rear of the site and will have sufficient stacking spaces per section 541.390 of the zoning code. CPED finds that the proposed development is in conformance with the above policies of *The Minneapolis Plan for Sustainable Growth*.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The site is located within the boundaries of the *Midtown Greenway Land Use and Development Plan* adopted by the City Council in 2007. The Plan recognizes the current Greenway access to and from the site and encourages the continuation of the connection. The plan also uses the subject property as a case study for future development. There are no planned additions or future development of the subject property at this time.

The subject property is located within a future transit station area for the proposed West Lake Station in the South West Light Rail Transit Project (SWLRT). Ahead of the station area planning, the City of Minneapolis-Public Works conducted a [West Lake Multimodal Transportation Study](#). The goal of the study was to identify opportunities to address non-motorized and motorized travel within the West Lake Station area with projects that can be implemented as a part of the construction of the Southwest LRT or as part of other capital initiatives. Potential longer-term improvements that would not occur before the LRT opening were also identified. The subject property and surrounding area is located within the study area of the multimodal study. Specifically, the study calls for a trail connection through the Calhoun Village parking lot along the east side of the drive aisle with raised crosswalks at aisle crossings within the parking lot. The long-range goal of the study is to allow for bicycle and pedestrian access from the Midtown Greenway to Lake Calhoun.

There is an existing access point to the Midtown Greenway at the north end of the property. The Midtown Greenway is owned by Hennepin County Regional Railroad Authority (HCRRA). The applicant is proposing to add dedicated bicycle and pedestrian connections from the north end of the site, which accesses the Midtown Greenway, through the site and to West Lake Street and the public sidewalk, in the manner recommended in the multimodal study. Public Works and CPED staff have been working closely with the applicant to ensure that the infrastructure is provided in such a way that it is supported by the study. However, staff is concerned that marking the pavement or creating wayfinding signage through the property, prior to the connection being completed, would be confusing for bicyclists and pedestrians, because the trail would essentially terminate at West Lake Street.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- **Landscaped yard (south).** The parking area facing West Lake Street contains an on-site landscaped yard of at least nine feet in width. The applicant is proposing to add a nine-foot wide landscaped yard adjacent to the south property line; however, the applicant is not proposing to add any shrubs on the west half of the west parking area. This area is where West Lake Street approach elevates as the roadway crosses over the Midtown Greenway and there is a wall at the edge of the right-of-way and the required shrubs would not be visible from the public sidewalk. Therefore, staff is recommending alternative compliance.
- **Landscaped yard (north).** The parking area abutting the residence district to the north does not contain an on-site landscaped yard of at least nine feet in width. The parking area at the rear of the site has a five-foot setback, which was required at the time the property was developed as a shopping center. The applicant has provided additional landscaped area and materials to conform to the minimum requirements elsewhere on the site. In addition, the adjacent property to the north is owned by HCRRA and the area is naturally landscaped. Staff is recommending that the planning commission grant alternative compliance to providing a nine-foot landscaped yard.
- **Screening of parking lot (north).** The zoning code requires a six-foot screen that is at least 95 percent opaque for the parking lot facing the residential district to the north. As previously mentioned, the HCRRA owns the property directly to the north and there is a dense stand of trees that runs along the shared property line and there is a fence between the HCRRA property and the Midtown Greenway. Staff is recommending that the planning commission grant alternative compliance.
- **50 feet to on-site tree.** With the newly proposed landscaped yards and islands, all of the parking spaces are located within 50 feet of the center of an on-site deciduous tree, except the existing

parking stalls at the northwest corner of the property. These stalls are not proposed to change and they are located up to the property line, without sufficient space to plant trees on the property.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the application by Doran Companies for the properties located at 3200-54 West Lake Street:

A. Site Plan Review.

Recommended motion: **Approve** the application for a site plan review, subject to the following conditions:

1. All site improvements shall be completed by June 27, 2018, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. CPED staff shall review and approve the final floor plans to ensure all proposed shelving, mechanical equipment, and other similar fixtures allow views into and out of the building between four and seven feet above the adjacent grade.
4. The applicant shall provide pedestrian connections from the existing Midtown Greenway access point to West Lake Street with crosswalk paint, elevated walkways and a three-way stop sign at the intersection between the two parking lots, as shown on the site plan.
5. The applicant shall provide bicycle connections from the existing Midtown Greenway access point to West Lake Street with a bicycle pathway through the property that is elevated to the same height as the pedestrian walkway, a minimum of 10 feet in width, as shown on the site plan.
6. The plant materials, and installation and maintenance of the plant materials, shall comply with sections 530.200 and 530.210 of the zoning code.

ATTACHMENTS

1. Zoning map
2. Oblique aerial photo
3. Written description and findings submitted by applicant
4. Survey
5. Site plan
6. Floor plan for proposed drug store
7. Building elevations
8. Renderings
9. Sign inventory
10. Photos
11. Excerpts from the West Lake Multimodal Transportation Study
12. Public comments



Statement of proposed use and description of the project:

The project is located at 3200 West Lake Street in Minneapolis and is commonly referred to as Calhoun Village Shopping Center.

The applicant is requesting the addition of a drive-through to accommodate a drug store as allowed under the existing zoning, C3S Community Shopping Center District. The proposed 15,400 SF drug store will be located in the NE corner of Building B, currently occupied by Barnes and Noble and H & R Block. This allows for the addition of a double drive-thru with canopy to the north façade of this building. Refer to the proposed architectural site plan (sheet A1-A), exterior elevation (sheet A1) and the colored rendering for the details on the building improvements.

The location of the drive-thru is ideal as it provides ample stacking for waiting customers and minimizes vehicular conflict with pedestrian and bicyclists, and does not require any additional curb cuts for access to the site. Minor site improvements are necessary to accommodate the drive-thru as depicted on the civil site plans (sheet C3.01).

In addition to the drive-thru, the applicant desires to improve the shopping center with some additional pedestrian, bicycle, vehicular, and landscape enhancements.

Improved vehicular, bicycle and pedestrian delineation are proposed along the north south internal drive access to the site. Left turn lane designations are proposed for inbound and outbound vehicles. Improved delineation of pedestrian crosswalks is proposed in several areas, and a delineated raised bicycle route connecting the trail access along the north property line to West Lake Street is proposed. We believe these site improvements will greatly improve the flow of vehicles, bicycles and pedestrians through the development.

Finally, landscape enhancements are proposed throughout the development. Several new and/or expanded landscape islands are proposed within the existing parking areas. The increased size of these islands will provide greater soil depth and area for root expansion which will allow for larger, healthier overstory shade trees. The preferred landscape plan (sheet L1.01) proposes 3,997 sf of additional green space, 49 additional overstory trees and in excess of the code required 385 shrubs will be provided.

DESCRIPTION OF PROPERTY SURVEYED

(Per First American Title Insurance Company Commitment for Title Insurance Commitment No. NCS-776762-MPLS, commitment date February 09, 2016)

Lot E, except the Easterly 290 feet thereof, West End Subdivision, Minneapolis, Minnesota, according to the recorded plat thereof, Hennepin County, Minnesota.

Abstract Property

PLAT RECORDING INFORMATION

The plat of West End Subdivision, Minneapolis, Minn. was filed of record on October 8, 1888, in Book 28 of Plats, Page 14.

[] Bearings and/or dimensions listed within brackets are per plat or record documents.

TITLE COMMITMENT

First American Title Insurance Company Commitment for Title Insurance Commitment No. NCS-776762-MPLS, commitment date February 09, 2016, was relied upon as to matters of record.

Schedule B Exceptions:

① Exceptions are indicated on survey with circled numbers unless otherwise noted. Items not listed below are standard exceptions and/or are not survey related.

9. Rights of the Chicago, Milwaukee and St. Paul Railway Company to construct and maintain slopes in connection with bridge maintenance and the right of the City of Minneapolis to maintain the sewer line along the foot of said slope as contained in the Release and Agreement, dated June 13, 1916, recorded August 27, 1917, in Book 167 of Miscellaneous, Page 616, as Document No. 860778. [A specific location cannot be determined from the record document. Not shown on survey]

10. Easement for sewer line purposes, together with any incidental rights, in favor of Metropolitan Waste Control Commission, a Minnesota metropolitan commission, as contained in the Grant of Easement, dated March 23, 1989, recorded April 13, 1989, as Document No. 5524300. [shown on survey]

11. Terms and conditions of the easement for the maintenance, repair and use of an existing building, fence, parking lot and retaining wall as contained in the Indenture, dated April 25, 1989, recorded May 2, 1989, as Document No. 5529677. [shown on survey]

12. Easement for trunk highway purposes, together with any incidental rights, acquired by the State of Minnesota, as contained in the Final Certificate, dated April 28, 1992, recorded May 15, 1992, as Document No. 5912602. The State of Minnesota conveyed its rights to the County of Hennepin by Quit Claim Deed, dated June 27, 1997, recorded October 8, 1997, as Document No. 6792869. [shown on survey]

LEGEND

- Denotes iron monument set marked with P.L.S. No. 15480
- Denotes mag nail with disk marked PLS No. 15480 set
- ⊙ Denotes 1.17 inch diameter copper magnetized marker with disc cap affixed stamped LS-15480 set
- ATM Denotes automated teller machine
- BE Denotes building entrance
- BR Denotes bike rack
- BTC Denotes top of bituminous curb
- BTL Denotes beavertail curb
- CB Denotes catch basin
- CBX Denotes communication box
- COL Denotes building column
- CST Denotes concrete step
- CWB Denotes concrete wall base
- DG Denotes drain grate
- EMH Denotes electric manhole
- FH Denotes fire hookup
- FOL Denotes fiber optic line
- FW Denotes face of walk
- GM Denotes gas meter
- GP Denotes guard post
- GRDL Denotes ground light
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- KWT Denotes keystone wall top
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- PPU Denotes power pole with underground utility
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- WWT Denotes wood wall top
- EVG Denotes evergreen tree
- SHB Denotes shrub
- TR Denotes deciduous tree

GENERAL NOTES

- Survey coordinate and bearing basis: Hennepin County Coordinates
- Adjoining ownership information shown hereon was obtained from the Hennepin County Property Tax Information web site.

UTILITY NOTES

- Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call locate requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.
- Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket number 160701224.
- Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

FLOOD ZONE NOTE

- The subject property appears to lie within Zone X (Areas determined to be outside the 0.2% annual chance floodplain) per the National Flood Insurance Program, Flood Insurance Rate Map Community Panel No. 2701720354E, dated September 2, 2004. This information was obtained from the FEMA Map Service Center web site.

PARKING

- 280 Regular Spaces
- 8 Disabled Spaces
- 83 Regular Spaces underground or lower level
- 3 Disabled Spaces underground or lower level
- 364 Total Spaces

AREAS

Gross = 319,753 square feet or 7.341 acres
Net (excludes right of ways) = 279,230 square feet or 6.410 acres

LIST OF POSSIBLE ENCROACHMENTS

The following list of possible encroachments is only the opinion of this surveyor; should not be interpreted as a legal opinion and should not be interpreted as a complete listing.

- A) Possible encroachments are indicated on survey with boxed letters as listed below.
 - A.) Construction in the northeast corner and along the east property line.
 - B.) Sign and keystone walls along the southerly right of way line east of Market Plaza.

SURVEYOR'S CERTIFICATION

To Doran Development, LLC, a Minnesota limited liability company and First American Title Insurance Company:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 7(a), 7(b)(1), 8, 9, 11 and 13 of Table A thereof. The fieldwork was completed on March 31, 2016.

Dated this 4th day of April, 2016

SUNDE LAND SURVEYING, LLC.

By: *Mark S. Hanson*
Mark S. Hanson, P.L.S. Minn. Lic. No. 15480



Revision	By	Date
March 31, 2016	SMF	

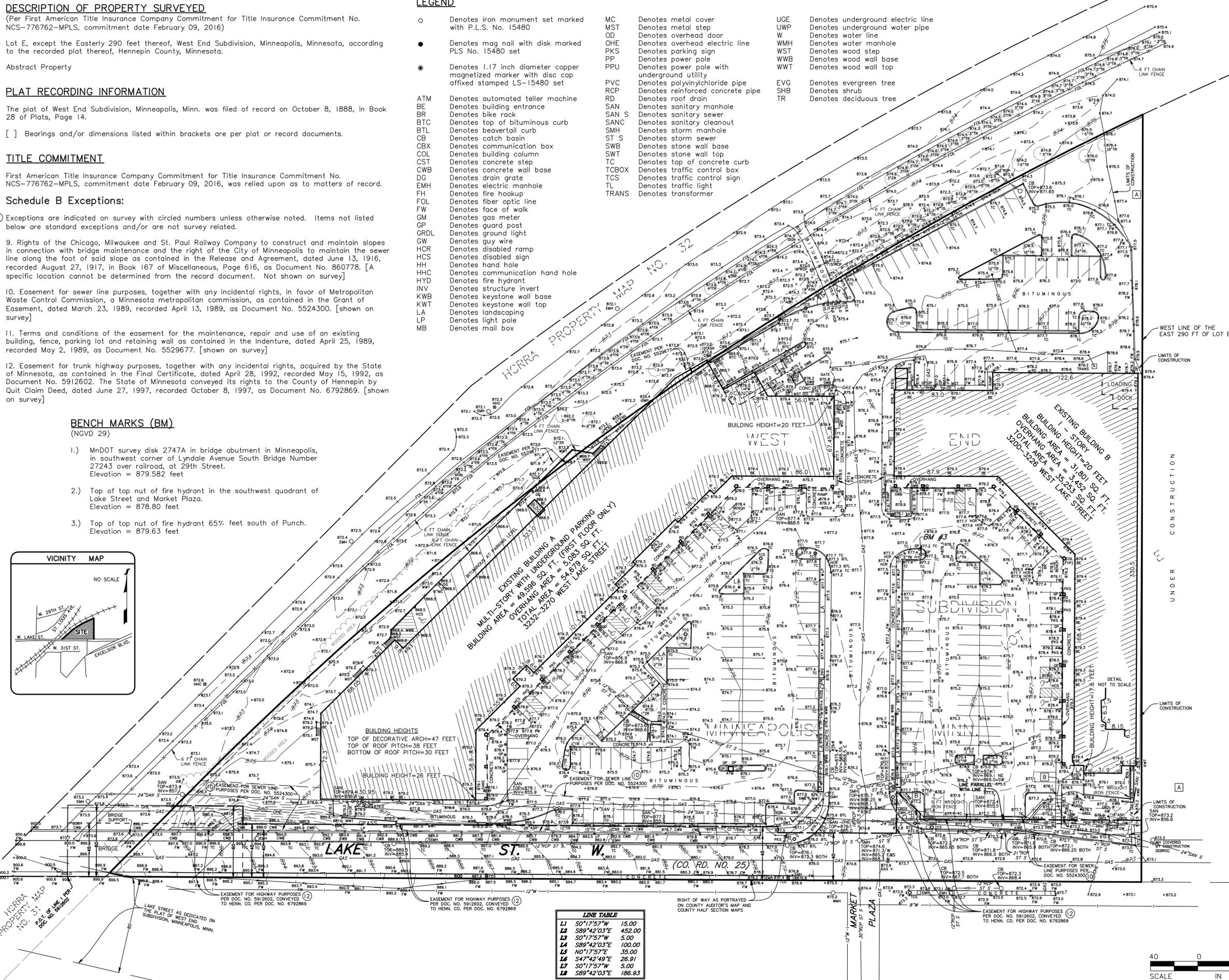
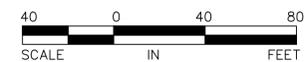
Drawing Title:
**ALTA/NSPS LAND TITLE SURVEY FOR:
DORAN DEVELOPMENT, LLC**
Calhoun Village
3220 Lake Street West, Minneapolis, MN

SUNDE LAND SURVEYING
www.sunde.com
Main Office: 9001 East Bloomington Freeway (35W) • Suite 118
Bloomington, Minnesota 55420-3435
952-881-2455 (Fax: 952-888-9526)
Branch Office: Cloquet, Minnesota 218-499-8267

Project: 87-173-D Bk/Pg: 766/76, Ref 508/32 Date: 04/04/2016
Township: 29 Range: 24 Section: 32 Sheet: 1 of 2
File: 871730001.dwg

LINE TABLE

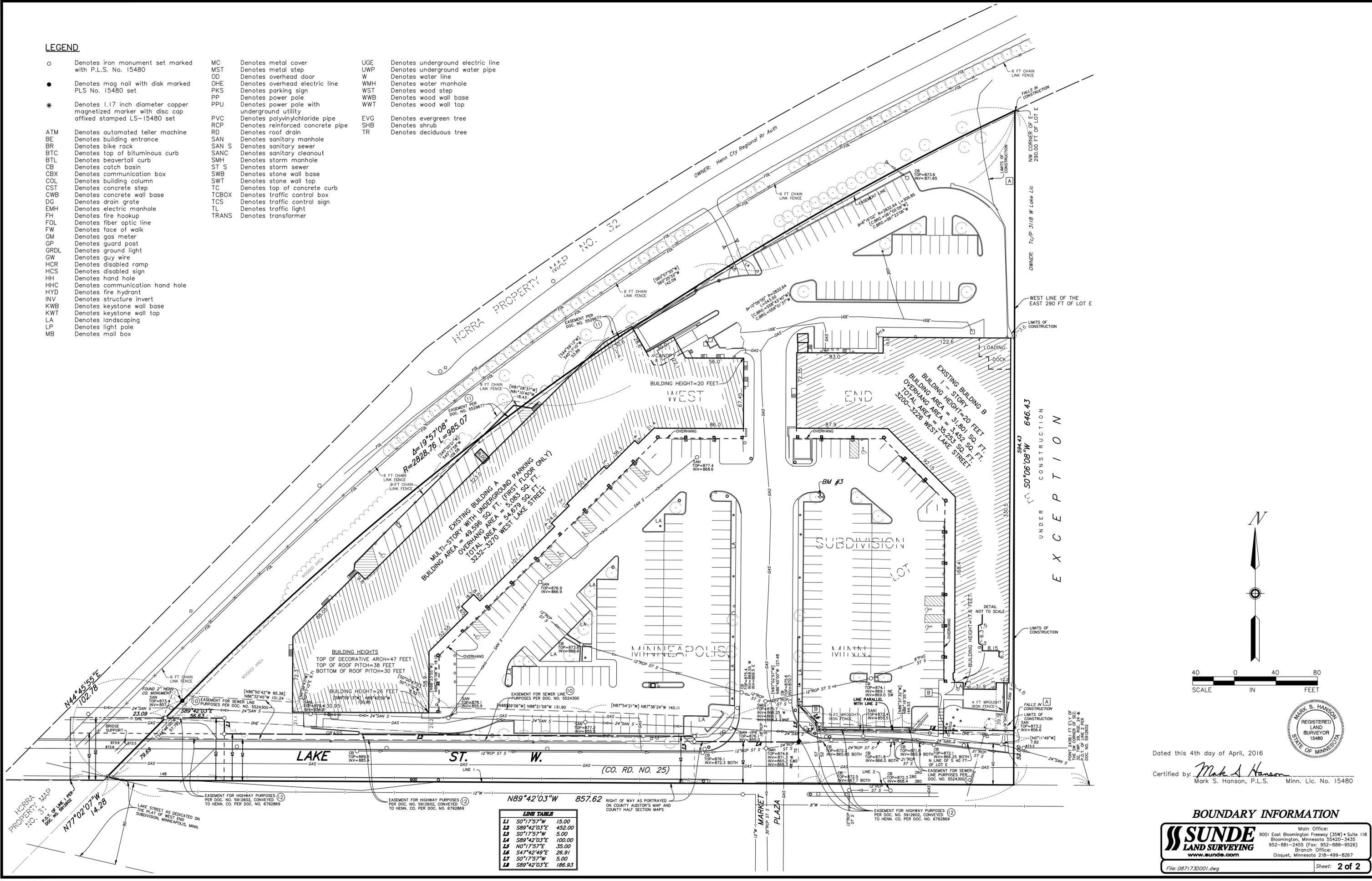
L1	S0°17'57"W	15.00
L2	S89°42'03"E	492.00
L3	S0°17'57"W	5.00
L4	S89°42'03"E	100.00
L5	N0°17'57"E	35.00
L6	S47°42'49"E	26.91
L7	S0°17'57"W	5.00
L8	S89°42'03"E	186.93



