

## LAND USE APPLICATION SUMMARY

*Property Location:* 347-359 Harrison Street Northeast and 980 Winter Street Northeast  
*Project Name:* Conditional use to allow for a bus garage and maintenance facility  
*Prepared By:* Aaron Hanauer, Senior City Planner, (612) 673-2494  
*Applicant:* 347 Harrison Properties, LLC  
*Project Contact:* Abdirizak Bille, Bille Bus Transportation Services  
*Request:* To lawfully establish a bus garage and maintenance facility.  
*Required Applications:*

<b>Conditional Use Permit</b>	To allow a bus garage and maintenance facility in the II/Light Industrial District.
<b>Variance</b>	To allow a surfacing material for off-street parking and loading areas that is not in compliance with Section 541.300-Surfacing.
<b>Site Plan Review</b>	To establish a bus garage and maintenance facility.

## SITE DATA

<b>Existing Zoning</b>	II/Light Industrial District
<b>Lot Area</b>	58,290 square feet / 1.34 acres
<b>Ward(s)</b>	3
<b>Neighborhood(s)</b>	Beltrami; adjacent to Marcy Holmes
<b>Designated Future Land Use</b>	Transitional Industrial
<b>Land Use Features</b>	None, Approximately one block away from a community corridor (East Hennepin Avenue)
<b>Small Area Plan(s)</b>	<u>N/A</u>

<b>Date Application Deemed Complete</b>	July 13, 2016	<b>Date Extension Letter Sent</b>	N/A
<b>End of 60-Day Decision Period</b>	August 12, 2016	<b>End of 120-Day Decision Period</b>	N/A

## **BACKGROUND**

### **SITE DESCRIPTION AND PRESENT USE.**

The project site is located just to the north of East Hennepin Avenue and to the east of Central Avenue. It is located at the intersection of Harrison Street Northeast and Winter Street Northeast. The 1.3 acre site consists of four parcels.

As outlined by their website, Bille Bus Transportation Services provides school bus transportation, mini-van transportation, and non-emergency medical transportation. According to state records, Bille Bus Transportation Services began operation in 2011. The company moved their bus garage and maintenance facility to 347 Harrison Street Northeast in or around 2014. In 2015, the applicant received a notice of non-compliance for operating a bus garage or maintenance facility at this location without first receiving a conditional use permit.

There are two buildings on the project site. The larger building, which is approximately 2,415 square feet was built in 1958. The 1,035 square foot garage was administratively approved in 2014 to provide additional onsite storage. The site currently is unpaved, has a barbed wire fence along the entire perimeter, and no intentional landscaping.

As outlined in the application, Bille Bus Transportation has approximately 45 employees. The employees typically come to work around 5:30am to start their morning bus runs. The buses return around 8:30 – 9:00am and most of the employees leave at this time. The employees return around 2:00-2:30pm to start the afternoon bus runs. The drivers return around 5:30 – 6:00pm.

The applicant states that Bille Bus Transportation has approximately 50 buses and 30 vans for their school transportation operation. The vans are used for longer distance travel (e.g. beyond the Interstate 494 and 694 loop). The drivers typically take the vans to their homes at night and on weekends; they are left onsite usually when needing repairs.

### **SURROUNDING PROPERTIES AND NEIGHBORHOOD.**

The project site is located in an industrially zoned area (I1 and I2) with a mix of industrial uses and a couple of restaurants. Burlington Northern and Santa Fe Railroad lines run on the north and west side of the property. The nearest residentially zoned property are those 450 feet to the south.

Harrison Street Northeast provides the most direct route for those looking to travel from Central Avenue Northeast to East Hennepin Avenue and vice versa. However, the traffic counts on Harrison Street (annual average daily traffic of 1,200 vehicles in 2012) are significantly lower compared to either of those arterial streets.

### **PROJECT DESCRIPTION.**

The applicant is proposing to legally establish the bus garage and maintenance facility with the requested conditional use permit and site plan review applications. There are no exterior building alterations proposed as part of this project. However, the applicant has proposed a significant amount of landscaping along the Harrison Street frontage as well as tree and shrub planting within the right-of-way along Winter Street Northeast. In addition, the applicant is proposing to resurface the parking area with true asphalt near the vehicle access point and an asphalt milling material for the rest of the area.

**RELATED APPROVALS.**

Planning Case #	Application	Description	Action
PDR-1001245	PDR Review	Approval of a new 1,020 square foot detached garage	Administrative approval

**PUBLIC COMMENTS.** As of the publication of the staff report on July 12, 2016, no public comments were received. Any correspondence received after the publication of the staff report and prior to the public meeting will be forwarded on to the Planning Commission for consideration.

**ANALYSIS**

**CONDITIONAL USE PERMIT**

The Department of Community Planning and Economic Development has analyzed the application to allow a bus garage and maintenance facility in the II/Light Industrial District based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare. The site is located in an industrial area and is surrounded by other industrial uses. There are no residential properties in the vicinity.

School bus transportation services are regulated and inspected by the State of Minnesota. Bille Bus Transportation Services has an active business corporation license with the Minnesota Secretary of the State. The company uses minivans and traditional school buses to complete school transportation services.

As outlined by the Minnesota State Patrol website, every school bus in Minnesota is required by statute to be inspected annually by the Minnesota State Patrol. Additionally, state troopers are assigned to enforce traffic regulations for school bus drivers and other motorists operating in proximity to school buses. Troopers conduct random school bus driver qualification checks, post-crash inspection and follow up reviews, and school bus-related public outreach programs. In an email conversation with the Minnesota State Patrol, they said that, "Bille Bus has had some issues with their annual inspections over the past couple of years. After the annual inspections last year they [Bille Bus] made a commitment to change the issues they were having and to turn things around. Since then, we've [the Minnesota State Patrol] completed their 2016 annual inspections and found no violations which is extremely rare. They seem to be following through on their commitment to turn things around."

The Bille Bus Transportation Services website also states that they provide non-emergency medical transportation. However, in communication with the Department of Transportation, Bille Bus currently does not meet state requirements to complete this service. As a condition of approval, the applicant shall not complete non-emergency medical transportation from the subject property until they are in good standing with the State of Minnesota to complete this type of service.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The proposal to legally establish a bus garage and maintenance facility would prove compatible with the surrounding uses and should not impede the normal and orderly development of the area. In addition, the enhanced landscaping along Harrison and Winter Street and removal of the barbed wire fence along Harrison Street Northeast will improve the overall appearance of the site.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The applicant is looking to maintain the current vehicle access configuration to the site, with one curb cut off of Harrison Street. As proposed, the applicant would provide parking/storage for 50 school buses, 5 minivans, and 5 employee vehicles.

The applicant received Preliminary Development Review (PDR) approval in 2014 when they proposed to build a 1,035 square foot garage/storage facility.

The site improvements proposed as part of this application were reviewed by other departments through the Preliminary Development Review (PDR) process. As part of this process, the Public Works Department will review the final plan for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant would be required to continue to work closely with the Public Works Department and the various utility companies during the duration of the development should the applications be approved. This would be required to ensure that all procedures are followed and that the development complies with all city and other applicable requirements.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Adequate measures have been taken to minimize traffic congestion in the public streets. The applicant parks all school buses within their property and is meeting their off-street parking requirement. The off-street parking requirement for a bus garage and maintenance facility is 1 space per 500 sq. ft. of GFA in excess of 4,000 sq. ft. excluding service bays (minimum of 4 spaces) + 1 space per 2 service bays. Outside of the two service bays, there is a total of 1,676 square feet of gross floor area onsite. Therefore, the off-street parking requirement is five spaces. The applicant is proposing to provide five parking spaces for employees.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

**Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.**

- 1.1.4 Support context-sensitive regulations for development and land use, such as overlay districts, in order to promote additional land use objectives.

**Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.**

- 1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

There is no small area plan for the project site and it is not located within an industrial employment district.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If all land use/zoning applications are approved, including the conditional use permit and site plan review, the proposal would appear to comply with all applicable provisions of the I1/Light Industrial District.

## VARIANCE-SURFACING MATERIAL

The Department of Community Planning and Economic Development has analyzed the application for a variance of the surfacing requirements for off-street parking and loading areas based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

As required by Section 541.300-Surfacing, all open off-street parking areas, all driveways leading to such parking areas, and all other areas upon which motor vehicles may be located, except where accessory to a single-family dwelling, shall be surfaced with a dustless all-weather hard surface material capable of carrying a wheel load of four thousand (4,000) pounds. In addition, all driveways in commercial and industrial districts leading to areas other than off-street parking areas shall be surfaced with a dustless all-weather hard surface material capable of carrying a wheel load of four thousand (4,000) pounds for a minimum of twenty (20) feet from the curb line. Acceptable surfacing materials shall include asphalt, concrete, brick, cement pavers or similar material installed and maintained per industry standards. Pervious pavement or pervious pavement systems are allowed subject to the provisions of this chapter.

The existing parking area is primarily gravel. The applicant is proposing to resurface the parking area with true asphalt near the vehicle access point and an asphalt milling material for the rest of the area. Asphalt millings are recycled asphalt that is processed to make a rock-like substance. Prior to the installation of asphalt millings, the site is graded. Once the existing surface is prepped, the asphalt millings are spread and regraded. After the site is graded, a double drum compaction roller is used to compact the asphalt millings into a smooth, hard surface. The zoning administrator made the determination that asphalt millings are not one of the acceptable materials outlined in Section 541.300-Surfacing. Therefore, a variance of the surfacing materials was required.

Practical difficulties exist in complying with the ordinance due to circumstances unique to the property. The subject property and the surface lot are larger than typical lots and surface areas in Minneapolis. The applicant is looking to pave approximately 1.1 acres of the 1.3 acre site for the bus parking lot.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

As conditioned, the property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and comprehensive plan. The intent of the surfacing material requirement is to provide a durable parking surface that does not produce dust,

erode, or allow dirt or other matter to be tracked into the public right-of-way. In general, paved surfaces also allow for a more permanent and organized parking layout through striping.

The applicant has provided product information that the asphalt millings are a durable material and do not produce dust or track other matter into the public right of way. Public Works colleagues have confirmed that when installed properly, asphalt millings can be a quality surfacing material.

