

LAND USE APPLICATION SUMMARY

Property Location: 815-827 6th Street South, 810 7th Street South, and 601 Chicago Avenue
Project Name: CHDC-Workforce Housing
Prepared By: Aaron Hanauer, Senior City Planner, (612) 673-2494
Applicant: First Covenant Church Minneapolis
Project Contact: Scott Beckman, Urban Works Architecture
Request: To allow for the construction of a mixed use development with 159 units, 12,364 square feet of commercial space, and a commercial parking facility.

Required Applications:

Conditional Use Permit for a Planned Unit Development	To allow a mixed-use, planned unit development (PUD) that will retain the existing church buildings and build a new mixed-use building with 159 dwelling units, 12,400 square feet of retail space, and a principal parking facility in the B4N/ Downtown Neighborhood District and DP/ Downtown Neighborhood Overlay District
Conditional Use Permit	To allow a 131-space, underground principal parking facility in the B4N/Downtown Neighborhood District.
Conditional Use Permit	To allow a surface parking lot in the DP/Downtown Neighborhood Overlay District.
Variance	To allow a 48-space surface parking lot in the DP/Downtown Neighborhood Overlay District.
Site Plan Review	For a planned unit development that includes a new six-story, mixed-use building with dwelling units, retail, and a principal parking facility.
Preliminary and final plat	Preliminary and final plat

SITE DATA

Existing Zoning	B4N/ Downtown Neighborhood District DP/ Downtown Neighborhood Overlay District
Lot Area	92,718 square feet / 2.13 acres
Ward(s)	Within Ward 6, Adjacent to Ward 3 and 7
Neighborhood(s)	Within Elliot Park; adjacent to Downtown East
Designated Future Land Use	Public and Institutional
Land Use Features	Commercial Corridor (Chicago Avenue) Growth Center (Downtown)
Small Area Plan(s)	<u>Downtown East/North Loop Master Plan</u> (2003)

Date Application Deemed Complete	June 30, 2016	Date Extension Letter Sent	N/A
End of 60-Day Decision Period	August 29, 2016	End of 120-Day Decision Period	N/A

BACKGROUND

SURROUNDING PROPERTIES AND NEIGHBORHOOD.

The project site is located just to the south of the new U.S. Bank Stadium. The Hennepin County Medical Center (HCMC) campus is located to the north and west of the project site; the Hennepin County Energy Center and an apartment building is located to the east. The U.S. Bank Stadium platform for the Blue and Green line Light Rail is located two blocks to the north. There is a dedicated bike lane along 6th Street South. Sixth Street South provides one-way traffic eastbound for vehicles and 7th Street South provides one-way traffic westbound.

SITE DESCRIPTION AND PRESENT USE.

After a proposed land swap with the owners of the Hubert’s Café and Sports Bar (now known as Erik the Red), First Covenant Church Minneapolis will own the entire city block bound by Chicago Avenue, 6th Street South, Bud Grant Way, and 7th Street South with the exception of approximately 16,500 square feet at the Chicago Avenue and 6th Street South intersection. The existing First Covenant Church buildings, which are connected to each other, front 7th Street South. Currently, First Covenant Church has a surface parking area on the project site that provides commercial parking for approximately 200 vehicles.

PROJECT DESCRIPTION.

First Covenant Church Minneapolis in partnership with Community Housing Development Corporation (CHDC) is proposing a planned unit development that includes the retention of the existing church buildings and the construction of a mixed-use, six-story, building that includes: 159 units of affordable housing ranging from studios to three bedrooms, an approximately 12,400 square foot commercial storefront along 6th Street South (anticipated to be a sports and health facility), and a principal parking facility underground that will accommodate 131 vehicles. In addition, the applicant is proposing to retain 48 surface parking stalls for the church uses and the proposed retail use. It should be noted that there is currently a billboard on the project site that is proposed to be moved. The moving of the billboard is being handled through a separate land use application.

RELATED APPROVALS.

Planning Case #	Application	Description	Action
BZZ-7498	Conditional use permit	To allow an overnight shelter	Approved
BZZ-7087	Conditional use permit	To allow for a high school	Approved

PUBLIC COMMENTS. On May 24, 2016, the Elliot Park Neighborhood Association wrote a letter of support for the project. Any additional correspondence received after the publication of the staff report and prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT - PLANNED UNIT DEVELOPMENT

The Department of Community Planning and Economic Development has analyzed the application to allow a mixed-use, planned unit development that will retain the existing church buildings and build a new mixed-used building with 159 dwelling units, 12,400 square feet of retail space, and a 131-space principal parking facility in the B4N/ Downtown Neighborhood District and DP/ Downtown Neighborhood Overlay District based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The establishment of a mixed-use, planned unit development at this location will not be detrimental to public health, comfort, or general welfare provided the development complies with all applicable building codes and life safety ordinances as well as Public Works Department standards. The site is located in the downtown growth center, which encourages mixed-use development like the proposed development.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The project would not be injurious to the use and enjoyment of other property in the vicinity. The surrounding area is nearly fully developed with a variety of uses including the new U.S. Bank Stadium to the north, the Hennepin County Medical Center (HCMC) campus to the south and west, and the Hennepin County Energy Center and apartment building to the east.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Adequate utilities and drainage would be provided as part of the project. The applicant received initial City of Minneapolis department comments (including Public Works) at a preliminary development review meeting on May 12, 2016. The development team will be required to continue to work closely with Public Works, Construction Code Services, Land Use, Design and Preservation staff and the various utility companies during the duration of the development should the applications be approved to meet these requirements. This would be required to ensure that all procedures are followed and that the development complies with all City and other applicable requirements. Adequate access roads are also provided. Vehicles are proposed to enter and exit the site via three curb cuts: two on Chicago Avenue and one on 7th Street South. This configuration received initial Public Works support.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Adequate measures have been taken to minimize traffic congestion in the public streets. This includes the following: providing a minimum of 10 shared bicycles that the public will have access to, bike parking that exceeds minimum parking requirements (see site plan review application for full analysis), two bicycle maintenance stations, public access to two on-site shared vehicle, and a commitment to the travel demand management plan recommendations that encourage alternative modes of transportation including having a TDM liaison, display of commuter information and assessment of commuting habits.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- 1.1.3 Encourage the use of flexible regulatory options that promote high quality development, such as the Planned Unit Development (PUD) tool.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- 1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- 1.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- 1.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

- 1.5.1 Support an appropriate mix of uses within a district or corridor with attention to surrounding uses, community needs and preferences, and availability of public facilities.

Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

- 1.15.1 Support development of Growth Centers through planning efforts to guide decisions and prioritize investments in these areas.
- 1.15.2 Support the intensification of jobs in Growth Centers through employment generating development.
- 1.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

- 6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the B4N/ Downtown Neighborhood District and the DP/ Downtown Neighborhood Overlay District.

Findings Required for Planned Unit Developments:

- A. The planned unit development conforms to the applicable standards for alternatives and amenities. All planned unit developments shall provide at least one amenity or a combination of amenities that total at least 10 points, beyond those required for any alternatives. For each alternative requested, amenities shall total at least five points.

Alternatives requested:

Number of principal residential structures.

Alternative requested. As outlined in Section 527.130-Number of Principal Residential Structures, more than one (1) principal residential structure may be placed on one (1) platted or recorded lot in a planned unit development. The appearance and compatibility of individual buildings to other site elements and to surrounding development shall be given primary consideration in reviewing and approving the placement and spacing of structures. The city planning commission may consider this alternative without the addition of amenities beyond those required for a planned unit development in section 527.120(1). When completed, the project will have one principal residential structure (i.e. the new proposed mixed-use building) and one principal nonresidential building (i.e. the existing church buildings). CPED is supportive of this alternative, given the applicant's proposed site plan provides outdoor space for the tenants in the residential building, includes an appropriate separation between the existing and proposed building, and there is landscaping between the buildings to help with the transition from residential to nonresidential.

Bulk regulations, floor area ratio (FAR).

No alternative requested. When completed, the project will have a floor area ratio of 2.4. The B4N Zoning District has a minimum floor area ratio of 2.0 and no maximum floor area ratio allowance.

Bulk regulations, building height.

No alternative requested. When completed, the height of the proposed building will be six stories and 70.4 feet, well below the allowed 10 stories and 140 feet allowed in the B4N Zoning District.

Lot area requirements.

No alternative requested. There is not a lot area requirement per dwelling unit in the B4N Zoning District.

Required yards.

Alternative requested. As required by Section 549.120-Yard requirements, the second through six floors of the building which have residential units, are required to provide a 15 foot setback from the west interior property line. The applicant is proposing a 13 foot building setback along this property line.

As outlined in Section 527.160-Yards and Table 527-2 Authorized Alternatives, the city planning commission may authorize reductions in or elimination of required yards provided landscaped yards of at least such minimum width as required by the zoning district in which the planned unit development is located shall be maintained along property lines abutting a side or rear lot line of a residential district or property lines abutting a side or rear lot line of a structure used for permitted or conditional residential purposes. The project is not subject to a landscaped yard along this property line. However, the applicant is proposing to have a seven foot wide landscape buffer along this property line (along with a walkway to connect the new building entrances to the public sidewalk).

Building placement in PO Pedestrian Oriented Overlay District.

Not applicable.

On-premise signs.

No alternative requested. As proposed, the master sign plan complies with all sign regulations.

Off-street parking and loading.

No alternative requested. As outlined in the site plan review section below, the proposed project complies with their off-street parking and loading requirements.

Points required for alternatives:

- Establishment of the PUD – **10 points.**
- Required yards – **5 points**

Total = 15 points.

Phasing plan.

Not applicable. Development is proposed to take place on a continuous basis.

Amenities provided:

The applicant is proposing the following amenities from Table 527-1, Amenities to meet the required points for authorized alternatives.:

Outdoor children's play area (5 points): The applicant is proposing an outdoor children's play area that meets all of the planned unit development requirements for outdoor children's play areas. The active, outdoor play area is proposed to be approximately 2,100 square feet, It will be secure (enclosed by a fence), separated from parking and maneuvering area, and designed to facilitate adult supervision. The applicant is proposing to reuse the existing playground equipment that is on the subject property (see L102 for images). The equipment will be reinstalled per the manufacturer's specifications and is suitable for children in both preschool and elementary schools. The play equipment will not be located in a required yard. The applicant states that the play area will be maintained throughout the year and cleared of snow. Building placement will also help reduce winter winds.

Reflective roof (3 points): Points for this amenity are awarded for a building that utilizes roofing materials for seventy-five (75) percent or more of the total roof surface having a Solar Reflectance Index (SRI) equal to or greater than the values as required by the US Green Building Council (USGBC) for low-sloped roofs. The applicant is proposing a fully adhered white membrane system for the entire building that meets or exceeds the Solar Reflectance Index (SRI) required by the US Green Building Council. As a condition of approval, roof details shall be submitted prior to building permit issuance that show compliance with the planned unit development reflective roof standards.

Shared bicycles (3 points) Points for this amenity are awarded for public access to shared bicycles available for short-term use as defined in section 541.180. A minimum of ten (10) shared bicycles per one (1) commercial use must be provided to qualify as an amenity. Bicycle parking spaces and racks shall be located in an area that is convenient and visible from the principal entrance of the building. The applicant is proposing to install a minimum of 10 bicycles along 6th Street South that will be available to the public. The applicant is proposing to work with Nice Ride to install a bike station at this location. If that partnership does not work out, the applicant is committed to providing their own bike share station that meets the standards for the shared bicycle amenity outlined in Table 527-1. As a condition of approval, a shared bicycle amenity that meets the standards outlined in Table 527-1 Amenities shall be maintained for the duration of the planned unit development.

Shared vehicles (3 points) Points for this amenity are awarded for projects that provide access to a shared passenger automobile available for short-term use. For residential uses, a minimum of one (1) car per one hundred (100) dwelling units is required. The applicant is proposing to have two shared vehicles on site parked in the surface parking lot. As a condition of approval, the applicant shall dedicate and mark two parking spaces in the surface parking lot for the shared vehicles.

Recycling storage area (1 point) Points for this amenity are awarded for provision of an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely below grade or entirely enclosed within the building. The applicant is proposing to have one recycling storage area for the entire project that would be located in the basement of the proposed building. The recycling storage area will be located approximately in the middle of the building and will be conveniently located for the uses within the church as the church will have direct access to the underground parking garage.

Table 1. Amenity Points Summary

Amenity	Points Requested by Applicant	Points Recommended by Staff
Outdoor children’s play area	5 point	5 point
Reflective roof	3 points	3 points
Shared bicycles	3 points	3 points
Shared vehicles	3 points	3 points
Recycling Storage Area	1 point	1 point
Total	15 points	15 points

B. The planned unit development conforms to the required findings for a planned unit development:

1. *That the planned unit development complies with all of the requirements and the intent and purpose of this chapter. In making such determination, the following shall be given primary consideration:*

a) *The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.*

The character of the proposed uses in the planned unit development relates to the existing uses on the subject site and the surrounding area. The proposed mixed-use development with ground floor retail and residential units will add pedestrian activity throughout the day in the area, which is encouraged in downtown. The residential component of the project has a variety of housing types. As proposed it will have 17 studios, 94 one-bedrooms, 42 two-bedrooms, and six three-bedrooms. In addition, the proposed eight walkup units along 9th Avenue South will complement the walkup units nearby in the Elliot Park Neighborhood and the 9th Street Historic District. Furthermore, the proposed outdoor spaces at grade and on the second floor on the interior of the site will be an amenity for those living within the development.

b) *The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provision of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.*

The use is not expected to contribute to traffic congestion in the public streets. The project site is within downtown and well served by mass transit. It is located within four blocks of nine bus lines and within two blocks of the US Bank Stadium light rail stop that is an access point to the METRO Blue and Green lines. In addition, as previously mentioned, the applicant is proposing to take efforts to promote alternative modes of transportation by exceeding the minimum bicycle parking requirements, and providing shared bike and vehicle opportunities on site.

A Travel Demand Management Plan (TDMP) was submitted that evaluates traffic generated by the proposed use and potential for congestion in the adjacent public streets. Results of the operational analysis indicate that the study area roadways and intersections will continue to operate acceptably without improvements.

- c) *The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.*

The site amenities of the proposed planned unit development meet this finding. The applicant is proposing open space for the residents at the interior of the site, both at grade and on the second floor within an outdoor area that will include planters. In addition, the eight walkup units that have a well landscaped edge along 9th Avenue South will provide a nice informal gathering space for the residents.

The proposed planned unit development also retains the original First Covenant Church building (historical name: Svenska Missions Tabernaklet), which is recognized as potentially being eligible for local designation or listing on the National Register.

From a sustainability standpoint, the project is proposing to include a reflective roof that will absorb less heat and in the process reduce energy bills by decreasing air conditioning needs. In addition, as previously mentioned, the project has made a strong commitment to promoting alternative modes of transportation.

From an urban design perspective, the applicant has tried to fit the building into its context by having a black metal façade along 6th Street South to complement the new U.S. Bank Stadium and brick row-house style design along 9th Avenue South and 7th Street to complement the surrounding brick buildings including the historic First Covenant Church.

- d) *The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development, and protection of views and corridors.*

The proposed six-story building is compatible with the First Covenant buildings on the project site and the scale and massing of the buildings in the immediate area. It is not anticipated that the project would have an adverse impact on the microclimate. The existing site of the proposed development is mainly a surface parking lot. With the new development, the applicant is proposing a landscape plan that exceeds minimum requirements. Finally, the proposed project will not have an adverse impact on views and corridors.

- e) *An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing, and applicable policies of the comprehensive plan and adopted small area plans.*

Currently, there is not an adjacent residential use. There is, however, a residential use on the other side of 9th Avenue South. The applicant is proposing walkup units along 9th Avenue South along with abundant landscaping onsite and in the public right of way, which will be a nice complement to the residential units to the east.

The project site takes up the entire city block with the exception of the northwest corner. There is the potential for the northwest corner of the block to be redeveloped into a residential or mixed-use development with a residential component at a later date. The applicant is proposing a landscaped edge along 9th Avenue South for the new building that will complement the green space and landscaped area in front of the existing apartment building on the east side of 9th Avenue.

- f) *The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.*

The Public Works Department reviewed how the project would impact public facilities, stormwater, and erosion control during the initial preliminary development review on May 12, 2016 (see attached). The applicant would be required to work closely with the Public Works Department, the Plan Review Section of the Inspections Department and the various utility companies during the duration of the development should the plan be approved.

- g) *The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.*

The development team is using sustainable building practices during the construction phases of the project including the recycling of the asphalt in the surface parking lot.

2. *That the planned unit development complies with all of the applicable requirements contained in Chapter 598, Land Subdivision Regulations.*

Please see the preliminary and final plat section of this staff report.

CONDITIONAL USE PERMIT-PRINCIPAL PARKING FACILITY

The Department of Community Planning and Economic Development has analyzed the application to allow a 131-space, underground principal parking facility in the B4N/Downtown Neighborhood District based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The project site is located in B4N/Downtown Neighborhood District. As outlined in Table 549-1 Principal Uses in the Downtown Districts, a parking facility that does not provide accessory parking to uses on site is a conditional use in the B4N District. The applicant is proposing a principal parking facility underneath the new building that will accommodate 131 vehicles. This parking facility will be used as a principal parking facility in which the spaces will be rented/leased out to the public.

The establishment of an underground parking facility will not be detrimental to public health, safety, comfort or general welfare provided the development complies with all applicable codes and life safety ordinances as well as Public Works Department standards.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

Approving the conditional use permit to allow a 131-space principal parking garage on the premises would not be injurious to the use and enjoyment of other property in the vicinity nor would it impede the normal or orderly development and improvement of surrounding property for uses permitted in the district. The parking ramp would be accessed from one curb cut off 7th Street South. This access point received preliminary Public Works approval. In addition, the principal parking garage will be in compliance with the Section 549.580-General District Regulations, as the principal parking garage is located entirely below grade. It is also important to note that the principal parking garage is part of a mixed-use development that will be a positive for the area and hopefully encourage more development and redevelopment with active uses nearby.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The applicant would continue to work closely with the Public Works Department, Building Plan Review, and the various utility companies during the duration of the development to ensure that all procedures are followed in order to comply with city and other applicable requirements.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Measures have been taken in regard to minimizing traffic congestion. As part of the new development, the applicant is proposing to promote alternative modes of transportation by having shared bicycles onsite, bicycle parking that exceeds their minimum requirements, and two bike maintenance facilities. In addition, the applicant is proposing to have two shared vehicles onsite to help reduce the amount of individual car driving to and from the site. It is also recognized that the project site is within the downtown core and has great transit accessibility.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

See the above listed response to finding #5 in the conditional use permit for the planned unit development as the land use policies and implementation steps identified apply to the CUP for a principal parking facility as well.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With the approval of the other conditional use permits, variance, site plan review and preliminary and final plat, this development would meet the applicable regulations of the B4N (Downtown Neighborhood) District and the DP (Downtown Parking) Overlay District.

CONDITIONAL USE PERMIT-SURFACE PARKING LOT

The Department of Community Planning and Economic Development has analyzed the application to allow a 48-space surface parking lot in the DP/Downtown Neighborhood Overlay District based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The project site is located in DP/Downtown Parking Overlay District. As outlined by Section 551.760-Conditional Uses, a parking lot requires a conditional use permit when located in the DP/Downtown Parking Overlay District. Although the project site currently has approximately 200 surface parking spaces, the applicant is proposing to reconfigure their site and have a 48-space accessory surface parking lot as part of the new development.

The establishment of a 48-space accessory surface space parking lot at this location would not prove detrimental to public health, safety, comfort or general welfare provided the development complies with all applicable codes and life safety ordinances as well as Public Works Department standards.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

Approving the conditional use permit to allow a 48-space accessory surface parking lot with conditions would not be injurious to the use and enjoyment of other property in the vicinity nor

would it impede the normal or orderly development and improvement of surrounding property for uses permitted in the district. The proposed mixed-used development project would significantly improve the appearance of the project site and reduce the amount of surface parking compared to existing conditions; the subject property currently has approximately 200 surface parking spaces visible from the public streets. With the proposed development, the 48-space surface parking lot would be screened significantly more compared to existing conditions given that it is near the interior of the site and well screened by buildings that are fronting the street and landscaping.