N:\87173\871730001.dwg 4/4/2016 2:07:38 PM CST

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Dated this 4th day of April, 2016
 Certified by: *Mark S. Hanson*
 Mark S. Hanson, P.L.S. Minn. Lic. No. 15480

BOUNDARY INFORMATION

SUNDE LAND SURVEYING
 Main Office: 9001 East Bloomington Freeway (35W) • Suite 118
 Bloomington, Minnesota 55420-3435
 952-881-2455 (Fax: 952-888-9526)
 Branch Office: Cloquet, Minnesota 218-499-8267

File: 0871730001.dwg Sheet: 2 of 2



OWNER: Tc/P 3118 W Lake Llc

CITY LANDSCAPE CODE

ONE TREE SHALL BE PROVIDED PER 500 SQ. FT. OF GREEN SPACE
38,474 / 500 = 76.95

ONE SHRUB SHALL BE PROVIDED PER 100 SQ. FT. OF GREEN SPACE
38,474 / 100 = 384.74

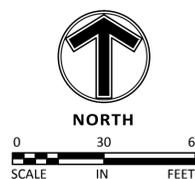
PLANT TYPE	REQUIRED	PROVIDED
EXISTING TREES	N/A	36
REMOVED EXISTING TREES	N/A	6
PROPOSED TREES	41	57
TOTAL SITE TREES	77	93
PROPOSED SHRUB	385	385

MINIMUM TREE SIZE SHALL BE 2.5"
ORDINANCE 530.220 (2) ALLOWS FOR A SUBSTITUTION OR REDUCTION OF LANDSCAPE STANDARDS PROVIDED ONE OR MORE OF THE FOLLOWING EXIST:

- THE PROPOSAL WILL ALLOW A SITE PLAN OF EXCEPTIONAL DESIGN THAT INCLUDES AMENITIES SUCH AS PUBLIC SEATING, AN OUTDOOR PLAZA OR TRANSIT SHELTER THAT WILL ENHANCE THE AREA OR THAT IS MORE CONSISTENT WITH THE DESIGN OF THE SITE OR THE SURROUNDING AREA.
- THE PROPOSAL WILL ALLOW A SITE PLAN THAT IS MORE CONSISTENT WITH THE CHARACTER OF THE AREA.
- EXISTING PLANT MATERIALS, WALLS, FENCES OR THE TOPOGRAPHY OF THE SITE AND ITS SURROUNDINGS MAKE THE REQUIRED LANDSCAPING OR SCREENING LESS NECESSARY.
- THE REQUIRED LANDSCAPING OR SCREENING WILL HINDER TRUCK ACCESS AND SERVICE NECESSARY TO THE OPERATION OF THE USE.
- THE REQUIRED LANDSCAPING AND SCREENING MAY OBSTRUCT VIEWS OF TRAFFIC OR REDUCE NATURAL SURVEILLANCE OF THE SITE.

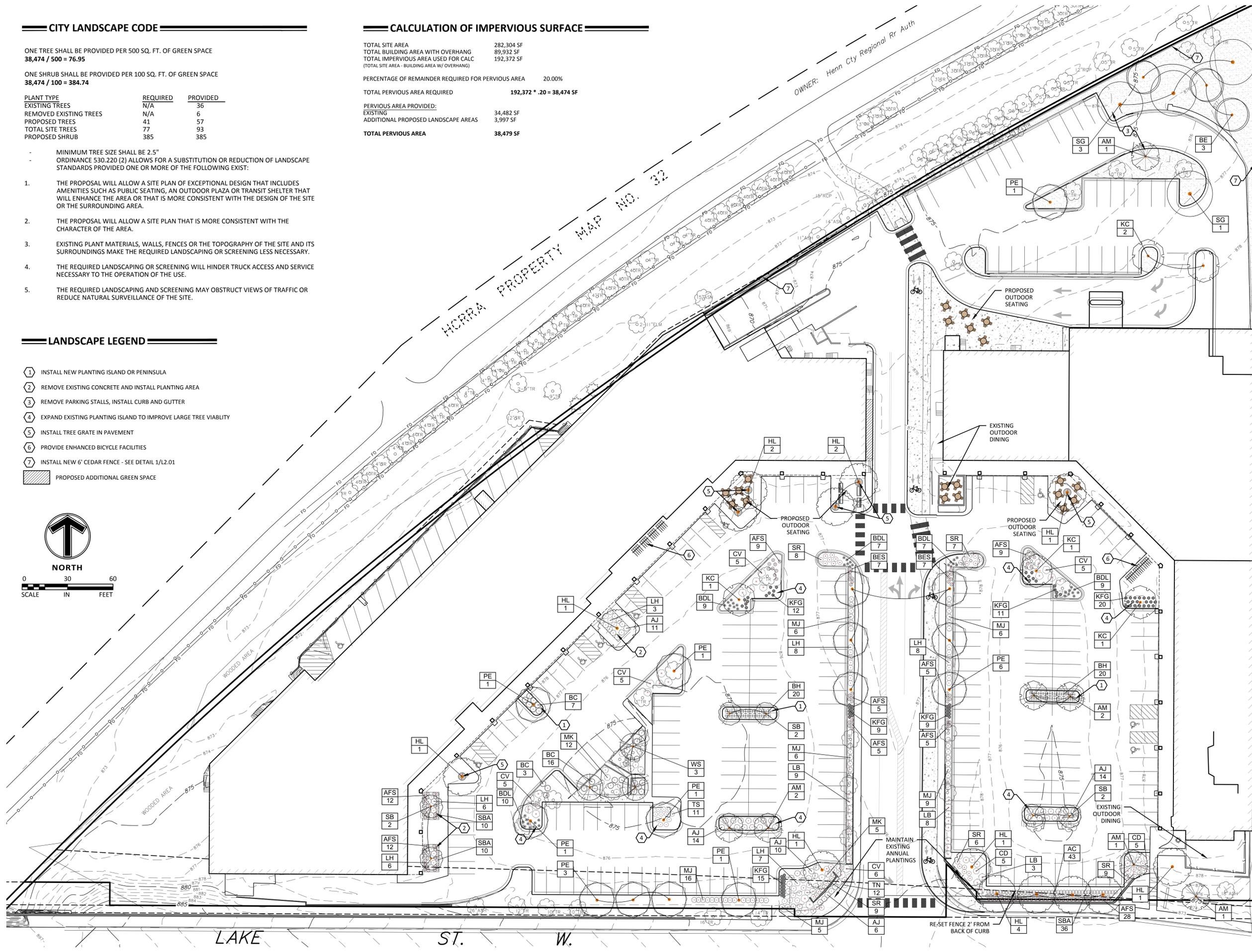
LANDSCAPE LEGEND

- INSTALL NEW PLANTING ISLAND OR PENINSULA
 - REMOVE EXISTING CONCRETE AND INSTALL PLANTING AREA
 - REMOVE PARKING STALLS, INSTALL CURB AND GUTTER
 - EXPAND EXISTING PLANTING ISLAND TO IMPROVE LARGE TREE VIABILITY
 - INSTALL TREE GRATE IN PAVEMENT
 - PROVIDE ENHANCED BICYCLE FACILITIES
 - INSTALL NEW 6' CEDAR FENCE - SEE DETAIL 1/L2.01
- PROPOSED ADDITIONAL GREEN SPACE



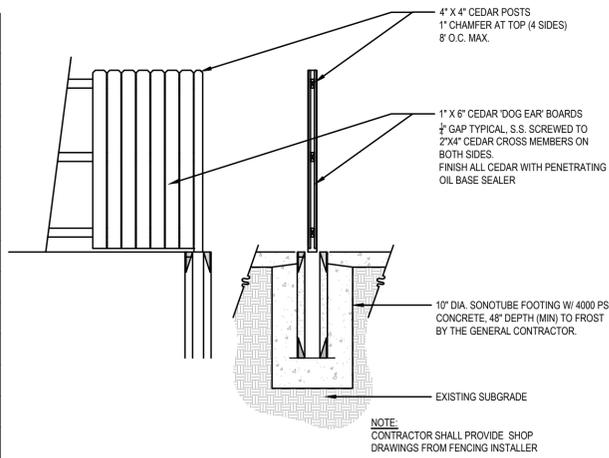
CALCULATION OF IMPERVIOUS SURFACE

TOTAL SITE AREA	282,304 SF
TOTAL BUILDING AREA WITH OVERHANG	89,932 SF
TOTAL IMPERVIOUS AREA USED FOR CALC (TOTAL SITE AREA - BUILDING AREA W/ OVERHANG)	192,372 SF
PERCENTAGE OF REMAINDER REQUIRED FOR PERVIOUS AREA	20.00%
TOTAL PERVIOUS AREA REQUIRED	192,372 * .20 = 38,474 SF
PERVIOUS AREA PROVIDED:	
EXISTING	34,482 SF
ADDITIONAL PROPOSED LANDSCAPE AREAS	3,997 SF
TOTAL PERVIOUS AREA	38,479 SF



PLANT SCHEDULE

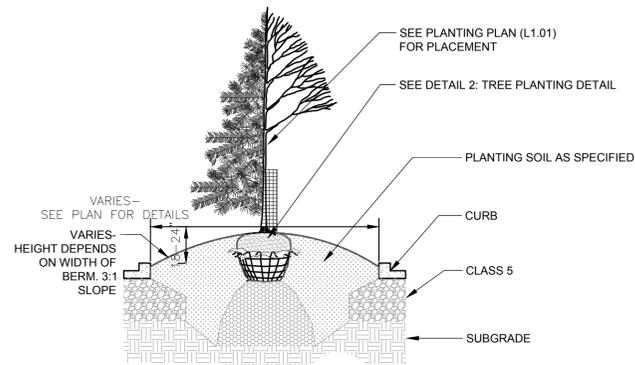
TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
SG	Acer freemanii 'Sienna Glen' / Sienna Glen Maple	B & B	2.5'Cal	4
BE	Aesculus x arnoldiana 'Autumn Splendor' / Autumn Splendor Buckeye	B & B	2.5'Cal	3
WS	Betula papyrifera 'Whitespire' / Whitespire Birch	B & B	2.5'Cal	3
HL	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B & B	2.5'Cal	14
KC	Gymnocladia dioica 'Espresso' / Kentucky Coffeetree	B & B	2.5'Cal	5
PE	Ulmus americana 'Princeton' / American Elm	B & B	2.5'Cal	15
ORN. TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
SB	Amelanchier canadensis 'Autumn Brilliance' / Autumn Brilliance Serviceberry	B & B	2'Cal	6
AM	Maackia amurensis / Amur Maackia	B & B	2'Cal	7
SHRUBS	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
BC	Aronia melanocarpa 'Autumn Magic' / Autumn Magic Black Chokeberry	5 gal		26
CD	Cornus sericea 'Allema's Compact' / Dwarf Red Twig Dogwood	5 gal		10
BH	Diervilla lonicera / Dwarf Bush Honeysuckle	5 gal		40
LH	Hydrangea paniculata 'Jane' / Little Lime Hydrangea	5 gal		38
MJ	Juniperus chinensis 'Mint Julep' / Mint Julep Juniper	5 gal		48
AJ	Juniperus horizontalis 'Compacta' / Andorra Compact Juniper	5 gal		55
TN	Physocarpus opulifolius 'SMPOTW' / Tiny Wine Ninebark	5 gal		12
AC	Ribes alpinum / Alpine Currant	5 gal		43
SR	Rosa rugosa 'Snow Pavement' / Snow Pavement Rose	5 gal		39
TS	Spiraea betulifolia 'Tor' / Birchleaf Spirea	5 gal		11
MK	Syringa patula 'Miss Kim' / Miss Kim Lilac	5 gal		17
LB	Syringa x 'Bloomerang' / Lilac	5 gal		20
CV	Viburnum opulus 'Compactum' / Compact European Cranberrybush	5 gal		26
GRASSES	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
KFG	Calamagrostis x acutiflora 'Karl Foerster' / Feather Reed Grass	1 gal		76
PERENNIALS	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
SBA	Allium x 'Summer Beauty' / Summer Beauty Allium	1 gal		56
BDL	Hemerocallis x 'Baja' / Baja Daylily	1 gal		42
BES	Rudbeckia fulgida 'Goldstrum' / Black Eyed Susan	1 gal		14
AFS	Sedum x 'Autumn Fire' / Autumn Fire Sedum	1 gal		90
GROUND COVERS	BOTANICAL NAME / COMMON NAME	QTY	CAL	QTY
	Turf Sod Highland Sod / Sod	8,451 sf		



1 CEDAR FENCE DETAIL

L2.01

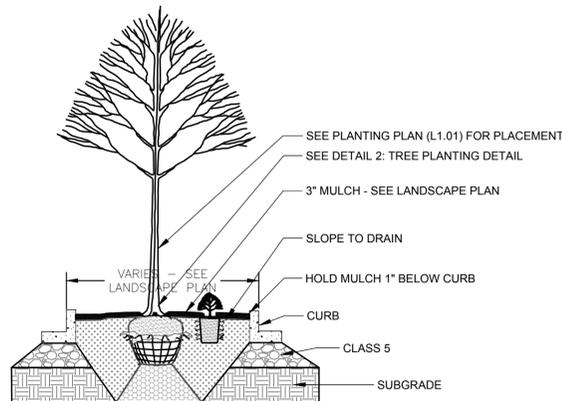
N.T.S.



2 BERMED ISLAND PLANTING DETAIL

L2.01

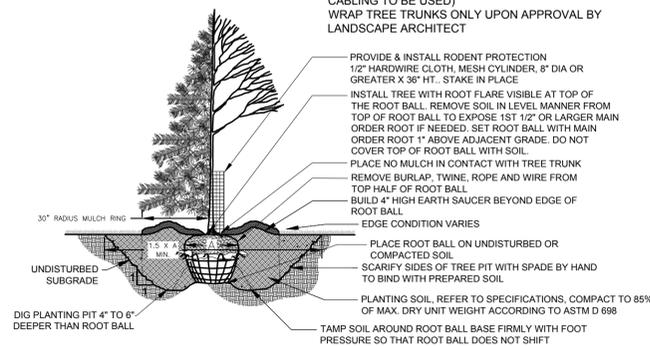
NTS



3 PARKING LOT ISLAND DETAIL

L2.01

NTS



4 TREE PLANTING DETAIL

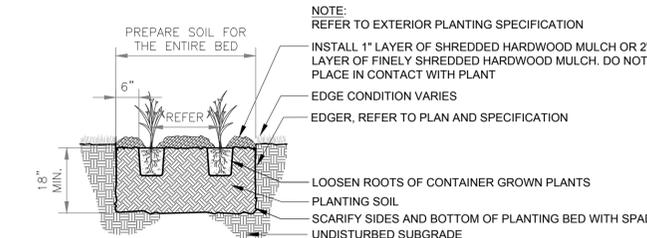
L2.01

NTS

5 SHRUB PLANTING DETAIL 2

L2.01

NTS



6 PERENNIAL PLANTING DETAIL

L2.01

NTS

NOTES

GENERAL NOTES:

- THE CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH THE EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF THE WORK.
- THE CONTRACTOR SHALL VERIFY PLAN LAYOUT AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN OR INTENT OF THE LAYOUT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
- THE CONTRACTOR SHALL PROTECT EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING CONSTRUCTION. DAMAGE TO SAME SHALL BE REPAIRED AND/OR REPLACED AT NO ADDITIONAL COST TO THE OWNER.
- LOCATE AND VERIFY ALL UTILITIES, INCLUDING IRRIGATION LINES, WITH THE OWNER FOR PROPRIETARY UTILITIES AND GOPHER STATE ONE CALL 48 HOURS BEFORE DIGGING. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ANY DAMAGES TO SAME. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS TO FACILITATE PLANT RELOCATION.
- THE LANDSCAPE CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.
- THE CONTRACTOR SHALL REVIEW THE SITE FOR DEFICIENCIES IN SITE CONDITIONS WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR WARRANTY. UNDESIRABLE SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK.
- THE PLAN TAKES PRECEDENCE OVER THE LANDSCAPE LEGEND IF DISCREPANCIES EXIST. QUANTITIES SHOWN IN THE PLANTING SCHEDULE ARE FOR THE CONTRACTOR'S CONVENIENCE. CONTRACTOR TO VERIFY QUANTITIES SHOWN ON THE PLAN.
- THE SPECIFICATIONS TAKE PRECEDENCE OVER THE PLANTING NOTES AND GENERAL NOTES.
- EXISTING TREES AND SHRUBS TO REMAIN SHALL BE PROTECTED TO THE DRIP LINE FROM ALL CONSTRUCTION TRAFFIC, STORAGE OF MATERIALS ETC. WITH 4' HT. ORANGE PLASTIC SAFETY FENCING ADEQUATELY SUPPORTED BY STEEL FENCE POSTS 6' O.C. MAXIMUM SPACING.
- LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE ALLOWED.
- CONTRACTOR SHALL REQUEST IN WRITING, A FINAL ACCEPTANCE INSPECTION.