As noted in Section 525.520-Authorized variances, the factors to be considered in varying the surfacing requirements for the industrial districts shall include but not be limited to the following: 1. the yard and parking uses are in the same area; 2. use of heavy equipment will not cause excessive hard surface breakup; 3. parking movements are infrequent; 4. the area is distant from other nonindustrial zone uses; or 5. water infiltration is ecologically desirable.

The applicant has provided product information showing that the asphalt millings are durable. In addition, the industrial site is located more than 450 feet away from a non-industrially zoned use. Furthermore, asphalt millings provide for some stormwater infiltration.

In consultation with Public Works colleagues, CPED is recommending a minimum depth of 6 inches of asphalt millings shall be applied over a compacted subgrade to help with durability of the surfacing material. The applicant is proposing to install traditional asphalt for the first 50 feet of the property. However, CPED is recommending that traditional asphalt be applied from the Harrison Street curb cut to the eastern portion of the building for the automobile parking area and drive aisle.

- 3. The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The subject property is surrounded by other industrially zoned properties. Because the asphalt milling area has been found to be a durable surfacing material and it is separated from the adjacent public streets by a true asphalt area, granting the variance would not likely affect the character of the area and should have little effect on adjacent properties. Furthermore, it would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

## SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

### **1. Conformance to all applicable standards of Chapter 530, Site Plan Review.**

## BUILDING PLACEMENT AND DESIGN

### **Building placement – Not applicable**

- The project site is a corner lot. The first floor of the building is not located within eight feet of either Harrison Street or Winter Street Northeast. However, this is an existing condition and is not proposed to change.
- The existing buildings are in line with the other buildings along Harrison Street Northeast in terms of building setback. The existing building does not have windows that allow for natural surveillance. However, these are existing conditions and are not proposed to change as part of the proposed project. As outlined in more detail in the access and circulation section below, the applicant is

proposing a direct walkway from the Harrison Street Northeast sidewalk to an entrance near the southwest corner of the main building.

- As proposed, the area between the building and the lot line along Harrison Street Northeast will have a landscaped area with bee-friendly plants, shrubs, and trees.
- The on-site accessory parking is not located entirely to the rear of the interior of the site. However, this is an existing condition.

#### **Principal entrances – *Not applicable***

- The main building has a principal pedestrian entrance that does not face the front property line; however, it is located within five feet of the Harrison Street elevation. This is an existing condition and is not proposed to change.
- With the exception of an overhead light, the existing principal entrance to the maintenance facility building does not have architectural features such as an awning or transom windows to express the importance of the entrance. This is an existing condition and is not proposed to change. However, as noted in the access and circulation section below, the applicant is proposing a walkway to provide direct access to this building entrance which will indirectly help express the importance of the entrance.

#### **Visual interest – *Not applicable***

- The existing buildings are utilitarian buildings with few ornate details. This is an existing condition and is not proposed to change as part of this application.
- There are no blank, uninterrupted walls exceeding 25 feet in length on the northern and southern sides of the building. However, this is an existing condition and is not proposed to change as part of this application.

#### **Exterior materials – *Not applicable***

- There are two buildings on the projects site. The applicant is not proposing to make alterations to the exterior materials of these buildings as part of the proposed project. The 1950s warehouse building has two main exterior materials. On the front elevation facing Harrison Street Northeast the primary material is brick. On the sides and rear of the building, the building is concrete block. The storage/garage building built in 2014 has horizontal lap siding.
- There is a painted plain face concrete block facing Winter Street Northeast. However, this is an existing condition and is not proposed to change as part of this application.

#### **Windows – *Not applicable***

- There are two buildings on the projects site. The applicant is not proposing to make alterations to the building fenestration as part of the proposed project. The 1950s warehouse building contains a window on the front elevation, two windows on the southern elevation, and one window on the eastern elevation that are located approximately 10 feet above grade. These windows help provide daylight into the building but do not provide views into and out of the building. The smaller building also has a small daylighting window on the west elevation.
- Typically, the north, west, and east elevation of the 1950s warehouse building and the west and east elevation of the smaller building would be required to provide 30 percent window coverage as these elevations face a public street or on-site parking lot. The buildings are well below this percentage. However, these buildings are existing and the applicant is not proposing to make changes to the fenestration as part of the proposed project.

#### **Ground floor active functions – *Not applicable***

- As outlined in Section 530.120-Building design, except for industrial uses in Table 550-1, Principal Industrial Uses in the Industrial Districts, the first floor or ground level of buildings shall be designed to accommodate active functions by ensuring that parking, loading, storage, or mechanical equipment rooms are limited to no more than thirty (30) percent of the linear building frontage along each wall facing a public street, public sidewalk, or public pathway. The bus garage and maintenance facility is a principal (conditional) use in the industrial zoning districts and therefore not subject to this requirement.

**Roof line – Not applicable**

- The 1950s warehouse building has a flat roof and the building built in 2014 has a gabled roof. These roof lines are not proposed to change as part of the proposed project.

**Parking garages – Not applicable**

- The subject property does not contain a parking garage nor is it proposed to have one with this project. The 1950s warehouse building provides a maintenance facility for the buses and minivans and the 2014 building is an office storage facility.

**ACCESS AND CIRCULATION**

**Pedestrian access – Meets requirements with Conditions of Approval**

- The applicant is proposing to install a four foot wide walkway connecting an entrance near the southwest corner of the building to Harrison Street Northeast. As a condition of approval, the applicant shall install a pedestrian gate along Harrison Street Northeast that provides direct access to the main building from Harrison Street Northeast during the hours of operation.

**Transit access – Not applicable**

- No transit shelters exist or are proposed as part of this development.

**Vehicular access – Meets requirements**

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses. As previously mentioned, the applicant is proposing to install a separate pedestrian entrance from Harrison Street Northeast to the main building.
- There is only one curb cut that provides access to the project site.
- There are no public alleys adjacent to the site.
- Service vehicle access will not conflict with pedestrian traffic as proposed.
- The proposed site plan minimizes the use of impervious surfaces. The applicant is proposing to convert approximately 1,700 square feet of impervious area to pervious.

**LANDSCAPING AND SCREENING**

**General landscaping and screening – Requires alternative compliance**

- The overall composition and location of landscaped areas will complement the scale of development and its surroundings. The applicant is proposing to add approximately 1,700 square feet of landscaped area, as well as trees, shrubs, and plants along Harrison Street Northeast and canopy trees and shrubs in the adjacent public right of way to the north of the project site along Winter Street.

- As outlined in Section 530.160-General landscaping and screening, not less than twenty (20) percent of the site not occupied by buildings shall be landscaped. As proposed, 11 percent of the project site will be landscaped. Alternative compliance is required.
- Given the size of the lot and the building, the project is required to provide 22 canopy trees onsite. The applicant is proposing nine canopy trees onsite. Alternative compliance is required.
- Given the size of the lot and the building, the project is required to provide 110 shrubs onsite. The applicant is proposing 51 shrubs onsite. Alternative compliance is required.
- In August 2015, the City of Minneapolis adopted a policy committed to increasing bee-friendly plants in the city and decreasing pesticide use. The City of Minneapolis urges all Minneapolis property owners, residents, businesses, institutions and neighborhoods to become more pollinator friendly by adopting practices that plant more pollinator forage on their property and using organic or chemical-free lawn and landscaping practices. The applicant is proposing to plant a number of bee and butterfly friendly plants including Rough Blazing Star, Meadow Blazing Star, and Butterfly Milkweed.
- As a condition of approval, the remainder of the required landscaped area is covered with turf grass, wood mulch, native grasses, perennial flowering plants, vines, shrubs and other trees.

**Table 1. Landscaping and Screening Requirements**

	<b>Code Requirement</b>	<b>Proposed</b>
<b>Lot Area</b>	--	58,290 sq. ft.
<b>Building Footprint</b>	--	3,450 sq. ft.
<b>Remaining Lot Area</b>	--	54,840 sq. ft.
<b>Landscaping Required</b>	10,968 sq. ft.	6,043 sq. ft.
<b>Canopy Trees (1:500 sq. ft.)</b>	22 trees	9 trees
<b>Shrubs (1:100 sq. ft.)</b>	110 shrubs	51 shrubs

**Parking and loading landscaping and screening – Requires alternative compliance**

- The parking and loading area facing the public street along Harrison Street Northeast has an onsite landscaped yard that is approximately 7 feet deep. The parking and loading area facing Winter Street Northeast does not have an onsite landscaped yard. Alternative compliance is required.
- Given that the applicant is proposing to have the parking area devoted principally to the parking or loading of commercial vehicles of more than fifteen thousand (15,000) pounds, screening six (6) feet in height and not less than sixty (60) percent opaque is required. The applicant is proposing to install a six foot high, black coated chain link fence and plant shrubs that will grow to be five feet in height. The applicant’s proposal is in compliance with this requirement.
- There is at least one tree provided for each 25 linear feet, or fraction thereof, of parking or loading area frontage along Harrison Street Northeast. However, the applicant is not proposing onsite planting of trees along Winter Street Northeast. Alternative compliance is required.
- The parking area does not abut or is across an alley from a residence or office residence district, or any permitted or conditional residential use.
- There are no portions of the parking lot that are unavailable for parking or vehicular circulation.
- Because the proposed surface parking lot contains ten or more spaces, each parking space must be located within 50 feet of the center of an on-site deciduous tree. The proposed site plan does not comply with this requirement. Alternative compliance is required.
- As conditioned, the plant materials, and the installation and maintenance of the plant materials, would comply with sections 530.200 and 530.210 of the zoning code and all other areas not occupied by buildings, parking and loading facilities, or driveways would be covered with turf grass, native grasses, perennials, wood mulch, shrubs, and trees.

## **ADDITIONAL STANDARDS**

### **Concrete curbs and wheel stops – Requires alternative compliance**

- The parking lots/driveway is not designed with wheel stops or discontinuous curbing to provide on-site retention and filtration of stormwater, nor will it have 6"x6" continuous curbing. Alternative compliance is required.