As outlined in Section 551.750-Prohibited uses, commercial parking lots are prohibited in the DP Overlay District. Therefore, CPED is recommending that the surface parking lot be for accessory parking only and that the use of the parking lot as a commercial parking lot is prohibited.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The applicant has completed a TDMP for the PUD. The TDMP findings support the overall amount of parking and loading spaces provided on the site, the amount of parking dedicated for each of the uses, and the access points for the various parking and loading areas on the site, including the surface parking lot.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

See the above listed response to finding #5 in the conditional use permit for the planned unit development as the land use policies and implementation steps identified there apply to supporting the CUP for a principal parking facility as well. In addition, the proposed surface parking lot would be consistent with the following transportation land use policies of *The Minneapolis Plan for Sustainable Growth*:

Transportation Policy 2.6: Manage the role and impact of automobiles in a multi-modal transportation system.

- 2.6.1 Encourage the implementation of Travel Demand Management (TDM) plans and programs that identify opportunities for reducing the generation of new vehicle trips from large developments.

Transportation Policy 2.8: Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community.

- 2.8.1 Implement off-street parking regulations which provide a certain number of parking spaces for nearby uses, while still maintaining an environment that encourages bicycle, pedestrian, and transit travel.

- 2.8.2 Design and implement incentives for shared parking and on-site car sharing programs, as well as carpooling and vanpooling.
 - 2.8.5 Continue to prohibit new commercial surface parking lots and to restrict the size of accessory surface parking lots in Downtown.
6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With the approval of the other conditional use permits, variance, site plan review and preliminary and final plat, this development would meet the applicable regulations of the B4N (Downtown Neighborhood) District and the DP (Downtown Parking) Overlay District.

VARIANCE-NUMBER OF SURFACE PARKING SPACES

The Department of Community Planning and Economic Development has analyzed the application for a variance to allow a 48-space surface parking lot in the DP/Downtown Neighborhood Overlay District based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The project site is located in DP/Downtown Parking Overlay District. As outlined by Section 551.760-Conditional Uses, an accessory surface parking lot is a conditional use in the DP/Downtown Parking Overlay District when the parking lot is located on the same lot as the principal use served and does not have more than 20 spaces. The applicant is proposing an accessory surface parking lot with 48 spaces for the onsite nonresidential uses.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The existing First Covenant church buildings, which are proposed to be retained, take up approximately 25 percent of the site. The existing buildings create limitations on where the applicant could place below grade parking.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The intent of the off-street parking regulations are to balance the off-street parking requirements that can help reduce traffic and congestions with the city's mass transit, land use, urban design, and sustainability goals. The purpose of the DP overlay district is to preserve significant and useful buildings, to protect the unique character of the downtown area and mixed-use downtown neighborhoods, and to encourage higher density development in the downtown area by restricting the establishment or expansion of surface parking lots.

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The applicant's proposal will be retaining the historic First Covenant Church building, significantly reducing the amount of surface parking on site compared to existing conditions, and screening the proposed surface parking more compared to existing conditions. In order to comply with all aspects of the DP overlay district, CPED is recommending that a condition of approval be added that requires that the surface parking lot be for accessory parking only and that the use of the parking lot as a commercial parking lot is prohibited (see conditional use permit for surface parking lot review above).

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of this variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The applicant is proposing to significantly reduce the amount of surface parking compared to existing conditions, place the parking in the interior of the site, and screen it from the public sidewalk.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

1. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building placement – Requires alternative compliance

- The first floor of the new building is located at the property line along 6th Street South and within eight feet of the front lot line along 9th Avenue South. Along 7th Street South, the building is located nine feet from the property line. Alternative compliance is required.
- The placement of the building would reinforce the street wall, maximizes natural surveillance and visibility, and facilitates pedestrian access and circulation (see sections below for additional review).
- The proposed building is located at the property line along 6th Street South. The area between the building and lot line along 9th Avenue South, 7th Street South, and Chicago Avenue include amenities such as an enhanced landscaping and bike parking.
- The applicant is proposing that onsite parking be located below grade and at the interior of the site.

Principal entrances – Meets requirements

- The building is oriented so that at least one principal entrance faces the front property line. The underlying platting shows that the front lot lines are 6th and 7th Street South. The primary pedestrian entrance for the commercial space is proposed to be along 6th Street South.
- The principal entrance along 6th Street South and the entrances along 9th Avenue South are clearly defined through the use of horizontal awnings.

Visual interest – Meets requirements

- The building walls provide architectural detail to create visual interest. This includes projecting balconies along 6th Street South, eight walkup units with awnings along 9th Avenue South, and the use of exterior materials/colors to blend in with the surroundings (e.g. black metal panel along 6th Street South to complement the new U.S. Bank Stadium) and brick along 9th Avenue South and 7th Street South to blend in with the surrounding masonry buildings). CPED encourages the applicant to change the color of the balconies from white to black along 6th Street South to blend in better with the proposed building and surroundings.
- Site plan review requires that in larger buildings, architectural elements, including recesses or projections, windows and entries, shall be emphasized to divide the building into smaller identifiable

sections. The proposed building design divides the building into two. A contemporary and slim metal panel clad box along 6th Street South and a more traditional brick clad/row house type of development along 9th Avenue South/7th Street South. Within the brick portion of the building there are recesses and projections that provide articulation and accentuate the entries.

- There are no blank, uninterrupted walls exceeding 25 feet in length.

Exterior materials – Meets requirements

- The applicant is proposing brick, metal panel, and fiber cement panel siding as the exterior building materials. All of these materials are considered durable as outlined in the Guide to Exterior Building Walls and Materials. The Guide to Exterior Building Walls and Materials states that material that create an oil-canning effect are discouraged. The applicant states that the type of metal panel that they are proposal to use along with a matte finish will prevent and/or significantly reduce any oil canning effect.
- Each elevation is in compliance with the percentage of exterior material allowed as outlined in the Guide to Exterior Building Walls and Materials, with the exception of the south elevation that has 80 percent metal panel; 75 percent is allowed. Alternative compliance is required (see Table 2).
- The proposed building will not include more than three exterior materials on an elevation excluding windows, doors, accent materials and foundation materials (see Table 2).
- The exterior materials and appearance of the elevations not facing the public street are similar to and compatible with the elevations facing the public street. The interior and rear portions of the building maintain the same exterior building design as proposed on the elevations facing the street. Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.
- Plain face concrete block is not proposed along any public streets, sidewalks, or adjacent to a residence or office residence district.

Table 2. Percentage of Exterior Materials per Elevation

Material	Allowed Max	North	South 1	South 2	East	West 1	West 2
Brick (face) (panel)	100%	19%	17%	44%	47%	19%	45%
Metal Panel	75%	74%	80%	35%	32%	54%	33%
Fiber Cement Siding	30%	7%	3%	21%	21%	27%	22%

Windows – Meets requirements

- The zoning code requires that no less than 20 percent of the walls on the first floor for residential uses that face a public street, public sidewalk, public pathway, or on-site parking lot are windows measured between two (2) and ten (10) feet above the adjacent grade. In addition, no less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows measured between the upper surface of a floor and the upper surface of the floor above. The proposed project is in compliance with these requirements (see Table 3).
- The zoning code requires that no less than 30 percent of the walls on the first floor for nonresidential uses that face a public street, public sidewalk, public pathway, or on-site parking lot are windows measured between two (2) and ten (10) feet above the adjacent grade. In order to be counted, the bottom of any window used to satisfy the ground floor window requirement may not be more than four (4) feet above the adjacent grade. The proposed project is in compliance with this requirement (see Table 3). As a condition of approval, the first floor windows required to meet the minimum window requirement for the nonresidential use, shall have clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher and shall allow views into and out of the building at eye level. Shelving, mechanical equipment or other similar fixtures shall

not block views into and out of the building in the area between four (4) and seven (7) feet above the adjacent grade for the windows required to minimum glazing percentage.

- All windows in the proposed development are vertical in proportion and are evenly distributed along the building walls.

Table 3. Percentage of Windows per Applicable Elevation

	Code Requirement		Proposed	
Residential Uses				
1 st floor-south elevation (7 th Street South)	20% minimum	110 sq. ft.	34%	190 sq. ft.
2nd floor and above-south (7 th Street South)	10% minimum	75 sq. ft.	26%	190 sq. ft.
2nd floor and above-south (facing onsite parking lot)	10% minimum	107 sq. ft.	26%	280 sq. ft.
1 st floor-west elevation (facing onsite parking lot)	20% minimum	294 sq. ft.	32%	466 sq. ft.
2 nd floor-west elevation (facing onsite parking lot)	10% minimum	257 sq. ft.	30%	761 sq. ft.
2 nd floor-north elevation (6 th Street South)	10% minimum	173 sq. ft.	27%	460 sq. ft.
1 st floor-east elevation (9 th Avenue South)	20% minimum	512 sq. ft.	37%	943 sq. ft.
2 nd floor-east elevation (9 th Avenue South)	10% minimum	326 sq. ft.	31%	1,018 sq. ft.
Nonresidential Uses				
1 st - floor-north elevation (facing 6 th Street South)	30% minimum	396 sq. ft.	46%	396 sq. ft.
1 st - floor-south elevation (facing onsite parking lot)	30% minimum	221 sq. ft.	51%	374 sq. ft.

Ground floor active functions – Meets requirements

- The first floor buildings are required to accommodate active functions by ensuring that parking, loading, storage, or mechanical equipment rooms are limited to no more than thirty (30) percent of the linear building frontage along each wall facing a public street. As proposed, all portions of the linear building frontage along 6th Street South, 9th Avenue South, and 7th Street South will have active functions.

Roof line – Meets requirements

- The principal roof line of the building will be flat, which is similar to that of surrounding buildings.

Parking garages – Meets requirements

- The proposed parking garage is underground. Therefore, the exterior design of the parking garage will not dominate the appearance of the walls and the vehicles will be screened from view.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

- There are clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities. In addition to the entrances along the street that lead directly to the public sidewalk, the applicant has proposed an internal walkway that will connect the back of the church building to Chicago Avenue, and an internal walkway that will provide access from the new building to 6th Street South.

Transit access – Meets requirements

- There is an existing bus shelter along 6th Street South that provides weather protection. The applicant is not proposing alterations to the bus stop. Public Works reached out to Metro Transit regarding this bus stop. Metro Transit did not identify this bus stop for improvements. However, CPED encourages the applicant to work with Minneapolis Public Works and Metro Transit to see if improvements could be made to this bus stop, including lighting.

Vehicular access – Meets requirements

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses. As previously mentioned, the applicant is proposing to have internal walkways separated from vehicular circulation areas that connect building entrances to the street for both the existing and proposed building.
- Curb cuts have been consolidated with the proposed project. Currently, the project site has six curb cuts. If the proposed project is approved and completed, it will have three curb cuts.
- There are no public alleys adjacent to the site.
- Service vehicle access does not conflict with pedestrian traffic. The proposed development will have protected walkways that will connect entrances in the interior of the site to the public street.
- Truck loading areas are not located next to residence or office residence districts.
- The proposed site plan minimizes the use of impervious surfaces. The applicant is proposing to significantly increase the amount of greenspace onsite and within the public right of way (see landscaping and screening section below for more detail).

LANDSCAPING AND SCREENING

General landscaping and screening – Meets requirements

- The overall composition and location of landscaped areas complement the scale of development and its surroundings. The applicant is proposing a significant amount of landscaping along 9th Avenue South and the eastern side of 7th Street South. The landscaping in these proposed areas will complement the greater amounts of landscaping found on the east side of 9th Avenue.
- As outlined in Section 530.30-Buildings and Uses Subject to Site Plan Review, the proposed project is exempt from the general landscaping and screening requirements given that it contains more than 50,000 square feet of gross floor area and is located in a Downtown zoning district. Even though the project is not required to provide an overall percentage of landscaped area and specific number of canopy trees and shrubs, it is providing a significant amount of landscaping onsite and within the public right of way (see Table 4).

Table 4. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area		92,718 sq. ft.
Building Footprint	--	54,018 sq. ft.

Remaining Lot Area	--	38,700 sq. ft.
Landscaping Required	N/A	6,106 sq. ft.
Canopy Trees	N/A	45 trees onsite 8 ROW
Shrubs	N/A	425 shrubs onsite 56 shrubs ROW

Parking and loading landscaping and screening – Requires alternative compliance

- Even though the general landscaping and screening requirements do not apply to this project, the project is still required to provide landscaping and screening requirements for the parking and loading area.
- Parking and loading areas that front a public street are required to provide a landscaped yard at least seven (7) feet wide, have a screen three (3) feet in height and not less than sixty (60) percent opaque, and provide not less than one (1) tree shall for each twenty-five (25) linear feet or fraction thereof of parking or loading area lot frontage. The portion of Chicago Avenue in front of the surface parking is subject to these requirements. The applicant is proposing a landscaped yard at this location that is 11 feet wide, includes a hedge of shrubs that will meet the height and opacity requirement, and three trees in front of the parking to meet the tree requirement.
- The project site does not abut or is it across the alley from a residence or office residence district, or any permitted or conditional residential use.
- The corners/areas of the parking lot that are unavailable for parking or vehicular circulation are landscaped as specified for a required landscaped yard.
- Because the proposed surface parking lot contains ten or more spaces, each parking space must be located within 50 feet of the center of an on-site deciduous tree. A portion of the surface parking lot does not comply with this requirement. Alternative compliance is required.
- The plant materials, and the installation and maintenance of the plant materials, shall comply with sections 530.200 and 530.210 of the zoning code.
- All other landscaped areas not occupied by buildings, parking and loading facilities, or driveways would be covered with turf grass, native grasses, perennials, wood mulch, shrubs, and trees.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – Meets requirements

- The parking lot and driveway is defined by a six (6) inch by six (6) inch continuous concrete curb. Given the size of the site and the amount of site disturbance the applicant is required to have a stormwater management plan that meets the filtration of stormwater requirements outlined in Chapter 54-Storm Water Management.

Site context – Meets requirements

- At six stories high, the proposed development will not have an adverse impact on the blocking of views of important elements of the city such as parks and greenways, significant buildings and water bodies.
- As demonstrated by the submitted shadow study, the proposed building should have minimal shadowing effects on public spaces and adjacent properties.
- At six stories tall, the proposed building will have a minimal impact on the generation of wind currents at ground level.

Crime prevention through environmental design – Meets requirements

- The site plan employs best practices to increase natural surveillance and visibility with the proposed fenestration that exceeds minimum requirements. The site plan will control and guide movement on the site as well as distinguish between public and non-public spaces with the proposed landscaping, fencing, and placement of building entrances. In addition, the proposed lighting plan will also assist with public safety. The lighting plan provides uniform lighting throughout the site. In addition, additional lighting of the public sidewalks will take place by having the building located along three public streets and a large amount of windows on the first floor.

Historic preservation – Meets requirements

- The original First Covenant Church is potentially eligible for local designation. The original ecclesiastic building was built in 1886 by Warren H. Hayes in the Romanesque Revival style for the Svenska Missions Tabernaklet. Hayes was indisputably the city's leading ecclesiastical architect in the latter half of the 19th century. There are currently three other churches designed by Hayes that are locally designated: Calvary Baptist Church (1889), Fowler Methodist Episcopal Church (1894), and Wesley Methodist Church (1891). A 2008 reconnaissance survey recommended the First Covenant buildings for intensive research in order to determine eligibility for local designation and listing on the National Register. The applicant plans to retain the original church buildings as part of the proposed planned unit development.

2. Conformance with all applicable regulations of the zoning ordinance.

- As previously stated, the proposed mixed-use development will have three new uses: multiple family residential, commercial (sports and health facility), and a principal parking facility. The residential and commercial components are permitted uses in the B4N District and principal parking facilities are conditional uses in the B4N District. As evaluated above, CPED is supportive of the conditional use permit application.
- The uses within the existing church building are the following: an active congregation, seasonal overnight homeless shelter, office space for the National Association of Minority Contractors Resource Center, and an early childhood learning center (Metrokids Early Childhood Center). All of these uses are permitted or an approved conditional use in the B4N District.

Off-street Parking and Loading – Meets requirements with Conditions of Approval

- The applicant is proposing a 131-space, underground, principal parking facility and a 48-space, accessory, surface parking lot for the nonresidential units on site. The principal parking facility is not subject to the minimum and maximum off-street parking requirements.
- Minimum off-street parking: Per Table 541-2 Specific Off-Street Parking Requirements-Downtown Districts, none of the existing or proposed uses have a minimum off-street parking requirement. However, multiple-family dwellings of 50 or more units that provide off-street parking for residents shall also provide designated visitor parking at a ratio of not less than one visitor space per 50 dwelling units. The proposed development is not planning on providing off-street parking for the residential component of the project. Therefore, it is not subject to the visitor parking standard (see Table 5).
- Maximum off-street parking: The proposed project will not exceed their maximum off-street parking requirement. The existing church building has over 61,000 square feet of gross floor area. Per Table 541-2 Specific Off-Street Parking Requirements-Downtown Districts, the institutional and commercial uses within the church building have a maximum parking allowance of one space per 1,000 square feet. Therefore, the existing church building would be allowed to have 61 off-street parking spaces. In addition, the new sports and health facility with over 12,000 square feet

would be allowed an additional 12 spaces (one space per 1,000 square feet of GFA). Therefore, from a maximum off-street parking standpoint, the proposed project would be allowed to have 73 off-street parking spaces while there is only 48 spaces proposed (see Table 5).

- As required by Table 541-3, Multiple-family dwellings with five or more units are required to provide 1 bike parking space per 2 dwelling units. Therefore, the proposed 159-unit development is required to provide 79 bike parking spaces. 90 percent of the bike parking spaces for the residential units are required to meet the standards of long-term bicycle parking. Required long-term bicycle parking spaces shall be located in enclosed and secured or supervised areas providing protection from theft, vandalism and weather and shall be accessible to intended users. Required long-term bicycle parking for residential uses shall not be located within dwelling units or within deck or patio areas accessory to dwelling units. The applicant is proposing to provide 78 long term bike parking spaces in the underground parking area along with a bike maintenance facility for the residential units and an additional eight spaces for the residents at grade. This complies with the minimum bike parking requirements for the residential component of the project.
- As required by Section 541.180, Developments in the downtown districts that provide less than 500,000 square feet of new or additional gross floor area shall provide one (1) secure bicycle parking space for every twenty (20) automobile spaces provided, but in no case shall fewer than four (4) or more than thirty (30) bicycle parking spaces be required. For the purposes of this section, a secure bicycle parking space shall include a bicycle rack which permits the locking of the bicycle frame and one (1) wheel to the rack, and which supports the bicycle in a stable position without damage to wheels, frame or components. Residential uses in the downtown districts are subject to the requirements of Table 541-3, Bicycle Parking Requirements. The applicant is proposing to providing a total of 179 vehicle parking spaces. Therefore, the bike parking requirement is eight spaces. The applicant is proposing to provide 48 bike parking spaces for the nonresidential portions of the project. (see Table 6).
- The only use within the planned unit development that has an off-street loading requirement is the multiple-family residential component. As outlined by Table 541-9 Specific Off-Street Loading Requirement, a multiple-family dwellings of 100 to 250 units is required to provide one small loading space. Required off-street loading spaces shall be at least ten (10) feet in width by at least twenty-five (25) feet in length for small space. The applicant is proposing a loading space that meets those requirements (see Table 7).
- Pavement is proposed to be bituminous which meets the requirements of 541.300-Surfacing.
- To comply with Section 541.320-Marking of spaces and 541.330-Size, all parking spaces shall be marked using paint approved by the city engineer and compact spaces shall be clearly labeled as such
- To comply with Section 541.300-Lighting, all lighting used to illuminate off-street parking areas shall be shielded and arranged in such a way as to comply with the performance standards of Chapter 535, Regulations of General Applicability.
- Snow accumulation is proposed to be taken offsite.

Table 5. Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
Underground Principal Parking Facility	N/A	N/A	N/A	N/A	131 spaces
All Other Uses	0	N/A	0	73	48 surface

Table 6. Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Short-Term	Long-Term	Proposed
Nonresidential uses	8	N/A	N/A	48
Residential Component	79	N/A	Not less than 90% (71)	78 (long term) 8 (short term)
	87	N/A	Not less than 90% (71)	134

Table 7. Loading Requirements (Chapter 541)

Use	Loading Requirement	Minimum Requirement	Proposed
Residential Dwellings	One small space	One small space	1

Building Bulk and Height – Meets requirements

- The existing church building contains 61,479 sq. ft. The new building will have 161,573 sq. ft. When completed, the entire development will have 223,321 sq. ft. Given the lot size, the proposed development will have floor area ratio of 2.41 which complies with the minimum floor area ratio requirement of the B4N zoning district (see Table 8 below).
- The flat roof building is 70.4 feet tall as defined by the Minneapolis zoning code, which is below the 140-foot height requirement in the B4N zoning district. The natural grade 10 feet from the center of the building along 6th Street South is 839.6 feet above sea level. The top of the first floor slab for the retail element of the project is 839.8 feet above sea level. The height of the building from the first floor slab to the top of the parapet is 70.4 (905 feet above sea level).