PLANTING NOTES:

- NO PLANTS SHALL BE INSTALLED UNTIL FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- A GRANULAR PRE-EMERGENT HERBICIDE SHALL BE APPLIED TO ALL PLANT BEDS AT THE MANUFACTURERS RECOMMENDED RATE PRIOR TO PLANT INSTALLATION.
- ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-Z60, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERYMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS.
- OVERSTORY TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE PAVED SURFACES.
- ALL PLANTS MUST BE HEALTHY, VIGOROUS MATERIAL, FREE OF PESTS AND DISEASE AND BE CONTAINER GROWN OR BALLED AND BURLAPPED AS INDICATED IN THE LANDSCAPE LEGEND.
- PLANT MATERIALS TO BE INSTALLED PER PLANTING DETAILS.
- ALL TREES MUST BE STRAIGHT TRUNKED AND FULL HEADED AND MEET ALL REQUIREMENTS SPECIFIED.
- THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION.
- NO SUBSTITUTIONS OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
- ALL PLANT MATERIAL QUANTITIES, SHAPES OF BEDS AND LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE COVERAGE OF ALL PLANTING BEDS AT SPACING SHOWN AND ADJUSTED TO CONFORM TO THE EXACT CONDITIONS OF THE SITE. THE LANDSCAPE ARCHITECT SHALL APPROVE THE STAKING LOCATION OF ALL PLANT MATERIALS PRIOR TO INSTALLATION.
- ALL PLANTING AREAS MUST BE COMPLETELY MULCHED AS SPECIFIED.
- MULCH: SHREDDED HARDWOOD MULCH, CLEAN AND FREE OF NOXIOUS WEEDS OR OTHER DELETERIOUS MATERIAL, IN ALL MASS PLANTING BEDS AND FOR TREES, UNLESS INDICATED AS ROCK MULCH ON DRAWINGS. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO DELIVERY ON-SITE FOR APPROVAL. DELIVER MULCH ON DAY OF INSTALLATION. USE 4" FOR SHRUB BEDS, AND 3" FOR PERENNIAL/GROUND COVER BEDS, UNLESS OTHERWISE DIRECTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MULCHES AND PLANTING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN.
- USE ANTI-DESICCANT (WILTPRUF OR APPROVED EQUAL) ON DECIDUOUS PLANTS MOVED IN LEAF AND FOR EVERGREENS MOVED ANYTIME. APPLY AS PER MANUFACTURER'S INSTRUCTION. ALL EVERGREENS SHALL BE SPRAYED IN THE LATE FALL FOR WINTER PROTECTION DURING WARRANTY PERIOD.
- PLANTING SOIL FOR TREES, SHRUBS AND GROUND COVERS: FERTILE FRIABLE LOAM CONTAINING A LIBERAL AMOUNT (4% MIN.) OF HUMUS AND CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT SHALL COMPLY WITH MNDOT SPECIFICATION 3877 TYPE B SELECT TOPSOIL. MIXTURE SHALL BE FREE FROM HARDPACK SUBSOIL, STONES, CHEMICALS, NOXIOUS WEEDS, ETC. SOIL MIXTURE SHALL HAVE A PH BETWEEN 6.1 AND 7.5

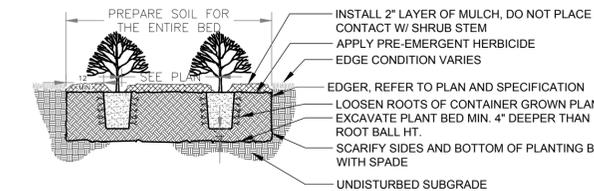
- AND 10-0-10 FERTILIZER AT THE RATE OF 3 POUNDS PER CUBIC YARD. IN PLANTING BEDS INCORPORATE THIS MIXTURE THROUGHOUT THE ENTIRE BED IN A 6" LAYER AND ROTO-TILLING IT INTO THE TOP 12" OF SOIL AT A 1:1 RATIO ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HELED IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE REJECTED.
- ALL PLANTS SHALL BE GUARANTEED FOR TWO COMPLETE GROWING SEASONS (APRIL 1 - NOVEMBER 31), UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND PLANTS.
 - CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 3 DAYS PRIOR TO PLANNED DELIVERY. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 24 HOURS IN ADVANCE OF BEGINNING PLANT INSTALLATION.
 - SEASONS/TIME OF PLANTING AND SEEDING: NOTE: THE CONTRACTOR MAY ELECT TO PLANT IN OFF-SEASONS ENTIRELY AT HIS/HER RISK. DECIDUOUS
 - 17.1. POTTED PLANTS: 4/1 - 6/1; 9/21 - 11/1
 - 17.2. DECIDUOUS B&B: 4/1 - 6/1; 9/21 - 11/1
 - 17.3. EVERGREEN POTTED PLANTS: 4/1 - 6/1; 9/21 - 11/1
 - 17.4. EVERGREEN B&B: 4/1 - 5/1; 9/21 - 11/1
 - 17.5. TURF/LAWN SEEDING: 4/1 - 6/1; 7/20 - 9/20
 - MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE, INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE WATERING, CULTIVATING, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
 - ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE AND MEETING ALL LANDSCAPE LEGEND SPECIFICATIONS.
 - WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK. IN EXTREMELY HOT, DRY WEATHER, WATER MORE OFTEN AS REQUIRED BY INDICATIONS OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.

TURF NOTES:

- TURF ESTABLISHMENT SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE PROVISIONS OF THE MNDOT 2105 AND 2575 EXCEPT AS MODIFIED BELOW:
- ALL AREAS TO RECEIVE SOD SHALL ALSO RECEIVE 4" OF TOPSOIL PRIOR TO INSTALLING SOD. TOPSOIL SHALL BE FREE OF TREE ROOTS, STUMPS, BUILDING MATERIAL, AND TRASH, AND SHALL BE FREE OF STONES LARGER THAN 1/2" INCHES IN ANY DIMENSION.
 - WHERE SOD ABUTS PAVED SURFACES, FINISHED GRADE OF SOD/SEED SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC.
 - SOD SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES, SOD SHALL BE STAKED SECURELY.
 - ALL DISTURBED AREAS TO BE TURF SEED, ARE TO RECEIVE 4" TOP SOIL, SEED, MULCH, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. FOR SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES INSTALL EROSION CONTROL BLANKET.
 - ALL DISTURBED AREAS TO RECEIVE NATIVE SEED, ARE TO RECEIVE PLANTING SOIL, SEED, MULCH, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. FOR SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES INSTALL EROSION CONTROL BLANKET.

IRRIGATION NOTES:

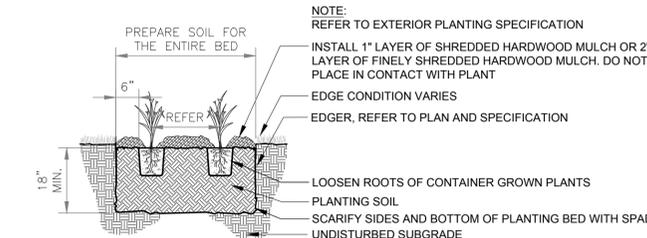
- IRRIGATION SYSTEM TO BE DESIGN/BUILD. CONTRACTOR TO SUBMIT SHOP DRAWINGS FOR APPROVAL OF SYSTEM LAYOUT PRIOR TO INSTALLATION.
- ALL SOD TO RECEIVE SPRAY OR ROTOR IRRIGATION HEADS WITH MINIMUM DESIGN OF 1" IRRIGATION PER WEEK.
- ALL PLANT BEDS TO RECEIVE DRIP LINE IRRIGATION, WITH A MINIMUM DESIGN OF .25" IRRIGATION PER WEEK.
- CONTRACTOR TO INSTALL A TOTAL OF 4 QUICK COUPLERS AT THE CORNERS OF THE PROPERTY. A 2.5" TYPE K SOURCE PIPE IS PROVIDED BY MECHANICAL.



5 SHRUB PLANTING DETAIL 2

L2.01

NTS



6 PERENNIAL PLANTING DETAIL

L2.01

NTS



12800 Whitewater Drive, Suite 300
Minnetonka, MN 55343
763.476.6010 telephone
763.476.8532 facsimile

Engineering | Surveying | Planning | Environmental

Client DORAN COMPANIES

Project CALHOUN VILLAGE

Location MINNEAPOLIS, MINNESOTA

Certification

Summary

Designed: JIM Drawn: JMW
Approved: EVM Book / Page:
Phase: CONCEPT Initial Issue: MM/DD/2016

Revision History

No. Date By Submittal / Revision
A 06/21/16 JMW Revised per City Comments

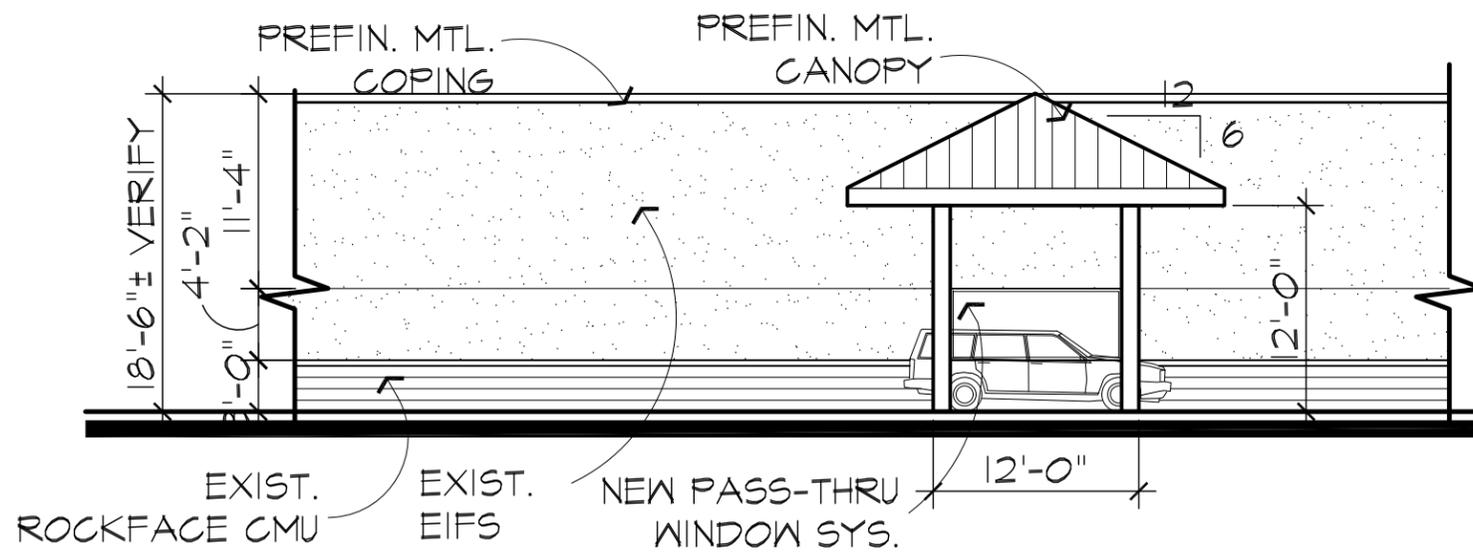
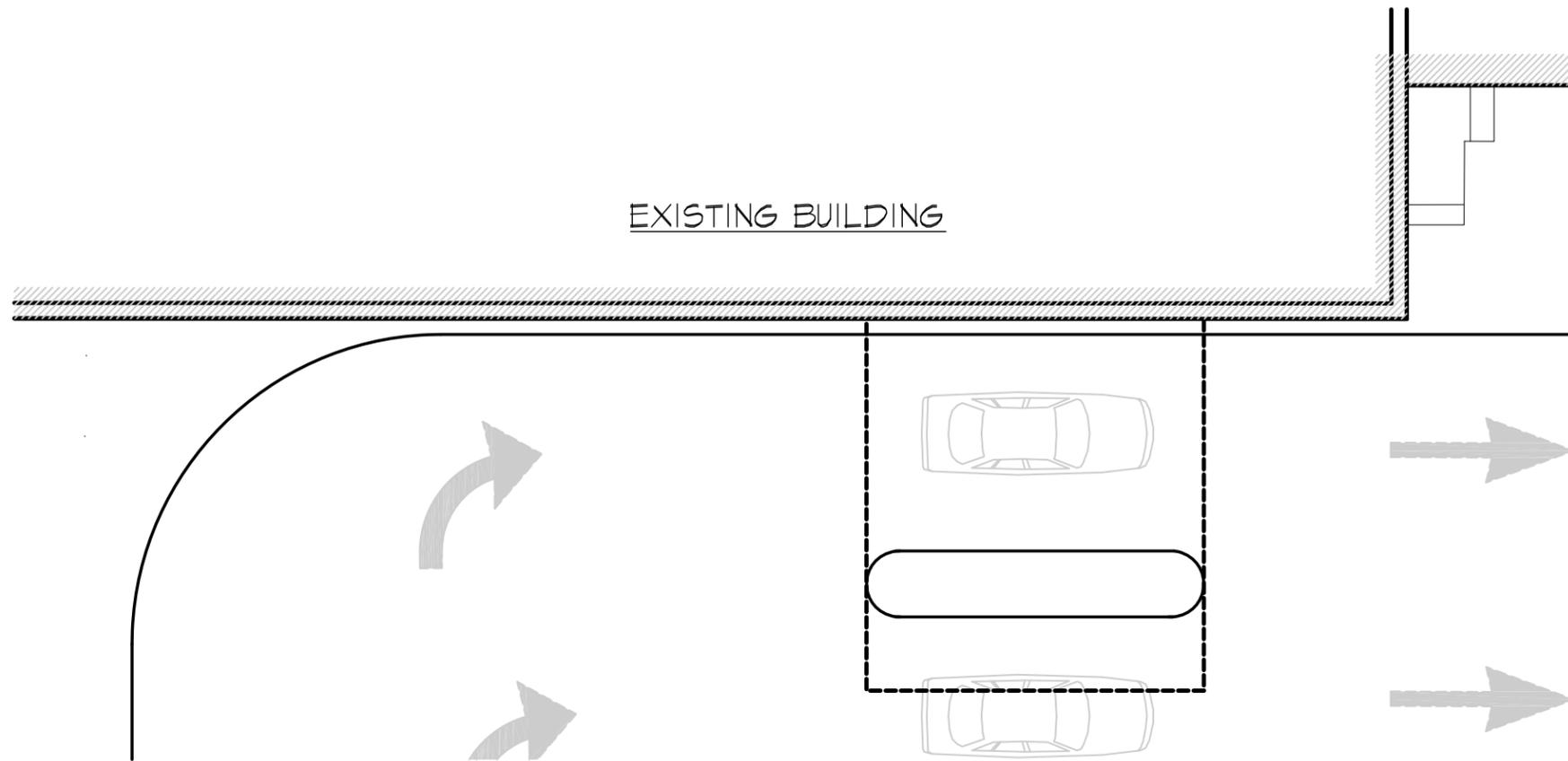
Sheet Title LANDSCAPE DETAILS

Sheet No. Revision

L2.01 A

Project No. DOR20550

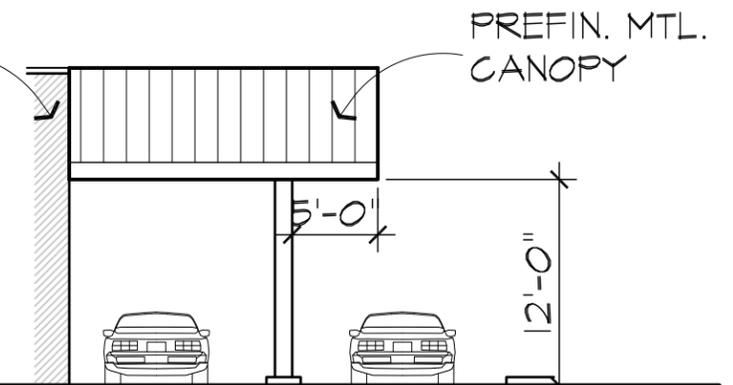
EXISTING BUILDING



PARTIAL BACK ELEVATION

3/32" = 1'-0"

LINE OF EXIST. BLDG.



SIDE ELEVATION

3/32" = 1'-0"

ARCHITECTURAL CONSORTIUM L.L.C.

901 North 3rd Street
Minneapolis, MN 55401

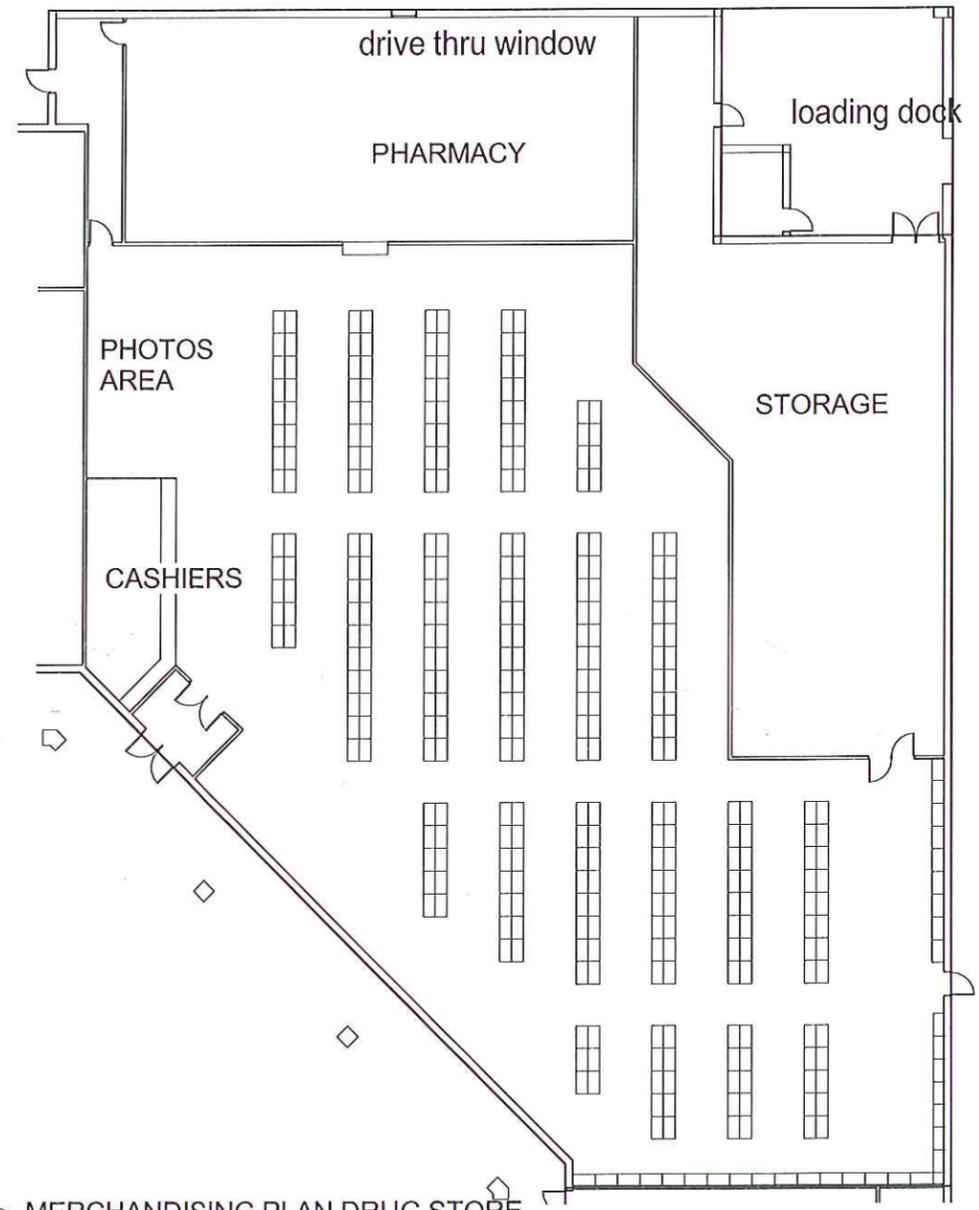
612-436-4030
Fax 612-692-9960

CALHOUN VILLAGE
MINNEAPOLIS, MN

EXTERIOR ELEVATIONS

PROJECT NUMBER: 16-1027-01
ISSUED DATE: 05-27-16
DRAWN BY: MR
CHECKED BY: KA

A1



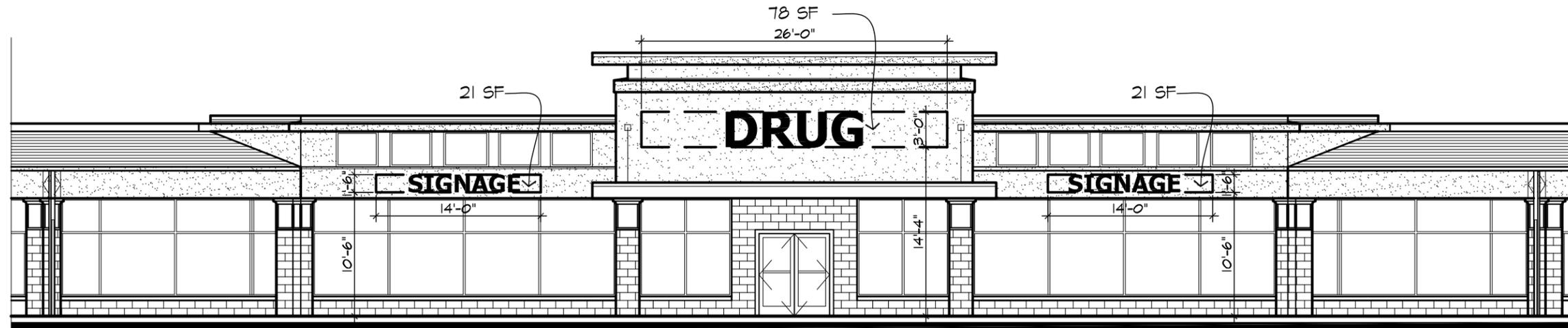
① MERCHANDISING PLAN DRUG STORE
 3/64" = 1'-0"