### **Site context – Meets requirements**

- The site plan will not block views of important elements of the city.
- There are no proposed changes to the building. Therefore, there will be no changes in regards to shadowing from the building nor from wind currents.
- The one story buildings will not have an adverse impact on the generation of wind currents at ground level.

### **Crime prevention through environmental design – Meets requirements**

- As proposed by the applicant, the site plan will employ crime prevention through environmental design (CPTED) best practices. This includes additional landscaping along the border of the site to help improve territorial reinforcement and natural access. CPED encourages the applicant to add openings in the building that provide opportunities to see into and out of the building.

### **Historic preservation – Not applicable**

- This site is neither historically designated nor is it located in a historic district.

## **2. Conformance with all applicable regulations of the zoning ordinance.**

The proposed bus storage and maintenance facility is a conditional use in the II/Light Industrial Zoning District.

### **Off-street Parking and Loading – Requires variance(s)**

- The off-street parking requirement for a bus garage and maintenance facility is 1 space per 500 sq. ft. of GFA in excess of 4,000 sq. ft. excluding service bays (minimum of 4 spaces) + 1 space per 2 service bays. Outside of the two service bays, there is a total of 1,676 square feet of gross floor area onsite. Therefore, the off-street parking requirement is five spaces. The applicant is proposing to provide five parking spaces for employees (see Table 2).
- The maximum off-street parking requirement is 1 space per 200 sq. ft. of GFA+ 1 space per service bay. Therefore, the maximum off-street parking requirement is eight spaces.
- There is no minimum bicycle parking requirement for bus garage and maintenance facility. The applicant is not proposing bike parking for the subject property (see Table 6).
- A bus garage and maintenance facility has a low loading requirement. The 3,450 square feet of building area does not have an off-street loading requirement as outlined in Table 541-8 Loading Requirements for Assigned Ratings (see Table 4).
- The proposed site plan layout allows all maneuvers associated with parking to occur in the off-street parking area.
- The project requires a variance to allow for surfacing material that does not meet the requirements outlined in Section 541.300-Surfacing. The variance is analyzed in the section above.
- As a condition of approval, all drive aisles and parking spaces shall be clearly marked on the pavement as outlined in Section 541.320-Marking of Parking Places.
- All vehicle parking spaces will meet the standard size requirements outlined in Section 541.330.

- The applicant is proposing to have snow storage take place at the eastern end of the site.

**Table 2. Vehicle Parking Requirements Per Use (Chapter 54I)**

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
Bus Storage and Maintenance Facility	5	--	5	8	5

**Table 3. Bicycle Parking Requirements (Chapter 54I)**

Use	Minimum	Short-Term	Long-Term	Proposed
Bus Storage and Maintenance Facility	0	N/A	N/A	0

**Table 4. Loading Requirements (Chapter 54I)**

Use	Loading Requirement	Minimum Requirement	Proposed
Bus Storage and Maintenance Facility	Low	0	0

**Building Bulk and Height – Meets requirements**

- There are no proposed changes to the building bulk and height (see Table 5). The existing buildings are in compliance with the maximum floor area ratio and height requirements.

**Table 5. Building Bulk and Height Requirements**

	Code Requirement	Proposed
Lot Area	--	58,290 sq. ft. / 1.34 acres
Gross Floor Area	--	3,450 sq. ft.
Floor Area Ratio (Minimum)	N/A	0.1
Floor Area Ratio (Maximum)	2.7	
Building Height (Maximum)	4 stories or 56 feet, whichever is less	1 story and 20 ft.

**Lot Requirements – Meets requirements**

- The proposed project does not include residential uses.
- Transportation uses in the industrial zoning districts require 12,000 square feet of minimum lot area and 100 feet of lot width. The proposed project meets these dimensional requirements (see Table 9).
- There is no impervious surface area requirement.

**Table 6. Lot Requirements Summary**

	Code Requirement	Proposed
Lot Area (Minimum)	12,000 square feet	58,290 square feet
Lot Width (Maximum)	100 ft.	197 ft.

**Yard Requirements – Meets requirements**

- There are no yard requirements for this industrial use given that the subject property does not abut a front, side, or rear lot line in a residence or office residence district, nor does it abut a side or rear lot line of a structure used for permitted or conditional residential purposes (see Table 7).

**Table 7. Minimum Yard Requirements**

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
<b>Front-Harrison</b> (East)	0 ft.	N/A	0 ft.	12 ft.
<b>Corner Side</b> (North)	0 ft.	N/A	0 ft.	114 ft.
<b>Interior Side</b> (South)	0 ft.	N/A	0 ft.	7.9 ft.
<b>Rear</b> (East)	0 ft.	N/A	0 ft.	219 ft.

**Signs – Not applicable**

- All signs are subject to Chapter 543, On-Premise Signs. The applicant is not proposing new signage with this application. Any new signs will be required to submit a separate sign permit application for any signage that is proposed.

**Screening of Mechanical Equipment – Meets requirements**

- There is no mechanical equipment currently visible from Harrison or Winter Street. The applicant is not proposing any mechanical equipment as part of the proposed project and there is no mechanical equipment currently visible from Harrison or Winter Street

**Refuse Screening – Meets requirements with Conditions of Approval**

- The applicant is proposing an 8 foot by 10 foot screen for the refuse and recycling storage container behind the main building. As a condition of approval, plans shall be submitted prior to building permit issuance showing that the refuse screening is in compliance Section 535.80- Screening of refuse and recycling storage containers.

**Lighting – Meets requirements with Conditions of Approval**

- As a condition of approval, the project site shall comply with the lighting requirements outlined in Chapter 535 and Chapter 541.

**Fences – Meets requirements**

- Fences must comply with the requirements in Chapter 535. The applicant is proposing to remove the barbed wire fence along Harrison Street Northeast and replace it with a six-foot tall black vinyl coated chain link fence. The barbed wire fence is proposed to remain along the remainder of the perimeter.

**Specific Development Standards – Not applicable**

- There are no development standards for a bus garage and maintenance facility in the industrial zoning districts.

**Overlay District Standards – Not applicable**

- The project site is not located within an overlay district.

**3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.**

*The Minneapolis Plan for Sustainable Growth* identifies the site as industrial on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

**Urban Design Policy 10.12: Design industrial uses with appropriate transitions and other design features which minimize negative impacts on surrounding residential uses.**

- 10.12.1 Provide appropriate physical transition and separation using green space, fencing, setbacks or orientation between industrial uses and other surrounding uses.
- 10.12.4 Design industrial sites to ensure direct access to major truck routes and freeways as a way to minimize automobile and truck impacts on residential streets and alleys.
- 10.12.5 Promote quality design and building orientation of industrial development that is appropriate with the surrounding neighborhoods.
- 10.12.6 Use the site plan review process to ensure that lighting and signage associated with industrial uses do not create negative impacts for residential properties.

**Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.**

- 10.18.1 Require that parking lots meet or exceed the landscaping and screening requirements of the zoning code, especially along transit corridors, adjacent to residential areas, and areas of transition between land uses.
- 10.18.2 Parking lots should maintain the existing street face in developed areas and establish them in undeveloped areas through the use of fencing, walls, landscaping or a combination thereof along property lines.
- 10.18.4 Provide walkways within parking lots in order to guide pedestrians through the site.
- 10.18.7 Minimize the width of ingress and egress lanes along the public right of way in order to provide safe pedestrian access across large driveways.
- 10.18.8 Encourage appropriate land uses to share parking lots to reduce the size and visual impact of parking facilities.

**Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.**

- 10.19.1 In general, larger, well-placed, contiguous planting areas that create and define public and private spaces shall be preferred to smaller, disconnected areas.
- 10.19.2 Plant and tree types should complement the surrounding area and should include a variety of species throughout the site that include seasonal interest. Species should be

indigenous or proven adaptable to the local climate and should not be invasive on native species.

- 10.19.3 Landscaped areas should include plant and tree types that address ecological function, including the interception and filtration of stormwater, reduction of the urban heat island effect, and preservation and restoration of natural amenities.
- 10.19.4 Landscaped areas should be maintained in accordance with Crime Prevention Through Environmental Design (CPTED) principles, to allow views into and out of the site, to preserve view corridors and to maintain sight lines at vehicular and pedestrian intersections.
- 10.19.5 Landscaping plans should be designed to facilitate future maintenance including the consideration of irrigation systems, drought and salt-resistant species, ongoing performance of storm water treatment practices, snow storage, access to sun, proximity to buildings, paved surfaces and overhead utilities.
- 10.19.7 Boulevard landscaping and improvements, in accordance with applicable city polices, are encouraged.

**Urban Design Policy 10.22 Use Crime Prevention Through Environmental Design (CPTED) principles when designing all projects that impact the public realm, including open spaces and parks, on publicly owned and private land.**

- 10.22.3 Design the site, landscaping, and buildings to promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.
- 10.22.5 Locate landscaping, sidewalks, lighting, fencing and building features to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- 10.22.7 Locate entrances, exits, signs, fencing, landscaping, and lighting to distinguish between public and private areas, control access, and to guide people coming to and going from the site.

CPED finds that the proposed development is in conformance with the above policies of The Minneapolis Plan for Sustainable Growth.

**4. Conformance with applicable development plans or objectives adopted by the City Council.**

There is no small area plan that provides additional guidance for the project site. The subject property is located outside of an industrial employment district.

**5. Alternative compliance.**

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Landscaping percentage:** As outlined in Section 530.160-General landscaping and screening, not less than twenty (20) percent of the site not occupied by buildings shall be landscaped. As proposed, 11 percent of the project site will be landscaped/green space, when including the snow storage area the landscaped/green space is 16 percent of the project site. This includes the applicant’s proposal to add approximately 1,700 square feet of landscaped area along Harrison

Street Northeast. Alternative compliance is required. The intent of the landscaping requirement is to help add pervious area to a site which helps with onsite water retention, help improve the aesthetics of the area, provide adequate screening when required, and help with transitions of differing uses. CPED finds the applicant's proposal to provide additional onsite landscaped area along Harrison Street meets the intent of this ordinance by providing more pervious surface where it will have the greatest visibility and help with onsite water retention. In addition, it is recognized that buses require a large maneuvering area which reduces the opportunity to provide additional landscaping.