Table 8. Building Bulk and Height Requirements

	Code Requirement	Bonuses	Total	Proposed
Lot Area	--	--	--	92,718 sq. ft. / 2.13 acres
Gross Floor Area	--	--	--	223,321 sq. ft.
Floor Area Ratio (Minimum)	2.0	--	--	2.41
Floor Area Ratio (Maximum)	No maximum	N/A	No maximum	
Building Height (Maximum)	10 stories or 140 feet, whichever is less	N/A	N/A	6 stories/70.4 ft.

Lot Requirements – Meets requirements

- A planned unit development is required to contain a minimum contiguous area of one acre. The project complies with that requirement.

Table 9. Lot Requirements Summary

	Code Requirement	Proposed

Dwelling Units (DU)	--	159 DUs
Density (DU/acre)	--	72 DU/acre
Lot Area (Minimum)	Not less than 43,560 sq. ft. (1 acre) for a planned unit development	92,718 sq. ft. / 2.13 acres
Impervious Surface Area (Maximum)	N/A	93%
Lot Coverage (Maximum)	N/A	59%

Yard Requirements – Requires conditional use permit

- The only yard requirement for the proposed development is for the upper floors of the western elevation given that the upper floors have a residential use. This elevation faces the neighboring property owner’s surface parking lot. Per 549.120-Yard Requirements, stories two through six shall be setback 15 feet. The applicant is proposing to set this portion of the building back 13 feet from this shared property line. As outlined in the conditional use permit for the planned unit development section above, the applicant has provided a landscaped yard along the shared property line and is proposing to provide PUD amenities to allow for this reduction (see Table 10).

Table 10. Minimum Yard Requirements

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
North (6 th Street South)	0 ft.	--	0 ft.	0 ft.
South (7 th Street South)	0 ft.	--	0 ft.	9 ft.
East (9 th Avenue South)	0 ft.	--	0 ft.	4.5 ft.
West (Interior yard)	0 ft.	15 ft.	15 ft.	13 ft.

Signs – Meets requirements

- Although the applicant has not provided specific sign details (e.g. copy and lighting), the applicant has provided a master sign plan that shows existing and proposed sign location and size. The applicant is intending to have a total of nine signs on the project site: three existing and six proposed. As outlined in Table 543-3 Specific Standards for Signs in the Downtown District, a project site is allowed two and one-half (2.5) sq. ft. of signage per one (1) ft. of primary building wall in the B4N District. None of the existing or proposed signs would be higher than 16 feet in height, which complies with the 28 foot height requirement in the B4N District.
- The proposed and existing signage on the south elevation is in compliance with Chapter 543-On-Premises Signs. There would be a total of three signs on this elevation; two of the signs are existing and attached to the church building. Given the length of the building walls (293 linear feet), the south elevation would be allowed to have 732 square feet of signage. The applicant is proposing to have a total of 98 square feet of signage with the existing and proposed signs. The applicant is also

proposing a 30 sq. ft. (6 feet x 5 feet) freestanding (monument) sign near the underground parking access point to indicate the existence of public parking below grade. The sign is not proposed to be illuminated. As outlined in Table 543-3 Specific Standards for Signs in the Downtown District, a project site is allowed one freestanding sign per zoning lot and shall not be greater than 32 sq. ft. in size and not taller than 8 ft. in height. As a condition of approval, the base of the signs shall be landscaped or have other decorative materials designed to screen the base of the freestanding sign. Such landscape or decorative materials shall be capable of screening the base of the sign all year and shall be well maintained.

- The proposed signage on the east elevation is in compliance with Chapter 543-On-Premises Signs. Given the length of the east elevation building wall, the project would be allowed to have 825 sq. ft. of signage. The applicant is proposing one sign on this elevation which will denote the main entrance to the apartment building. The sign would be a pressed aluminum letters on top of the entrance canopy. The applicant outlines that the sign would not be greater than 110 sq. ft., which complies with the requirement that any one sign not be greater than 120 sq. ft. (the sign shown on the elevation is currently 60 sq. ft.).
- The proposed signage on the north elevation is in compliance with Chapter 543-On-Premises Signs. Given the length of the north elevation building wall, the project would be allowed to have 439 sq. ft. of signage. The applicant is proposing two signs on this elevation for the sports and health facility. A sign above the main entrance that would not exceed 120 sq. ft. and another sign on the building wall that would not exceed 60 sq. ft. Both signs are proposed to be illuminated and made of pressed aluminum lettering.
- The proposed signage on the west elevation is in compliance with Chapter 543-On-Premises Signs. Given the length of the west elevation building wall (244 linear feet), the project would be allowed to have 610 sq. ft. of signage. The applicant is proposing two signs on this elevation: one for the sports and health facility and one for the residential component. The signs are proposed to be internally illuminated and neither sign will exceed 70 square feet in area.
- As a condition of approval, the applicant will be required to submit a separate sign permit application for any signage that is proposed. All signs are subject to Chapter 543, On-Premise Signs.

Screening of Mechanical Equipment – Meets requirements

- As proposed, all mechanical equipment at ground level will be screened. The applicant is proposing to relocate a transformer that is currently located behind the church to a location near the underground parking access point. The applicant is also proposing to install four mechanical units at grade near the southwest corner of the sports and health facility. All of these mechanical units will be screened by landscaping.
- For the residential component of the project, the applicant is proposing to have magic paks placed on the exterior building wall for the individual units. To comply with Section 535.70-Screening of mechanical equipment, the magic pak screens on the outside walls shall be painted to match the surrounding wall finish to help reduce their visibility.

Refuse Screening – Meets requirements

- The project will comply with the requirements for screening of refuse and recycling storage containers. The church building currently has a trash enclosure area at the back of their building that is screened by chain link fence with slats. However, the applicant is proposing to consolidate their trash and recycling storage area for the entire site into a location in the underground parking facility.

Lighting – Meets requirements

- The applicant has submitted a lighting plan that appears to be in compliance with the lighting requirements in Chapter 535 and Chapter 541 of the zoning code.

Fences – Meets requirements

- The project will be in compliance with the zoning code fence regulations. The applicant is proposing a five-foot tall aluminum picket fence surrounding the playground. As a condition of approval, details of the aluminum picket fence shall be provided prior to building permit issuance.

Specific Development Standards – Not applicable

- Per Table 549-I Principal Uses in the Downtown District, there are no specific development standards for the proposed uses.

General Development Standards – Meets requirements

- The project will be in compliance with Section 535.90-General Standards for Residential Uses as all units will exceed the minimum 350 square foot gross floor area for an efficiency unit and 500 square foot floor area for non-efficiency units.

Accessory Structure– Not applicable

- There are no proposed accessory structures as part of this project.

Downtown District and B4N Standards – Meets requirements with Conditions of Approval

- With the approval of the conditional use permit to allow for the principal parking facility, the proposal will meet all other Downtown District requirements.
- As required by Section 549.580-General District Regulations, the principal parking facility will be located entirely underground. In addition, the proposed retail use at 12,364 square feet is below the maximum 30,000 square foot gross floor area requirement.
- The project contains less than 500,000 square feet or more of new or additional gross floor area; therefore, it will not be required to provide a bicycle facility as outlined in Section 549.170-Bicycle facilities in new development.

Downtown Parking-DP Overlay District Standards – Requires conditional use permit and variance

- Per the Downtown Parking Overlay District, the proposed 48- space surface parking lot requires a conditional use permit and a variance to exceed 20 parking spaces. The conditional use permit and variance applications are evaluated above. To comply with Section 551.750-Prohibited Uses, CPED is recommending as part of the conditional use permit for the surface parking lot that it shall be for accessory parking only and the use of the parking lot as a commercial lot is prohibited.

3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.

The Minneapolis Plan for Sustainable Growth identifies the site as public and institutional on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Urban Design Policy 10.2: Integrate pedestrian scale design features into Downtown site and building designs and infrastructure improvements.

- 10.2.1 The ground floor of buildings should be occupied by active uses with direct connections to the sidewalk.

- 10.2.2 The street level of buildings should have windows to allow for clear views into and out of the building.
- 10.2.3 Ensure that buildings incorporate design elements that eliminate long stretches of blank, inactive building walls such as windows, green walls, architectural details, and murals.
- 10.2.4 Integrate components in building designs that offer protection to pedestrians, such as awnings and canopies, as a means to encourage pedestrian activity along the street.
- 10.2.5 Locate access to and egress from parking ramps mid-block and at right angles to minimize disruptions to pedestrian flow at the street level.
- 10.2.7 Locate buildings so that shadowing on public spaces and adjacent properties is minimized.
- 10.2.8 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.9: Support urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed-use and transit-oriented development.

- 10.9.1 Encourage both mixed-use buildings and a mix of uses in separate buildings where appropriate.
- 10.9.2 Promote building and site design that delineates between public and private spaces.
- 10.9.4 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.16: Design streets and sidewalks to ensure safety, pedestrian comfort and aesthetic appeal.

- 10.16.2 Provide streetscape amenities, including street furniture, trees, and landscaping, that buffer pedestrians from auto traffic, parking areas, and winter elements.
- 10.16.4 Employ pedestrian-friendly features along streets, including street trees and landscaped boulevards that add interest and beauty while also managing storm water, appropriate lane widths, raised intersections, and high-visibility crosswalks.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

- 10.18.1 Require that parking lots meet or exceed the landscaping and screening requirements of the zoning code, especially along transit corridors, adjacent to residential areas, and areas of transition between land uses.
- 10.18.3 Locate parking lots to the rear or interior of the site.
- 10.18.4 Provide walkways within parking lots in order to guide pedestrians through the site.
- 10.18.5 Design parking structures so sloping floors do not dominate the appearance of the walls.
- 10.18.7 Minimize the width of ingress and egress lanes along the public right of way in order to provide safe pedestrian access across large driveways.

- 10.18.8 Encourage appropriate land uses to share parking lots to reduce the size and visual impact of parking facilities.

Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.

- 10.19.1 In general, larger, well-placed, contiguous planting areas that create and define public and private spaces shall be preferred to smaller, disconnected areas.
- 10.19.2 Plant and tree types should complement the surrounding area and should include a variety of species throughout the site that include seasonal interest. Species should be indigenous or proven adaptable to the local climate and should not be invasive on native species.
- 10.19.4 Landscaped areas should be maintained in accordance with Crime Prevention Through Environmental Design (CPTED) principles, to allow views into and out of the site, to preserve view corridors and to maintain sight lines at vehicular and pedestrian intersections.
- 10.19.7 Boulevard landscaping and improvements, in accordance with applicable city policies, are encouraged.

Urban Design Policy 10.22 Use Crime Prevention Through Environmental Design (CPTED) principles when designing all projects that impact the public realm, including open spaces and parks, on publicly owned and private land.

- 10.22.1 Integrate “eyes on the street” into building design through the use of windows to foster safer and more successful commercial areas in the city.
- 10.22.2 Orient new housing to the street to foster safe neighborhoods.
- 10.22.3 Design the site, landscaping, and buildings to promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.
- 10.22.4 Provide on-site lighting at all building entrances and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.
- 10.22.5 Locate landscaping, sidewalks, lighting, fencing and building features to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- 10.22.7 Locate entrances, exits, signs, fencing, landscaping, and lighting to distinguish between public and private areas, control access, and to guide people coming to and going from the site.

CPED finds that the proposed development is in conformance with the above policies of The Minneapolis Plan for Sustainable Growth.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The site is located within the boundaries of the *Downtown East/North Loop Neighborhood Master Plan* adopted by the City Council in 2003. The *Downtown East/North Loop Neighborhood Master Plan* calls for complete communities (i.e. neighborhood or districts that have a variety of land uses including a

mixture of housing types). It also calls for a streetscape along Chicago Avenue to tie together the central riverfront, the Mills District, Downtown East and Elliot Park, encouragement street-level improvements around the Metrodome and HCMC to create visual interest and functional links through the area around these megastructures, and establishment of a pedestrian-friendly streetscape of widened sidewalks, and tree planters. The proposed project accomplishes these goals.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Building placement.** As required by Section 530.110-Building placement, the first floor of a building is required to be not more than eight feet from the front lot line. The project is in compliance with this standard along 6th Street South and 9th Avenue South; however, it is proposed to be located nine feet from the property line along 7th Street South rather than eight feet. Alternative compliance is required. The applicant's proposal to set the building back nine feet meets the intent of this chapter. The applicant's proposal is complementary to the setback of the historic church building, which is also setback nine feet from 7th Street at the corner of 7th and Chicago.
- **Distance of trees to parking spaces in surface parking lots with 10 or more spaces.** As required by Section 530.170-Parking and Loading Landscaping and Screening, no parking space shall be located more than fifty (50) feet from the center of an on-site deciduous tree in parking lots of ten (10) spaces or more. Four of the surface spaces do not comply with this requirement. Alternative compliance is required. Strict adherence to the requirements is impractical because of site conditions and the proposed alternative meets the intent of this chapter. Below the proposed surface parking lot is an underground parking facility. Successfully planting trees above the surface parking lot would take extensive additional structural elements to support a root system. As an alternative, the applicant has proposed landscaping surrounding the surface parking lot, throughout the site, and within the public right of way that well exceed the minimum requirements.
- **Percentage of exterior materials.** As required by the [Guide to Exterior Building Walls and Materials](#), an elevation is allowed to have 75 percent coverage of metal panels. The applicant is proposing that the south elevation have 80 percent coverage. Alternative compliance is required. The purpose of this standard is to ensure that building elevations have high-quality durable materials. The proposed alternative is consistent with applicable development plans or development objectives adopted by the city council and meets the intent of this chapter. The applicant's proposed use of the dark metal panel relates well to the new U.S. Bank Stadium that has a dark surface along 6th Street South. Therefore, CPED is supportive of granting alternative compliance.

PRELIMINARY AND FINAL PLAT

The Department of Community Planning and Economic Development has analyzed the application for a preliminary and final plat based on the following findings:

- I. *The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

The applicant is proposing to combine two parcels into one lot. Public Works and the Attorney's office reviewed the preliminary and final plat and did not identify any issues.

The preliminary and final plat is in conformance with the design requirements of the land subdivision regulations. The preliminary and final plat would also conform with the applicable regulations of the zoning ordinance and the policies of the comprehensive plan as outlined in the land use applications above.

Section 598.260 Planned unit developments and cluster design, requires the design of a subdivision for a planned unit development to implement the site plan as approved by the Planning Commission and shall include a deed restriction designating the following:

1. The relationship between all common spaces and each individual lot (rights in the common spaces and proportionate ownership accruing to the individual lot).

As proposed, the preliminary and final plat will have one lot. First Covenant Church will have ownership over the entire project site through a ground lease.

2. Provision for access to each lot that does not have frontage on a public street.

As part of the development, there will only be one lot which fronts all four streets.

3. A requirement that an owner's association be created. The duties and responsibilities of the owner's association shall include maintaining the elements of the planned unit development as authorized under the zoning ordinance or other applicable regulations.

There will be one owner for the project site. First Covenant Church will be responsible for maintaining the elements of the planned unit developed as authorized by this zoning ordinance and other applicable regulations.

4. A provision that the taxes, special assessments, and other charges and fees that would normally be levied against the common spaces shall be levied against the individual lot occupied or to be occupied by buildings in direct proportion to the interest that is stated in the deed restriction and shall provide that such levies shall be a lien against the individual lots.

As proposed, the preliminary and final plat will have one lot. First Covenant Church will have ownership over the entire project site through a ground lease and will be responsible for taxes, special assessments and other charges and fees.

5. A requirement that any disposition of any of the common property situated within the planned unit development shall not be made without the prior approval of the Planning Commission.

If an amendment is made to the Planned Unit Development the proposed amendments shall be reviewed and approved by the City Planning Commission.

2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

Approval of the subdivision would not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in public streets as outlined in the land use applications above. The proposed subdivision would create one lot for the development that is 92,718 square feet, or approximately 2.13 acres, in size.

3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

The land intended for building can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions,

improper drainage, steep slopes, rock formations, utility easements or other hazard. Since the site is over an acre, the project site will have to meet Chapter 54 of the Minneapolis code ordinances to minimize the negative impacts of stormwater runoff. In addition, the applicant has provided a geotechnical report that outlines how the project would not adversely impact the high water table. The project is relatively flat and does not have a steep slope.

4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.*

The parcels created by this application present no foreseeable difficulties for the proposed or future redevelopment. No significant alterations to the land appear necessary.

5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

The Surface Water and Sewers Division of the Public Works Department will review and approve the drainage and sanitary system plans for this development prior to building permits being issued.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the application(s) by Scott Beckman of UrbanWorks Architecture LLC for the properties located at 815-827 6th Street South, 810 7th Street South, and 601 Chicago Avenue in the B4N District:

A. Conditional Use Permit for a Planned Unit Development.

Recommended motion: **Approve** the conditional use permit to allow a mixed-use, planned unit development with 159 dwelling units, 12,400 square feet of retail space, a principal parking facility and retention of the existing church buildings in the B4N/ Downtown Neighborhood District and DP/ Downtown Neighborhood Overlay District, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. Roof details shall be submitted prior to building permit issuance that show compliance with the reflective roof amenity requirement outlined in Table 527-1 Amenities. In addition, a shared bicycle amenity that meets the standards outlined in Table 527-1 Amenities shall be maintained for the duration of the planned unit development. The applicant shall dedicate and mark two parking spaces in the surface parking lot for the shared vehicles.

B. Conditional Use Permit to allow for a principal parking facility

Recommended motion: **Approve** the conditional use permit application to allow a 131-space, underground principal parking facility in the B4N/Downtown Neighborhood District, subject to the following condition:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity

requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

C. Conditional Use Permit to allow a surface parking lot

Recommended motion: **Approve** the conditional use permit application to allow a surface parking lot in the DP/Downtown Neighborhood Overlay District, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. The surface parking lot shall be for accessory parking only and the use of the parking lot as a commercial lot is prohibited as outlined in Section 551.750-Prohibited Uses.

D. Variance to allow a surface parking lot more than 20 spaces.

Recommended motion: **Approve** the variance to allow a 48-space surface parking lot in the DP/Downtown Neighborhood Overlay District.

E. Site Plan Review.

Recommended motion: **Approve** the site plan review for a planned unit development that includes a new six-story, mixed-use building with dwelling units, retail, and a principal parking facility, subject to the following conditions:

1. All site improvements shall be completed by August 1, 2018, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. The first floor windows required to meet the minimum window requirement for the nonresidential use, shall have clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher and shall allow views into and out of the building at eye level. Shelving, mechanical equipment or other similar fixtures shall not block views into and out of the building in the area between four (4) and seven (7) feet above the adjacent grade for the windows required to minimum glazing percentage.
4. A separate sign permit application is required for all new signage. All signs are subject to Chapter 543, On-Premise Signs. To comply with Section 543.360-Freestanding signs, the base of the signs shall be landscaped or have other decorative materials designed to screen the base of the freestanding sign. Such landscape or decorative materials shall be capable of screening the base of the sign all year and shall be well maintained.
5. To comply with Section 535.70-Screening of mechanical equipment, the magic pak screens on the outside walls shall be painted to match the surrounding wall finish to help reduce their visibility.
6. Details of the aluminum picket fence shall be provided prior to building permit issuance.