05/27/16



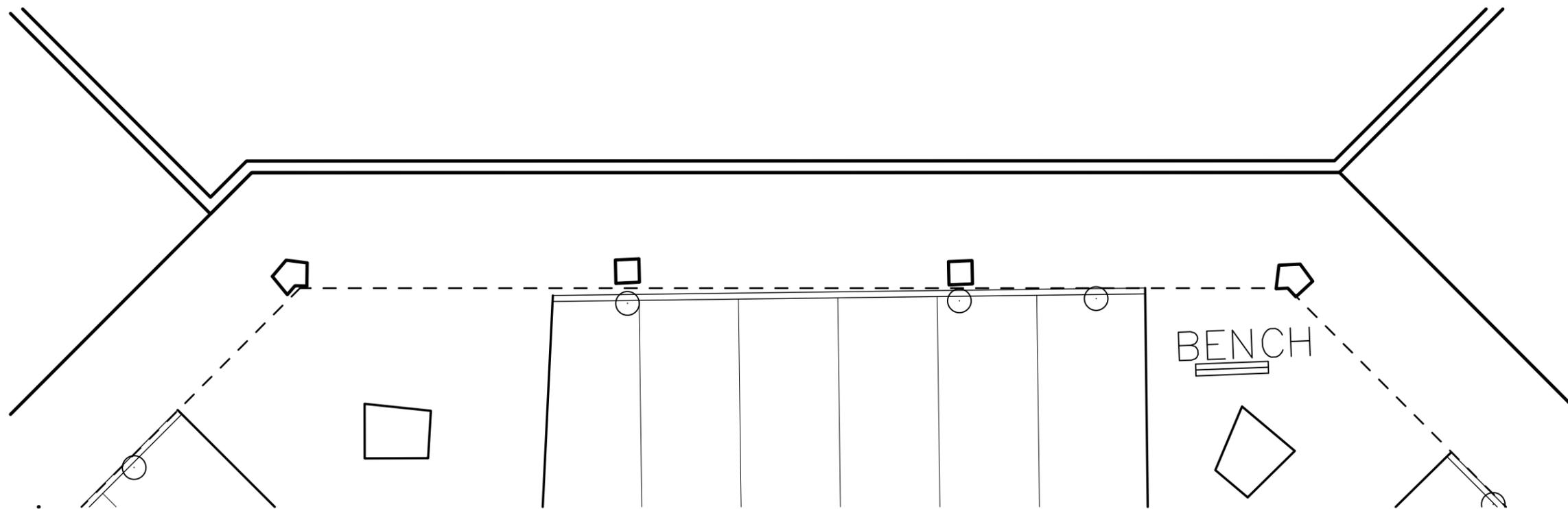
CALHOUN VILLAGE
 3200 WEST LAKE STREET

MERCHANDISING PLAN DRUG
 STORE



PARTIAL FRONT ELEVATION

3/32" = 1'-0"



PARTIAL PLAN

3/32" = 1'-0"

**ARCHITECTURAL
CONSORTIUM L.L.C.**

901 North 3rd Street
Minneapolis, MN 55401

612-436-4030
Fax 612-692-9960

CALHOUN VILLAGE
MINNEAPOLIS, MN

EXTERIOR ELEVATIONS

PROJECT NUMBER: 16-1027-01
ISSUED DATE: 05-27-16
DRAWN BY: CM
CHECKED BY: KA

A2



Signage Survey

Calhoun Village Shopping Center
3200 West Lake Street
Minneapolis, MN

Calhoun Village Shopping Center

Monument Sign 150 sft (per side) 15' x 10'

Center Entrance at West Lake Street

East Elevation



West Elevation



East Building from Lake Street



East Building Tenants

Burger Jones

3200 West Lake Street

Monument Sign (on West Lake St) 52 sft (per side) 10' h



Building sign



South Elevation:	35 sq ft (approx. 5'x7')
West Elevation:	71 sq ft (approx. 5'x7'+6'x6')
Height	12' AFF

Subway

3202 West Lake Street

Building Sign

45 sq ft (3'x15')

12' AFF



Laurie Kottke Fine Jewelers

3204 West Lake Street

Building Sign

24.5 sq ft (30"x10.5')

12' AFF



Tonka Cleaners
3206 West Lake Street
Building Sign

26 sf ft (4' x 11')

12' AFF



PostNet
3208 West Lake Street
Building Sign

18 sq ft (18"x11.5')

12' AFF



Goddess Nails
3210 West Lake Street

Building Sign 45 sq ft (3'x15') 12' AFF



H&R Block Enterprises
3212 West Lake Street

Building Sign 43 sq ft (36'x21') 12' AFF



Barnes & Noble
3216 West Lake Street
Building Sign

78 sq ft (3'x26')

16' AFF



Rustica Bakery
3220 West Lake Street
Building Sign

116 sq ft (3'x52')

12' AFF



Punch Neopolitan Pizza
3226 West Lake Street

Building Sign

46.5 sq ft (3'x15.5')

12' AFF



West Building from Lake Street



European Wax Center
3232 West Lake Street

Building Sign

48 sq ft (42'x14')

12' AFF



Calhoun Vision Center
3234 West Lake Street

Building Sign

32 sq ft (32"x12')

12' AFF



1st Wok

3236 West Lake Street

Building Sign

68 sq ft (4'x18')

12' AFF



Falafel King

3238 West Lake Street

Building Sign

48 sq ft (4'x15')

12' AFF



Walgreens

3240 West Lake Street

Building Sign

146 sq ft (4'x32', 18"x7' (x2))

16' AFF



Sprint PCS

3242 West Lake Street

Building Sign

43 sq ft (42"x13.5')

12' AFF



Chuck & Don's
3246 West Lake Street
Building Sign

81 sq ft (3'x36')

12' AFF



FastFrame
3248 West Lake Street
Building Sign

54 sq ft (3'x19') 12' AFF



Orange Theory Fitness
3252A West Lake Street

Building Sign

42 sq ft (42"x14')

12' AFF



End of Building Sign

62.5 sq ft (4'x15'8")

26.5' AFF



Modo Hot Yoga
3252B West Lake Street

Building Sign 48 sq ft (38"x16.5') 12' AFF



End of Building Sign 65 sq ft (42"x23') 30.5 AFF



Run N Fun
3252C West Lake Street

Building Sign 52 sq ft (42"x15') 12' AFF



Massage Envy
3254 West Lake Street

Building Sign 54 sq ft (36"x19') 12' AFF



The Locker / City Homes Design & Build
3260 West Lake Street

Building Sign 34 sq ft (42"x9.5') 12' AFF



Calhoun Dental
3262 West Lake Street

Building Sign 60 sq ft (36'x20.5") 19' AFF



Walkway Sign 3 sq ft (12"x3')(side) 11' AFF



Twin Cities Medical Clinic
3264 West Lake Street

Building Sign 84 sq ft (36'x28') 20' AFF



Walkway Sign 3 sq ft (12"x3')(side) 11' AFF





View 1

View of Existing Building A from the south end of the west parking lot





View 2

Calhoun Village signage at front of property on west side of parking area





View 3

East parking lot from the south side on top of retaining wall



View 4

Burger Jones signage on south end of east parking lot





View 5

Intersection between E & W parking lots looking N toward back loading area





View 6

Back parking lot at north of property looking towards the back of Existing Building A (far right) and Existing Building B (far right)





View 7

View of rear east loading dock behind Existing Building B





Views 8 & 9

View of rear loading dock and underground parking entrance behind Existing Building A





View 10

Underground parking behind and under Existing Building A on the northwest side of the property



Panoramas



Panorama 1

View of west side of parking lot and property signage from intersection of Lake St W and property entrance





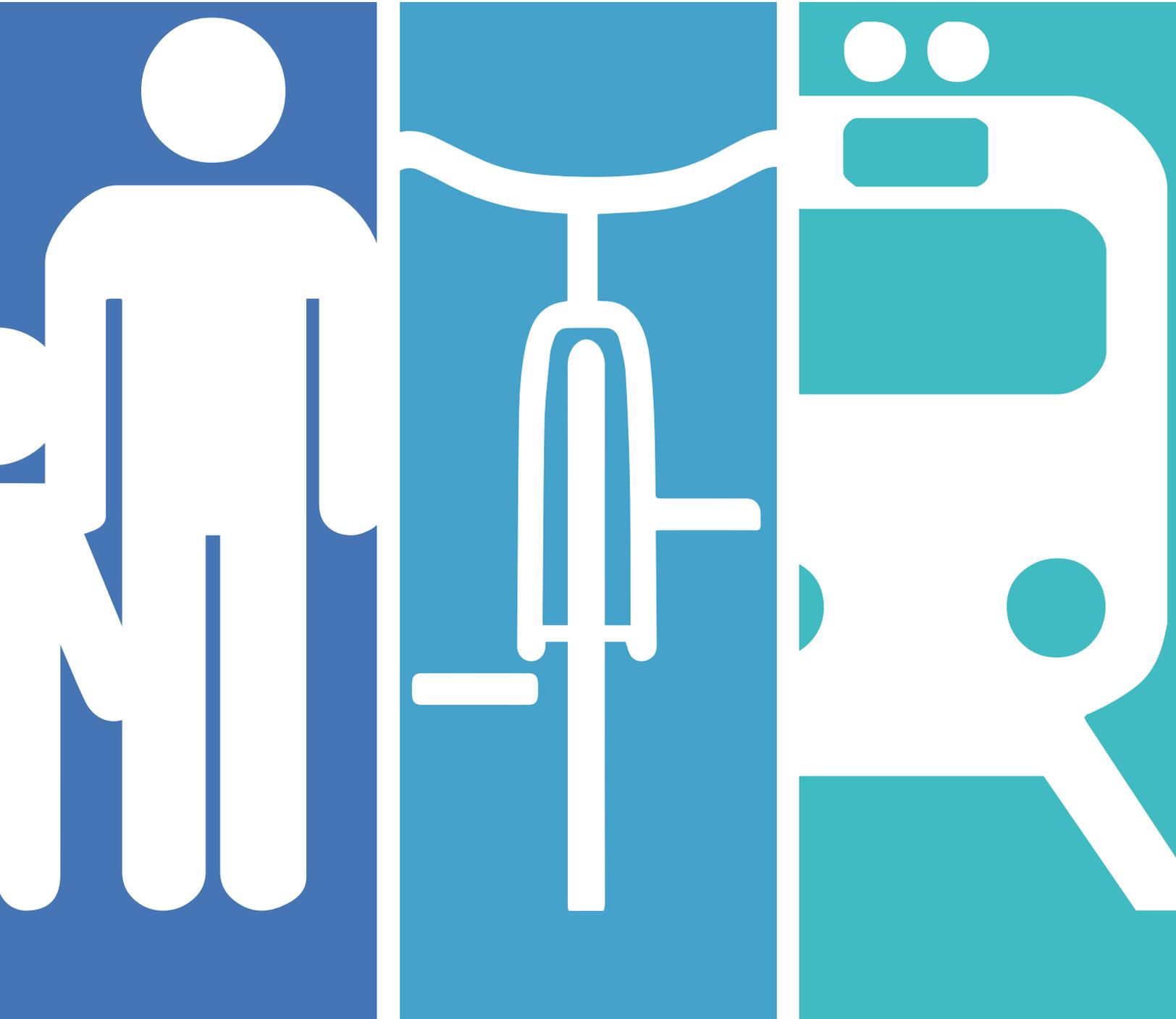
Panorama 2

Panoramic view from the north side of the site looking south towards Existing Building A (right) and Existing Building B (left)



WEST LAKE MULTIMODAL TRANSPORTATION STUDY

FINAL RECOMMENDATIONS REPORT





PROJECT PARTNERS

City of Minneapolis

Metropolitan Council

Hennepin County

Minneapolis Parks and Recreation Board

TECHNICAL ADVISORY COMMITTEE

City of Minneapolis

Nathan Koster, Project Manager
Allan Klugman
Paul Miller
Brian Schaffer

Metropolitan Council

Mark Bishop
Ryan Kronzer
Sophia Ginis

Hennepin County

John Doan
Kristy Morter
Catherine Walker
Kelley Yemen

Minneapolis Parks and Recreation Board

Michael Schroeder

City of St. Louis Park

Jack Sullivan

SEH

Heather Kienitz, Project Manager
Kristin Petersen
Nikki Farrington

Toole Design Group

Hannah Pritchard

COMMUNITY COLLABORATORS

Cedar Isles Dean
Neighborhood Association

West Calhoun Neighborhood
Council

Midtown Greenway Coalition

Hennepin County Bicycle
Advisory Committee

Minneapolis Bicycle Advisory
Committee

Minneapolis Pedestrian
Advisory Committee

Councilmember Lisa
Goodman, Ward 7

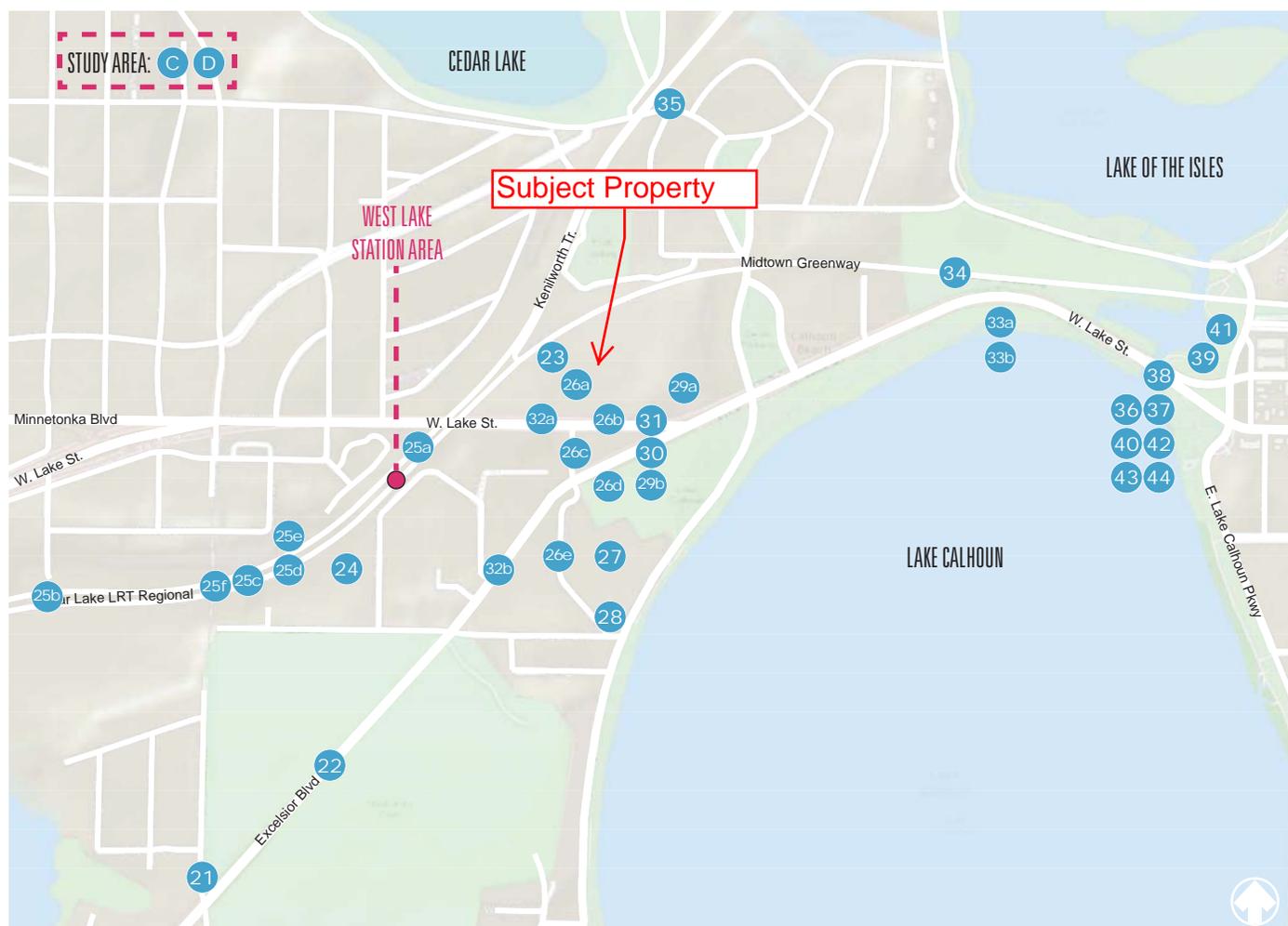
Councilmember Linea
Palmisano, Ward 13



4.0 LONG TERM RECOMMENDATIONS

Improvements were identified that would likely be implemented beyond opening day of the Green Line Extension (scheduled for 2020). These improvements do not have funding sources identified at this time, were not as highly prioritized by project stakeholders as near term recommendations were, and/or require additional coordination, significant investment and construction. Recommendations C, D and 21 through 44 are the long term recommendations.

FIGURE 4-1
LONG TERM RECOMMENDATION LOCATIONS



LONG TERM RECOMMENDATIONS, CONTINUED

ID	Location	Improvement	Agency*	Estimated Cost**	
26a	Market Plaza through Calhoun Village	Trail connection through Calhoun Village parking lot along east side of drive aisle (requires coordination with private property).	MPLS	\$97,000	\$196,000
26b	Market Plaza/Lake Intersection	Improve intersection to accommodate trail along Market Plaza by relocating signal controller and adding trail crossing markings. Address curb radius in northeast quadrant.	MPLS/HC/MPRB	\$35,000	
26c	Market Plaza	Reconfigure the street to provide a shared use trail along the east side of the roadway.	MPLS	\$54,000	
26d	Market Plaza/Excelsior Intersection	Improve intersection to accommodate potential trail on Market Plaza: trail crossing pavement markings.	HC/MPLS	\$6,000	
26e	W. Calhoun Blvd	Design street to include bike facility to provide access from Excelsior Blvd to Lake Calhoun. NB Counterflow Bicycle Lane and SB Bike Boulevard.	HC/MPLS	\$4,000	
27	Area between Excelsior and Lake Calhoun	Construct off-street trail providing connection between Excelsior Boulevard and W. Lake Calhoun Parkway.	MPRB	\$65,000	
28	32nd/Calhoun Blvd. Intersection	Realign W Calhoun Blvd Intersection to increase distance from Calhoun Pkwy	MPLS/MPRB	\$260,000	
29a	Lake/Excelsior Intersection	Reconfigure median to add pedestrian staging space, shorten crossings, and add gateway landscaping *ROW NEEDED (not included in cost)	HC/ MPLS	\$909,000	
29b	Excelsior between Market Plaza and Lake Street	Reconstruct medians to clarify lanes and provide green space for landscaping	HC/MPLS	\$109,000	
30	Excelsior and Lake - In coordination with improvements over time	If areas are redeveloped and projects occur, locate infrastructure outside sidewalk accessible path	HC/ MPLS	NA	
31	Excelsior and Lake - In coordination with improvements over time	If areas are redeveloped and projects occur, fill lighting gaps	HC/ MPLS	NA	
32a	Lake Street between Market Plaza and Dean Parkway	As projects and redevelopment occur, maximize sidewalk widths reallocating surplus widths from travel lanes	HC/ MPLS	NA	
32b	Excelsior Boulevard between West 32nd Street and Lake Street	As projects and redevelopment occur, maximize sidewalk widths reallocating surplus widths from travel lanes	HC/ MPLS	NA	

*Listed agencies are assumed partners for identified improvement.