- **Onsite canopy trees:** As required by Section 530.160-General landscaping and screening, 22 canopy trees are required for the project site. The applicant is proposing to plant 10 canopy trees onsite. Alternative compliance is required. The intent of the minimum tree requirement is to enhance the aesthetics and landscape of the projects site and area, enhance the City of Minneapolis tree canopy and help reduce the heat island effect. In addition to the nine trees the applicant is proposing to plant onsite, they are also proposing to plant 12 canopy trees within the public right of way just to the north of the project site. In total, the applicant is proposing to plant a total of 22 canopy trees. CPED finds that this proposal meets the intent of the ordinance.
- **Onsite shrubs:** Given the size of the lot and the building, the project is required to provide 110 shrubs onsite. The applicant is proposing 51 shrubs onsite. Alternative compliance is required. The intent of the minimum shrub requirement is to enhance the landscape of the projects site and area, provide adequate screening when required, and help with transitions of differing uses. In addition to the 52 shrubs the applicant is proposing to plant onsite, they are also proposing to plant 58 shrubs within the public right of way just to the north of the project site. In total, the applicant is proposing to plant a total of 110 shrubs. As a condition of approval, the applicant shall provide a mulch bed where the shrubs are proposed to be planted within the public right of way along the northern end of the site.
- **Landscaped yard for parking front a public street (Winter Street Northeast):** Per Section 530.170-(b) Parking and Loading Landscaping and Screening, surface parking that abuts a public street is required to have a landscaped yard that is at least seven feet in width. Along the northern property line (Winter Street Northeast), there is not an onsite landscaped yard and the applicant is not proposing to install one. Alternative compliance is required. The intent of this requirement is to reduce the negative visual impact off-street parking from the public street and to help delineate the public sidewalk [if present] from off-street parking area. The applicant's proposal meets the intent of this requirement. The applicant is proposing to plant trees and shrubs within the public right of way to help screen the parking from those traveling south on Harrison Street Northeast and Winter Street Northeast. CPED finds that this proposal meets the intent of the ordinance. However, if the landscaped yard is removed from the public right of way along Winter St NE, a seven-foot landscaped yard shall be provided on-site adjacent to the Winter St right of way.
- **A minimum of one tree for every 25 linear feet of parking or loading fronting a street (Winter Street Northeast).** Per Section 530.170-Parking and Loading Landscaping and Screening, not less than one tree is required for each 25 linear feet or fraction thereof of parking and loading area lot frontage. The applicant is not proposing to plant trees within their property along Winter Street Northeast. Alternative compliance is required. The intent of this requirement is to support and enhance the City of Minneapolis tree canopy and the benefits trees bring to a city including reducing the heat island effect. The applicant is proposing to plant 12 canopy trees in the public-right-of-way along Winter Street Northeast. CPED finds that this proposal meets the intent of the ordinance.
- **Distance from an onsite tree:** Because the proposed surface parking lot contains ten or more spaces, each parking space must be located within 50 feet of the center of an on-site deciduous tree. The proposed site plan, in particular the parking spaces on the south and east side of the

project site do not comply with this requirement. Alternative compliance is required. The intent of this requirement is to reduce the negative visual impact off-street parking from the public street and to help reduce the heat island effect. The applicant is proposing to plant 10 trees onsite along Harrison Street Northeast and 12 trees within the public right of way along Winter Street. CPED finds that the applicant's alternative meets the intent of the ordinance and that full compliance is impractical because of site conditions. The applicant's proposed snow storage area along the eastern side of the site and the parking layout for the southern spaces do not make it feasible to plant trees that will be able to survive. In addition, it is recognized that buses require a large maneuvering area which reduces the opportunity to provide additional landscaping.

- **Defined parking lot with a discontinuous curbing or concrete curb.** Per Section 530.230- Concrete Curbs and Wheel Stops, all parking lots and driveways shall be designed with wheel stops or discontinuous curbing to provide on-site retention and filtration of stormwater. Where on-site retention and filtration is not practical, the parking lot shall be defined by six (6) inch by six (6) inch continuous concrete curb. The existing surface parking is not in full compliance with this requirement. Alternative compliance is required. The intent of this requirement is to help define the off-street parking area and to direct stormwater to a proper location for on-site retention. The applicant is proposing to retain not having a curb define the parking area. Public Works Surface Water and Sewers Division did not have an issue with the submitted proposal. The applicant's proposal to have the landscaped yards at the same height as the surface parking area where surface water can drain into the landscaped yards is consistent with the intent of this site plan review requirement.

## RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by 347 Harrison Properties, LLC for the properties located at 347-359 Harrison Street Northeast and 980 Winter Street Northeast:

### **A. Conditional Use Permit to allow a bus garage and maintenance facility in the II/Light Industrial District.**

Recommended motion: **Approve** the application for a conditional use permit to allow a bus garage and maintenance facility in the II/Light Industrial District, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. The applicant shall not complete non-emergency medical transportation from the subject property until they are in good standing with the State of Minnesota to complete this type of service.

### **B. Variance of the surfacing requirement.**

Recommended motion: **Approve** the application for a variance of the surfacing requirements for off-street parking and loading areas, subject to the following conditions:

1. A minimum depth of 6 inches of asphalt millings shall be applied over a compacted subgrade to help with durability of the surfacing material.
2. Traditional asphalt shall be applied from the Harrison Street curb cut to the eastern portion of the building for the automobile parking area and drive aisle.

3. The surfacing of the entire parking area shall be maintained in good working condition.

**C. Site Plan Review for a bus garage and maintenance facility.**

Recommended motion: **Approve** the site plan review to allow for a bus garage and maintenance facility, subject to the following conditions:

1. All site improvements shall be completed by July 18, 2017, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. The plant materials, and installation and maintenance of the plant materials, shall comply with sections 530.200 and 530.210 of the zoning code.
4. A pedestrian gate shall be installed within the fence that allows direct access to the building from Harrison Street Northeast during the hours of operation.
5. As required by Section 530.160-General landscaping and screening, the newly proposed landscaped area shall be covered with turf grass, wood mulch, native grasses, perennial flowering plants, vines, shrubs and other trees. In addition, the applicant shall provide a mulch bed where the shrubs are proposed to be planted within the public right of way along the northern end of the site. Furthermore, if the landscaped yard is removed from the public right of way along Winter St NE, a seven-foot landscaped yard shall be provided on-site adjacent to the Winter Street right of way.
6. The subject property shall comply with the lighting requirements outlined in Chapter 535 and Chapter 541.
7. Plan shall be submitted prior to building permit issuance showing that the refuse screening is in compliance with Section 535.80-Screening of refuse and recycling storage containers.
8. As required by Section 541.320-Marking of Parking Places, all drive aisles and parking spaces shall be clearly marked on the pavement.

## ATTACHMENTS

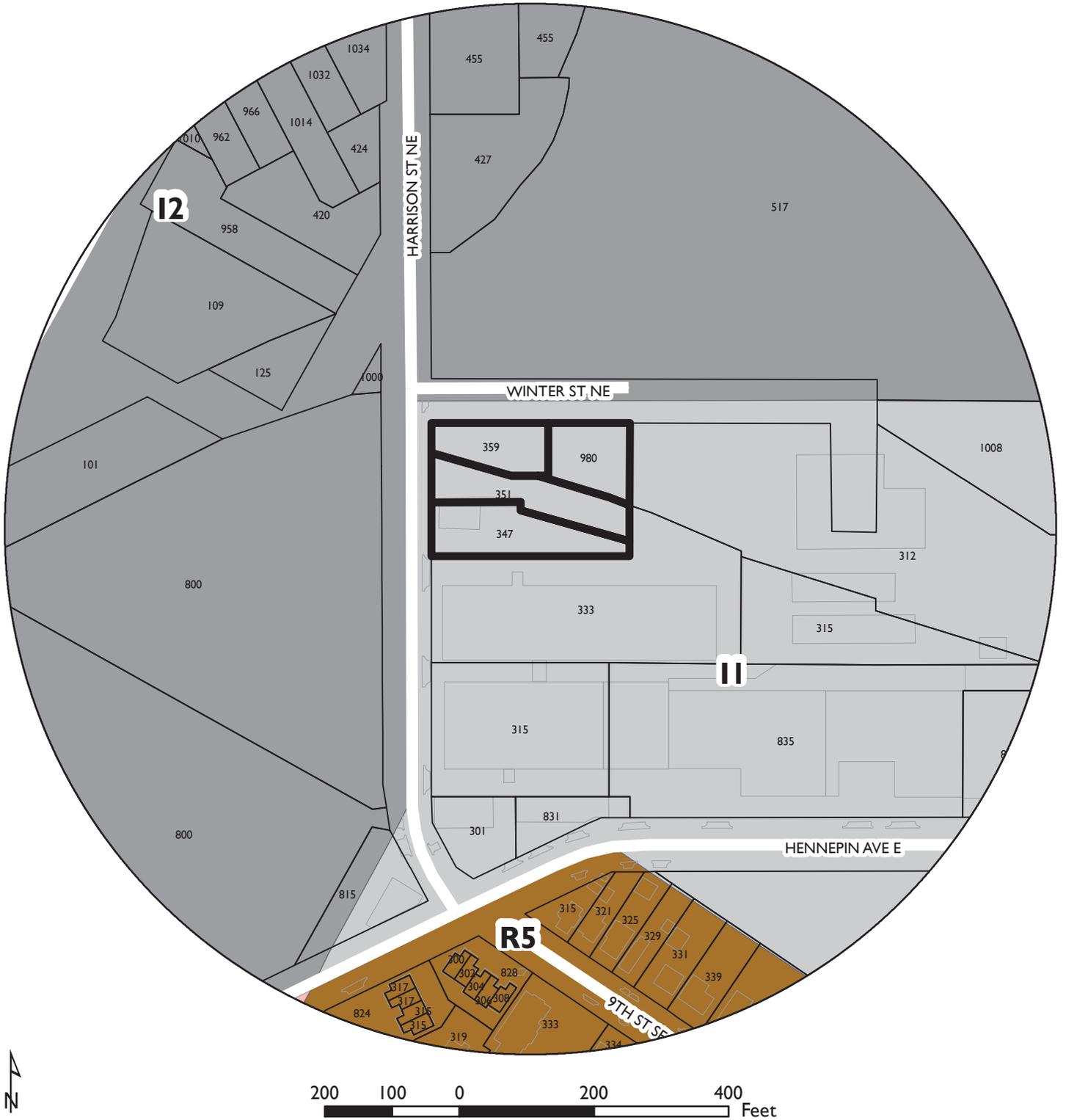
1. Zoning map
2. Aerials
3. Statement of proposed use and written statement to the findings
4. Asphalt milling information
5. Survey | site plan | floor plan
6. Photos
7. PDR report