F. Preliminary and Final Plat.

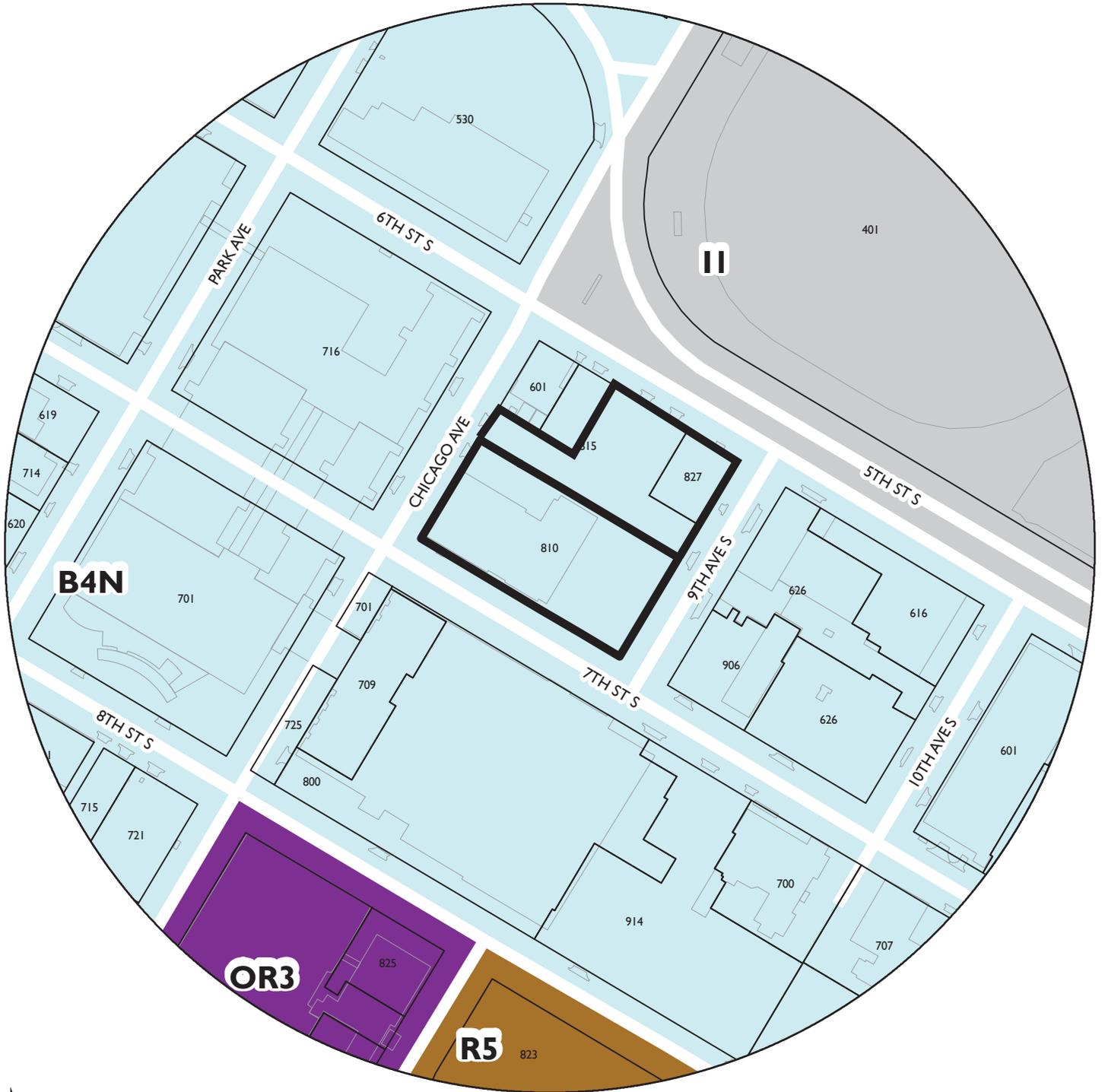
Recommended motion: **Approve** the preliminary and final plat.

ATTACHMENTS

1. Zoning map
2. Aerial photo
3. Written description and findings submitted by applicant
4. Public comments
5. PDR report
6. Survey | Civil plan set | Architectural plan set | Plat
7. Photos (existing conditions)
8. Renderings
9. Preliminary and final plat

NAME OF APPLICANT

WARD



PROPERTY ADDRESS

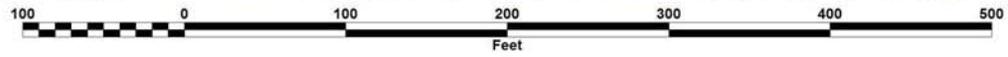
815 6th Street South, 810 7th Street South, 618 9th Avenue

FILE NUMBER

BZZ-7720



Hennepin County Medical Center



**FIRST COVENANT CHURCH
EAST TOWN APARTMENTS MIXED-USE PROJECT
STATEMENT OF PURPOSE AND DESCRIPTION OF PROJECT**

The purpose of this development is to expand the existing First Covenant Church site and campus to incorporate new workforce housing, non-profit commercial company that focuses on youth development, healthy living and social responsibility, and below grade public parking structure. Also located in the center of the block is a 48 stall structured surface parking lot fronted by a public bike hub to serve both the First Covenant Facility and the larger neighborhood community. The on-grade parking serves the daily needs of the church, their non-profit tenants and guest parking for the apartment building.

The total size of the new building is just over 211,000 GSF, in a 6 story building. This new building will replace existing 200 stall surface parking lot. As part of this project moving forward a land swap between First Covenant Church and Stadium Partners LLC (Owners' of Hubert's site) has occurred to allow for Hubert's to have a single contiguous land parcel.

As noted above the reconfigured site is 92,718 GSF (2.13 AC), and will include a new 6 story building with a first floor footprint of just over 30,000 GSF. East Town Apartments, the new building, will include 159 apartments with supporting amenities, single tenant commercial on the first floor and public parking below grade.

The existing church facility is approximately 61,750 GSF and includes First Covenant congregation as well as space for community assets including MetroKids Early Childhood Education, Upstream (part of HCMC) and a homeless shelter staffed by the Salvation Army

This development project has four primary components:

1. East Town Apartments – 159 affordable apartments (50 & 60% AMI) in 6 story L-shaped building along 6th Street S and 9th Avenue S.
2. 6th Street Retail – Over 12,000 SF for single user non-profit tenant that supports youth development, healthy living and social responsibility.
3. Public Parking – Below grade public parking structure of approximately 150 stalls that provides revenue for the church and neighborhood needs. In addition there are over 70 long-term bike parking stalls.
4. First Covenant Church renovation - This historic facility is being upgraded with mostly interior work such as new elevator and fire protection system. Exterior work is limited to new entry vestibule structure at building entry off parking area.

Features of the site and building design to be aware of, include the following:

- Removal of 4 existing curb cuts on the block
- Reduction of approximately 200 stall surface commercial parking lot to 48 structured surface stalls to support church, non-profit tenants and guest parking for apartments
- Bike Hub created along Chicago Avenue.
- Outdoor children's play area shared between existing preschool operating out of the church facility and residents of East Town Apartments.
- Opportunity for urban gardening on 2nd floor roof deck.

- Rainwater capture and on-site treatment.
- High albedo roof.
- CPTED strategies incorporated into building design including:
 - Numerous windows on first floor
 - Visibility from every parking stall to Entry door.
 - Buffering the property line with landscaping to define boundary between public and private.

Based on the above statement of purpose and the project design, we are proceeding with this project as a PUD, and as such are requesting the following conditional use permits and variance, per Chapter 527 of the current Minneapolis Zoning Code.

1. Conditional Use Permit for a planned unit development.
 - a. 2 primary structures on the same land parcel (527.130)
2. Conditional Use Permit to allow for a surface parking lot in Downtown Parking Overlay District
 - a. Project proposes modification of an existing 200+ surface parking lot to an at grade 48 parking stall lot that is located on the same lot as the principal use served. (551.760)
3. Variance to allow a surface parking lot to exceed 20 spaces in the Downtown Parking Overlay.
 - a. Per 551.760, CUP can be used for surface stall up to 20.
4. Variance to allow for a Reverse Corner Side Yard setback of less than 15'-0" in a hotel or residential use.
 - a. Per 527.160, CUP can be used to reduce the required yard setback.

In order to obtain the requested authorized alternative to multiple primary structures on the same parcel, an at grade structured parking over 20 stalls, and a reverse side yard setback reduction the project must provide amenities qualifying for the fifteen points required for the PUD process. The applicant is proposing utilization of the following amenities.

Outdoor children's play area (5 points)

An active, outdoor children's play area of 2,100 SF will be provided between the existing church facility and East Town Apartment building. The play area will be shared between the licensed daycare and apartment residents, will be separated from parking and maneuvering areas and will be design to facilitate adult supervision. The play area will include paly equipment, installed to the manufacturer's specifications or natural features suitable for children in both preschool and elementary school.

Reflective roof (3 points)

The proposed development will utilize roofing materials for 75% or more of the total roof surface having a Solar Reflectance Index (SRI) equal to or greater than the values as required by the US Green Building Council (USGBC) for low-sloped and steep-sloped roofs.

Shared bicycles (3 points)

Public access to shared bicycles available for short-term uses as defined in section 541.180 shall be provided for this proposed development. This proposed development will provide a minimum of ten (10) bicycles per one (1) commercial use. Bicycle parking spaces and racks

shall be located in an area that is convenient and visible from the principal entrance of the building.

Shared vehicles (3 points)

Access to a shared passenger automobile available for short-term use. For residential uses, a minimum of one (1) car per one hundred (100) dwelling units is required.

Recycling Storage Area (1 point)

The proposed development will provide an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely enclosed within the building.

**Conditional Use Permit for Planned Unit Development
Required Findings**

The amendment to the PUD to approve the building height, and yard reduction meets the required findings for a CUP for a planned unit development.

- (1) *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety comfort or general welfare.*

The construction of the East Town Apartments and associated programs at the site provides a very important to the economic health, diversity and vitality of the site and neighborhood.

- (2) *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property of uses permitted in the district.*

The design of the buildings and associated parking improve on the existing condition of the site, by adding public amenities such as a bike hub on Chicago Avenue, and first floor apartments with front porches along 9th Avenue S. In addition the design of the reduced surface parking area brings an opportunity to create outdoor community opportunities akin to block parties run by First Covenant Church. Possibly most importantly the site will be fully programmed and the inclusion of CPTED strategies will create a greater sense of safety for the other properties in the vicinity.

- (3) *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Adequate utilities, access, drainage and other facilities have been provided. The management of stormwater on the site will be significantly improved over existing conditions to the extent that the rainwater will be captured and treated per City of Minneapolis requirements.

- (4) *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

A Travel Demand Management Plan (TDMP) has been prepared for project. The parking provided is within the requirements of the parking regulations.

- (5) *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The project site is zoned B4N with DP overlay. The policies of the Minneapolis Plan for Sustainable Growth support the development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services and affordable housing available, improves the ability to accommodate automobile traffic, and enhances the design features that give the corridor its urban character.

This project is important to the redevelopment and maintenance of successful commercial uses that serve the needs of current and future users. (1.4) In addition this project is key to "preserv(ing) the stability and diversity of the city's neighborhoods...in order to attract and retain long-term businesses".

The proposed project is also consistent with the following policies in the comprehensive plan specific to Economic Development; Housing; Environment; and Urban Design. Specific examples of this project's alignment with the Minneapolis Plan for Sustainable Growth include but are not limited to:

- Policy 2.3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant and accessible
- Policy 2.5: Ensure that bicycling throughout the city is safe, comfortable and pleasant.
- Policy 2.8: Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community
- Policy 3.1: Grow by increasing the supply of housing.
- Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.
- Policy 3.3: Increase housing that is affordable to low and moderate income households.
- Policy 6.9: Be a steward of clean water by protecting and enhancing its surface and groundwater systems.
- Policy 6.14: Preserve and enhance the quality of the urban environment to promote sustainable lifestyles for its citizens.

Policy 10.6: New multi-family development should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

Policy 10.18 Reduce the visual impact of automobile parking facilities.

This project will be a welcome addition to the Elliot Park Neighborhood. This project is consistent with the Minneapolis Comprehensive Plan

(6) *The conditional use shall in all other respects, conform to the applicable regulations of the district in which it is located.*

In addition to the CUP standards the project complies with the additional findings required for planned unit developments.

(1) *That the planned unit development complies with all of the requirements and the intent and purpose of the PUD chapter. In making such determination, the following shall be given primary consideration:*

a. *The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.*

The proposed use is compatible with the surrounding area of both commercial and residential uses. Many pedestrian, streetscape and landscape amenities will be provided on all sides of the development to enhance the appearance of the public sidewalks. The building fronts 9th Street with unit entries and front porches. The structured at grade parking is buffered by the new and existing buildings, bike hub and landscape. The parking lot can serve additional uses for community gatherings

b. *The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provisions of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.*

The traffic generation characteristics of this project are managed in an holistic fashion with an overall reduction of automobile parking stalls, the addition of long term bike parking and public bike hub to allow for bike repairs. Reduction of 4 curb cuts and internal pedestrian pathways improves the availability and quality of the site and project

c. *The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.*

This project provides for the removal of a 200+ stall surface parking lot to be replace with affordable housing, a shared playground for young children, workforce housing and a limited at grade structured parking area (< 50 stalls) designed in such a fashion as to allow community events. In addition we are providing a landscape buffer where the

parking is exposed to the public realm that include tree canopy (shade) over the parking areas. Also it is a project goal to include a 'Nice Ride' kiosk and bikes on site.

- d. *The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development and protection of view and corridors.*

The design of the building scale and massing is in keeping with the design guidelines found in the Zoning Code. In regards to the parking the design improves drastically over the existing site condition.

- e. *An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing and applicable policies of the comprehensive plan and adopted small area plans.*

The location of the building is such that it reinforces and improves the existing public experience and the location of the at grade structured parking creates a buffer between the adjacent churches and residences.

- f. *The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.*

This project will comply with all City requirements for permanent and temporary erosion control. In addition this project will use stormwater to irrigate the extensive plantings on the site.

- g. *The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.*

This project is participating in the Xcel Energy Design Assistance Program and will comply with the newly adopted energy codes.

- (2) *That the planned unit development complies with all of the applicable requirement contained in Chapter 598, Land Subdivision Regulations.*

This project complies with all of the applicable requirements contained in Chapter 598, Land Subdivision Regulations.

CONDITIONAL USE PERMIT TO ALLOW FOR A SURFACE PARKING LOT

The B4N and DP Overlay Zoning District allows for an accessory parking lot if it is located on the same zoning lot as the principal use served and the number of parking spaces not exceed twenty (20) spaces (requested variance). The proposed parking facility meets the required findings for the issuance of a conditional use permit under § 525.340 of the Zoning Code.

- 1) *That the establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The proposed parking lots will not be detrimental or endanger the public health, safety, comfort or general welfare. The lots will have appropriate lighting and landscaping will incorporate CPTED principles for visibility. Public safety will be improved over current conditions.

- 2) *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district or substantially diminish property value.*

The parking lots will not be injurious to the use and enjoyment of other property, impede the normal and orderly development of the area, or diminish property values. Ensuring adequate parking for the church, new commercial tenant, daycare and other non-profit tenants in the 40+ at grade stalls will not create for other properties in the vicinity from negative impacts of parking congestion. The parking lots will provide shared opportunities for community gatherings. Landscaping will be incorporated to screen the lot including the addition of a bike hub resulting in a more attractive condition that currently exists.

- 3) *Adequate utilities, access roads, drainage, necessary facilities and other measures have been or will be provided.*

Adequate utilities, access, drainage, and other necessary facilities will be provided for the project. Multiple driveway accesses to the parking lots will be provided from Fremont and Girard. All stormwater will be captured and treated on the site.

- 4) *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The preliminary TDMP indicates that no traffic mitigation measures are required. The parking lots will ensure that overflow parking does not cause parking congestion on surrounding commercial and residential streets.

- 5) *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed parking complies with the following comprehensive plan policies related to educational institutions:

Policy 2.3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant and accessible

Policy 2.5: Ensure that bicycling throughout the city is safe, comfortable and pleasant.

Policy 2.8: Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community.

The amount and location of parking provided by the proposed parking lots is necessary to ensure that First Covenant Church and their non-profit tenants in the existing and new facilities are safe, accessible and functionally appropriate for the array of programs that will occur there. The project will take advantage of the proximity of transit on the adjacent corridors, and increased opportunities to bike can serve the majority of people who will come to the site.

The at grade parking lot is relative small in size given the large size of the buildings and the number of different users they will serve. Nevertheless, the proposed parking lot also comply with policies intended to encourage multi-modal transit and reduce the creation of parking.

- 6) *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With approval of the other zoning applications for the project, the parking lot will conform to the applicable regulations of the B4N zoning district.

CONDITIONAL USE PERMIT TO ALLOW FOR A PPRINCIPAL PARKING FACILITY

The B4N Zoning District allows for a principal parking garage if it is entirely below grade per 549.410 (5). The proposed parking facility meets the required findings for the issuance of a conditional use permit under § 525.340 of the Zoning Code.

- 1) *That the establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The proposed below grade parking structure will not be detrimental or endanger the public health, safety, comfort or general welfare. Public safety will be improved over current conditions of the surface lot as additional security control will be in place.

- 2) *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district or substantially diminish property value.*

The proposed below grade parking structure will not be injurious to the use and enjoyment of other property, impede the normal and orderly development of the area, or diminish property values. The parking structure is replacing the existing 200+ principal parking surface parking lot. The proposed below grade parking structure will provide shared opportunities for managing the high parking demand out of site from the pedestrian realm.

- 3) *Adequate utilities, access roads, drainage, necessary facilities and other measures have been or will be provided.*

Adequate utilities, access, drainage, and other necessary facilities will be provided for the project. Multiple driveway accesses to the parking lots will be provided from Fremont and Girard. All stormwater will be captured and treated on the site.

- 4) *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The preliminary TDMP indicates that no traffic mitigation measures are required. The parking lots will ensure that overflow parking does not cause parking congestion on surrounding commercial and residential streets.

- 5) *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed parking complies with the following comprehensive plan policies related to educational institutions:

Policy 2.3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant and accessible

Policy 2.5: Ensure that bicycling throughout the city is safe, comfortable and pleasant.

Policy 2.8: Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community.

The amount and location of parking provided by the proposed below grade parking structure is necessary to ensure that First Covenant Church maintains their parking ministry and to provide additional revenue for the church to continue to operate the various social services programs. The project will take advantage of the proximity of transit on the adjacent corridors, and increased opportunities to bike can serve the majority of people who will come to the site.

The proposed below grade parking structure reduces the current principal parking lot by over twenty five percent (25%) at a time when development has increased in the near

neighborhood. Nevertheless, the proposed parking lot also comply with policies intended to encourage multi-modal transit and reduce the creation of parking.

- 6) *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With approval of the other zoning applications for the project, the below grade structured parking structure will conform to the applicable regulations of the B4N zoning district.

VARIANCE REQUIRED FINDINGS

A variance is required from Section 551.760(2) of the 48 stall structured at grade parking, which states that, in the DP Overlay districts, "the number of parking spaces shall not exceed twenty (20) spaces." The parking is located central to the block and buffered from the public realm by buildings and landscaping. In addition the at grade parking is design such that it can be closed off to automotive traffic for public community events.

The Project meets the required findings under Zoning Code § 525.500 for this variance as discussed below:

- 1) *The property cannot be put to reasonable use under the conditions allowed by the official controls and strict adherence to the regulations of this zoning ordinance would cause undue hardship.*

The locations of the existing and proposed buildings and their uses determine the need for convenient parking on this site. A large institutional use in addition to affordable housing is both reasonable and desirable on this site and in the B4N district

- 2) *The circumstances are unique to the parcel of land for which the variance is sought and have not been created by any persons presently having an interest in the property. Economic considerations alone shall not constitute an undue hardship if reasonable use for the property exists under the terms of the ordinance.*

The four front lot lines of the project site is a unique condition not created by the applicant. There is an existing 200+ parking lot that is being reduced to under 50 stalls to serve the specific tenants on the block. Based on the total building on the site, the at grade lot parks .22 stalls per 1000 GSF.

- 3) *The granting of the variance will be in keeping with the spirit and intent of the ordinance and will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity.*

The variance will comply with the plain meaning of the ordinance and will be in keeping with the intent of the ordinance. The site design including the at grade parking will be

an improvement over the existing character of the streetscape. Allowing 28 additional stalls for the lot will not be injurious to the use or enjoyment of other property.

- 4) *The proposed variance will not substantially increase the congestion of the public streets, or increase the danger of fire, or be detrimental to the public welfare or endanger public safety.*

The proposed variance will allow for on-site parking stalls that will decrease congestion of parking on the public streets. The variance will not increase the danger of fire or be detrimental to the public welfare or safety.

A variance is required from Section 549.120(c) to allow a Reverse Corner Side Yard setback of 13'-0". This section states "Side yard and rear yard requirements for residential uses and hotels. Unless subject to a greater yard requirement in section (b) above, or in Chapter 535, Regulations of General Applicability, residential uses and hotels containing windows facing an interior side yard or rear yard shall provide an interior side yard or rear yard of at least five (5) feet plus two (2) feet for each story above the first floor provided that this section (c) shall not require a minimum interior side yard or rear yard greater than fifteen (15) feet."

The Project meets the required findings under Zoning Code § 525.500 for this variance as discussed below:

- 1) *The property cannot be put to reasonable use under the conditions allowed by the official controls and strict adherence to the regulations of this zoning ordinance would cause undue hardship.*

The locations of the existing and proposed buildings and their uses determined the layout of the affordable housing over commercial retail space on the site. The institutional use existing on site, in addition to the proposed mix of use of affordable housing over commercial space, is both reasonable and desirable on this site and in the B4N district.

- 2) *The circumstances are unique to the parcel of land for which the variance is sought and have not been created by any persons presently having an interest in the property. Economic considerations alone shall not constitute an undue hardship if reasonable use for the property exists under the terms of the ordinance.*

The adjacent existing use is a commercial parking lot serving an existing restaurant and is not a residential use. Our proposed mixed use development is unique with a commercial use on the first floor and a residential use above.