**Estimated costs include 30% contingency in 2015 dollars without engineering fees.

MPLS = Minneapolis MT = Metro Transit HC = Hennepin County TR = Three Rivers Park Dist. MPPB = Minneapolis Park and Recreation Board

RECOMMENDATION 26 – MARKET PLAZA CONNECTION TO LAKE CALHOUN

MARKET PLAZA (FROM MIDTOWN GREENWAY TRAIL NORTH OF CALHOUN VILLAGE TO EXCELSIOR BOULEVARD),
WEST CALHOUN BOULEVARD (FROM EXCELSIOR BOULEVARD TO WEST CALHOUN PARKWAY)

Create a connection for bicyclists and pedestrians between the Midtown Greenway and Lake Calhoun through Calhoun Village, along Market Place and West Calhoun Boulevard. The recommendation includes:

- **26a** – A shared use trail connection through Calhoun Village parking lot along the east side of driveway aisle with raised crosswalks at aisle crossings within the parking lot. The proposed improvement would utilize the current sidewalk and patio space adjacent to the easternmost building and narrows the lanes of the entrance driveway to gain space to widen the existing sidewalk.
- **26b** – Relocation of the existing signal controller from the northeast quadrant to the northwest quadrant of West Lake Street and the Calhoun Village access to provide more pedestrian space on the corner. Include enhanced shared use trail crosswalk, using poly-preform markings and continental design at the east West Lake Street approach. Address curb radius in northeast quadrant.
- **26c** – Reallocate the current roadway right-of-way by narrowing the lanes on Market Plaza between West Lake Street and Excelsior Boulevard and widening the east side sidewalk to accommodate a shared use trail.
- **26d** – Provide an enhanced shared use trail crosswalk, using poly-preform markings and continental design at the east approach of the Market Plaza and Excelsior Boulevard intersection.
- **26e** – On West Calhoun Boulevard, from Excelsior Boulevard to West 32nd Street, the recommendation includes a northbound counter-flow bicycle lane and a southbound bicycle boulevard.

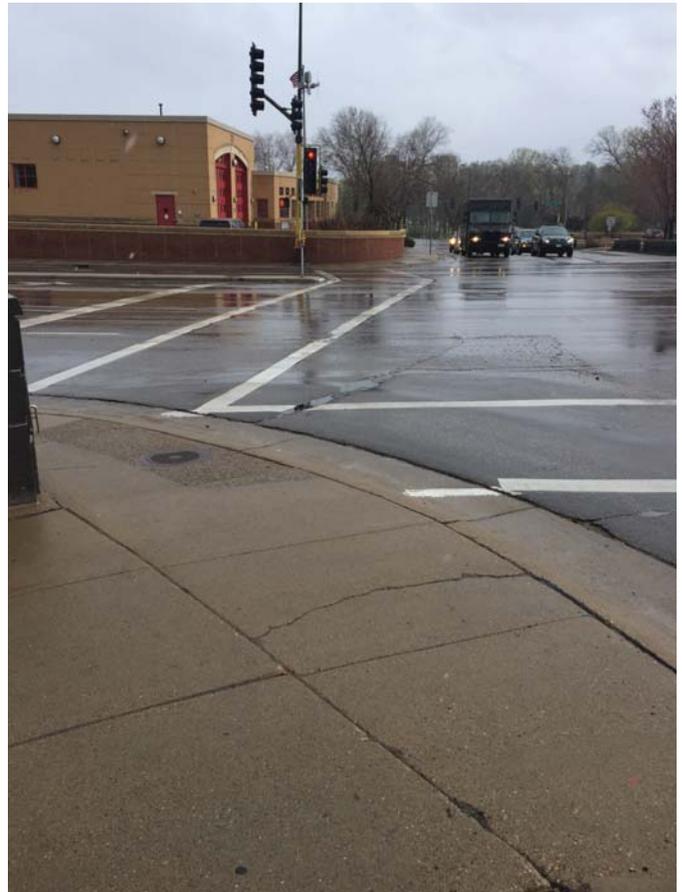
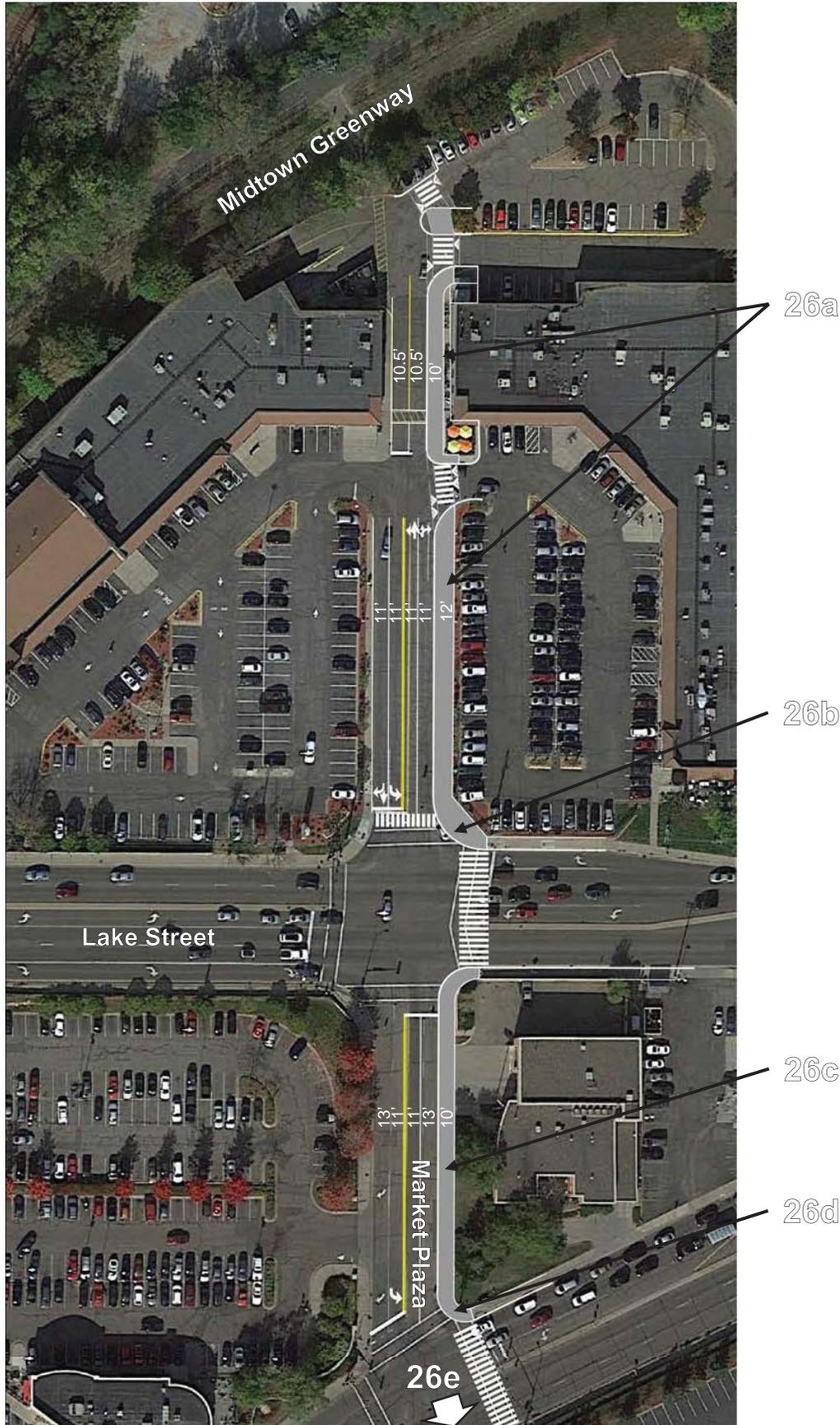


FIGURE 4-8
RECOMMENDATION 26A-D – MARKET PLAZA CONNECTION TO LAKE CALHOUN



CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION ("CIDNA")
**RESOLUTION OPPOSING A DRUG STORE DRIVE-THRU
IN CALHOUN VILLAGE SHOPPING CENTER**
JUNE 8, 2016

WHEREAS the Calhoun Village Shopping Center ("Calhoun Village") is under contract to be sold by Pfaff Calhoun LLC ("Pfaff") to Doran Companies ("Doran").

WHEREAS Doran, through Pfaff, has filed a Master Land Use Application for site plan review for an automobile drive-thru facility on the northeast side of Calhoun Village (the "Drive-Thru", see attached Doran Application). It appears that a Drug Store would use the Drive-Thru and operate from space currently occupied by Barnes and Noble.

WHEREAS the Cedar-Isles-Dean Neighborhood and surrounding area already has serious traffic problems, creating unsafe and unattractive conditions, and as a result, there was a tragic pedestrian fatality resulting from a delivery truck emerging from Calhoun Village at the corner of Lake Street and Market Plaza on February 14, 2014.

WHEREAS the Drive-Thru would create additional automobile traffic into Calhoun Village and through the narrow roadway between the two buildings (the "Narrows") and into the northern parking lot of the shopping center adjacent to the Midtown Greenway.

WHEREAS significant bicycle and pedestrian traffic comes into Calhoun Village from the surrounding neighborhoods including from the Midtown Greenway through the northern parking lot as well as points south.

WHEREAS delivery trucks also pass through the Narrows, into the northern parking lot, to loading docks in the basement of the building.

WHEREAS increased automobile traffic resulting from the Drive-Thru would be dangerous to pedestrians and cyclists, particularly at the blind corner behind Punch Pizza where automobiles would emerge from the Drive-Thru, turn left, and cross bicycle or pedestrian paths.

WHEREAS the West Lake Multi-Modal Transportation Study in 2015 considered a bicycle-pedestrian connection between the Midtown Greenway and Lake Calhoun through Calhoun Village on the east side of Market Plaza. (See attached Multi-Modal Study pp 71-74.)

WHEREAS Doran has proposed a dedicated bicycle trail on the west side of the Market Plaza entry drive by reducing the number of traffic lanes from 4 to 3. (See attached Doran Application.)

WHEREAS Doran has proposed marked bicycle trail and pedestrian cross walks in the north parking lot to the Midtown Greenway. (See attached Doran Application.)

WHEREAS Doran has proposed new landscaping. (See attached Doran Application.)

NOW THEREFORE BE IT RESOLVED THAT

1. CIDNA opposes a Drive-Thru in Calhoun Village; increased automobile traffic should not be encouraged in the Calhoun Village for reasons of safety, health, and active urban lifestyle.
2. CIDNA commends Doran for reducing the number of automobile lanes from 4 to 3 in the Calhoun Village entry drive, and proposing a dedicated bicycle path and pedestrian cross walks through its property connecting the Midtown Greenway with Market Plaza.
3. CIDNA commends Doran for proposing landscape improvements that are sorely needed.
4. CIDNA recommends the following changes to the Doran bicycle-pedestrian traffic flow, subject to technical review by City Planning, Public Works, and the Bicycle Review Committee:
 - a. The trail should be raised to sidewalk level, and/or marked with bollard posts or planters to protect cyclists from automobile traffic. (As an example, see attached lane analysis of the Market Plaza entry drive looking north from Lake Street. In this example, the bike lanes could be reversed if they were to be located on the west side as proposed by Doran.) CPED, Public Works, and the Bicycle Advisory Committee should consider the pro's and con's of locating the bike trail south of the Narrows on the east or west side of the Market Plaza entry drive.
 - b. In the Narrows CIDNA recommends a dedicated and protected two-way bike lane in the street on the west side of the Narrows. Sidewalks on the east and west side of the Narrows should be reserved for pedestrians. Punch Pizza patrons should be able to continue using the sidewalk on the east side of the Narrows.
 - c. The bike lane between the Midtown Greenway and the Calhoun Village building should be clearly marked with stripes and protected with stop signs for delivery trucks and automobiles.
5. CIDNA recommends an east-west pedestrian trail across the very northern edge of the Calhoun Village property, directly south of the HCRRA property line, eventually connecting Chowen Avenue with Dean Parkway. The Trammell Crow apartments and Loop Calhoun Condominiums already have

dedicated land use for this purpose. (See attached Trammel Crow concept drawing of the east-west trail on its property.)

6. CIDNA recommends more landscaping in order to improve the arid look of the center, dominated by automobiles, which could include a stronger landscape focal entry drive using a select species of columnar trees, and medium-sized to large flowering shrubs on parking perimeter.
7. CIDNA recommends lighting for the safety and visibility of pedestrians and bikers. Lighting should be shielded to minimize encroachment upon neighboring apartments and condominiums.
8. CIDNA recommends way finding to the trails and other points.
9. CIDNA authorizes its Land Use and Development Committee to meet with Doran to discuss changes to this resolution, prior to its July, 2016 Board Meeting, that would seek common ground with the developer for improvements to Calhoun Village; as a result of these discussions, the CIDNA Board of Directors would consider changes to this resolution through email vote.

CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION

_____/s/_____
Craig Westgate
CIDNA President

_____/s/_____
Robert Corrick
Chair, CIDNA Land Use and Development Committee

_____/s/_____
Rosanne Halloran
CIDNA Secretary

From: [Robert Corrick](#)
To: [Craig Westgate](#); [Jay Isenberg](#); [Joan Soranno](#); [John Wessinger](#); [Karen Stublaski](#); [Michael Wilson](#); [Richard Goodermont](#); [Robert Corrick](#); [Stephen Goltry](#); [Goodman, Lisa R.](#); [Sadler, Patrick A.](#); [Richard Logan](#); [Sether, Shanna M](#)
Subject: Revised Doran Calhoun Village Resolution
Date: Sunday, June 19, 2016 9:12:03 AM
Attachments: [DoranDriveThruCIDNA2015Jun19.docx](#)
[DoranDriveThruCIDNA2015Jun19.pdf](#)
[DoranDriveThruCIDNA2015Jun19.odt](#)

All,

Attached is a revised resolution concerning the Doran Calhoun Village Resolution. The new draft reflects the following:

- The site plan is still not complete. We are waiting for final changes and landscaping plans, but this new resolution reflect significant changes to the proposal.
- The major change is to place the trail on the east side to coordinate with a possible future trail connection south of Lake Street on the east side because the west side would conflict with Whole Foods.
- Elimination of the blind corner north of Punch by moving the Drive-Thru entry further north.
- Setback of parking adjacent to Lake, making room for much needed landscaping in front of the shopping center.
- Eliminate of the lighting and wayfinding resolutions per our discussions.
- Three way stop south of the narrows. Public works fears stacking from the south with a four-way stop.
- Preference for a one-lane over a two-lane drive-thru.
- The tone of the resolution is now less oppositional because the improvements are voluntary and much improved. The new connectivity would be very valuable for the neighborhood.
- Support of the entire Connector Trail in the City's long-term plans whether or not SW LRT is built. This does not commit to city at any particular time to completing the trail south of Lake Street.

Please give me your comments by Monday morning. We must get this revised resolution to CIDNA Board members by email very soon in preparation for the public hearing on June 27.

Thanks.

--

Bob Corrick
Chair
CIDNA Land Use and Development Committee
612.927.5599

**CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION (“CIDNA”)
RESOLUTION OPPOSING A DRUG STORE DRIVE THRU, AND
COMMENDING DORAN COMPANIES FOR PROPOSING MITIGATING AMENITIES
INCLUDING NEW LANDSCAPING AND
A DEDICATED BICYCLE-PEDESTRIAN CONNECTOR TRAIL
JUNE 19, 2016**

WHEREAS the Calhoun Village Shopping Center (“Calhoun Village”) is under contract to be sold by Pfaff Calhoun LLC (“Pfaff”) to Doran Companies (“Doran”).

WHEREAS Doran, through Pfaff, has filed a Master Land Use Application for site plan review for an automobile drive-thru facility on the northeast side of Calhoun Village (the “Drive-Thru”, see attached Doran Application). It appears that a Drug Store would use the Drive-Thru and operate from space currently occupied by Barnes and Noble.

WHEREAS the Cedar-Isles-Dean Neighborhood and surrounding area already has serious traffic problems, creating unsafe and unattractive conditions, and as a result, there was a tragic pedestrian fatality resulting from a delivery truck emerging from Calhoun Village at the corner of Lake Street and Market Plaza on February 14, 2014.

WHEREAS the Drive-Thru would create additional automobile traffic into Calhoun Village and through the narrow road between the two buildings (the “Narrows”) and into the northern parking lot of the shopping center adjacent to the Midtown Greenway.

WHEREAS the bicycle-pedestrian connection through Calhoun Village provides important benefits to the neighborhood and Calhoun Village by encouraging walking, cycling, and other non-vehicular movement through this busy area.

WHEREAS delivery trucks also pass through the Narrows, into the northern parking lot, to loading docks in the basement of the building.

WHEREAS improved traffic design through Calhoun Village (including for automobiles, bicycles and pedestrians) represent an essential response to increased automobile traffic from the Drive-Thru.

WHEREAS the West Lake Multi-Modal Transportation Study in 2015 proposed a bicycle-pedestrian connection between the Midtown Greenway and Lake Calhoun through Calhoun Village on the east side of Market Plaza. (See attached Multi-Modal Study pp 71-74.)

WHEREAS, in addition to the Drive-Thru, Doran has proposed mitigating amenities that would include new landscaping throughout the shopping center and a dedicated bicycle-pedestrian trail. (See attached Doran Application.)

NOW THEREFORE BE IT RESOLVED THAT

1. Increased automobile traffic should not be encouraged in the Calhoun Village for reasons of safety, health, and active urban lifestyle. As a result, CIDNA opposes a Drive-Thru in Calhoun Village, but acknowledges that it would represent an approved use under the City's existing zoning ordinance. CIDNA also favors a one-lane over a two-lane drive-thru.
2. CIDNA commends Doran for improvements to Calhoun Village that would mitigate the negative effects of the Drive-Thru, which would include the following features and amenities:
 - a. Reduce the number of automobile lanes in the Market Plaza entry drive from 4 to 3.
 - b. A two-way bicycle trail on the east side of Market Plaza entry drive, raised to sidewalk level, separated from automobiles and pedestrians, connecting the Midtown Greenway with Lake St.
 - c. Location of the Drive-Thru entry on the north side of the north parking lot in order to eliminate the blind corner and improve visibility of cyclists and pedestrians by passing vehicles.
 - d. Clearly marked and raised pedestrian crosswalks throughout.
 - e. Parking setback against Lake Street, together with over-story and under-story landscape improvements to the front of the shopping center.
3. If the Drive-Thru should be approved and built, CIDNA requests that Doran commit to all proposed improvements, and keep the Connector Trail open unless the Hennepin County Regional Railroad Authority ("HCRRA") withdraws formal or informal permissions to cross its property.
4. CIDNA recommends an east-west pedestrian trail across the very northern edge of the Calhoun Village property, directly south of the HCRRA property line, and north of Calhoun Village's existing curbs and buildings, eventually connecting Chowen Avenue with Dean Parkway. The Trammell Crow apartments and Loop Calhoun Condominiums already have dedicated land use for this purpose. (See attached Trammel Crow concept drawing of the east-west trail on its property.) CIDNA and Doran would work with the HCRRA to permit part of the trail on rail property where necessary and appropriate.

5. For bicycle and pedestrian safety and connectivity, CIDNA recommends adoption of the Calhoun Village-Lake Calhoun Connector trail in the City's long-term trail plans whether or not Southwest Light Rail is built.

CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION

Craig Westgate
CIDNA President

Robert Corrick
Chair, CIDNA Land Use and Development Committee

Rosanne Halloran
CIDNA Secretary

**CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION (“CIDNA”)
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CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION

Craig Westgate
CIDNA President

Robert Corrick
Chair, CIDNA Land Use and Development Committee

Rosanne Halloran
CIDNA Secretary

From: [Robert Corrick](#)
To: [Craig Westgate](#); [Jay Isenberg](#); [Joan Soranno](#); [John Wessinger](#); [Karen Stublaski](#); [Michael Wilson](#); [Richard Goodermont](#); [Robert Corrick](#); [Stephen Goltry](#); [Goodman, Lisa R.](#); [Sadler, Patrick A.](#); [Sether, Shanna M](#); [Richard Logan](#)
Subject: Doran Revised CIDNA Resolution: Additional Comments
Date: Sunday, June 19, 2016 11:50:53 AM

All,

I forgot to mention two important changes to the revised resolution:

- 1) We are requesting that Doran commit to making all proposed improvements if the Drive-Thru were to be approved and built, and that Doran would keep the connector trail open as long as the HCRRA lets it be.
- 2) The east west trail is moved north of existing Calhoun Village curbing and buildings, requiring placement of the future trail partly and substantailly on HCRRA property, which would be a collaborative effort between HCRRA, Doran, CIDNA, and the City.