**347 Harrison Properties, LLC**

**3rd**

NAME OF APPLICANT

WARD

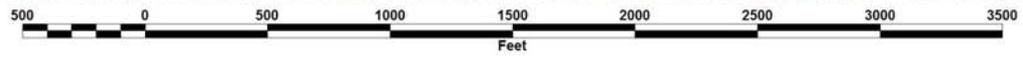
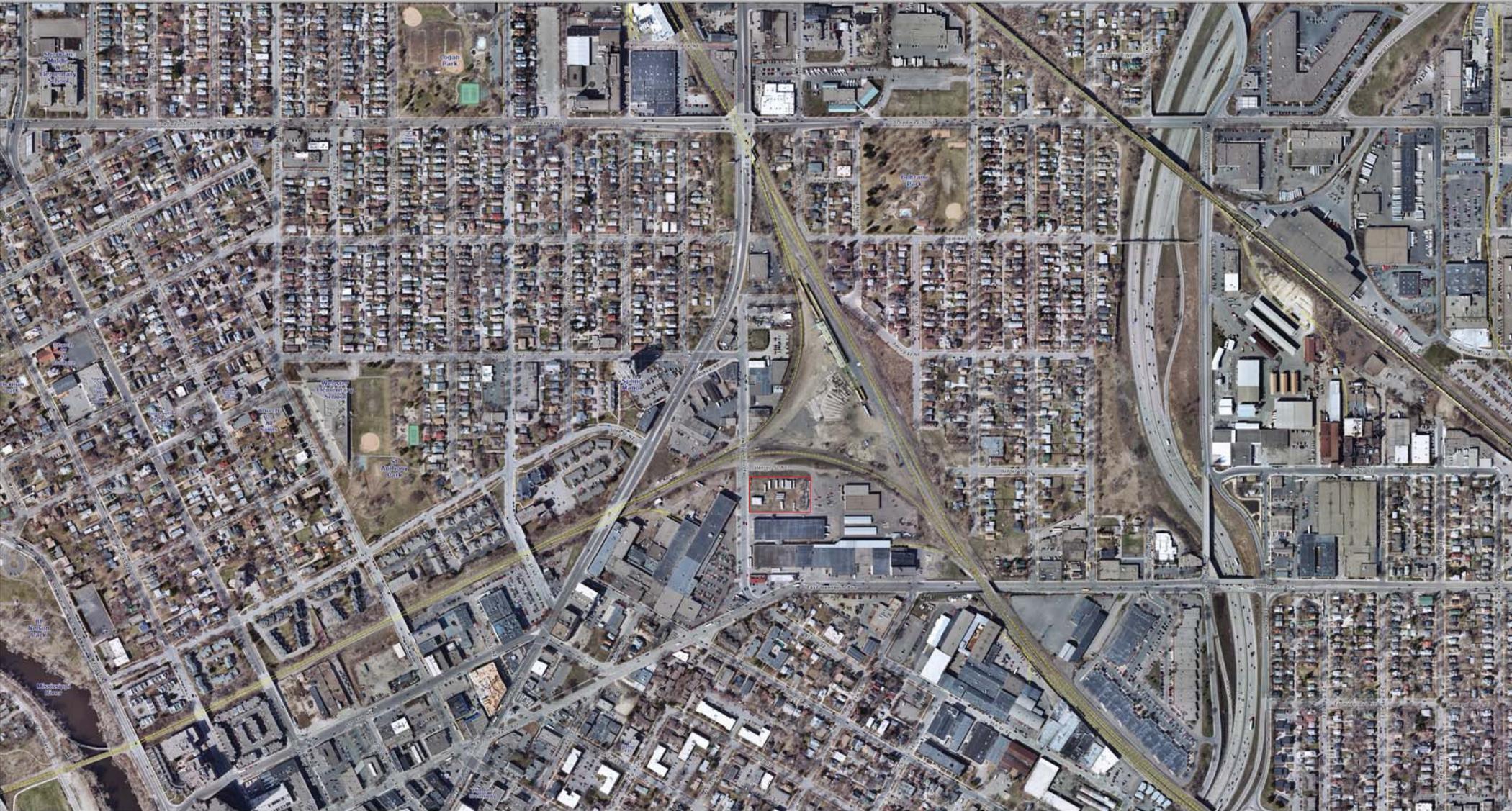