- 3) *The granting of the variance will be in keeping with the spirit and intent of the ordinance and will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity.*

The variance will be in keeping with the intent of the ordinance by providing a setback to the adjacent property. The commercial use on the first floor and the mixed use nature of the new development will be an improvement to the existing character of the neighborhood.

- 4) *The proposed variance will not substantially increase the congestion of the public streets, or increase the danger of fire, or be detrimental to the public welfare or endanger public safety.*

The proposed variance will provide public access from the street to the center of the block increasing public circulation around the site. The variance will not increase the danger of fire or be detrimental to the public welfare or safety.



May 24, 2016

Aaron Hanauer, Senior City Planner
Minneapolis Community Planning and Economic Development
250 South Fourth Street, Room 300
Minneapolis, MN 55415

Re: First Covenant Church housing development in Elliot Park

Dear Aaron:

In recent months we have been meeting with the First Covenant Church team that is leading the housing development project on church property between 6th and 7th Streets and Chicago and 9th Avenues. We understand that the proposal includes: Close to 160 units of workforce housing (tenants will be prequalified) of various sizes to accommodate individual and family living; retail space of approximately 12,500 square feet on the first floor; underground parking for 140 public commercial spaces and surface parking for 50 vehicles to accommodate church tenants and retail visitors.

The Elliot Park community particularly likes the residential row house style entries along 9th Avenue, the numerous bike racks, the roof top garden area; and, the overall massing and materials (although the 6th Street façade had mixed reviews, from some loving the industrial feel and others suggesting the introduction of another material to break up the massing). The opportunity to close off some of the surface parking lot near the west building entrance to be used as an occasional gathering and event space was also favored.

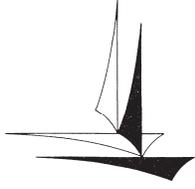
Overall the reaction is very positive and the following motion was approved at the May 19th Building, Land Use & Housing (BLUH) meeting by the community: ***Motion to approve the conceptual plans for the 1st Covenant Church housing development pending final design details. M. Young/Winkel. Passed Unanimously.*** (This motion will be forwarded to the EPNI Board of Directors for review and ratification at their June 13th meeting.)

EPNI approval is contingent upon final design review by the community which is tentatively scheduled for the June 16th BLUH meeting. Please contact me if you have any questions.

Sincerely,

Lynn Regnier, Executive Director

cc. 6th Ward Council Member Abdi Warsame
7th Ward Council Member Lisa Goodman
Pastor Dan Collison, First Covenant Church
Elizabeth Flannery, Community Housing Development Corporation
Tod Elkins, UrbanWorks Architecture
Carl Runck, Ryan Companies



Minneapolis Development Review
 250 South 4th Street
 Room 300
 Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**
(612) 673-2726
don.zart@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001436
Applicant:	COMMUNITY HOUSING DEVELOPMENT CORP 614 1ST ST N, SUITE 100 MINNEAPOLIS, MN 55401
Site Address:	815 6TH ST S 810 7TH ST S 827 6TH ST S
Date Submitted:	04-MAY-2016
Date Reviewed:	12-MAY-2016

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

Proposed 6 story, 150 unit affordable housing building with 17,000 sq ft of commercial space. The development will include 190 structured parking stalls.

Review Findings (by Discipline)

Street Design

- Note to the Applicant: Minneapolis standard plates have recently been updated. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. Add the following details from the ROAD-2000 Series - Driveways to the plans: ROAD-2000-R1, ROAD-2001-R1, ROAD-2002, and ROAD-2003-R1.

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Zoning - Planning

- The following land use applications have been identified/will likely be required to allow for the mixed use development with 150 residential units and commercial storefront.
 - Conditional use permit for a planned unit development;
 - Conditional use permit to allow for a surface parking lot;
 - Variance to allow a surface parking lot to exceed 20 spaces in the Downtown Parking Overlay District;
 - Site plan review;
 - Preliminary and final plat.
- In addition, applications/zoning code text amendments are being identified for the proposed movement of billboards onto adjacent property.

□ Addressing

- The proposed address for the residential entrance will be 618 9th Ave.
- The proposed address for the commercial / retail space will be 815 6th St S.
- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Contact Development Coordinator for new address assignment. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number.
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

□ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- Note to the Applicant: Bud Grant Drive is a commemorative street name for 9th Ave. S.; the site plan shall be revised to reflect the actual names.

□ Parks - Forestry

- Contact Craig Pinkalla (cpinkalla@minneapolisparcs.org), Telephone (612)-499-9233 regarding removal or protection of trees during construction in the city right of way.
- There will be an attachment with Craig's recommendations for tree replacements and associated costs for tree removals.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your project the calculated dedication fee is as follows:

Residential - 150 x \$1521	=	\$228,150
Commercial - Retail space	=	3,448
Administrative Fee	=	\$ 1,000
Total	=	\$232,598

- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Don Zart at (612)673-2726.

□ Sidewalk

- ADA compliant pedestrian ramps are required at each impacted crosswalk surrounding the site (recently upgraded ADA compliant pedestrian ramps shall be protected during construction). Construct two (2) ADA compliant pedestrian ramps at each of these locations. All pedestrian ramps will need to meet current ADA regulations and be "Accessible Pedestrian Signal" (APS) ready. Please contact Ryan Anderson at 612-673-3986 for more information on current APS designs. Include the appropriate details and standard plans in the site plan; refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>
- The Project streetscape shall be designed in accordance with the Access Minneapolis design guidelines; see <http://www.ci.minneapolis.mn.us/publicworks/transplan/index.htm> for further information. Specifically, the pedestrian zone is of concern, a minimum 6'-0" clear zone (un-obstructed) sidewalk shall be maintained for the length of any block in a straight line (the clear pedestrian zone cannot "jog" around planters and tree grates). Locations for site furniture, tree grates, planters and other proposed design elements that fall within the Public right-of-way shall be modified to provide for the required pedestrian clear zone space. For further clarification, site plans shall be fully dimensioned in relation to the property lines, Public right-of-way, sidewalks, street furniture, landscaping, utilities, and other obstructions.
- All proposed trees in the Public right-of-way are subject to the review and approval of the Minneapolis Park Board. Please contact Craig Pinkalla at (612) 499-9233 to discuss tree species selection, planting method, spacing and locations. Tree planting details shall be included in the plans. For all trees proposed in "hardscape environments" within the Public right-of-way, the Applicant shall provide engineered/structured soil in the form of a tree trench or tree pit for all proposed street trees. Sidewalk layouts and landscaping in the Public right-of-way shall follow established design standards; refer to the following: City of Minneapolis Urban Forest Policy (http://www.minneapolismn.gov/www/groups/public/@cped/documents/webcontent/convert_282934.pdf)
- Note to the Applicant: Any currently defective sidewalks or other concrete infrastructure within the public right of way, or any concrete infrastructure damaged during construction, must be removed and replaced.

□ Traffic and Parking

- The nature of the proposed development is such that traffic impacts will be an issue; please contact Allan Klugman at (612) 673-2743 to discuss the requirements of a Travel Demand Management Plan (TDMP). The TDMP shall be used to assist justification for multiple curb cuts and in setting driveway opening sizes.
- Parking circulation should not take place on the City roadway system; rather the parking ramp should have direct access to/from the surface parking lot (and not the street). Current ordinance states that all maneuvers associated with vehicle circulation, loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the parking, trash removal and delivery operations and show turning maneuvers for all truck type vehicles that will be using the parking entrance areas. Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- All street lighting (existing and proposed) shall be shown clearly on the site plan. Prior to site plan approval, the Applicant shall contact Joseph Laurin at 612-673-5987 to determine street lighting requirements. Note: If decorative street lighting exists on the proposed site it must be preserved or replaced at existing levels. Per the City of Minneapolis Street Lighting Policy, street lighting is required as the project falls within the Central Business District. Street lighting will be strongly encouraged in areas immediately adjacent to existing lighting systems, in high density areas such as Uptown and the University of Minnesota, and along major pedestrian corridors and business nodes as identified in the Minneapolis Street Lighting Policy.
- Note to the Applicant: In addition to the various required construction permits, impacts to existing traffic signal and street lighting systems (including installation of new street lights) will require the Applicant and respective Contractors to enter into a separate Right-of-Way Excavation Permit (including Testing and Inspection requirements) with the Public Works Department, for further information regarding this permit please coordinate with Paul Miller at (612) 673-3603.
- Note to the Applicant: The construction of this development will likely require the use of Public right-of-way (roadway and sidewalks) for construction purposes. A request for an estimate of street use and obstruction permit fees can be made to the City's Traffic Department; please contact Scott Kramer at (612) 673-2383 for further information.
- Note to the Applicant: Please add the following notes to the site plan:
 - Street lighting installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact Dave Prehall at (612) 673-5759 for further information. Any lighting installations not meeting City specifications will be required to be reinstalled at Owner expense.
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roway.net/> for a permit.
 - Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis lighting or traffic signal system that may be in the way of construction.
 - All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.
 - Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.

□ Fire Safety

- Provide required fire suppression system throughout building
- Fire department must be located on the address side of building and within 150 feet of a fire hydrant
- Provide required fire alarm system throughout building
- Provide and maintain fire apparatus access at all times

❑ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

❑ Business Licensing

- Contact Don Zart (612)673-2726 if any portion of the commercial space will be occupied by a food related business.

❑ Construction Code Services

- Earth retainage design will be required for excavations below grade in close proximity to property lines.
- Exterior stairs are located on public property.

❑ Environmental Health

- In April 2016 petroleum odors and staining were encountered while soil borings were being performed. This was reported to the Minnesota State Duty Officer and filed on report # 157039. Subsequent to this report the Minnesota Pollution Control Agency assigned petroleum release leak # 20069. In review for this project three records were identified for underground underground storage tanks. At 810 7th St S city records indicate a 4,000 gallon underground tank was installed and in use as late as 1954. No records of its removal have been found and there is no registration for the fuel oil tank with the MPCA. At 815 and 829 6th St S Minneapolis Fire Department records indicate two historic businesses had underground tanks. The same records indicate that tanks have been removed but did not report the condition of the tanks at removal. It is recommended the project enter the petroleum brownfield program at the MPCA and have plan developed and approved for addressing impacted soils when encountered. The status of the 4,000 gallon fuel oil tank must also be determined. If the tank is still on site and not in use it will need to be removed.
- If dewatering is required during site construction see below for city permit requirements. The scope of work calls for underground parking. The highest groundwater level expected for this site should be determined and used in establishing the lowest level for underground parking. The underground parking and other subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

❑ Historical Preservation Committee

- There is no preservation flag on these properties.

❑ Sewer Design**Groundwater:**

- Please identify if any pumping of groundwater will be necessary in order to keep the below grade areas dry after completion of construction. If not, this should be noted on the plans. Any proposed groundwater discharges require review by the City.

Stormwater Management:

- Please provide information on the proposed storm sewer system, internal to the site. This should indicate where the various catch basins, trench drains and roof drains are routed and connected.
- It appears that no stormwater live storage volume is being provided on the site and the entire site is proposed to be discharged through the connection at the northeast of the property. Flooding of the below grade portion of the structure seems highly likely given these conditions. Please evaluate this and address appropriately.
- Please provide a summary report of the stormwater management system describing how the stormwater management requirements are being met.
- The rate control requirements must be met for each discharge location to the City storm sewer system. For instance, it appears only Existing Drainage Area 3 discharges to the 12" storm sewer in 9th Ave S in the existing conditions, however in the proposed conditions, a majority of the site discharges to the 12" storm sewer. Please provide an analysis and appropriate drainage areas based on this.
- Please clearly identify the subcatchment drainage areas and what is considered pervious surface within each on the existing and proposed drainage maps.
- Please provide certification that the proposed Bayfilter device achieves 70% TSS removal for the 1.25" rain event, based on a NURP particle size distribution.
- It is not clear what Pond 1P in the HydroCAD model represents. Please provide design details on this system.
- An operations and maintenance plan is required for the stormwater treatment devices. The O&M plan shall define the maintenance regimen, including type and interval of maintenance and party to conduct such maintenance. Please provide a copy of the O&M Plan.

Surface Drainage:

- Is curb and gutter proposed between the existing surface parking lot, adjacent to the 601 Chicago Ave building, and this site? It appears there would be a significant slope on the pavement between the existing parking surface and the edge of the concrete walk. Also the parking lot is identified as a proposed land swap parcel. If the parking lot and development site are to be split, drainage should be controlled within the existing parking lot surface.
- The slope is pretty flat on the west side of the proposed retail building, less than 0.1%. It is recommended that sufficient drainage be provided here.
- Please identify the proposed property lines and the location of the roof drains for the existing buildings within the property to remain. Drainage from the existing buildings should be properly accounted for.
- Please indicate what is meant by 0.8 Watertable and 0.2 Watertable on the survey.

Utility Connections:

- The proposed 12" storm sewer connection in 9th Ave S, near 6th St S, should connect to the storm main in 6th. This connection should be core-drilled and a saddle tee fitting installed, per City of Minneapolis Standard Supplemental Specifications.
- It appears that pumps may be needed to accommodate some aspects of the storm sewer system. Please indicate where these pumps will be located and provide relevant design information.

Non Stormwater Discharges:

- Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

END OF REPORT



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.485.3100

CONSULTANT

Land Use Application
6.30.2016

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Justi Bark
SIGNED: *Justi Bark* Seal Backward
DATE: 6.30.2016
LICENSE # 47334

REVISIONS
Date Description

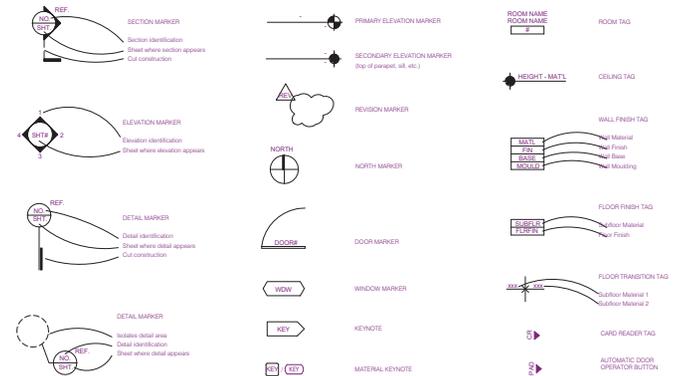
DATE: 6.30.2016
PROJECT # 15-0000
PHASE: LAND USE APPLICATION
DRAWN BY: AD
CHECKED BY: SB

KEY PLAN

TITLE SHEET

G001

SYMBOLS



GROSS BUILDING AREA

BUILDING LEVEL	AREA
AREA PLAN - SUBLEVEL 1	20792 SF
AREA PLAN - LEVEL 1	20204 SF
AREA PLAN - LEVEL 2	20204 SF
AREA PLAN - LEVEL 3	20204 SF
AREA PLAN - LEVEL 4	20204 SF
AREA PLAN - LEVEL 5	20204 SF
AREA PLAN - LEVEL 6	20204 SF
GRAND TOTAL	130808 SF

PARKING STALLS

TOTAL	RESERVED	RESERVED 1	TOTAL
Condominium Parking	11	20	31
Public Parking	3	0	3
Public Parking (Bike)	3	0	3
GRAND TOTAL	17	20	37

BIKE STALLS

TYPE	AT GRADE	LOCKER	TOTAL
RESIDENTIAL	0	16	16
COMMERCIAL - PUBLIC	0	0	0
RESERVED - PUBLIC	26	0	26
TOTAL	26	16	42

UNIT COUNT

TYPE	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6	LEVEL TOTAL
BEDROOM - TYPE 1	0	0	0	1	0	0	1
BEDROOM - TYPE 2	0	0	0	0	0	0	0
BEDROOM - TYPE 3	0	1	0	0	0	0	1
BEDROOM - TYPE 4	0	0	0	0	0	0	0
BEDROOM - TYPE 5	0	0	0	0	0	0	0
BEDROOM - TYPE 6	0	0	0	0	0	0	0
BEDROOM - TYPE 7	0	0	0	0	0	0	0
BEDROOM - TYPE 8	0	0	0	0	0	0	0
GRAND TOTAL	0	0	0	1	0	0	1

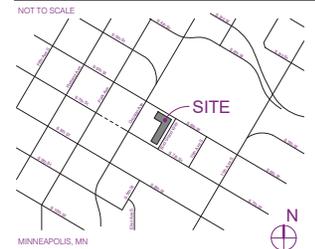
SITE & BUILDING STATISTICS

A. SITE AREA:	2.129 ACRES	=	92,718 sf
B. GROSS PROJECT AREA:	EXISTING CHURCH ABOVE GRADE S.F.	=	61,749 sf
	NEW BUILDING ABOVE GRADE S.F.	=	181,972 sf
	TOTAL ABOVE GRADE S.F.	=	243,721 sf
C. PERCENT OF SITE COVERAGE & FLOOR AREA RATIO (CHURCH AND NEW BUILDING):	% SITE COVERAGE F.A.R.	=	70% / 2.41
D. DWELLING UNITS PER ACRE:	159 UNITS / 2.129 ACRE	=	74.7 UNITS PER ACRE
E. PERCENT PERIPLUS SITE AREA:	6,106 sf	=	7% PERIPLUS
F. ELEVATION ABOVE MEAN SEA LEVEL:	839' - 9' AT RETAIL	=	847' - 0' AT RESIDENTIAL
G. HEIGHT OF ROOF ABOVE GRADE:		=	67' - 0"

GENERAL NOTES

- DO NOT SCALE DRAWINGS.
- SUBCONTRACTORS SHALL VISIT THE JOB SITE AND SHALL REVIEW THE CONTRACT DOCUMENTS TO FAMILIARIZE THEMSELVES WITH THE REQUIREMENTS AND INTENT OF THE SCOPE OF THE WORK FROM COMMENCEMENT OF WORK. DISCREPANCIES SHALL BE REPORTED TO GENERAL CONTRACTOR FOR CLARIFICATION.

VICINITY MAP



CONTACTS

OWNER
NAME: CHOC
CONTACT: ELIZABETH PLANNERY
6523 19TH AVENUE
ADDRESS: 614 N. FIRST ST., SUITE 100
MINNEAPOLIS, MN 55401
PHONE No.: 612.326.8261
FAX No.: 952.477.2150
E-MAIL: EPOCHCO@COMCAST.COM

DESIGN BUILDER
NAME: RYAN COMPANIES US
CONTACT: JEFF HALSTROM
ADDRESS: 50 SOUTH TENTH STREET
SUITE 300
MINNEAPOLIS, MN 55402
PHONE No.: 612.482.4000
FAX No.: 612.482.4000
E-MAIL: JEFF.HALSTROM@RYANCOMPANIES.COM

ARCHITECT
NAME: URBANWORKS ARCHITECTURE LLC
CONTACT: SCOTT BEOMAN, AIA
702 BUNGE
ADDRESS: 901 NORTH THIRD STREET
SUITE 145
MINNEAPOLIS, MN 55401
PHONE No.: 612.485.3100
FAX No.: 612.485.3109
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SARAH GARDNER
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CONTACT: RONALD J. LAMERE, P.E.
JOHN TRAM, P.E.
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MINNEAPOLIS, MN 55412
PHONE No.: 783.843.0400
FAX No.: 783.843.0421
E-MAIL: JTRAM@RBKM.COM

CIVIL ENGINEER
NAME: RBKM ENGINEERS
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MINNEAPOLIS, MN 55412
PHONE No.: 783.843.0400
FAX No.: 783.843.0421
E-MAIL: KMATTE@RBKM.COM

LANDSCAPE ARCHITECT
NAME: DAMON FARMER ASSOCIATES
CONTACT: JESSE S. WYNNIKOWICZ, ASLA, PLA
ADDRESS: 401 2ND AVE NORTH
SUITE 410
MINNEAPOLIS, MN 55401
PHONE No.: 612.202.7502
FAX No.: 612.202.0509
E-MAIL: JFWYNN@DAMONFARMER.COM

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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	G001	TITLE SHEET
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	G002	SHEET INDEX
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	G003	BUILDING SYSTEMS
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	G004	BUILDING SYSTEMS
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	R000	LIFE SAFETY ACCESSIBILITY INFORMATION
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	R001	LIFE SAFETY REFERENCE PLAN - SUBLEVEL 1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	R101	LIFE SAFETY REFERENCE PLAN - LEVEL 1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	R102	LIFE SAFETY REFERENCE PLAN - LEVEL 2 & LEVEL 3
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	R103	LIFE SAFETY REFERENCE PLAN - LEVEL 4 & LEVEL 5
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	R104	LIFE SAFETY REFERENCE PLAN - LEVEL 6 & ROOF
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	R901	CODE REQUIRED SIGNAGE
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	SURVY	SURVEY
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	PLAT	PLAT
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	C1.0	DEMOLITION PLAN AND EROSION CONTROL PLAN
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	C2.0	GRADING, DRAINAGE, EROSION CONTROL PLAN
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	C3.0	UTILITY PLAN
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	AL100	ARCHITECTURAL SITE PLAN - AREA PLAN
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	AL101	ARCHITECTURAL SITE PLAN
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	AL102	ARCHITECTURAL SITE PLAN - DIMENSIONED
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	AL103	ARCH SITE PLAN - LANDSCAPE & SCREENING
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	AL104	SHADOW STUDY
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A000.0	SUBLEVEL 1 PLAN - OVERALL
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A105.0	LEVEL 5 PLAN - OVERALL
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A106.0	LEVEL 6 PLAN - OVERALL
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A602	DOOR FRAME, DOOR, & WINDOW ELEVATIONS
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	E1	SITE PHOTOMETRICS

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ELECTRICAL SHEET INDEX

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401

612.495.3100

CONSULTANT

Land Use Application
6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backman
 SIGNED: Scott Backman
 DATE: 6.6.2016
 LICENSE #: 47341

REVISIONS
 # Date Description

DATE: 6.6.2016
 PROJECT #: 15-0007
 PHASE: LAND USE APPLICATION
 DRAWN BY: SB
 CHECKED BY: SB

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G002

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ALTA/NSPS LAND TITLE SURVEY FOR: Community Housing Development Corp.