Please contact me asap with question and comments.

Thanks.

--

Bob Corrick
Chair
CIDNA Land Use
612.927.5599

From: [Robert Corrick](#)
To: [Craig Westgate](#); [Jay Isenberg](#); [Joan Soranno](#); [John Wessinger](#); [Karen Stublaski](#); [Michael Wilson](#); [Richard Goodermont](#); [Robert Corrick](#); [Stephen Goltry](#); [Goodman, Lisa R.](#); [Sadler, Patrick A.](#); [Richard Logan](#); [Sether, Shanna M](#)
Subject: Clarification of Revised CIDNA Calhoun Village Doran Resolution
Date: Sunday, June 19, 2016 9:20:24 PM
Attachments: [Doran Site Plan Revised.pdf](#)

All,

Jay Isenberg had excellent comments on the revised resolution, so I am responding to questions that he has raised.

1) Has Doran agreed to the proposed changes including relocation of the bike trail to the east side and relocation of the drive-thru entry/exit?

Yes, Doran seems to have agreed to the revised design, although nothing is done until it is done. I failed to attach a copy of the revised design, which has been provided by Doran with input from various City departments. A copy is attached. Remember that this is not final and subject to revision, but I felt that it was necessary show this to you and to get your approval and the CIDNA Board asap in order to be ready for a June 27 Planning Commision meeting. I will keep you apprised with new versions of the design as they are provided by Doran and the City.

2) How will the revision affect current and future outdoor seating along the west side of Punch?

My understanding is that the outdoor seating will remain, although Tony mentioned eliminating this over our objections in our meeting with him. The revised resolution does not make our position clear, so I propose adding the following Resolved:

"The outdoor seating on the sidewalk west of Punch Pizza should remain in use."

3) How will Doran accomplish elimination of the blind corner?

Please see the attached revised design for an answer to this question. A picture is worth a thousand words. The entry/exit to the drive-thru is moved very much further north of the blind corner to create substantially improved visibility.

4) A 3-way stop south of the Narrows should be sufficient to address bike-ped congestion/confusion. A 4-way stop potentially would create stacking from the south (from Lake St.)

I agree with Jay on this point. A 3-way stop south of the Narrows seems to be the best solution, as proposed in the revised design.

Please provide your comments by Monday morning.

Thanks.

--

Bob Corrick
Chair
CIDNA Land Use and Development Committee

From: [Robert Corrick](#)
To: [Craig Westgate](#); [Jay Isenberg](#); [Joan Soranno](#); [John Wessinger](#); [Karen Stublaski](#); [Michael Wilson](#); [Richard Goodermont](#); [Robert Corrick](#); [Stephen Goltry](#); [Goodman, Lisa R.](#); [Sadler, Patrick A.](#); [Sether, Shanna M](#); [Richard Logan](#)
Subject: Doran Calhoun Village CIDNA Committee Tuesday Update
Date: Tuesday, June 21, 2016 7:12:04 AM

All,

Status of Revised Resolution

We will not receive revised designs from Doran until perhaps tonight. This means that the Planning Commission package probably will be sent out today without fresh perspective from CIDNA, although the package probably will include our existing Board resolution. We could have submitted a revised resolution from the Committee this morning without benefit of the final designs, but the short time permitted to vet the revisions and secure timely Committee votes was too challenging.

Design Update

It is easy to make mistakes when we are not participating in some meetings, and we do not have very much time or final designs, but my best take on design update is as follows:

- The trail on the east side will be widened to 15 feet from the Drive-Thru entry to Lake St. This will be a raised concrete sidewalk.
- The Connector Trail south of Lake St. may be included in long-term City plans even if funding is not obtained from SW LRT. I am not sure of this, but that seems to be the direction. This would not commit the City to funding the trail at any particular time.
- Until the Connector Trail is completed south of Lake St., the trail through Calhoun Village will be an unmarked wide sidewalk. In the interim, Public Works does not want any way finding from the Midtown Greenway to Lake St. because it might increase bike-ped traffic to busy Lake St. with nowhere to go. (Those of us who walk and bike to Calhoun Commons from the north and east probably will continue to use the Midtown Greenway to Chowen.)
- At some point, not sure when, the tables and chairs on the narrow sidewalk next to Punch Pizza will not be allowed because there is no room for a two-way bike lane and pedestrian sidewalk.
- There will not be any stop signs at the entry/exit to/from the Drive-Thru. Instead the crosswalk will be raised. Considering the sharp turn, Public Works apparently believes that the raised trail is better for traffic calming and safety than stop signs.
- The sharp left turn out of the Drive-Thru at the crosswalk meets turning radius requirements in this setting. Public Works supports the existing design with a sharp turn.
- We are losing the fight to plan for an east-west trail. Planning does not really support it, and Doran's lenders apparently would not want any easements for this purpose on the property. Our only recourse now is to talk with HCRRA about such a trail on their property north of Calhoun Village. This is still possible.
- We have not seen any new landscaping plans yet. This may be available tonight, not sure.
- We are told that the site plans cannot be changed without review by the city. This gives us a level of protection to keep the trail open. I must check with

Shanna to ensure that Doran would be obligated to follow through with striping and trail marking once the entire Connector trail is complete.

The Process

Shanna in planning has done a good job keeping us informed. In addition, Doran has been relatively open after our initial difficulties. The process, nevertheless, seems unfair, and does not seem to be transparent enough for such an important issue in our neighborhood. This relates to existing zoning code and other legal issues like property rights of Doran. We might have formally requested a delay to give us more time and input into the process, but I was reluctant to do this because we were impressed with how much Doran was willing to do on connectivity and safety on his private property, all paid by Doran. A delay could have changed this dynamic. I am still not sure why the schedule was so quick, but my intuition tells me that it has something to do with getting a drive-thru approved before any laws changed, and also the closing with Pfaff.

Next Steps

Although we still oppose the Drive-Thru, and 2 lanes, the improved connectivity, safety and landscaping benefit the neighborhood. Nevertheless, there are a few issues outstanding:

- Stop signs at the Drive-Thru entrance/exit
- Sharp turn out of the Drive-Thru exit
- Elimination of table at Punch Pizza
- Support of entire Connector Trail in long-term City plans

The Committee may have more issues.

I think that we will get the last one (the Connector Trail), but the first three probably will go against staff recommendations. If we want to take a position on these, we should pass a resolution in Committee and try to get Board approval by Friday in time for the hearing on Monday.

I admit to being worn out on this project. Since we have accomplished a lot, I don't feel strongly about these issues (other than support for the entire Connector Trail), so let me know what you would like to do as a Committee. We will need to move fast and vote quickly. If we want to do any more work, I would appreciate any help that Committee member could provide.

Public Hearing

We probably should have CIDNA representation at the hearing. Mike had mentioned that he might testify against the Drive-Thru. If anyone else plans to attend, please let me know. We should receive the Planning Commission package soon. We will send out an email blast to the neighborhood after we receive the agenda and time for this morning.

Let me know what you think.

Bob

--

Bob Corrick
612.927.5599

From: [Joan Soranno](#)
To: [Robert Corrick](#); [Craig Westgate](#); [Jay Isenberg](#); [John Wessinger](#); [Karen Stublaski](#); [Michael Wilson](#); [Richard Goodermont](#); [Stephen Goltry](#); [Goodman, Lisa R.](#); [Sadler, Patrick A.](#); [Sether, Shanna M](#); [Richard Logan](#)
Subject: Re: Doran Calhoun Village CIDNA Committee Tuesday Update
Date: Tuesday, June 21, 2016 8:05:13 AM

Bob,

First, thank you for all your time and energy spent on this.....it's been a lot of work in a short amount of time and I really appreciate it. I support your position on everything. As a neighborhood, besides our collective voice, we don't have a lot of leverage in terms of zoning and building codes. I agree with you that they are trying to push this through quickly because they are afraid the City will revise zoning with regards to drive-thrus. And I think what's most important is Doran is maintaining the connection to the Greenway.

The sharp turn at the drive-thru exit looks dangerous and ultimately if it proves to be a problem, they could always add a stop sign at the drive-thru exit afterwards. I'm going to ask my brother about the seating alongside Punch to get his reaction.

Joan

Joan M. Soranno, FAIA
Design Principal

HGA Architects and Engineers

420 5th Street North, Suite 100
Minneapolis, MN 55401
Direct 612.758.4553

From: Robert Corrick <robertcorrick@mentorplanet.com>
Date: Tuesday, June 21, 2016 7:11 AM
To: Craig Westgate <cwreg@msn.com>, Jay Isenberg <jhi@isenbergassociates.com>, "Joan M. Soranno" <jsoranno@hga.com>, John Wessinger <john.wessinger@comcast.net>, Karen Stublaski <Karen.stublaski@hbfuller.com>, Michael Wilson <michaelwilsonmpls@gmail.com>, Richard Goodermont <rtgoodermont@gmail.com>, Robert Corrick <robertcorrick@mentorplanet.com>, Stephen Goltry <swg@kre8eve.me>, "Goodman, Lisa R." <Lisa.Goodman@minneapolismn.gov>, "Sadler, Patrick A." <Patrick.Sadler@minneapolismn.gov>, "Sether, Shanna M" <Shanna.Sether@minneapolismn.gov>, Richard Logan <docsafari@hotmail.com>
Subject: Doran Calhoun Village CIDNA Committee Tuesday Update

All,

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Let me know what you think.

Bob

--

Bob Corrick
612.927.5599

From: [Stephen Goltry](#)
To: [Robert Corrick](#)
Cc: [Craig Westgate](#); [Jay Isenberg](#); [Joan Soranno](#); [John Wessinger](#); [Karen Stublaski](#); [Michael Wilson](#); [Richard Goodermont](#); [Goodman, Lisa R.](#); [Sadler, Patrick A.](#); [Sether, Shanna M](#); [Richard Logan](#)
Subject: Re: Doran Calhoun Village CIDNA Committee Tuesday Update
Date: Tuesday, June 21, 2016 11:36:20 AM

Bob,
Thanks for this latest update.
The bike/ped/ safety "linkage-connection" is a needed priority.
Yet logical /safe delivery & waste truck ingress/egress to the site.

Will see how my schedule appears for 06.27 hearing . Time? City Hall room to be certain?

You've done an a lot work & "Thanks! "

: : : Steve

Sent from my iPad

On Jun 21, 2016, at 7:11 AM, Robert Corrick <robertcorrick@mentorplanet.com> wrote:

All,

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Let me know what you think.

Bob

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Bob Corrick
612.927.5599

From: [Michael Wilson](#)
To: [Stephen Goltry](#)
Cc: [Robert Corrick](#); [Craig Westgate](#); [Jay Isenberg](#); [Joan Soranno](#); [John Wessinger](#); [Karen Stublaski](#); [Richard Goodermont](#); [Goodman, Lisa R.](#); [Sadler, Patrick A.](#); [Sether, Shanna M](#); [Richard Logan](#)
Subject: Re: Doran Calhoun Village CIDNA Committee Tuesday Update
Date: Tuesday, June 21, 2016 3:52:42 PM

To paraphrase the Serenity Prayer, we need the patience to accept the things we cannot change right now, the firmness to keep advocating for those things we see as most important, and the pragmatism to recognize the difference.

We apparently cannot change the direction of the drive-thru traffic. If the city won't go for stop signs, that's their call. If the city and Doran want to change their position after the first half-dozen collisions between northbound drivers through the Narrows ramming into cars edging forward from the drive-thru lane before turning south back towards Lake Street, then that's their business.

Let Doran deal with tenants whose rear delivery doors -- which Doran says they seldom use -- will become unusable. Let Doran require businesses to take deliveries through the front -- diesel-engine trucks with motors left running, blocking traffic and parking places, adding to the visual pollution.

Our top priority is safe, inviting, logical bike/ped connection between the Greenway and Lake Street, and ultimately from Lake Street over to Lake Calhoun.

Our second priority is to press for and encourage a greening of this most desolate of strip malls. A few years ago when we meeting on the short-lived "Shops at the Edge" business association plan, I referred to Calhoun Village as Moonscape Village - the ugliest strip mall between Uptown and Brooklyn Park. Unbeknownst to me, one of the Pfaffs was present and was not overly pleased. My words were perhaps a bit intemperate but the description was not far off the mark.

We're stuck with a strip mall, at least until it gets redeveloped. But we want to press and encourage Doran to make it much more than an urban transplant of a dreary strip mall set in a depressed, post-war inner-ring suburb.

We apparently won't succeed in opposing the drive-thru, but I would think it's in CIDNA's best interest, and in the best interest of other similarly situated neighborhoods, to continue to officially oppose the drive-thru. By doing so, I would think we add weight to the call for the City Council to tighten restrictions on future drive-thrus.

I add my vote of thanks to everyone else's, Bob, for your labors on this vexing issue.

Mike

On Tue, Jun 21, 2016 at 11:35 AM, Stephen Goltry <swg@kre8eve.me> wrote:

Bob,

Thanks for this latest update.

The bike/ped/ safety "linkage-connection" is a needed priority.

Yet logical /safe delivery & waste truck ingress/egress to the site.

Will see how my schedule appears for 06.27 hearing . Time? City Hall room to be

certain?

You've done an a lot work & "Thanks! "

: : : Steve

Sent from my iPad

On Jun 21, 2016, at 7:11 AM, Robert Corrick <robertcorrick@mentorplanet.com> wrote:

All,

Status of Revised Resolution

We will not receive revised designs from Doran until perhaps tonight. This means that the Planning Commission package probably will be sent out today without fresh perspective from CIDNA, although the package probably will include our existing Board resolution. We could have submitted a revised resolution from the Committee this morning without benefit of the final designs, but the short time permitted to vet the revisions and secure timely Committee votes was too challenging.

Design Update

It is easy to make mistakes when we are not participating in some meetings, and we do not have very much time or final designs, but my best take on design update is as follows:

- The trail on the east side will be widened to 15 feet from the Drive-Thru entry to Lake St. This will be a raised concrete sidewalk.
- The Connector Trail south of Lake St. may be included in long-term City plans even if funding is not obtained from SW LRT. I am not sure of this, but that seems to be the direction. This would not commit the City to funding the trail at any particular time.
- Until the Connector Trail is completed south of Lake St., the trail through Calhoun Village will be an unmarked wide sidewalk. In the interim, Public Works does not want any way finding from the Midtown Greenway to Lake St. because it might increase bike-ped traffic to busy Lake St. with nowhere to go. (Those of us who walk and bike to Calhoun Commons from the north and east probably will continue to use the Midtown Greenway to Chowen.)
- At some point, not sure when, the tables and chairs on the narrow sidewalk next to Punch Pizza will not be allowed because there is no room for a two-way bike lane and pedestrian sidewalk.
- There will not be any stop signs at the entry/exit to/from the Drive-Thru. Instead the crosswalk will be raised. Considering the sharp turn, Public Works apparently believes that the raised trail is better for traffic calming and safety than stop signs.
- The sharp left turn out of the Drive-Thru at the crosswalk meets

turning radius requirements in this setting. Public Works supports the existing design with a sharp turn.

- We are losing the fight to plan for an east-west trail. Planning does not really support it, and Doran's lenders apparently would not want any easements for this purpose on the property. Our only recourse now is to talk with HCRRA about such a trail on their property north of Calhoun Village. This is still possible.
- We have not seen any new landscaping plans yet. This may be available tonight, not sure.
- We are told that the site plans cannot be changed without review by the city. This gives us a level of protection to keep the trail open. I must check with Shanna to ensure that Doran would be obligated to follow through with striping and trail marking once the entire Connector trail is complete.

The Process

Shanna in planning has done a good job keeping us informed. In addition, Doran has been relatively open after our initial difficulties. The process, nevertheless, seems unfair, and does not seem to be transparent enough for such an important issue in our neighborhood. This relates to existing zoning code and other legal issues like property rights of Doran. We might have formally requested a delay to give us more time and input into the process, but I was reluctant to do this because we were impressed with how much Doran was willing to do on connectivity and safety on his private property, all paid by Doran. A delay could have changed this dynamic. I am still not sure why the schedule was so quick, but my intuition tells me that it has something to do with getting a drive-thru approved before any laws changed, and also the closing with Pfaff.

Next Steps

Although we still oppose the Drive-Thru, and 2 lanes, the improved connectivity, safety and landscaping benefit the neighborhood. Nevertheless, there are a few issues outstanding:

- Stop signs at the Drive-Thru entrance/exit
- Sharp turn out of the Drive-Thru exit
- Elimination of table at Punch Pizza
- Support of entire Connector Trail in long-term City plans

The Committee may have more issues.

I think that we will get the last one (the Connector Trail), but the first three probably will go against staff recommendations. If we want to take a position on these, we should pass a resolution in Committee and try to get Board approval by Friday in time for the hearing on Monday.

I admit to being worn out on this project. Since we have accomplished a lot, I don't feel strongly about these issues (other than support for the entire Connector Trail), so let me know what you would like to do as a Committee. We will need to move fast and vote quickly. If we want to do any more work, I would appreciate any help that Committee member could provide.

Public Hearing

We probably should have CIDNA representation at the hearing. Mike had mentioned that he might testify against the Drive-Thru. If anyone else plans to attend, please let me know. We should receive the Planning Commission package soon. We will send out an email blast to the neighborhood after we receive the agenda and time for this morning.

Let me know what you think.

Bob

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Bob Corrick

[612.927.5599](tel:612.927.5599)

**CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION (“CIDNA”)
RESOLUTION OPPOSING A DRUG STORE DRIVE THRU, AND
COMMENDING DORAN COMPANIES FOR PROPOSING MITIGATING AMENITIES
INCLUDING NEW LANDSCAPING AND
A DEDICATED BICYCLE-PEDESTRIAN CONNECTOR TRAIL
JUNE 23, 2016**

WHEREAS the Calhoun Village Shopping Center (“Calhoun Village”) is under contract to be sold by Pfaff Calhoun LLC (“Pfaff”) to Doran Companies (“Doran”).

WHEREAS Doran, through Pfaff, has filed a Master Land Use Application for site plan review for a two-lane automobile drive-thru facility on the northeast side of Calhoun Village (the “Drive-Thru”, see attached Doran Application). It appears that a Drug Store would use the Drive-Thru and operate from space currently occupied by Barnes & Noble.

WHEREAS the Cedar-Isles-Dean Neighborhood and surrounding area already has serious traffic problems, creating unsafe and unattractive conditions, and as a result, there was a tragic pedestrian fatality resulting from a delivery truck emerging from Calhoun Village at the corner of Lake Street and Market Plaza on February 14, 2014.

WHEREAS the Drive-Thru would create additional automobile traffic into Calhoun Village and through the narrow road between the two buildings (the “Narrows”) and into the northern parking lot of the shopping center adjacent to the Midtown Greenway.

WHEREAS the bicycle-pedestrian connection through Calhoun Village provides important benefits to the neighborhood and Calhoun Village by encouraging walking, cycling, and other non-vehicular movement through this busy area, thus aligning with and promoting the City's recently-adopted "complete streets" policy, which gives added priority to walking, bicycling and transit over single-occupancy vehicles.

WHEREAS delivery trucks also pass through the Narrows, into the northern parking lot, to loading docks in the basement of the building.

WHEREAS improved traffic design through Calhoun Village (including for automobiles, bicycles and pedestrians) represents an essential response to increased automobile traffic from the Drive-Thru.

WHEREAS the West Lake Multi-Modal Transportation Study in 2015 proposed a bicycle-pedestrian connection between the Midtown Greenway and Lake Calhoun through Calhoun Village on the east side of Market Plaza. (See attached Multi-Modal Study pp 71-74.)

WHEREAS, in addition to the Drive-Thru, Doran has proposed mitigating amenities that would include new landscaping throughout the shopping center and a dedicated bicycle-pedestrian trail. (See attached Doran Application.)