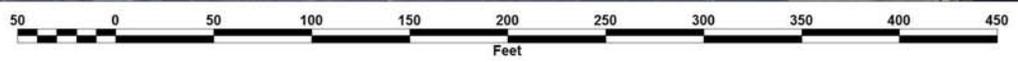
PROPERTY ADDRESS

**347-359 Harrison Street NE and 980 Winter Street NE**

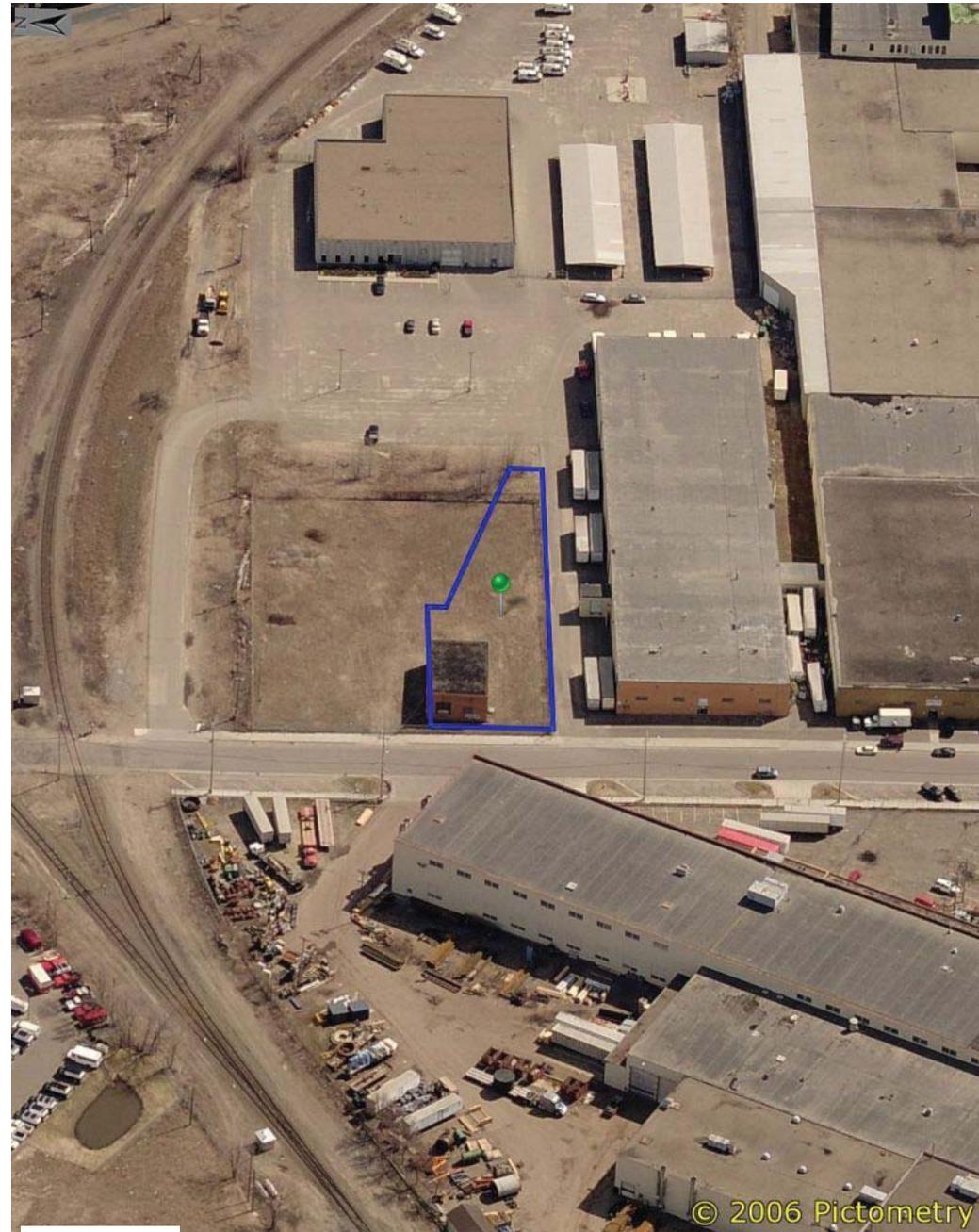
FILE NUMBER

**BZZ-7696**

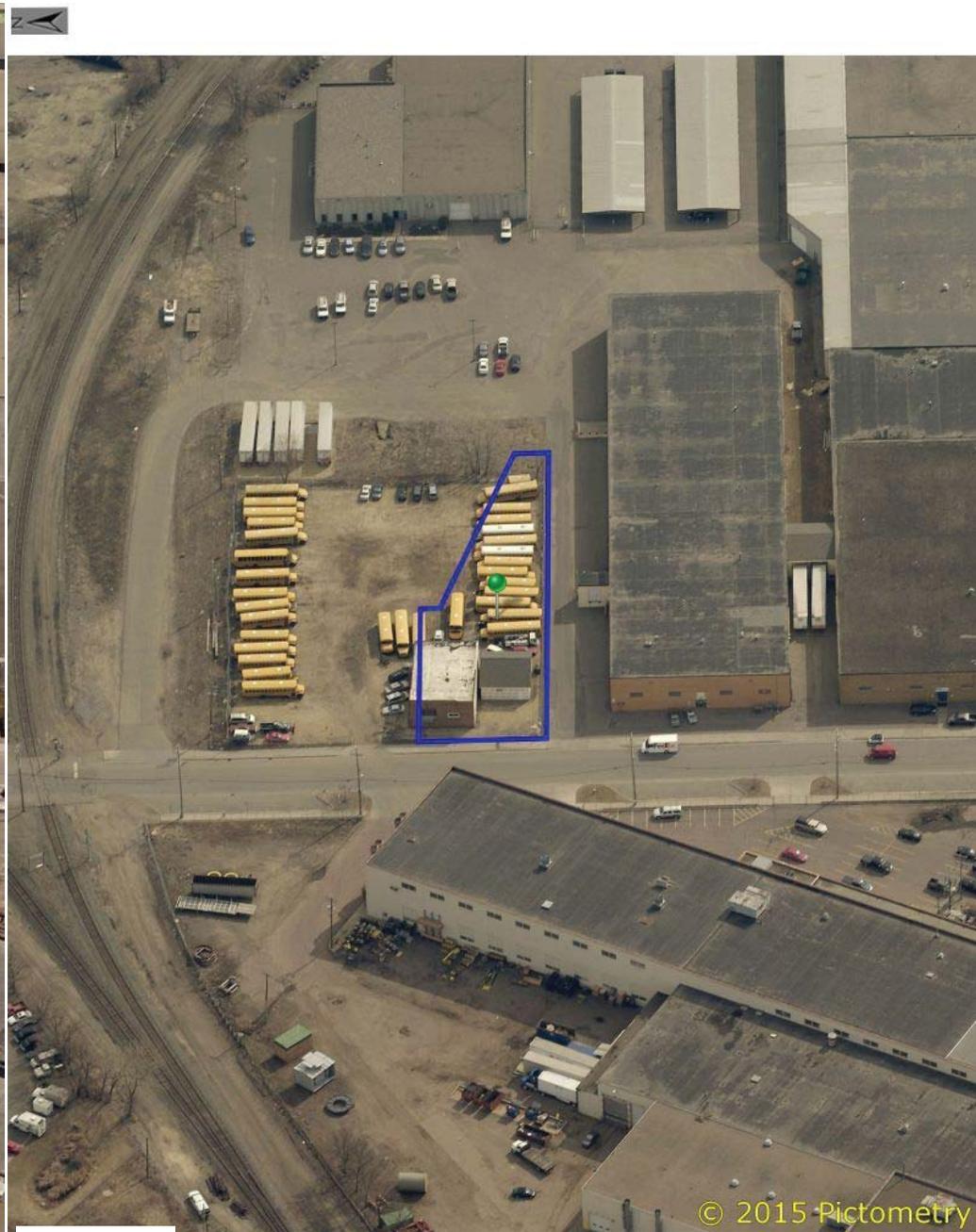




# 2006 and 2015 Comparison



04/08/2006



04/01/2015



May 25, 2016

Conditional Use Permit Application for 347 Harrison Street NE

Statement of Proposed Use

The property owner, 347 Harrison Properties, LLC, will be leasing the property at 347 Harrison Street NE to Bille Bus Transportation for use as storage lot for school buses and vans, as well as a garage for bus repair.

Bille Bus Transportation has approximately 45 employees. The employees come to work around 5:30am to start their morning bus runs. They typically park on available parking spaces on Harrison Street. The buses return around 8:30 – 9:00am and most of the employees leave at this time. The employees return around 2:00-2:30pm to start the afternoon bus runs. The drivers return around 5:30 – 6:00pm.

The vans are part of the normal bus operations. It is not a stand-alone service. The vans are used when the travel will be a long distance or they need to go places that buses can't access. The vans are dispatched by the bus dispatchers. The drivers take the vans to their homes and keep them there. The vans only return to the site for repairs. 4 – 5 spare vans are kept on site and approximately 30 vans are kept by the drivers.

The building on the site contains two repair bays for the buses and vans.

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1108 W. Broadway Ave.  
Minneapolis, MN 55411



Written Statement of Finds for Conditional Use Permit for 347  
Harrison Street NE for use as a Bus Storage Lot

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.**

The bus parking lot is located in an area zoned for industrial uses. The streets that the buses use immediately surrounding the site are all located in the industrial district. The nearest residence is located more than 750' from the site. The use of the site as a bus storage lot will have no impact on any residential area. The proposed enhancements to the site will improve the visual appearance of the bus storage lot from the street.

- 2. The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.**

The area adjacent to the bus storage lot is industrial uses. The parking lot is consistent with other uses in the area. The nearest residential area is over 750' the bus storage lot. Due to the distance, the residential neighborhood would not be affected by an activities at the parking lot. The storage lot has been there for a long period of time and has not impeded development on adjoining properties in the past, mainly because it is a use consistent in character with the industrial area that it is located in.

- 3. Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.**

All utilities required for the operation of the bus storage lot are present and no additional utilities are needed. The majority of the bus storage lot drains to the east where there is an existing stormwater catch.

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1108 W. Broadway Ave.  
Minneapolis, MN 55411

**4. Adequate measures have been or will be taken to minimize traffic congestion in the public streets.**

Access to the site is from Harrison Street NE. Major collector streets connected to Harrison St NE are Central Ave. and E. Hennepin Ave. From the major collector streets the buses will pass by other industrial uses and no residential uses.

**5. The conditional use is consistent with the applicable policies of the comprehensive plans.**

Under the current comprehensive plan the parking lot is located in a Industrial use area. Since the parking lot will have a low impact; ie, few utilities, light intensity of activity, not polluting the site, it would not preclude the parking lot from evolving to other uses in the further if surrounding development would change.

**6. The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.**

The parking lot will conform to all applicable regulations of the district. Primarily, landscape screening will be added to the perimeter of the lot to meet the screening requirements of the zoning code.



## Request for Variance for 347 Harrison Street NE for use as a Bus Storage Lot

The applicant is requesting a variance to allow the use of Recycled Asphalt Paving (RAP) as a surface material for the bus storage lot at 347 Harrison Street NE. The applicant believes that the Recycled Asphalt Paving meets all the requirements of the zoning ordinance for acceptable paving materials.

The applicable zoning ordinance, Section 541.300 – Surfacing states “All open-street parking areas, all driveways leading to such parking areas, and all other areas upon which motor vehicles may be located, except where accessory to a single-family dwelling, shall be surfaced with dustless, all-weather hard surface material capable of carrying a wheel load of four thousand (4,000) pounds for a minimum of twenty (20) feet from the curb line. Acceptable hard surfacing materials shall include asphalt, concrete, brick, cement pavers or similar materials installed and maintained per industry standards.”

Recycled Asphalt Paving is similar to standard asphalt paving except that it consists of old asphalt that has been ground during milling operations instead of virgin material. The Recycled Asphalt Paving is installed similar to standard asphalt paving. It is installed a minimum of 4” over a compacted subgrade. The material will harden and compact the same way as virgin asphalt material.

Recycled Asphalt Paving is dustless and all-weather and will support a wheel load as specified in the zoning ordinance.

The Recycled Asphalt Paving is a more sustainable material since virgin material, made from petroleum is not being used. The paving is also pervious, which will allow storm water to percolate to the ground underneath, rather than running to the storm drain.

Recycled Asphalt Paving has all of the characteristics as typical

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Minneapolis, MN 55411

asphalt paving, except it doesn't use petroleum like virgin material does and it is pervious.

The applicant believes that this material is comparable in performance to asphalt paving without using additional petroleum, thus helping the environment. We believe that this material should not only be allowed but also encouraged.

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1108 W. Broadway Ave.  
Minneapolis, MN 55411

# Truck Central North

## Millings Specialist



Asphalt Millings are a substantial upgrade to a gravel driveway or parking lot and offer many of the same benefits as a traditional asphalt surface.

- Long Lasting - Asphalt Millings adapt well to extreme weather and hold up without refinishing or repaving. Upon installation asphalt millings will be hard and compact but will continue to harden over time, thus eliminating dust and mud.
- Maintenance Free - Unlike traditional asphalt, asphalt millings adapts will to extreme weather conditions and holds up without refinishing or reaping as frequently as other materials. Typically Asphalt Millings are more ice resistance and snow melts faster than on pure asphalt. High quality Asphalt Millings are said to be as good as "virgin" asphalt for durability.

Jessica Timmons  
Cell: 651.239.9928  
Email:  
[sales@millingspecialist.com](mailto:sales@millingspecialist.com)

13830 Square Lake Tr N  
Stillwater, MN 55082



### Asphalt Millings Durability:

Millings Specialist uses only quality Asphalt Millings from reliable and consistent sources that have been crushed for a better and more durable finished product.