LEGAL DESCRIPTION:

Parcel 1:

Lots 1, 2, 3, 4, 5, 8, and 9; the Southwesterly 55 feet of Lots 6 and 7; the Westerly 50 feet of the front 110 feet of Lot 7; and the Southwesterly 31 feet of Lot 10, all in Block 113, Nelsons Addition to Minneapolis, according to the recorded plat thereof, Hennepin County, Minnesota.

Part of the above being registered land as evidenced by Certificate of Title No. 113935

Note: The above portion being more particularly described as follows: Lot 9, Block 113, Nelsons Addition To The Town of Minneapolis.

Parcel 2:

Tract A: The Northeasterly 110 feet of Lot 6, except the Southeasterly 40 feet of the Northeasterly 85 feet thereof; The Southeasterly 16 feet of the Northeasterly 110 feet of Lot 7, Block 113, Nelsons Addition to the Town of Minneapolis, Hennepin County, Minnesota.

Being Registered land as is evidenced by Certificate of Title No. 1414381.

Tract B: Southeasterly 40 feet of the Northeasterly 85 feet of Lot 6, Block 113, Nelsons Addition to the Town of Minneapolis, Hennepin County, Minnesota.

Being Registered land as is evidenced by Certificate of Title No. 1414382.

NOTES:

- The orientation of this bearing system is based on the Hennepin County coordinate grid (NAD 83-96 Adj).
- The total area of the property described herein is 101,933 square feet or 2.3401 acres.
- The legal description and easement information used in the preparation of this survey is based on the Commitment for Title Insurance prepared by Old Republic National Title Insurance Company, Commitment No. 62397 dated February 24, 2016 at 7:00 am.
- Existing utilities, services and underground structures shown hereon were located either physically, from existing records made available to us, by resident testimony, or by locations provided by Gopher State One Call, per Ticket No. 16520277. However, locating, excavating, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, the client is advised that excavation may be necessary. Other utilities and services may be present and verification and location of all utilities and services should be obtained from the owners of the respective utilities prior to any design, planning or excavation.
- No zoning report or letter was received from the insurer pursuant to Optional Table A Item 6(a) & 6(b) of Table A, as set forth in the 2011 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys.
- The property described hereon lies within Flood Zone X (Areas determined to be outside of the 0.2% annual chance flooding) per Federal Insurance Rate Map No. 27053-C-0357-E, dated September 2, 2004.
- BENCHMARK #1:** Minnesota Department of Transportation Geodetic Monument "TEATR" GSD Station #72258 Elevation = 838.56 feet (N.A.V.D. 88)
- BENCHMARK #2:** Top out of hydrant in the north quadrant of Chicago Avenue South & 7th Street Elevation = 845.60 feet (N.A.V.D. 88)
- SITE BENCHMARK #2:** Top out of hydrant in the east quadrant of Chicago Avenue South & 6th Street Elevation = 844.36 feet (N.A.V.D. 88)
- As of the date of this survey there is no observable evidence of current earth moving work, building construction or building additions as described hereon.
- As of the date of this survey and according to the City of Minneapolis Engineering Department there are no completed or proposed changes in street right-of-way lines. As of the date of this survey there is no observable evidence of recent street or sidewalk construction or repairs that affect the property described hereon.

SURVEY ITEMS PER SCHEDULE B:

- ITEM 6: Minerals and mineral rights reserved by the State of Minnesota, as shown on the Certificate of Title. (As to Parcel 1 and Tract B of Parcel 2)
- ITEM 7: Electric transmission line easement(s) over the westerly 10 feet of Lot 9 in favor of Northern States Power Company, as created by Instrument No. 1336947.
- ITEM 8: Terms and conditions of City of Minneapolis Conditional Use Permit as evidenced by Document No. 15321916 and A10279790.
- ITEM 9: Terms and conditions of City of Minneapolis Conditional Use Permit as evidenced by Document No. 14954305 and A10190626.
- ITEM 10: Possible gap between Parcel 1 and 2. A gap may exist if Lots 6 and 7 are not exactly 165 feet in length from southeast to northeast and 66 feet in width from northwest to southeast. (SEE DETAIL)

CERTIFICATION:

To Community Housing Development Corporation, the First Covenant Church of Minneapolis, Minnesota, Stadium Partners LLC, a Minnesota limited liability company, Old Republic Title Insurance Company.

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and operated by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 6(b), 7(a), 7(b)(1), 8, 11, 13, 16, and 17 of Table A thereof. The field work was completed on March 22, 2016.

Date of Plat or Map: March 23, 2016.

[Signature]
L. J. HORN
Minnesota License No. 22033

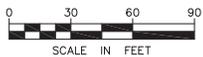
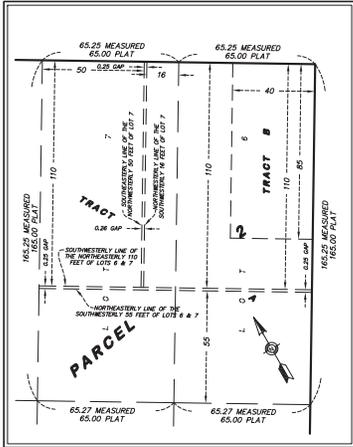
LEGEND:

- SANITARY MANHOLE
- STORM MANHOLE
- CATCH BASIN
- DRAIN (PLET)
- WATER MANHOLE
- HYDRANT
- ELECTRIC MANHOLE
- UTILITY POLE
- POWER POLE WITH LIGHT
- GUY WIRE
- LIGHT
- ELECTRIC BOX
- HANDHOLE
- TRAFFIC CONTROL BOX
- ELECTRIC METER
- TELECOMMUNICATIONS MANHOLE
- COMMUNICATION BOX
- GAS VALVE
- MANHOLE
- BENCH
- SIGN
- BOLLARD
- HANDICAPPED PARKING SPACE
- HANDICAPPED PARKING SIGN
- PARKING METER
- BORING
- CHAIN LINK FENCE
- GUARDRAIL
- SANITARY SEWER
- STORM SEWER
- WATERMAIN
- UNDERGROUND ELECTRIC OVERHEAD WIRE
- UNDERGROUND ELECTRIC OVERHEAD WIRE
- TELECOMMUNICATION LINE
- UNDERGROUND GAS
- SPOT ELEVATION
- EXISTING CONTOUR LINE
- TREE
- BITUMINOUS SURFACE
- CONCRETE SURFACE
- GRAVEL SURFACE

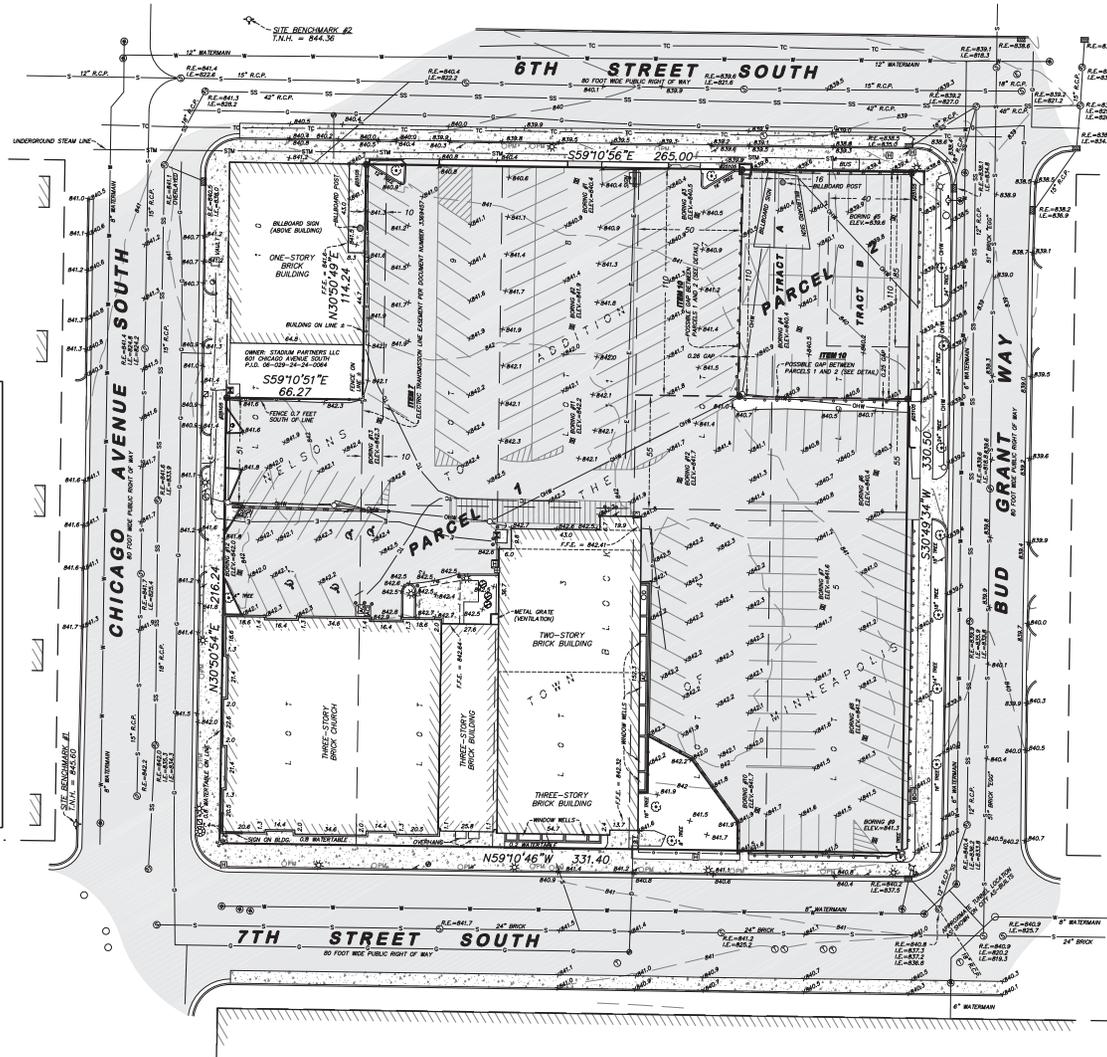


VICINITY MAP
NOT TO SCALE

DETAIL
NOT TO SCALE



- FOUND IRON MONUMENT
- SET IRON MONUMENT MARKED WITH LICENSE NUMBER 22033
- ✱ FOUND "X" IN CONCRETE



FIELD BOOK	PAGE	FIELDWORK CHIEF:	REVISIONS	
			NO.	DATE
2876	24	SW		
DRAWN BY: P.M.D.				
CHECKED BY: L.W.N.				
DRAWING NAME: 36814_C30.dwg				
JOB NO.: 36614				
FILE NO.: 45				

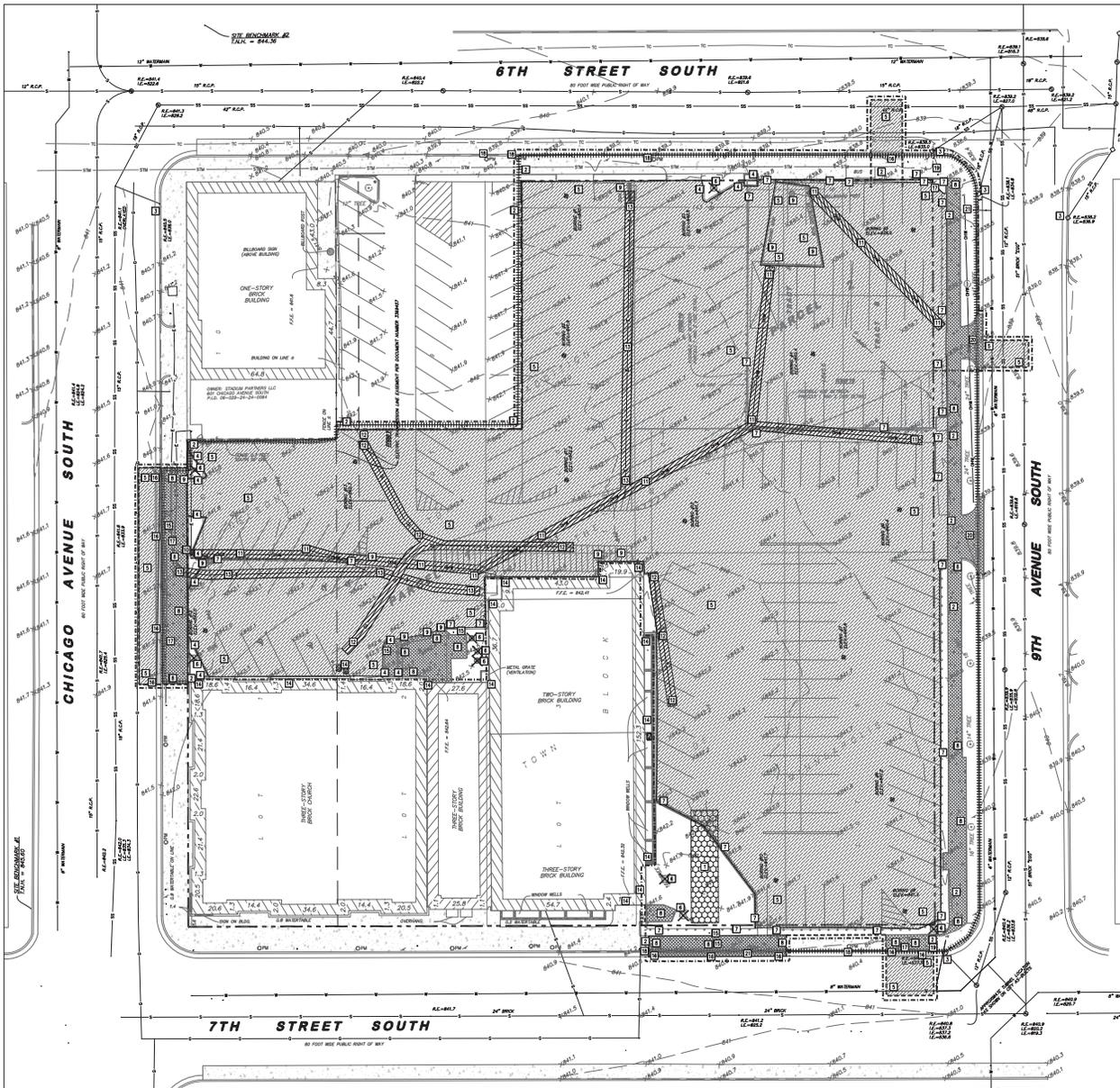
ALTA/NSPS LAND TITLE SURVEY

SURVEY FOR:
Community Housing Development Corp.

PROPERTY ADDRESS:
601 Chicago Avenue South
815 & 827 6th Street South
810 7th Street South
Minneapolis, Minnesota 55415



1229 Tyler Street NE, Suite 100
Minneapolis, Minnesota 55413
PHONE: (612) 466-3300
FAX: (612) 466-3383
WWW.EFNSURVEY.COM
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PROPOSED PLAN SYMBOLS

CONSTRUCTION LIMITS
 PERIMETER SEDIMENT CONTROL (SLO-HOLE)
 PROPERTY LINE
 SHADOW LINE (APPROX.)
 ROCK CONSTRUCTION ENTRANCE
 BITUMINOUS REMOVAL
 CONCRETE REMOVAL
 UTILITY REMOVAL
 CURB REMOVAL
 TREE REMOVAL

ABBREVIATIONS

TRC	Building
BM	Block Mark
CLC	Center Line
CL	Center Line
DIP	Ductile Iron Pipe
ELEV	Elevation
EE	Existing Floor Elevation
FIN	Finish
INV	Invert
MIN	Minimum
MN	Manhole
PCP	Polypropylene Chloride
PCP	Reinforced Concrete Pipe
RED	Redwood

NOTE: STORM SEWER INLETS NOT SHOWN ON PLAN MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INSTALL INLET SEDIMENT PROTECTION PER DETAIL A/C/S.O. ON ALL STORM INLETS THAT MAY RECEIVE RUNOFF.

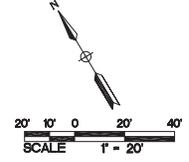
APPROXIMATE DISTURBED AREA IS 1.59 ACRES

NOTE: CONSTRUCTION LIMITS ARE ANTICIPATED TO BE PROPERTY LINE UNLESS OTHERWISE SHOWN.

- KEYED NOTES**
- KEYED NOTES ARE DENOTED BY [] ON PLAN.
- INSTALL ROCK CONSTRUCTION ENTRANCE. REFER TO DETAIL A/C/S.O. 2A3/C.S.O.
 - INSTALL PERIMETER SEDIMENT PROTECTION. REFER TO DETAIL A/C/S.O. 2A3/C.S.O.
 - INSTALL INLET SEDIMENT PROTECTION. REFER TO DETAIL A/C/S.O.
 - REMOVE CURB/PLANTER ISLANDS IN THEIR ENTIRETY.
 - REMOVE BITUMINOUS PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
 - REMOVE TREE IN ITS ENTIRETY INCLUDING STUMP.
 - REMOVE WALL AND OR GUARD RAIL IN ITS ENTIRETY TO THE EXTENTS SHOWN.
 - REMOVE CONCRETE PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
 - REMOVE SIGN AND OR BELIERS. COORDINATE ANY RELOCATION OF MATERIALS WITH OWNER, AND THE CITY OF MINNEAPOLIS.
 - EXISTING POWER POLE TO REMAIN. PROTECT AT ALL TIMES. COORDINATE WITH LOCAL UTILITY COMPANY.
 - EXISTING UNDERGROUND TELECOMMUNICATIONS TO BE REMOVED TO THE EXTENTS SHOWN. COORDINATE WITH LOCAL UTILITY COMPANY AND OWNER.
 - EXISTING UNDERGROUND ELECTRIC TO BE REMOVED TO THE EXTENTS SHOWN. COORDINATE WITH LOCAL UTILITY COMPANY AND OWNER.
 - EXISTING BUILDING AND ASSOCIATED FRAM RELATING TO BUILDING ARE TO REMAIN. PROTECT AT ALL TIMES.
 - REMOVE AND SALVAGE LIGHT POLE, BASE, AND ASSOCIATED ELECTRICAL ENTRIES. COORDINATE ANY RELOCATION/SALVAGE WITH LOCAL UTILITY AND OWNER. FOR LIGHT POLES AND ASSOCIATED ELECTRICAL ENTRIES IN PUBLIC RIGHT-OF-WAY, FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
 - REMOVE CURB AND GUTTER IN ITS ENTIRETY TO THE EXTENT SHOWN. REMOVE AND REMOVE AT NEAREST JOINT.
 - SALVAGE AND RELOCATE EXISTING CITY OWNED SIGNAGE. CONTRACTOR SHALL COORDINATE REMOVAL AND RELOCATION WITH CITY OF MINNEAPOLIS.
 - PARKING METER TO REMAIN. PROTECT AT ALL TIMES.
 - STREET LIGHT TO REMAIN. PROTECT AT ALL TIMES.
 - REMOVE CONCRETE DRIVE ENTRANCE IN ITS ENTIRETY TO THE EXTENTS SHOWN.
 - FIRE HYDRANT TO REMAIN. PROTECT AT ALL TIMES.
 - REMOVE AND SALVAGE PARKING METER. COORDINATE WITH THE CITY OF MINNEAPOLIS.