NOW THEREFORE BE IT RESOLVED THAT

1. Increased automobile traffic should not be encouraged in the Calhoun Village for reasons of safety, health, and active urban lifestyle. As a result, CIDNA opposes a Drive-Thru in Calhoun Village, but acknowledges that it would represent an approved use under the City's existing zoning ordinance. CIDNA also supports a one-lane rather than a two-lane Drive-Thru.
2. CIDNA commends Doran for improvements to Calhoun Village, which would mitigate the negative effects of the Drive-Thru, and which would include the following features and amenities:
 - a. Three automobile lanes on the Market Plaza entry drive (rather than the current four).
 - b. A two-way raised bicycle and pedestrian sidewalk on the east side of the Market Plaza entry drive and through the Narrows, connecting the Midtown Greenway with Lake St. (The sidewalk would not have trail markings until the entire Connector Trail south of Lake Street would be completed at some time in the future to be determined.)
 - c. Location of the Drive-Thru entry on the north side of the north parking lot in order to eliminate the blind corner and improve visibility of cyclists and pedestrians to passing vehicles. The crosswalk would be raised to calm traffic.
 - d. Clearly marked pedestrian crosswalks at the Drive-Thru entry/exit, the intersection south of the Narrows (four-way crosswalk), and at Lake Street.
 - e. Parking setback against Lake Street, together with over-story and under-story landscape improvements.
 - f. Other landscape improvements throughout the parking lots.
 - g. Location of new outdoor dining tables north of Punch Pizza and in several locations in the south parking lot facing Lake Street.
3. If the Drive-Thru were to be approved, CIDNA recommends the following conditions:

- a. Doran should not be permitted to change the site plan, including the bicycle-pedestrian trail from the Midtown Greenway, without complete review by the City.
- b. The “existing outdoor dining” area on the sidewalk west of Punch Pizza as noted in the Site Plan should remain in use.
- c. For the sake of pedestrian and bicycle safety and connectivity, the Calhoun Village-Lake Calhoun Connector Trail proposed in the Multi-Modal Study should be adopted in the City’s long-term trail plans whether or not the Southwest Light Rail project is built.

CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION

Craig Westgate
CIDNA President

Robert Corrick
Chair, CIDNA Land Use and Development Committee

Rosanne Halloran
CIDNA Secretary

OWNER: Tc/P 3118 W Lake Llc

CITY LANDSCAPE CODE

ONE TREE SHALL BE PROVIDED PER 500 SQ. FT. OF GREEN SPACE
38,474 / 500 = 76.95

ONE SHRUB SHALL BE PROVIDED PER 100 SQ. FT. OF GREEN SPACE
38,474 / 100 = 384.74

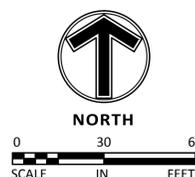
PLANT TYPE	REQUIRED	PROVIDED
EXISTING TREES	N/A	36
REMOVED EXISTING TREES	N/A	6
PROPOSED TREES	41	57
TOTAL SITE TREES	77	93
PROPOSED SHRUB	385	385

MINIMUM TREE SIZE SHALL BE 2.5"
ORDINANCE 530.220 (2) ALLOWS FOR A SUBSTITUTION OR REDUCTION OF LANDSCAPE STANDARDS PROVIDED ONE OR MORE OF THE FOLLOWING EXIST:

- THE PROPOSAL WILL ALLOW A SITE PLAN OF EXCEPTIONAL DESIGN THAT INCLUDES AMENITIES SUCH AS PUBLIC SEATING, AN OUTDOOR PLAZA OR TRANSIT SHELTER THAT WILL ENHANCE THE AREA OR THAT IS MORE CONSISTENT WITH THE DESIGN OF THE SITE OR THE SURROUNDING AREA.
- THE PROPOSAL WILL ALLOW A SITE PLAN THAT IS MORE CONSISTENT WITH THE CHARACTER OF THE AREA.
- EXISTING PLANT MATERIALS, WALLS, FENCES OR THE TOPOGRAPHY OF THE SITE AND ITS SURROUNDINGS MAKE THE REQUIRED LANDSCAPING OR SCREENING LESS NECESSARY.
- THE REQUIRED LANDSCAPING OR SCREENING WILL HINDER TRUCK ACCESS AND SERVICE NECESSARY TO THE OPERATION OF THE USE.
- THE REQUIRED LANDSCAPING AND SCREENING MAY OBSTRUCT VIEWS OF TRAFFIC OR REDUCE NATURAL SURVEILLANCE OF THE SITE.

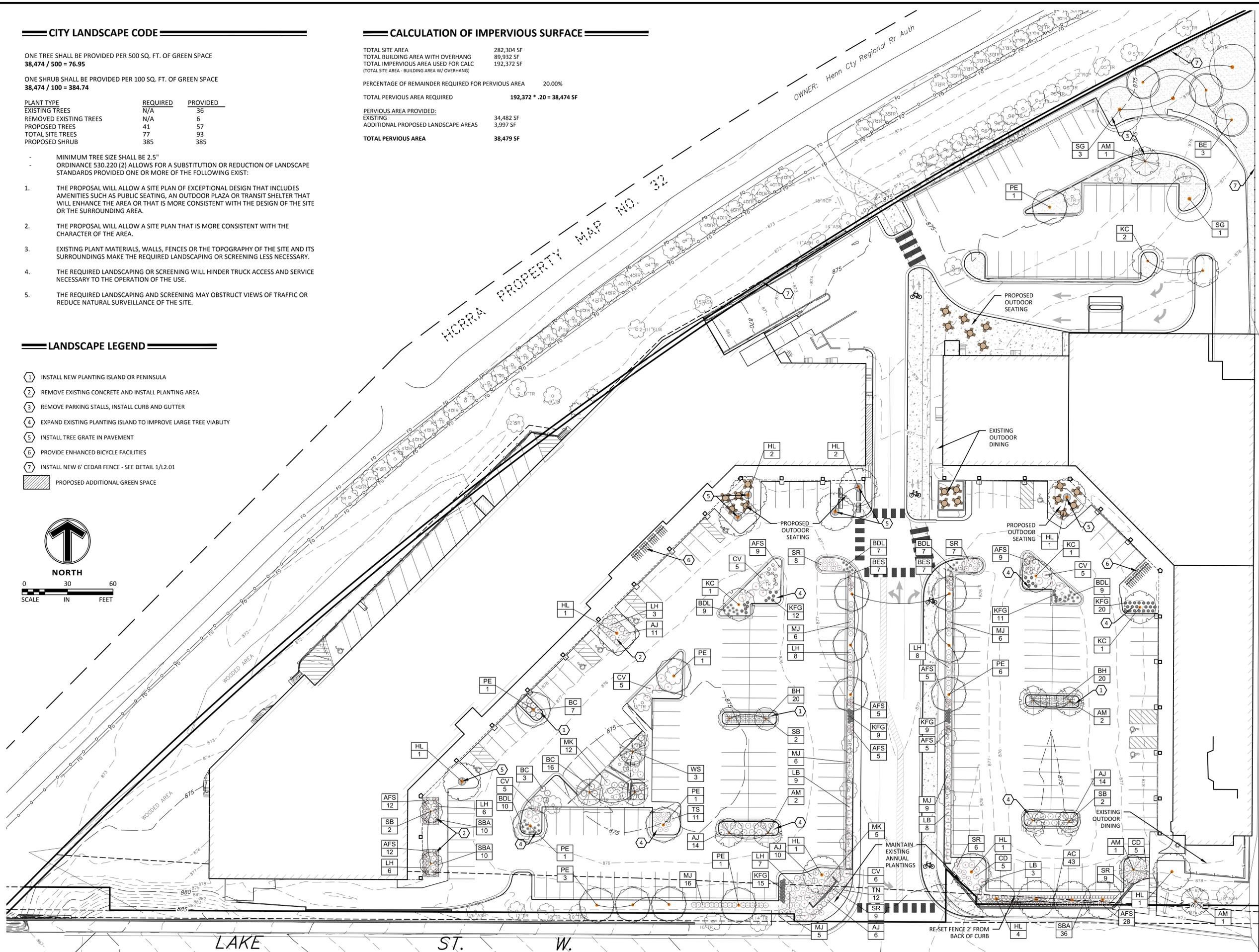
LANDSCAPE LEGEND

- INSTALL NEW PLANTING ISLAND OR PENINSULA
 - REMOVE EXISTING CONCRETE AND INSTALL PLANTING AREA
 - REMOVE PARKING STALLS, INSTALL CURB AND GUTTER
 - EXPAND EXISTING PLANTING ISLAND TO IMPROVE LARGE TREE VIABILITY
 - INSTALL TREE GRATE IN PAVEMENT
 - PROVIDE ENHANCED BICYCLE FACILITIES
 - INSTALL NEW 6' CEDAR FENCE - SEE DETAIL 1/L2.01
- PROPOSED ADDITIONAL GREEN SPACE



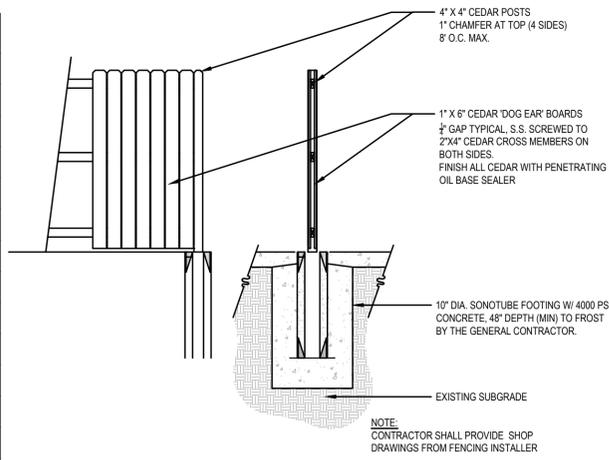
CALCULATION OF IMPERVIOUS SURFACE

TOTAL SITE AREA	282,304 SF
TOTAL BUILDING AREA WITH OVERHANG	89,932 SF
TOTAL IMPERVIOUS AREA USED FOR CALC (TOTAL SITE AREA - BUILDING AREA W/ OVERHANG)	192,372 SF
PERCENTAGE OF REMAINDER REQUIRED FOR PERVIOUS AREA	20.00%
TOTAL PERVIOUS AREA REQUIRED	192,372 * .20 = 38,474 SF
PERVIOUS AREA PROVIDED:	
EXISTING	34,482 SF
ADDITIONAL PROPOSED LANDSCAPE AREAS	3,997 SF
TOTAL PERVIOUS AREA	38,479 SF



PLANT SCHEDULE

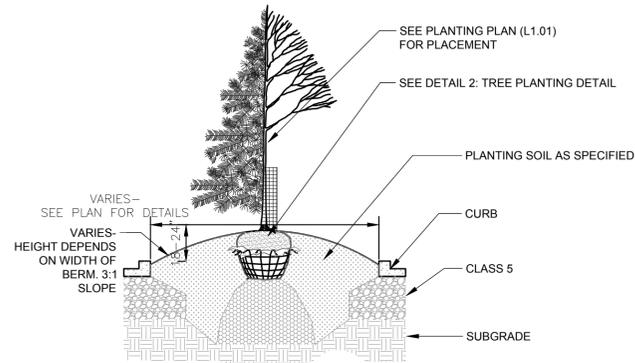
TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
SG	Acer freemanii 'Sienna Glen' / Sienna Glen Maple	B & B	2.5'Cal	4
BE	Aesculus x arnoldiana 'Autumn Splendor' / Autumn Splendor Buckeye	B & B	2.5'Cal	3
WS	Betula papyrifera 'Whitespire' / Whitespire Birch	B & B	2.5'Cal	3
HL	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B & B	2.5'Cal	14
KC	Gymnocladia dioica 'Espresso' / Kentucky Coffeetree	B & B	2.5'Cal	5
PE	Ulmus americana 'Princeton' / American Elm	B & B	2.5'Cal	15
ORN. TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
SB	Amelanchier canadensis 'Autumn Brilliance' / Autumn Brilliance Serviceberry	B & B	2'Cal	6
AM	Maackia amurensis / Amur Maackia	B & B	2'Cal	7
SHRUBS	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
BC	Aronia melanocarpa 'Autumn Magic' / Autumn Magic Black Chokeberry	5 gal		26
CD	Cornus sericea 'Allema's Compact' / Dwarf Red Twig Dogwood	5 gal		10
BH	Diervilla lonicera / Dwarf Bush Honeysuckle	5 gal		40
LH	Hydrangea paniculata 'Jane' / Little Lime Hydrangea	5 gal		38
MJ	Juniperus chinensis 'Mint Julep' / Mint Julep Juniper	5 gal		48
AJ	Juniperus horizontalis 'Compacta' / Andorra Compact Juniper	5 gal		55
TN	Physocarpus opulifolius 'SMPOTW' / Tiny Wine Ninebark	5 gal		12
AC	Ribes alpinum / Alpine Currant	5 gal		43
SR	Rosa rugosa 'Snow Pavement' / Snow Pavement Rose	5 gal		39
TS	Spiraea betulifolia 'Tor' / Birchleaf Spirea	5 gal		11
MK	Syringa patula 'Miss Kim' / Miss Kim Lilac	5 gal		17
LB	Syringa x 'Bloomerang' / Lilac	5 gal		20
CV	Viburnum opulus 'Compactum' / Compact European Cranberrybush	5 gal		26
GRASSES	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
KFG	Calamagrostis x acutiflora 'Karl Foerster' / Feather Reed Grass	1 gal		76
PERENNIALS	BOTANICAL NAME / COMMON NAME	CONT	CAL	QTY
SBA	Allium x 'Summer Beauty' / Summer Beauty Allium	1 gal		56
BDL	Hemerocallis x 'Baja' / Baja Daylily	1 gal		42
BES	Rudbeckia fulgida 'Goldstrum' / Black Eyed Susan	1 gal		14
AFS	Sedum x 'Autumn Fire' / Autumn Fire Sedum	1 gal		90
GROUND COVERS	BOTANICAL NAME / COMMON NAME	QTY	CAL	QTY
	Turf Sod Highland Sod / Sod	8,451 sf		



1 CEDAR FENCE DETAIL

L2.01

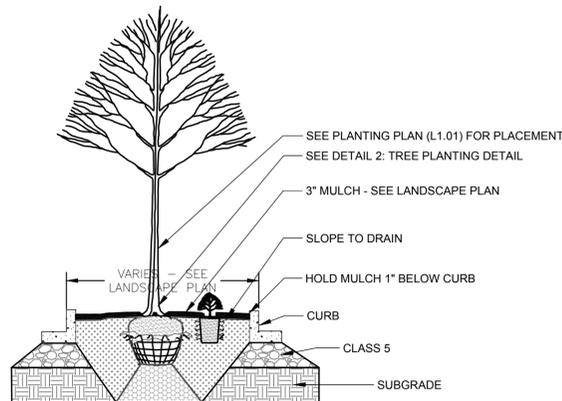
N.T.S.



2 BERMED ISLAND PLANTING DETAIL

L2.01

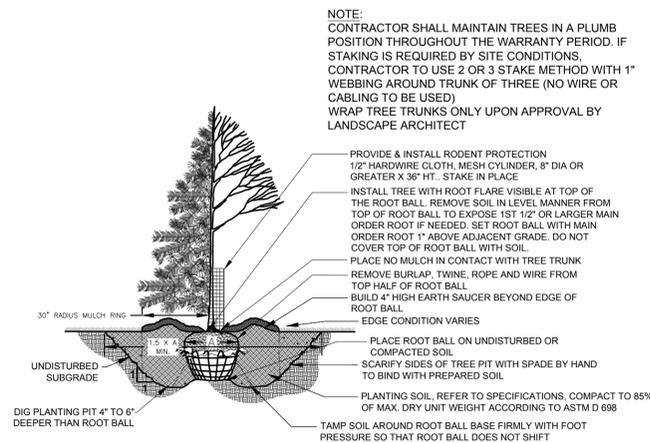
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3 PARKING LOT ISLAND DETAIL

L2.01

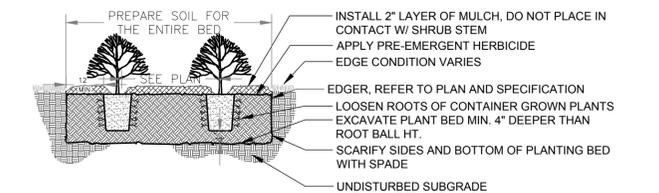
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4 TREE PLANTING DETAIL

L2.01

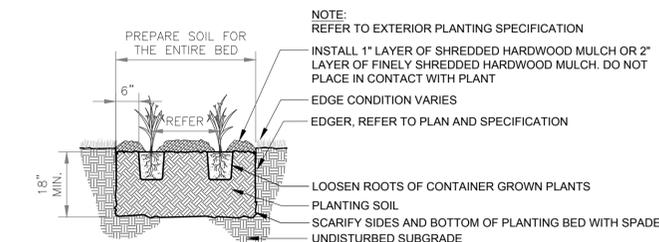
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5 SHRUB PLANTING DETAIL 2

L2.01

NTS



6 PERENNIAL PLANTING DETAIL

L2.01

NTS

NOTES

GENERAL NOTES:

- THE CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH THE EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF THE WORK.
- THE CONTRACTOR SHALL VERIFY PLAN LAYOUT AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN OR INTENT OF THE LAYOUT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
- THE CONTRACTOR SHALL PROTECT EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING CONSTRUCTION. DAMAGE TO SAME SHALL BE REPAIRED AND/OR REPLACED AT NO ADDITIONAL COST TO THE OWNER.
- LOCATE AND VERIFY ALL UTILITIES, INCLUDING IRRIGATION LINES, WITH THE OWNER FOR PROPRIETARY UTILITIES AND GOPHER STATE ONE CALL 48 HOURS BEFORE DIGGING. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ANY DAMAGES TO SAME. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS TO FACILITATE PLANT RELOCATION.
- THE LANDSCAPE CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.
- THE CONTRACTOR SHALL REVIEW THE SITE CONDITIONS IN SITE CONDITIONS WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR WARRANTY. UNDESIRABLE SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK.
- THE PLAN TAKES PRECEDENCE OVER THE LANDSCAPE LEGEND IF DISCREPANCIES EXIST. QUANTITIES SHOWN IN THE PLANTING SCHEDULE ARE FOR THE CONTRACTOR'S CONVENIENCE. CONTRACTOR TO VERIFY QUANTITIES SHOWN ON THE PLAN.
- THE SPECIFICATIONS TAKE PRECEDENCE OVER THE PLANTING NOTES AND GENERAL NOTES.
- EXISTING TREES AND SHRUBS TO REMAIN SHALL BE PROTECTED TO THE DRIP LINE FROM ALL CONSTRUCTION TRAFFIC, STORAGE OF MATERIALS ETC. WITH 4' HT. ORANGE PLASTIC SAFETY FENCING ADEQUATELY SUPPORTED BY STEEL FENCE POSTS 6' O.C. MAXIMUM SPACING.
- LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE ALLOWED.
- CONTRACTOR SHALL REQUEST IN WRITING, A FINAL ACCEPTANCE INSPECTION.