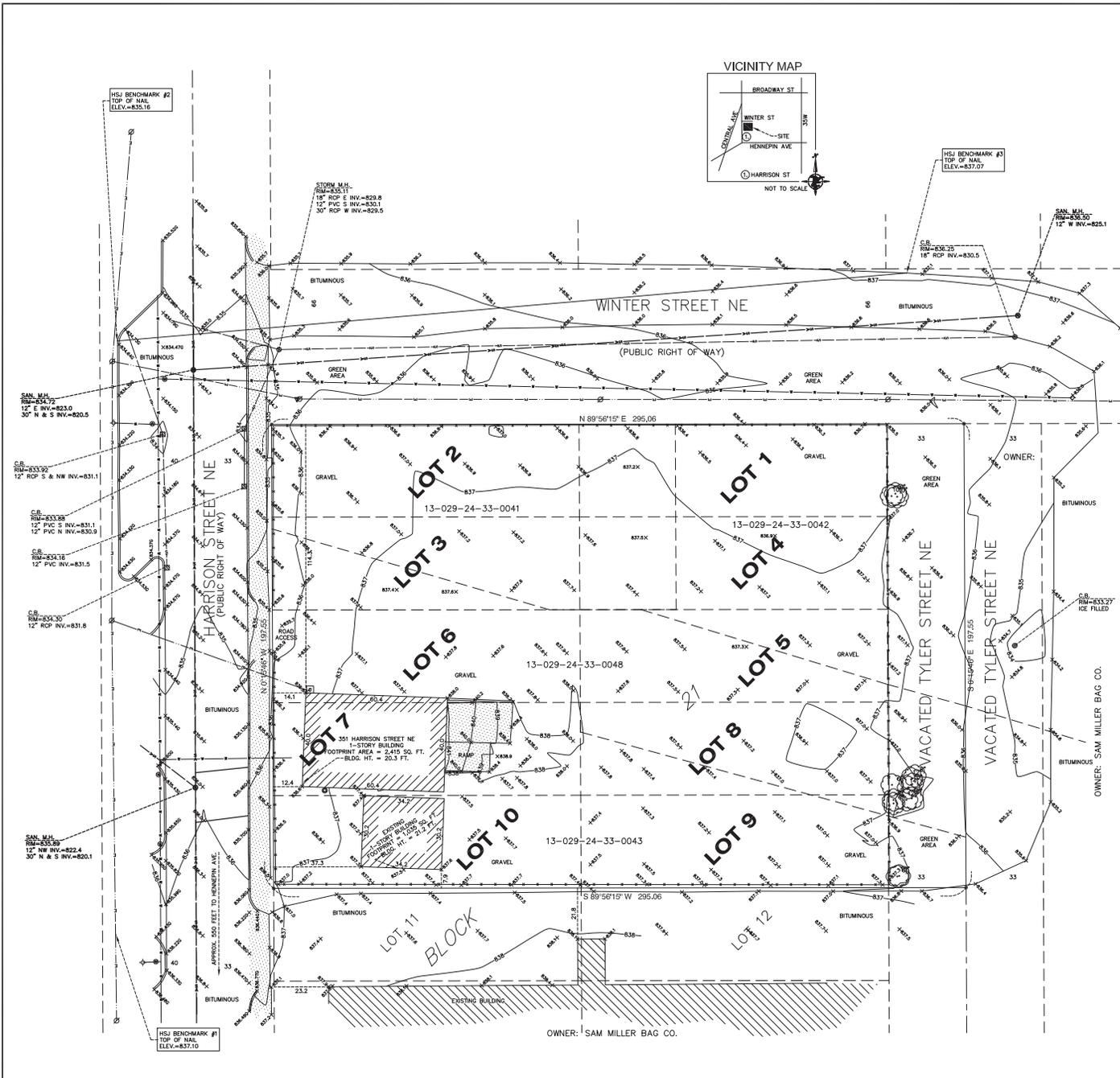
- Always installed at a minimum 4" depth
- Can withstand heavy traffic including heavy equipment (dump trucks, trailers, buses, etc.) Extra 1" depth recommended
- Demonstrated at 441 Villuame Ave, South Saint Paul, MN 55075

### Asphalt Milling Sustainability:

- Creates a pervious surface avoiding excess water run off and will not wash away during heavy rain
- Uniquely installed using road graders and compaction rollers to create an immediate hard and compact surface avoiding material displacement due to wind or rain

Jessica Timmons  
Cell: 651.239.9928  
Email:  
[sales@millingspecialist.com](mailto:sales@millingspecialist.com)

13830 Square Lake Tr N  
Stillwater, MN 55082



**LEGAL DESCRIPTION**

PID: 13-029-24-33-0041  
 Commencing at the northwest corner of Lot 2; thence east to a point on the north line of Lot 1 distant 80.00 feet west from northeast corner thereof; thence south parallel with east line of Lots 1 and 4 to south line of Lot 4; thence west to a point in south line of Lot 3 distant 13.00 feet west from southeast corner thereof; thence northwesterly to a point in west line of said Lot 3 distant 5.00 feet south from northwest corner thereof; thence north to beginning.

PID: 13-029-24-33-0042  
 Commencing at the northeast corner of Lot 1 thence west along the north line thereof 90.00 feet; thence south parallel with east line of Lot 1 and 4 to south line of Lot 4; thence west along south line of said Lot 4 a distance of 15.00 feet; thence southeasterly to a point in east line of Lot 5 distant 32.00 feet south from northeast corner thereof; thence north to the point of beginning. Including adjacent half of vacated street.

PID: 13-029-24-33-0043  
 Lots 7 and 10 and that part of Lots 8 and 9 lying southwesterly of a straight line running from a point in the west line of Lot 8 distant 12.60 feet south from the northwest corner thereof, to a point in the east line of Lot 9 distant 9.00 feet south from the northeast corner of said Lot 9. Including adjacent half of vacated street.

PID: 13-029-24-33-0048  
 Commencing at a point in east line of Lot 9 distant 9.00 feet south from northeast corner thereof; thence northwesterly to a point in west line of Lot 8 distant 12.60 feet south from northwest corner thereof; thence north to southeast corner of Lot 6; thence west to southwest corner thereof; thence north along west line of Block 21 to a point distant 5.00 feet south from northeast corner of Lot 3; thence southeasterly to a point in the south line of said Lot 3 a distance of 13.00 feet west from southeast corner thereof; thence east along the south line of said Lot 3 and north line of Lot 5 to a point distant 105.00 feet west from northeast corner of Lot 5; thence southeasterly to a point in east line of said Lot 5 distant 32.00 feet south from northeast corner thereof; thence south to the point of beginning. Including adjacent half of vacated street.

**GENERAL NOTES:**

- The bearing system used is assumed.
- The location of the underground utilities shown hereon, if any, are approximate only, PURSUANT TO MSA 2160 CONTACT Gopher State One Call At (612) 454-0002 PRIOR TO ANY EXCAVATION.
- Subject property is identified as being in "Zone X, Other Areas" on Flood Insurance Rate Map No. 270303076E, effective date September 2, 2004.
- Site area = 58,290 square feet = 1.338 acres.
- Elevation datum is based on NAVD 88 data. HSBenchmarks #1 is located Top of Nail (AS SHOWN ON SURVEY) Elevation = 837.07.
- No current title work was furnished for the preparation of this survey. Legal description, recorded or unrecorded easements and encumbrances are subject to revision upon receipt of current title work. NOTE: Property descriptions per Hennepin County Tax Information.
- This survey was made on the ground.

**LEGEND**

	Property Monument
	Concrete
	Concrete Curb
	Fence
	Overhead Electric
	Underhead Electric
	Overhead Telephone
	Underhead Telephone & Electric
	Water
	Gas
	Sanitary Sewer
	Storm Sewer
	Catchbasin
	Hydrant
	Power Pole
	Unknown Manhole
	Catchbasin
	Deciduous Tree (Diameter in Inches)
	Light Pole
	Gas Meter
	Telephone Box
	Water Manhole
	Sanitary Manhole
	Storm Manhole
	Existing Contour
	Existing Spot Elevation
	Existing Spot Elevation



**SCALE: 1 INCH = 20 FEET**

**REVISIONS**

Date:	Update Utility Information
2/4/16	

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Registered Land Surveyor under the laws of the State of Minnesota

Thomas E. Hodorff, L.S.  
 Minnesota Reg. No. 23677  
 Date: January 27, 2016

**BOUNDARY AND TOPOGRAPHICAL SURVEY**

**For:**

MILL PLACE

**SITE:**

980 WINTERS STREET NE AND  
 347, 351 & 359 HARRISON STREET NE

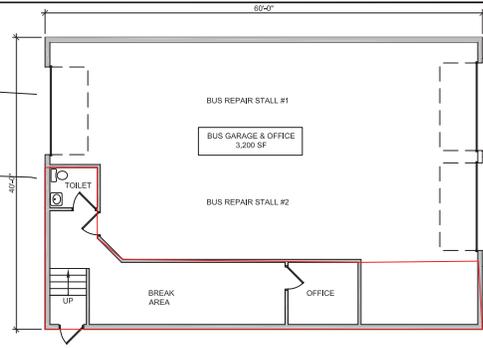
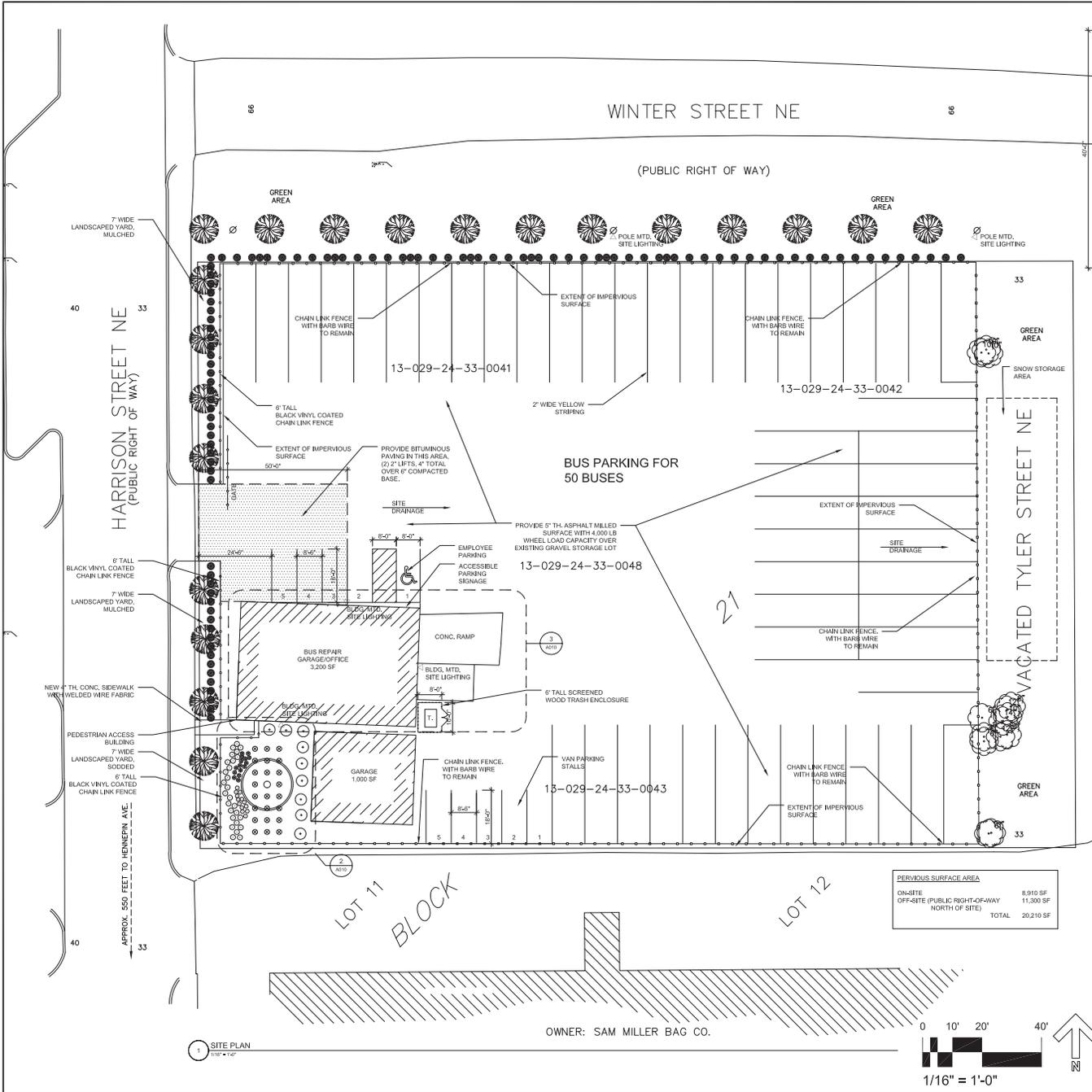
MINNEAPOLIS, MINNESOTA  
 HENNEPIN COUNTY