- DEMOLITION AND REMOVAL NOTES:**
- PRIOR TO START OF DEMOLITION, ALL EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE INSTALLED BY THE CONTRACTOR AND INSPECTED BY THE CITY OF MINNEAPOLIS. ALL SILT FENCES SHALL BE INSTALLED AND INSPECTED PRIOR TO ANY CONSTRUCTION ACTIVITIES. SILT FENCES SHALL BE INSTALLED ALONG THE CONTOUR.
 - IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE LOCATION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY THE LOCATION, ELEVATION AND MARK ALL EXISTING UTILITIES. IF INQUIRY BEFORE CONSTRUCTION STARTS, THE ENGINEER, ARCHITECT OR OTHER DOES NOT GUARANTEE THAT ALL UTILITIES ARE MARKED. IF UTILITIES ARE MARKED, OR IF MARKERS ARE SHOWN CORRECTLY, CONTACT OTHER PARTY ONE CALL AT 651-464-0002 FOR FIELD LOCATING EXISTING UTILITIES. CONTACT UTILITY OWNER IF DAMAGE OCCURS DUE TO CONSTRUCTION.
 - THERE MAY BE MISCELLANEOUS ITEMS TO BE REMOVED THAT ARE NOT IDENTIFIED ON THESE PLANS. THE CONTRACTOR SHALL VISIT THE SITE AND REVIEW THE DOCUMENTS TO OBTAIN A CLEAR UNDERSTANDING OF THE INTENDED SCOPE OF WORK.
 - PRIOR TO START OF CONSTRUCTION, DISCONNECT ALL GAS AND ELECTRIC SERVICES. COORDINATE DISCONNECTION OF EACH UTILITY WITH THE UTILITY OWNER. REMOVE ALL GAS AND ELECTRIC LINES UNDER PROPOSED BUILDING FOOTPRINT.
 - ANY UTILITIES NOT IDENTIFIED FOR REMOVAL OR ABANDONMENT, ARE TO BE PROTECTED AT ALL TIMES.
 - ALL EXISTING CURB AND GUTTER IS TO BE REMOVED WITHIN THE SCOPE OF THE PROJECT FROM THE SAW CUT LINES TO THE NEAREST JOINT.
 - THE BACKGROUND INFORMATION WAS PREPARED BY EGAN, FIELD & NORRICK, INC. (612) 466-3300.
 - ALL WORK IN THE PUBLIC RIGHT OF WAY IS TO BE COORDINATED WITH THE CITY OF MINNEAPOLIS. ROADWAY REPAIRS, SIGNALING REPAIRS, AND TRAFFIC CONTROL ARE TO BE PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.

- GENERAL NOTES:**
- CONCRETE CURB AND GUTTER REMOVAL, PAVEMENT REMOVAL, AND UTILITY REMOVAL LIMITS ARE TO BE COORDINATED WITH THE CITY OF MINNEAPOLIS AND UTILITY OWNERS REFER TO ALL CONSTRUCTION DOCUMENTS.
 - THE CONTRACTOR SHALL DEVELOP AND IMPLEMENT A TRAFFIC CONTROL PLAN WHILE WORKING WITHIN THE RIGHT-OF-WAY. THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE CITY ENGINEERING DEPARTMENT PRIOR TO STREET ENCROACHMENT.
 - CONTRACTOR SHALL VISIT THE SITE PRIOR TO BEGING AND REVIEW ALL CONSTRUCTION DOCUMENTS AND GEOTECHNICAL REPORTS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ITEMS THAT SHOULD HAVE BEEN ANTICIPATED BY PERFORMING THE ABOVE.
 - THE ROCK CONSTRUCTION ENTRANCE INDICATED ON THE PLAN IS SHOWN IN AN APPROXIMATE LOCATION. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR IS TO COORDINATE WITH THE CITY OF MINNEAPOLIS FOR THE EXACT ROCK CONSTRUCTION ENTRANCE LOCATION.



1 DEMOLITION AND EROSION CONTROL PLAN
 C1.0

East Town
 Apartments
 Minneapolis, MN

COMMUNITY HOUSING
 DEVELOPMENT CORPORATION
 CH
 DC

RYAN

URBAN WORKS
 NORTH DAKOTA

901 NORTH THIRD STREET, SUITE 148
 MINNEAPOLIS, MN 55401
 612.495.3100

CONSULTANT
 BKBM
 800 Broadway Building
 Minneapolis, MN 55415
 www.bkbn.com

Land Use Application
 6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Engineer under the laws of the state of Minnesota.

Keith Muth
 SIGNED: Keith A. Muth
 DATE: 06.28.2016
 LICENSE #: 49571

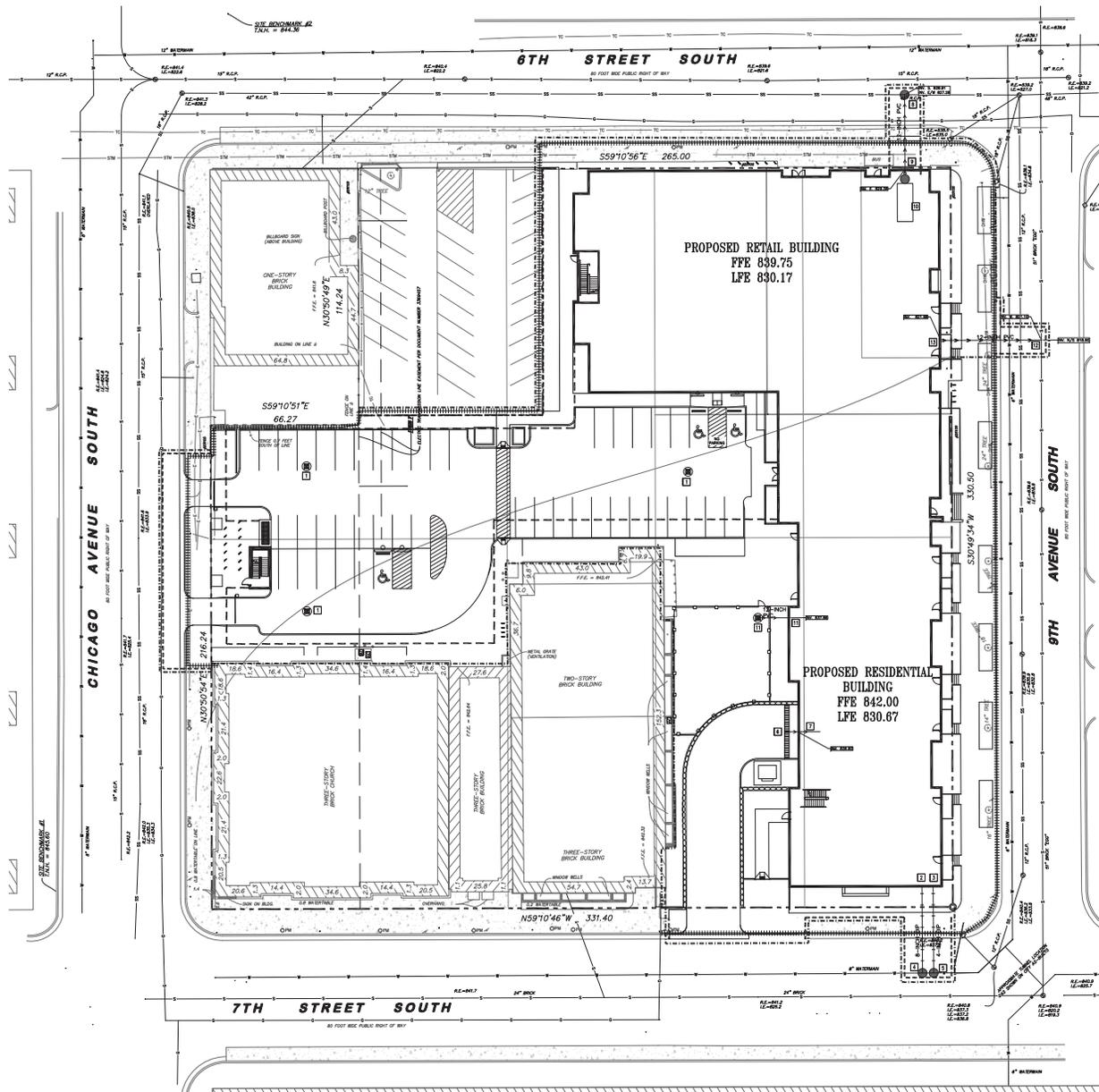
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DATE: 6.6.2016
 PROJECT #: 16315
 PHASE: Land Use Application

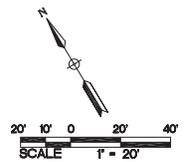
DRAWN BY: SJA
 CHECKED BY: KAM

Demolition and
 Erosion Control
 Plan

C1.0



PROPOSED PLAN SYMBOLS		ABBREVIATIONS	
CONSTRUCTION LIMITS	---	BM	Bench Mark
PROPERTY LINE	---	CB	Catch Basin
CONC	---	CONC	Concrete
DIP	---	DI	Ductile Iron Pipe
ELEV	---	ELEV	Elevation
DI	---	DI	Ductile Iron Pipe
FFE	---	FFE	Finished Floor Elevation
H.C.E.	---	H.C.E.	High Density Polyethylene
INVERT	---	INVERT	Invert
MAN	---	MAN	Manhole
MIN	---	MIN	Minimum
MAX	---	MAX	Maximum
MIN	---	MIN	Minimum
PC	---	PC	Polyethylene Chloride
RC	---	RC	Reinforced Concrete Pipe
RD	---	RD	Reel Drive



- NOTE: CONSTRUCTION LIMITS ARE ANTICIPATED TO BE PROPERTY LINE UNLESS OTHERWISE SPECIFIED.
- KEYED NOTES ARE DENOTED BY [] ON PLAN.
- PARKING DECK DRAINS, COORDINATE WITH STRUCTURAL, ARCHITECTURAL AND MECHANICAL FINAL LOCATION. PARKING LOT WILL DRAIN INTO NEW UNDERGROUND STRUCTURE. ROUTING TO BE DETERMINED BY MECHANICAL ENGINEER.
 - STUB 8-INCH PIPE PROTECTION LINE TO WITHIN 5-FOOT OF PROPOSED BUILDING, FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
 - STUB 4-INCH DOMESTIC WATER SERVICE TO WITHIN 5-FOOT OF PROPOSED BUILDING, FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
 - WETPAP EXISTING 8-INCH WATER MAIN FOR 8-INCH SERVICE, THE CITY OF MINNEAPOLIS WILL PROVIDE AND INSTALL THE WETPAP AND FROM TO PART OF THE PERMIT FEE, THE CONTRACTOR IS TO EXCAVATE FOR THE WETPAP, COORDINATE THE WATER MAIN MANHOLE CONNECT TO THE VALVE AND BACKFILL THE TRENCH PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS, REFER TO DETAIL 301.03. COORDINATE ALL WORK WITH CITY OF MINNEAPOLIS. CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS TO MNDOT PRIOR TO CONSTRUCTION.
 - WETPAP EXISTING 3-INCH WATER MAIN FOR 3-INCH SERVICE, THE CITY OF MINNEAPOLIS WILL PROVIDE AND INSTALL THE WETPAP AND VALVE AS PART OF THE PERMIT FEE, THE CONTRACTOR IS TO EXCAVATE FOR THE WETPAP, CONSTRUCT THE WATER VALVE MANHOLE, CONNECT TO THE VALVE AND BACKFILL THE TRENCH PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS, REFER TO DETAIL 301.03. COORDINATE ALL WORK WITH CITY OF MINNEAPOLIS. CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS TO MNDOT PRIOR TO CONSTRUCTION.
 - INSTALL TRENCH DRAIN, REFER TO DETAIL 1/05.1.
 - CONNECT PROPOSED TRENCH DRAIN TO BUILDING AT INVERT ELEVATION OF 827.62. COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
 - CONSTRUCT NEW MANHOLE OVER EXISTING STORM SEWER, FFM ELEVATION = 839.4. INVERT ELEVATION 828.81.
 - STUB 12-INCH PVC STORM SEWER TO WITHIN 5-FOOT OF BUILDING AT 1.0% TO INVERT ELEVATION 829.26. COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION.
 - INSTALL BANSWAIR JAWL OR EQUIVALENT, REFER TO DETAIL 3/02.1. SYSTEM SHALL BE ABLE TO TREAT THE FLOW FROM A 1.25-INCH BAWL EXACT TO THE TOTAL SURFACE SOLIDS MANUAL. FLOW RATE FOR THE 1.25-INCH EXACT IS 1.33 CUBIC FEET PER SECOND.
 - INSTALL 12-INCH CATCH BASIN, FFM ELEVATION 841.60, INVERT ELEVATION 838.00. CONNECT TO BUILDING STORM SEWER DRAIN. COORDINATE CONNECTION WITH MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION.
 - CONNECT TO 31-INCH BRICK SANITARY SEWER AT INVERT ELEVATION 821.10. GROUT SEAL AROUND PERIMETER.
 - CONNECT TO 12-INCH SDR 40 PVC PIPE AT 2.0% TO INVERT OF 821.80 TO WITHIN 5-FOOT OF PROPOSED BUILDING. COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.

- UTILITY NOTES FOR WORK IN PUBLIC RIGHT-OF-WAY:
- FOLLOW ALL CITY OF MINNEAPOLIS AND HENNEPIN COUNTY STANDARDS AND SPECIFICATIONS.
 - BEFORE CONSTRUCTION, CONTRACTORS ARE TO COORDINATE ALL WORK WITH RIGHT OF WAY AND OBTAIN ALL APPLICABLE PERMITS.
- UTILITY NOTES:
- COORDINATE SERVICE CONNECTION LOCATIONS AT THE BUILDING WITH THE MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR UNCOORDINATED WORK.
 - ALL SERVICE CONNECTIONS WITH LESS THAN 5 FEET OF COVER OVER THE TOP OF PIPE ARE TO BE INSULATED. INSULATION SHALL BE INSTALLED FROM THE CONNECTION OF THE SERVICE AT THE SERVICE AT THE POINT WHICH THE SERVICE ATTAINS 5 FEET OF COVER. CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM ARCHITECT OR ENGINEER PRIOR TO INSTALLATION OF INSULATION.
 - PROTECT ALL EXISTING STRUCTURES AND UTILITIES WHICH ARE NOT SCHEDULED TO BE REMOVED.
 - ALL SEWER AND WATER CROSSINGS SHALL HAVE A MINIMUM VERTICAL SEPARATION OF 1.5 FEET AND HORIZONTAL SEPARATION OF 10 FEET. FOLLOW ALL HEALTH DEPARTMENT AND CITY OF MINNEAPOLIS AND HENNEPIN COUNTY STANDARDS.
 - ALL WATER MAINS SHALL BE DUCTILE IRON PIPE, CLASS 52, UNLESS NOTED OTHERWISE.
 - ALL WATER MAIN SHALL HAVE A MINIMUM BATHY OF COVER OF 7.0 FEET OVER TOP OF WATER MAIN.
 - PROTECT BURIED BLOCKING ON ALL WATER MAIN PER CITY OF MINNEAPOLIS. PROVIDE MECHANICAL JOINT RESTRAINTS ON ALL BENDS, VALVES, TEES, PLUGS AND HYDRANT LEADS.
 - SANITARY SEWER PIPING SHALL BE SDR 36 FOR MINNEAPOLIS PVC UNLESS NOTED OTHERWISE.
 - STORM SEWER PIPING SHALL BE SDR 26 POLY VINYL CHLORIDE (PVC), UNLESS NOTED OTHERWISE. ALL STORM SEWER THAT IS EXTENDED TO THE BUILDING FOR ROOF DRAIN SERVICES IS TO BE SCHEDULE 40 PVC. PVC PIPE AND FITTINGS SHALL CONFORM TO THE REQUIREMENTS OF ASTM D3034 (A2M D4060) FOR SCHEDULE 40.
 - CONTRACTORS ARE TO COORDINATE ALL WORK WITH GAS, ELECTRIC, TELEVISION AND TELEPHONE COMPANIES PRIOR TO START OF CONSTRUCTION.
 - ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10-FOOT OF THE BUILDING OR WATER SERVICE LINE SHALL BE TESTED IN ACCORDANCE WITH MN PLUMBING CODE.
 - ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE GAS TIGHT OR WATER TIGHT IN ACCORDANCE TO MN PLUMBING CODE. APPROVED RESIDENT RUBBER JOINTS MUST BE USED TO MAKE WATER TIGHT CONNECTIONS TO MANHOLES, CATCH BASINS, AND OTHER STRUCTURES. GROUT RINGS ARE AN ACCEPTABLE ALTERNATIVE. GROUT RINGS ARE PERMITTED ONLY FOR REPAIRS AND CONNECTIONS OF EXISTING LINES CONSTRUCTED WITH SUCH JOINTS.

- CITY OF MINNEAPOLIS UTILITY NOTES:
- ALL WORK DONE OUTSIDE OF THE LOT LINE ON PUBLIC RIGHT-OF-WAY MUST BE DONE BY A CONTRACTOR LICENSED TO WORK IN THE CITY OF MINNEAPOLIS WHO HAS THE \$100,000.00 SCHEDULE CONTRACTOR'S BOND ON FILE WITH THE SCHEDULE DIVISION.
 - ALL UTILITIES MUST BE INSTALLED BY A MINNEAPOLIS LICENSED CONTRACTOR.
 - THE CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL OBTAIN MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORM WATER MANAGEMENT BMP. CONTACT PAUL CHILLEN, 612-673-2406 PAUL.CHILLEN@MINNEAPOLIS.GOV.
 - UPON THE PROJECT'S COMPLETION, THE GENERAL CONTRACTOR SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL BE SENT AS A BIDDING SPECIFICATION THAT THE REPORT OF THE APPROVED STORM WATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBstantiate THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ACCURATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.

1 UTILITY PLAN
C3.0
1" = 20'

East Town
Apartments



RYAN



901 NORTH THIRD STREET, SUITE 148
MINNEAPOLIS, MN 55401
612.498.3100

CONSULTANT



800 Broadway, Suite 1000
Minneapolis, MN 55415
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www.bkbn.com

Land Use Application
6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Engineer under the laws of the state of Minnesota.