PLANTING NOTES:

- NO PLANTS SHALL BE INSTALLED UNTIL FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- A GRANULAR PRE-EMERGENT HERBICIDE SHALL BE APPLIED TO ALL PLANT BEDS AT THE MANUFACTURERS RECOMMENDED RATE PRIOR TO PLANT INSTALLATION.
- ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-Z60, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERYMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS.
- OVERSTORY TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE PAVED SURFACES.
- ALL PLANTS MUST BE HEALTHY, VIGOROUS MATERIAL, FREE OF PESTS AND DISEASE AND BE CONTAINER GROWN OR BALLED AND BURLAPPED AS INDICATED IN THE LANDSCAPE LEGEND.
- PLANT MATERIALS TO BE INSTALLED PER PLANTING DETAILS.
- ALL TREES MUST BE STRAIGHT TRUNKED AND FULL HEADED AND MEET ALL REQUIREMENTS SPECIFIED.
- THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION.
- NO SUBSTITUTIONS OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
- ALL PLANT MATERIAL QUANTITIES, SHAPES OF BEDS AND LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE COVERAGE OF ALL PLANTING BEDS AT SPACING SHOWN AND ADJUSTED TO CONFORM TO THE EXACT CONDITIONS OF THE SITE. THE LANDSCAPE ARCHITECT SHALL APPROVE THE STAKING LOCATION OF ALL PLANT MATERIALS PRIOR TO INSTALLATION.
- ALL PLANTING AREAS MUST BE COMPLETELY MULCHED AS SPECIFIED.
- MULCH: SHREDDED HARDWOOD MULCH, CLEAN AND FREE OF NOXIOUS WEEDS OR OTHER DELETERIOUS MATERIAL, IN ALL MASS PLANTING BEDS AND FOR TREES, UNLESS INDICATED AS ROCK MULCH ON DRAWINGS. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO DELIVERY ON-SITE FOR APPROVAL. DELIVER MULCH ON DAY OF INSTALLATION. USE 4" FOR SHRUB BEDS, AND 3" FOR PERENNIAL/GROUND COVER BEDS, UNLESS OTHERWISE DIRECTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MULCHES AND PLANTING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN.
- USE ANTI-DESICCANT (WILTPRUF OR APPROVED EQUAL) ON DECIDUOUS PLANTS MOVED IN LEAF AND FOR EVERGREENS MOVED ANYTIME. APPLY AS PER MANUFACTURER'S INSTRUCTION. ALL EVERGREENS SHALL BE SPRAYED IN THE LATE FALL FOR WINTER PROTECTION DURING WARRANTY PERIOD.
- PLANTING SOIL FOR TREES, SHRUBS AND GROUND COVERS: FERTILE FRIABLE LOAM CONTAINING A LIBERAL AMOUNT (4% MIN.) OF HUMUS AND CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT SHALL COMPLY WITH MNDOT SPECIFICATION 3877 TYPE B SELECT TOPSOIL. MIXTURE SHALL BE FREE FROM HARDPACK SUBSOIL, STONES, CHEMICALS, NOXIOUS WEEDS, ETC. SOIL MIXTURE SHALL HAVE A PH BETWEEN 6.1 AND 7.5

- AND 10-0-10 FERTILIZER AT THE RATE OF 3 POUNDS PER CUBIC YARD. IN PLANTING BEDS INCORPORATE THIS MIXTURE THROUGHOUT THE ENTIRE BED IN A 6" LAYER AND ROTO-TILLING IT INTO THE TOP 12" OF SOIL AT A 1:1 RATIO ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HELED IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE REJECTED.
- ALL PLANTS SHALL BE GUARANTEED FOR TWO COMPLETE GROWING SEASONS (APRIL 1 - NOVEMBER 31), UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND PLANTS.
 - CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 3 DAYS PRIOR TO PLANNED DELIVERY. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 24 HOURS IN ADVANCE OF BEGINNING PLANT INSTALLATION.
 - SEASONS/TIME OF PLANTING AND SEEDING: NOTE: THE CONTRACTOR MAY ELECT TO PLANT IN OFF-SEASONS ENTIRELY AT HIS/HER RISK. DECIDUOUS
 - 17.1. POTTED PLANTS: 4/1 - 6/1; 9/21 - 11/1
 - 17.2. DECIDUOUS B&B: 4/1 - 6/1; 9/21 - 11/1
 - 17.3. EVERGREEN POTTED PLANTS: 4/1 - 6/1; 9/21 - 11/1
 - 17.4. EVERGREEN B&B: 4/1 - 5/1; 9/21 - 11/1
 - 17.5. TURF/LAWN SEEDING: 4/1 - 6/1; 7/20 - 9/20
 - MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE, INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE WATERING, CULTIVATING, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
 - ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE AND MEETING ALL LANDSCAPE LEGEND SPECIFICATIONS.
 - WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK. IN EXTREMELY HOT, DRY WEATHER, WATER MORE OFTEN AS REQUIRED BY INDICATIONS OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.

TURF NOTES:

- TURF ESTABLISHMENT SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE PROVISIONS OF THE MNDOT 2105 AND 2575 EXCEPT AS MODIFIED BELOW:
- ALL AREAS TO RECEIVE SOD SHALL ALSO RECEIVE 4" OF TOPSOIL PRIOR TO INSTALLING SOD. TOPSOIL SHALL BE FREE OF TREE ROOTS, STUMPS, BUILDING MATERIAL, AND TRASH, AND SHALL BE FREE OF STONES LARGER THAN 1 1/2" IN ANY DIMENSION.
 - WHERE SOD ABUTS PAVED SURFACES, FINISHED GRADE OF SOD/SEED SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC.
 - SOD SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES, SOD SHALL BE STAKED SECURELY.
 - ALL DISTURBED AREAS TO BE TURF SEED, ARE TO RECEIVE 4" TOP SOIL, SEED, MULCH, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. FOR SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES INSTALL EROSION CONTROL BLANKET.
 - ALL DISTURBED AREAS TO RECEIVE NATIVE SEED, ARE TO RECEIVE PLANTING SOIL, SEED, MULCH, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. FOR SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES INSTALL EROSION CONTROL BLANKET.

IRRIGATION NOTES:

- IRRIGATION SYSTEM TO BE DESIGN/BUILD. CONTRACTOR TO SUBMIT SHOP DRAWINGS FOR APPROVAL OF SYSTEM LAYOUT PRIOR TO INSTALLATION.
- ALL SOD TO RECEIVE SPRAY OR ROTOR IRRIGATION HEADS WITH MINIMUM DESIGN OF 1" IRRIGATION PER WEEK.
- ALL PLANT BEDS TO RECEIVE DRIP LINE IRRIGATION, WITH A MINIMUM DESIGN OF .25" IRRIGATION PER WEEK.
- CONTRACTOR TO INSTALL A TOTAL OF 4 QUICK COUPLERS AT THE CORNERS OF THE PROPERTY. A 2.5" TYPE K SOURCE PIPE IS PROVIDED BY MECHANICAL.



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Engineering | Surveying | Planning | Environmental

Client DORAN COMPANIES

Project CALHOUN VILLAGE

Location MINNEAPOLIS, MINNESOTA

Certification

Summary

Designed: JIM Drawn: JMW
Approved: EVM Book / Page:
Phase: CONCEPT Initial Issue: MM/DD/2016

Revision History

No. Date By Submittal / Revision
A 06/21/16 JMW Revised per City Comments

Sheet Title LANDSCAPE DETAILS

Sheet No. Revision

L2.01 A

Project No. DOR20550

RECOMMENDATION 26 – MARKET PLAZA CONNECTION TO LAKE CALHOUN

MARKET PLAZA (FROM MIDTOWN GREENWAY TRAIL NORTH OF CALHOUN VILLAGE TO EXCELSIOR BOULEVARD),
WEST CALHOUN BOULEVARD (FROM EXCELSIOR BOULEVARD TO WEST CALHOUN PARKWAY)

Create a connection for bicyclists and pedestrians between the Midtown Greenway and Lake Calhoun through Calhoun Village, along Market Place and West Calhoun Boulevard. The recommendation includes:

- **26a** – A shared use trail connection through Calhoun Village parking lot along the east side of driveway aisle with raised crosswalks at aisle crossings within the parking lot. The proposed improvement would utilize the current sidewalk and patio space adjacent to the easternmost building and narrows the lanes of the entrance driveway to gain space to widen the existing sidewalk.
- **26b** – Relocation of the existing signal controller from the northeast quadrant to the northwest quadrant of West Lake Street and the Calhoun Village access to provide more pedestrian space on the corner. Include enhanced shared use trail crosswalk, using poly-preform markings and continental design at the east West Lake Street approach. Address curb radius in northeast quadrant.
- **26c** – Reallocate the current roadway right-of-way by narrowing the lanes on Market Plaza between West Lake Street and Excelsior Boulevard and widening the east side sidewalk to accommodate a shared use trail.
- **26d** – Provide an enhanced shared use trail crosswalk, using poly-preform markings and continental design at the east approach of the Market Plaza and Excelsior Boulevard intersection.
- **26e** – On West Calhoun Boulevard, from Excelsior Boulevard to West 32nd Street, the recommendation includes a northbound counter-flow bicycle lane and a southbound bicycle boulevard.



FIGURE 4-8
RECOMMENDATION 26A-D – MARKET PLAZA CONNECTION TO LAKE CALHOUN

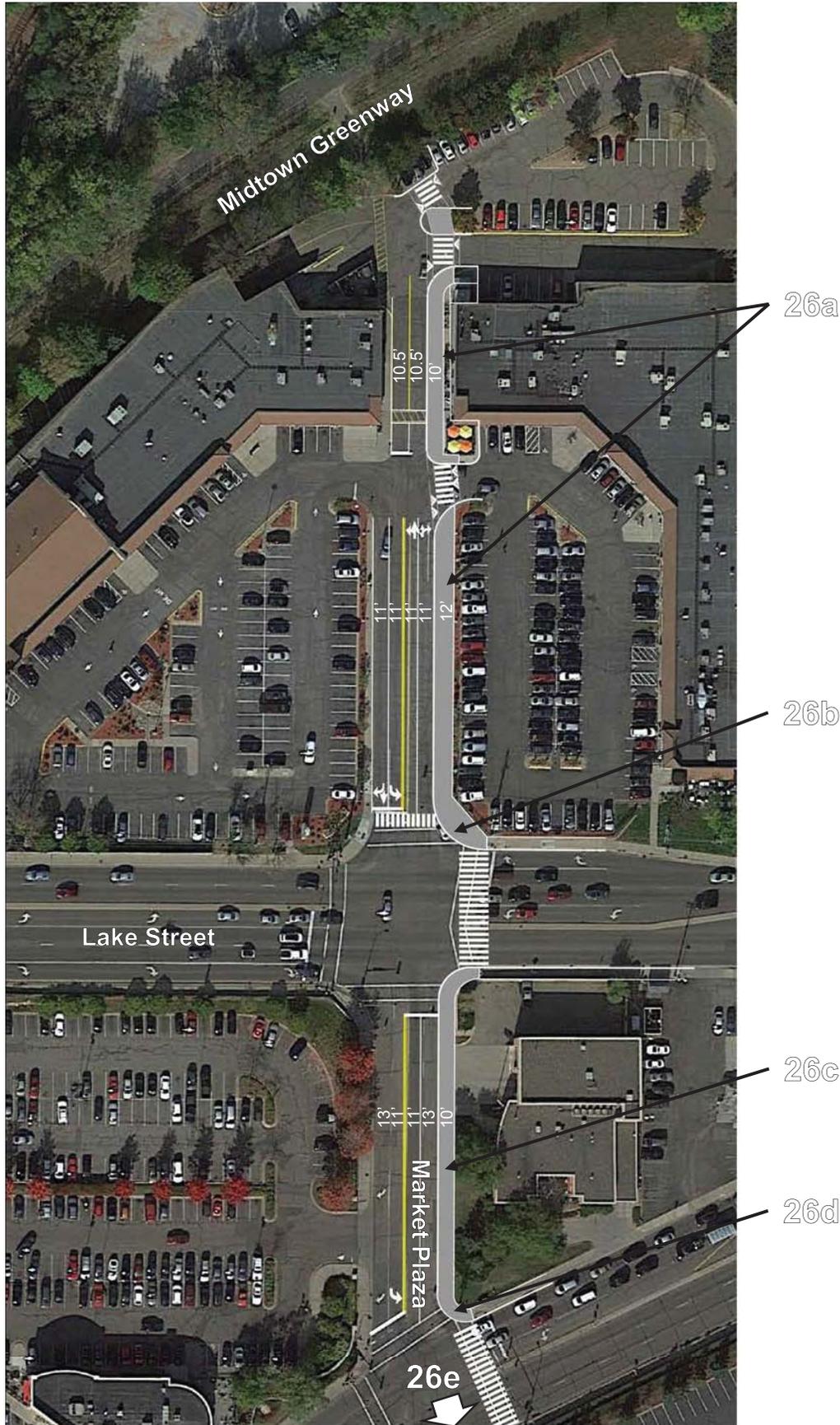
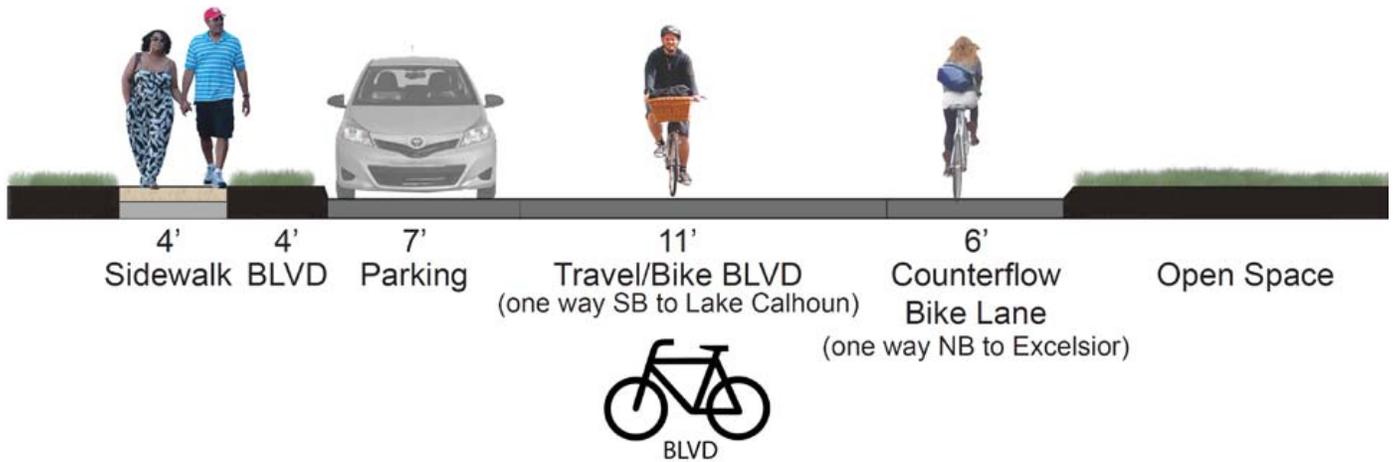


FIGURE 4-9
RECOMMENDATION 26E – WEST CALHOUN BOULEVARD – EXCELSIOR BLVD TO WEST 32ND STREET



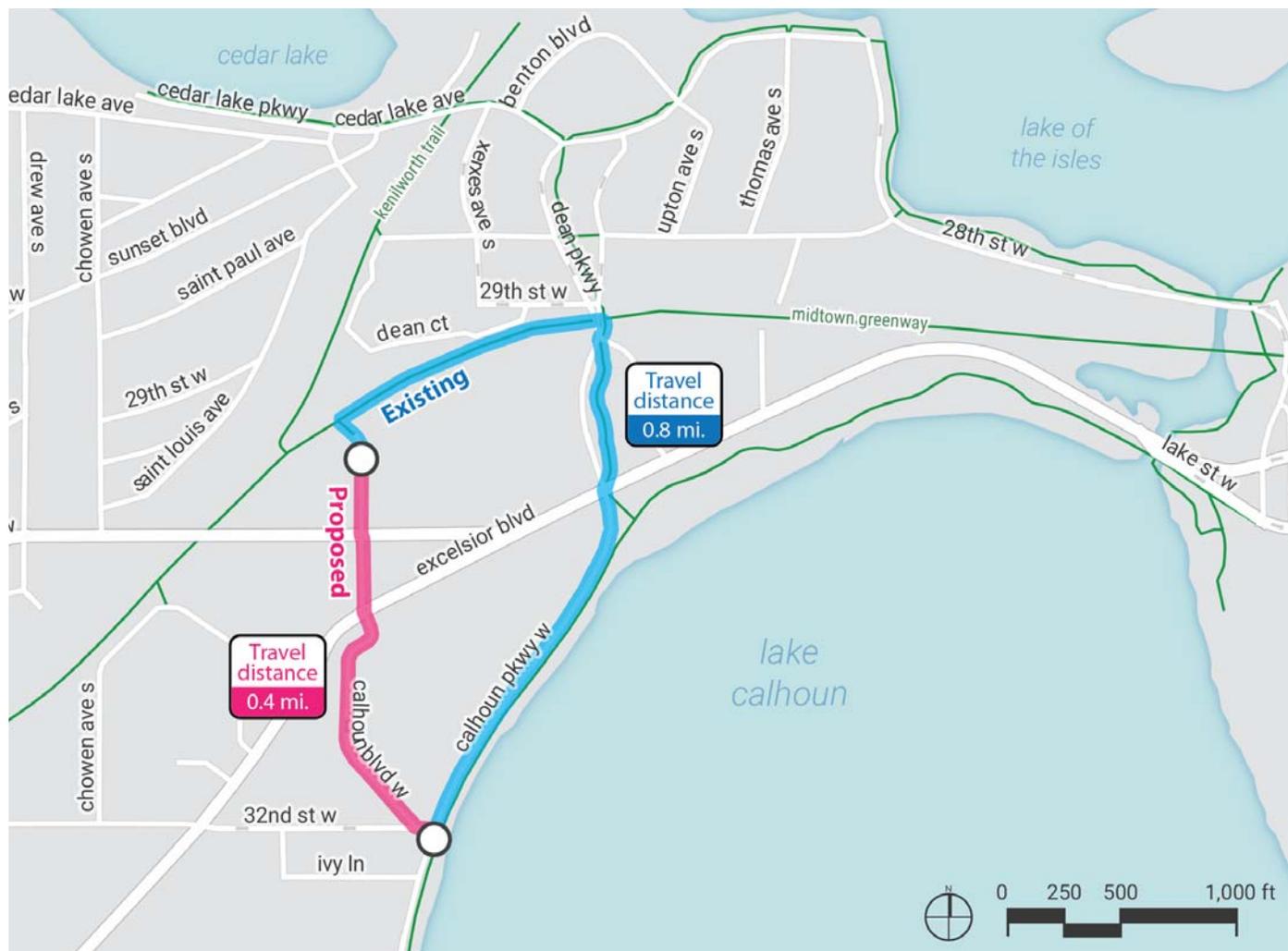
DISCUSSION & ANALYSIS

- Combined with wayfinding, the recommendation provides a clearly delineated pedestrian and bicycle facility that connects the Midtown Greenway and Lake Calhoun
- Raised crosswalks in the Calhoun Village property increases visibility of pedestrians and bicyclists and improves sense of right of way
- Enhanced crosswalk markings alert motorists to locations where conflicts between bicyclists, pedestrians and automobiles would most likely occur
- Use of the continental design for crosswalk markings also improves crosswalk detection for people with low vision and cognitive impairments
- Counter-flow bicycle lane south of Excelsior provides a circulation improvement for bicycle travel
- Off-street shared use trail north of Excelsior provides a low-stress facility for visitors and people with children

Analysis

As shown in Figure 4-10, the addition of an off-street shared use path along Market Plaza and a counter-flow bike lane on West Calhoun Boulevard would reduce the travel distance from the Calhoun Village shopping center to Lake Calhoun by about half. Under existing conditions, bicyclists would take the Midtown Greenway to Dean Parkway to West Calhoun Parkway to reach their destination in 0.8 miles, whereas the trip down Market Plaza is a more direct 0.4 miles.

FIGURE 4-10
CONNECTIVITY ANALYSIS MAP - CONNECTION BETWEEN MIDTOWN GREENWAY AT CALHOUN VILLAGE AND LAKE CALHOUN



IMPLEMENTATION CONSIDERATIONS

- The trail includes both private property and/or park property. Coordination would be required with property owners for implementation
- Coordination between Hennepin County, the Minneapolis Park and Recreation Board and the City of Minneapolis is necessary
- Detailed design is necessary to resolve ADA access at driveway crossing (between Punch Pizza and Nail Salon) and building entrances