**HARRY S. JOHNSON CO., INC.**  
**LAND SURVEYORS**

9063 Lyndale Avenue South  
 Bloomington, MN. 55437  
 Tele. 952-884-5341 Fax 952-884-5344  
 www.hsjlandsurveyors.com

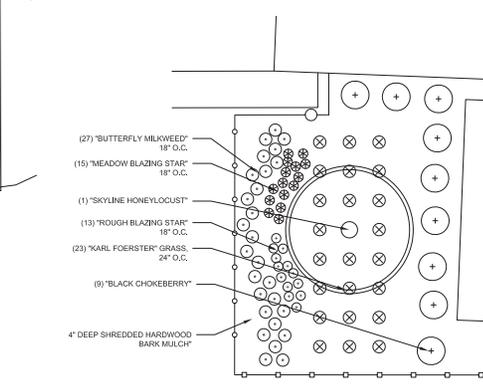
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Page	32
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**13-9282**  
 W.O. Number  
**2016110**



SYMBOL	QUANT.	COMMON NAME	PLANT SCHEDULE			COMMENTS
			SCIENTIFIC NAME	SIZE	ROOT COND.	
	21	SWAMP WHITE OAK	QUERCUS BICOLOR	2.5' CAL	B & B	1
	1	SKYLINE HONEYLOCUST	GLEDITSIA TRIACANTHOS	2.5' CAL	B & B	1
	101	LITTLE GIANT GLOBE ARBORVITAE	THUUS OCCIDENTALIS 'LITTLE GIANT'	#5	CONTAINER	1.4
	9	BLACK CHOKEBERRY	ARONIA MELANOCARPA ELATA	#2	CONTAINER	1
	23	KARL FOERSTER	CALAMAGROSTIS ACUTIFLORA	#1	CONTAINER	1.3
	13	ROUGH BLAZING STAR	LIATRIS ASPERA	#4	CONTAINER	1.2
	15	MEADOW BLAZING STAR	LIATRIS LIGULISTYLIS	#4	CONTAINER	1.2
	27	BUTTERFLY MILKWEED	ASCLEPIAS TUBEROSA	#4	CONTAINER	1.2

- NOTES:
- MULCH TO BE SHREDDED HARDWOOD BARK @ 4" DEPTH
  - SPACE @ 18" O.C.
  - SPACE @ 24" O.C.
  - SPACE @ 36" O.C.



**Mobilize Design Architecture**  
P.O. Box 11093  
1108 W. Broadway Ave.  
Minneapolis, MN 55411  
p 612.208.0504  
www.mobilizedesign.net

**Bille Bus Transportation**  
347 Harrison Street  
Minneapolis, MN

NO.	DATE	DESCRIPTION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the State of Minnesota.

David J. Witt, R. License # 22911  
**SITE PLAN**

SCALE AS NOTED  
DRAWN BY DAW  
CHECKED BY  
PROJ. ARCH.  
PROJ. MGR.  
JOB NO.  
Mobilize Design Architecture, LLC 2012

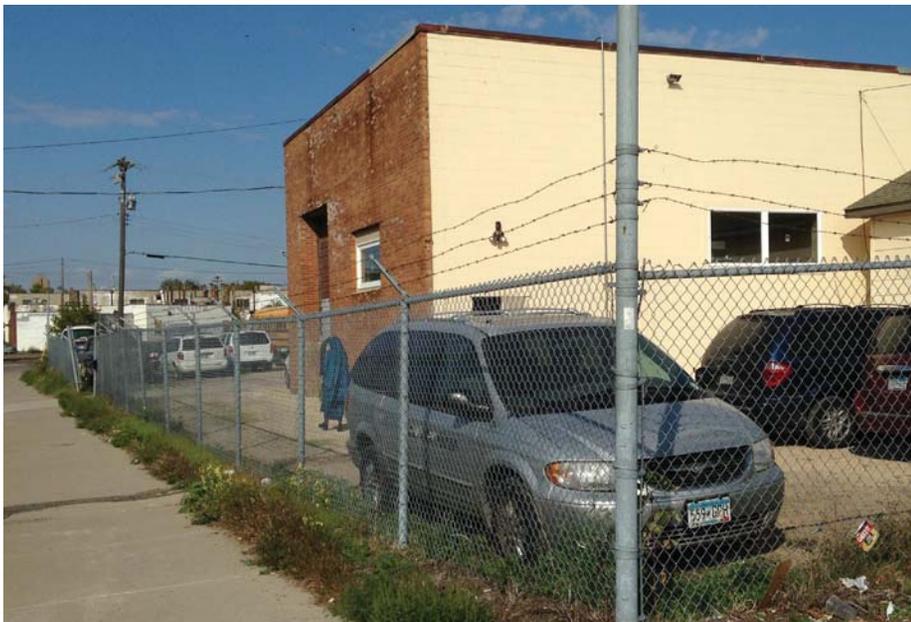
**A010**



Subject building along Harrison Street Northeast



Neighboring property along Harrison Street Northeast to the west (800 Central Avenue Northeast)





View of adjacent parcel to the East



View of adjacent parcels to the North



View of adjacent parcels to the Northeast



View of adjacent parcels to the Northwest



View of subject property and adjacent parcels looking south



View of East property line



View of subject property looking North



View into subject property looking Southwest



View of North property line looking East



View into subject property looking Southeast



View into subject property looking South



View of South property line



Minneapolis Development Review  
250 South 4<sup>th</sup> Street  
Room 300  
Minneapolis, MN 55415

## Preliminary Development Review Report

**Development Coordinator Assigned:** **MATTHEW JAMES**  
**(612) 673-2547**  
**matthew.james@minneapolismn.gov**

<b>Status *</b>
<b>RESUBMISSION REQUIRED</b>

<b>Tracking Number:</b>	PDR 1001463
<b>Applicant:</b>	HARRISON PROPERTIES P.O. BOX 581398 MINNEAPOLIS, MN 55458
<b>Site Address:</b>	347 HARRISON ST NE
<b>Date Submitted:</b>	29-JUN-2016
<b>Date Reviewed:</b>	11-JUL-2016

### Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: [http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions\\_home.asp](http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp).

**DISCLAIMER:** *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

### Project Scope

Improvements for bus storage lot.

### Review Findings (by Discipline)

#### Zoning - Planning

- Proposed project requires the following land use applications to lawfully establish a bus garage and maintenance facility:
  - Conditional use permit.
  - Variance of the surfacing requirements for off-street parking and loading areas.
  - Site plan review.

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\***Approved:** You may continue to the next phase of developing your project.

\***Resubmission Required:** You cannot move forward or obtain permits until your plans have been resubmitted and approved.

- It is anticipated that the project will go to the July 18 planning commission meeting.

#### ❑ Right of Way

- The plan as submitted meets the requirements of the Public Works Right-Of-Way Division.

#### ❑ Street Design

- The plan as submitted meets the requirements of the Public Works Street Design Division.

#### ❑ Sidewalk

- The plan as submitted meets the requirements of the Public Works Sidewalk Inspections Division.

#### ❑ Traffic and Parking

- The plan as submitted meets the requirements of the Public Works Traffic & Parking Services Division.

#### ❑ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

#### ❑ Sewer Design

- Stormwater Management: Please provide a tabulation of the disturbed area square footage, and the square footage of the existing and proposed impervious surfaces within that disturbed area. If the disturbed area is greater than 1 acre, the site would be subject to the stormwater management requirements of Chapter 54 of the Minneapolis Code of Ordinances.
- Surface Drainage: Please provide a proposed grading plan in sufficient detail to depict the proposed drainage patterns of the site. This would include proposed spot elevations and contours. The existing conditions survey should be used as a base for this. It must be demonstrated that the proposed conditions do not adversely impact adjacent properties as it relates to stormwater runoff.
- Please identify the location of any existing or proposed roof drains or discharges.
- If surface drainage is directed onto an adjacent property, private easements and/or agreements may be needed allowing the site to discharge onto adjacent properties. If this is the case, please provide copies of these easements and/or agreements. The documents will need to be recorded with the County.
- Erosion Control: An erosion and sediment control plan in conformance with Chapter 52 of the Minneapolis Code of Ordinances is required for the project. Please provide a copy of the erosion and sediment control plan for review.
- Non Stormwater Discharges: Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or [jeremy.strehlo@minneapolismn.gov](mailto:jeremy.strehlo@minneapolismn.gov).

#### ❑ Construction Code Services

- Access aisle (accessible parking stall) shall be provided with a "No Parking" sign (502.4.4) Resubmittal not required.

END OF REPORT