Keith Mittle
BORN: Keith A. Mittle
DATE: 06.08.2016
LICENSE #: 46971

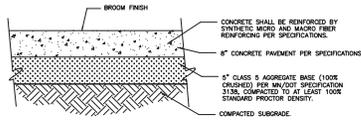
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Date Description

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PROJECT #: 16313
PHASE: Land Use Application

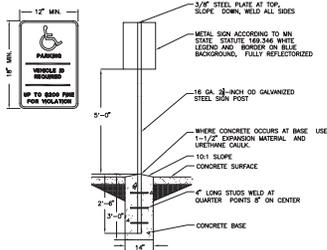
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Utility
Plan

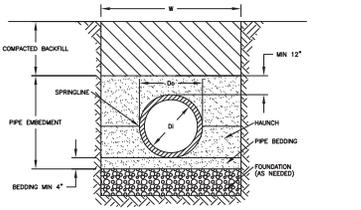
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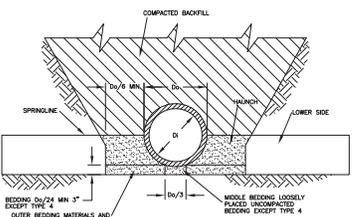
15 CONCRETE PAVEMENT
NOT TO SCALE



16 ACCESSIBLE SIGN AND POST - CONCRETE PLACEMENT
NOT TO SCALE



17 TYPICAL PIPE BEDDING FOR PVC
NOT TO SCALE

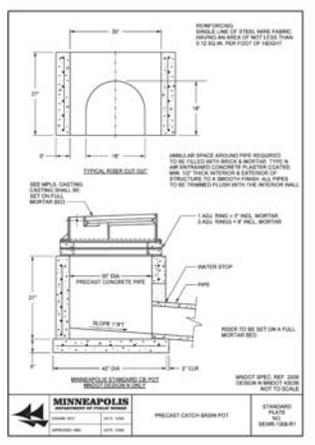


STANDARD TRENCH INSTALLATION SOIL AND MINIMUM STANDARD PROCTOR COMPACTION REQUIREMENTS	HAUNCH AND OUTER BEDDING	LOWER SIDE
TYPE 1	80% CATEGORY I	80% CATEGORY I
TYPE 2	80% CATEGORY I	80% CATEGORY I
TYPE 3	80% CATEGORY I	80% CATEGORY I
TYPE 4	NO COMPACTION REQUIRED, EXCEPT IF CATEGORY II USE 80% CATEGORY II	NO COMPACTION REQUIRED, EXCEPT IF CATEGORY II USE 80% CATEGORY II

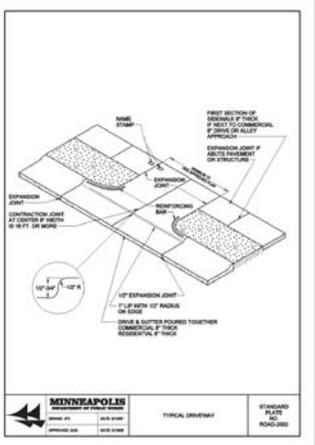
EQUIVALENT SOIL CLASSIFICATIONS FOR SOIL DESIGNATIONS	UNTESTED SOIL CLASSIFICATION SYSTEM (UCS)	MIN/DOZ SPECIFICATION
CATEGORY I	CLEAN GRAINED SANDS, SILT, SP, OR GP, OR ANY SOIL BEGINNING WITH ONE OF THESE SYMBOLS WITH 12% CLS PASSING A #200 SIEVE	COARSE FILTER AGGREGATE MIN/DOZ 3149.2H
CATEGORY II	COURSE GRAINED SANDS WITH FINES, OR, GC, SC, SL, OR ANY SOIL BEGINNING WITH ONE OF THESE SYMBOLS CONTAINING MORE THAN 12% PASSING A #200 SIEVE	AGGREGATE BEDDING MIN/DOZ 3149.2H
CATEGORY III	FINE GRAINED SANDS, CL, ML, OR CL-ML, CL-MC, ML, OL, WITH LESS THAN 30% RETAINED ON A #200 SIEVE	NOT APPLICABLE

1. COMPACTION AND SOIL SYMBOLS - USE CATEGORY II IF REFERRED TO CATEGORY I SOIL MATERIAL WITH MINIMUM STANDARD PROCTOR COMPACTION OF 95%.
2. SOIL IN BEDDING AND HAUNCHES - PROCTOR COMPACTION OF AT LEAST THE SAME COMPACTION AS SPECIFIED FOR THE MAJORITY OF SOIL IN THE BACKFILL ZONE.
3. THE TRENCH WIDTH SHALL BE WIDER THAN SPECIFIED FOR THE AGGREGATE SPACE TO ALLOW UNIMPACTED CONSTRUCTION IN THE HAUNCH AND BEDDING ZONES.
4. FOR TRENCH WALLS WITH GREATER THAN 10 DEGREE SLOPES TAKE ADVISORY COMMENTS ON THE CONTRACT TO PROVIDE A CONSISTENT AGGREGATE AS SPECIFIED FOR THE SOIL IN THE BACKFILL ZONE.
5. NO BEDDING IS REQUIRED FOR TYPE 4 STANDARD INSTALLATION.
6. REFER TO ASTM C1479-D FOR DETAILS.
7. TYPE II BEDDING SHALL BE USED UNLESS OTHERWISE.

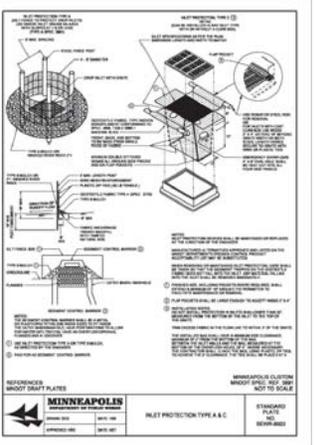
18 TYPICAL PIPE BEDDING FOR RCP AND DIP
NOT TO SCALE



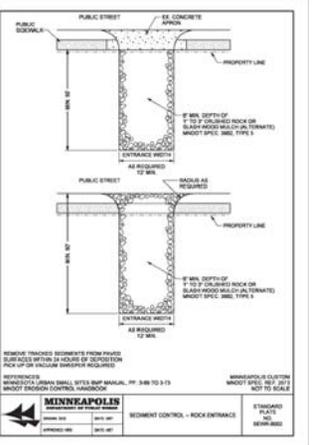
11 PRECAST CATCH BASIN
NOT TO SCALE



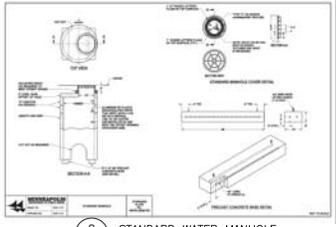
7 TYPICAL DRIVEWAY
NOT TO SCALE



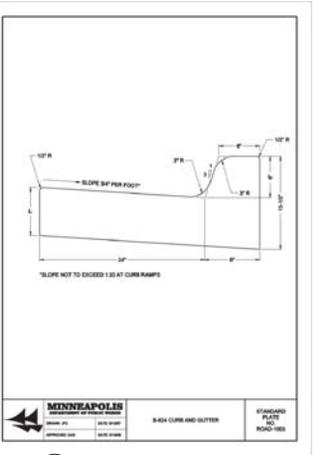
4 INLET PROTECTION DEVICES
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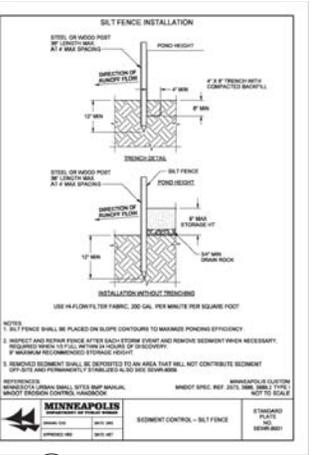
1 ROCK CONSTRUCTION ENTRANCE
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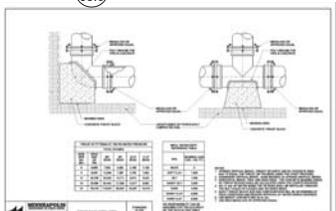
8 STANDARD WATER MANHOLE
NOT TO SCALE



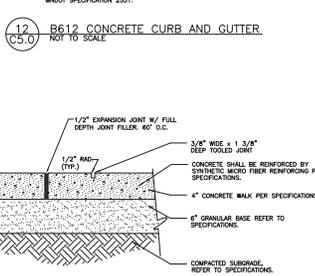
5 B-624 CURB AND GUTTER
NOT TO SCALE



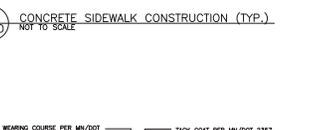
2 SEDIMENT CONTROL - SILT FENCE
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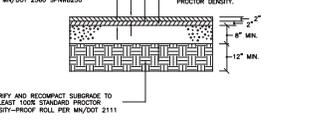
9 THRUST BLOCK
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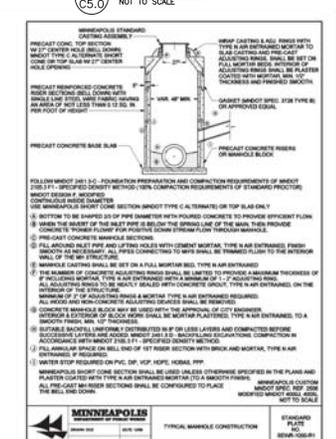
12 B612 CONCRETE CURB AND GUTTER
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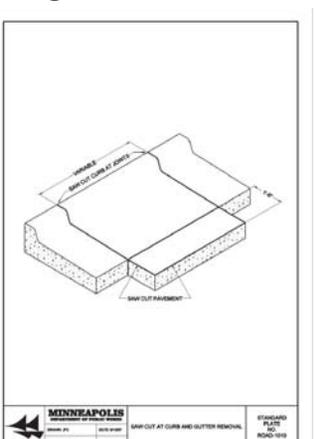
13 CONCRETE SIDEWALK CONSTRUCTION (TYP.)
NOT TO SCALE



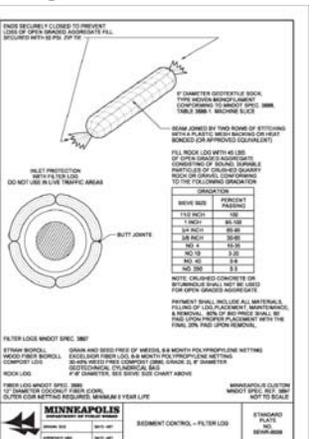
14 HEAVY-DUTY BITUMINOUS PAVEMENT
NOT TO SCALE



10 TYPICAL MANHOLE CONSTRUCTION
NOT TO SCALE



6 SAW CUT AT CURB AND GUTTER
NOT TO SCALE



3 SEDIMENT CONTROL - FILTER CLOTH
NOT TO SCALE

OTHER NOTES:

- LONG TERM MAINTENANCE OF THE SITE WILL BE PERFORMED BY THE OWNER. DISC
- CONTRACTOR SHALL MAINTAIN ALL EXPOSED SOIL AREAS (INCLUDING STOCKPILES) STABILIZED WITH MULCH OR OTHER EROSION CONTROL MEASURES TO PREVENT SOIL EROSION AND TO PREVENT POLLUTANTS FROM ENTERING THE STORMWATER DRAINAGE SYSTEM.
- CONTRACTOR SHALL MAINTAIN ALL EXPOSED SOIL AREAS (INCLUDING STOCKPILES) STABILIZED WITH MULCH OR OTHER EROSION CONTROL MEASURES TO PREVENT SOIL EROSION AND TO PREVENT POLLUTANTS FROM ENTERING THE STORMWATER DRAINAGE SYSTEM.
- CONTRACTOR SHALL MAINTAIN ALL EXPOSED SOIL AREAS (INCLUDING STOCKPILES) STABILIZED WITH MULCH OR OTHER EROSION CONTROL MEASURES TO PREVENT SOIL EROSION AND TO PREVENT POLLUTANTS FROM ENTERING THE STORMWATER DRAINAGE SYSTEM.

POLLUTION PREVENTION MANAGEMENT MEASURES

SOIL MUST BE STORED PROPERLY, COMPLY WITH MPCA REQUIREMENTS.

HAZARDOUS WASTE STORED (SECONDARY CONTAINMENT, RESTRICTED ACCESS) AND DEPOSED IN CONFORMANCE WITH MPCA REQUIREMENTS.

EXTERNAL WASHING OF TRUCKS AND OTHER CONSTRUCTION VEHICLES MUST BE LIMITED TO A DEFINED AREA OF THE SITE. RUNOFF MUST BE CONTAINED AND WASTE PROPERLY DISPOSED.

NO ENGINE OPERATING ALLOWED ON-SITE.

CONTRACTOR SHALL MAINTAIN ALL EXPOSED SOIL AREAS (INCLUDING STOCKPILES) STABILIZED WITH MULCH OR OTHER EROSION CONTROL MEASURES TO PREVENT SOIL EROSION AND TO PREVENT POLLUTANTS FROM ENTERING THE STORMWATER DRAINAGE SYSTEM.

HANDLING AND STORAGE OF HAZARDOUS MATERIALS:

IF THE CONTRACTOR INTENDS TO USE POLYMERS, FLOCCULANTS, OR OTHER SEDIMENTATION TREATMENT CHEMICALS ON THE PROJECT SITE, THE CONTRACTOR MUST COMPLY WITH THE FOLLOWING MINIMUM REQUIREMENTS:

- THE CONTRACTOR MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITION TO ENSURE EFFECTIVE TREATMENT. CHEMICALS MAY ONLY BE APPLIED WHILE HEAVY TURBIDITY IS PRESENT TO A SEDIMENT CONTROL SYSTEM WHICH ALLOWS FOR FILTRATION OR SETTLEMENT OF THE FLOC PRIOR TO DISCHARGE.
- CHEMICALS MUST BE STORED THAT ARE APPROPRIATELY SUITED TO THE TYPES OF SOLIDS LIKELY TO BE EXPOSED TOING OPERATIONS AND TO THE EXPECTED TURBIDITY, PH AND FLOW RATE OF STORMWATER FLOWING INTO THE CHEMICAL TREATMENT SYSTEM OR AREA.
- CHEMICALS MUST BE STORED IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES, AND WITH OSHA REGULATIONS AND TENDON REMOVAL DESIGN SPECIFICATIONS PROVIDED BY THE MANUFACTURER OR PRODUCER/SUPPLIER OF THE APPLICABLE CHEMICALS.

MECHANICAL AND NON-STORMWATER DISCHARGES, EXISTING AND PROPOSED

- WATER LINE FLUSHING
- LAUNDRY WASHING
- DISCHARGE FROM PORTABLE WATER SOURCES ARE CONDITIONAL
- INDIVIDUAL RESIDENT CAR WASHING
- RESIDENTIAL BUILDING WASH WATERS WITHOUT DETERGENTS

ABBREVIATIONS

BLDG	Building
BMP	Best Management Practice
BSM	Basin at Man
CB	Catch Basin
CIRC	Circular
ELEV	Elevation
ENT	Entrance
FTE	Finished Floor Elevation
FEET	Feet
MIN	Minimum
MAX	Maximum
MPCA	Minnesota Pollution Control Agency
MPO	Minnesota Pollution Control Agency
PVC	Polyvinyl Chloride
PCP	Precast Concrete Pipe
TR	Top of Wall
N.O.	Noted

AGENCY CONTACTS

CITY OF MINNEAPOLIS
ENGINEERING DEPARTMENT
PHONE: (612) 673-3000

MINNESOTA POLLUTION CONTROL AGENCY
PHONE: (612) 296-6300

MISSISSIPPI WATERSHED MANAGEMENT ORGANIZATION
PHONE: (612) 468-8786

OWNER
814 NORTH FIRST STREET #100
MINNEAPOLIS, MN 55401-3101
PHONE: (612) 333-6844

NOTE:

THE STORM WATER POLLUTION PREVENTION PLAN FOR THIS PROJECT INCLUDES THE 2015 MANUAL FOR CONSTRUCTION POLLUTION PREVENTION. CONTRACTOR TO SUPPLY CONSTRUCTION PRACTICES MANUAL, ESTIMATED PRELIMINARY QUANTITIES OF ALL EROSION PREVENTION AND SEDIMENT CONTROL MEASURES FOR THE LIFE OF THE PROJECT. PROJECT AND FOR THE LIFE OF THE PROJECT. CONTRACTOR WILL BE PHASED TO MAINTAIN DURATION OF THE CONSTRUCTION PERIOD. CONTRACTOR IS TO REVIEW MINNESOTA POLLUTION CONTROL AGENCY'S INSTRUCTIONS FOR THE APPLICATION FOR MINNESOTA'S NIDES/SS GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY PRIOR TO SUBMITTING APPLICATION.

INSPECTIONS

EXPOSED SOIL AREAS: ONCE EVERY 7 DAYS AND WITHIN 24 HOURS FOLLOWING A 1/2 INCH OVER 24 HOURS RAIN EVENT.

STABILIZED AREAS: ONCE EVERY 30 DAYS.

FROZEN GROUND: AS SOON AS RUNOFF OCCURS CONTRACTOR SHALL REPAIR AND MAINTAIN RECORDS. A COPY OF THE GRADING, DRAINAGE EROSION CONTROL PLAN AND PERMITS MUST BE KEPT EITHER IN THE FIELD OFFICE, INSPECTOR'S VEHICLE OR CONTRACTOR'S VEHICLE.

FINAL STABILIZATION

STABILIZATION BY UNIFORM PERENNIAL VEGETATIVE COVER (FOR DENSITY) DRAINAGE DITCHES STABILIZED. ALL TEMPORARY CONTROLS AND STRUCTURAL BMP'S REMOVED.

CLEAN OUT SEDIMENT FROM CONVEYANCES AND SEDIMENTATION BASINS (RETURN TO DESIGN CAPACITY).

TEMPORARY SEDIMENT BASINS:

TEMPORARY SEDIMENT BASINS SHALL BE PROVIDED TO RECEIVE RUNOFF FROM THE PROJECT SITE. ENGINEER ANTICIPATES THAT DRAIN TO REGIONAL COLLECTION SYSTEM SHALL BE CLEAN THE CONTRACTOR WILL USE PROPOSED FILTRATION BASINS AS TEMPORARY SEDIMENT BASINS AND CLAY LINE PRIOR TO USE. SURFACE WATER SHALL BE REMOVED BY SOMMER DEVICE OR USE A FANLION SKIMMER OR TRAP RACK, OR USING A PUMP WITH A FILTER. ALTERNATE TEMPORARY SEDIMENT BASINS SHALL BE APPROVED BY THE ENGINEER FOR USE.

NOTE:

THE OWNER AND CONTRACTOR ARE RESPONSIBLE FOR IMPLEMENTATION OF THE SWPPP AND INSTALLATION, OPERATION AND MAINTENANCE OF THE EROSION PREVENTION AND SEDIMENT CONTROL MEASURES, BEFORE, DURING, AND AFTER CONSTRUCTION UNTIL THE NOTICE OF TERMINATION HAS BEEN FILED.

STOCKPILES:

ON-SITE STOCKPILES OF SOIL SHALL HAVE PERMITS SEDIMENT CONTROL STOCKPILES SHALL BE STABILIZED WITH BLENDED STRIPS, OR HYDRO MULCH 3 FEET ON-SITE FOR MORE THAN 14 DAYS.

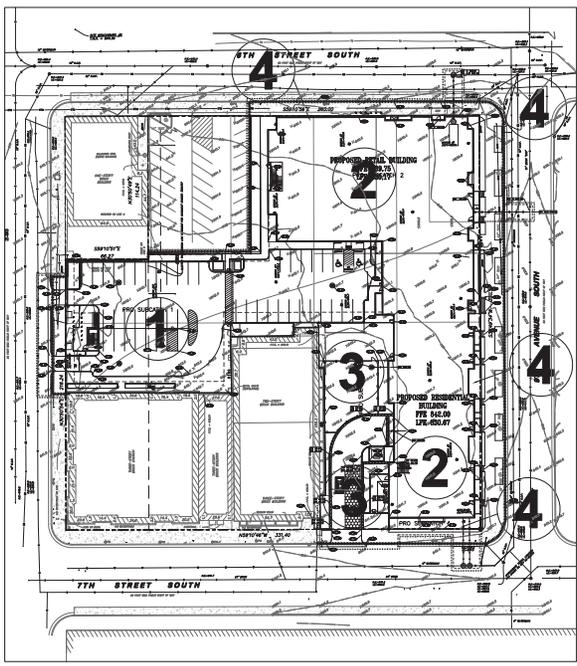
SWPPP IMPLEMENTATION, INSTALLATION, OPERATION AND BMP MAINTENANCE BY GENERAL CONTRACTOR.

NAME: _____
CERTIFICATION #: _____
DATE: _____

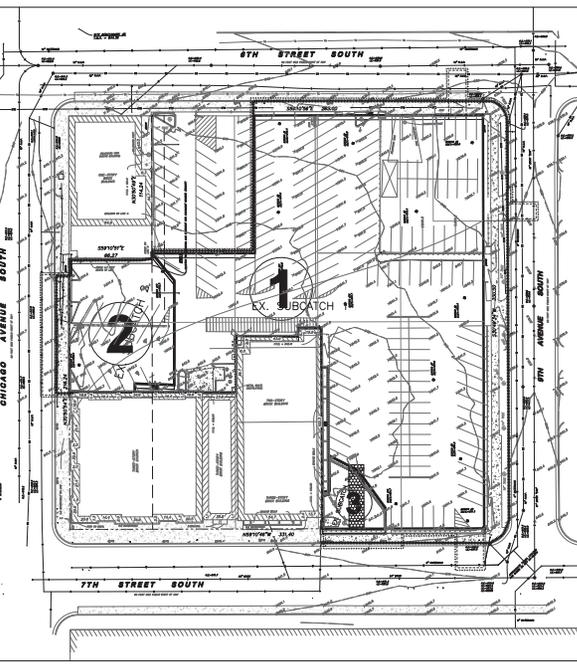
MINIMUM ESTIMATED QUANTITIES FOR EROSION CONTROL

ITEM DESCRIPTION	ESTIMATED QUANTITY
DRAINAGE STRIP INLET FILTER	1 EACH
SOIL CONSTRUCTION ENTRANCE	1 EACH
CONCRETE WASHOUT	1 EACH
BEST MANAGEMENT PRACTICE	1500 LF

NOTE: QUANTITIES SHOWN ARE THE MINIMUM REQUIRED. ADDITIONAL QUANTITIES MAY BE NEEDED IF REQUIRED BY THE MPCA, WATERFED DISTRICT, OR CITY. LOCATION OF STOCKPILES FOR FINAL DETERMINATION OF QUANTITIES PRIOR TO CONSTRUCTION.



2 PROPOSED CONDITIONS
1=40'



1 EXISTING CONDITIONS
1=40'



EXISTING DRAINAGE AREAS

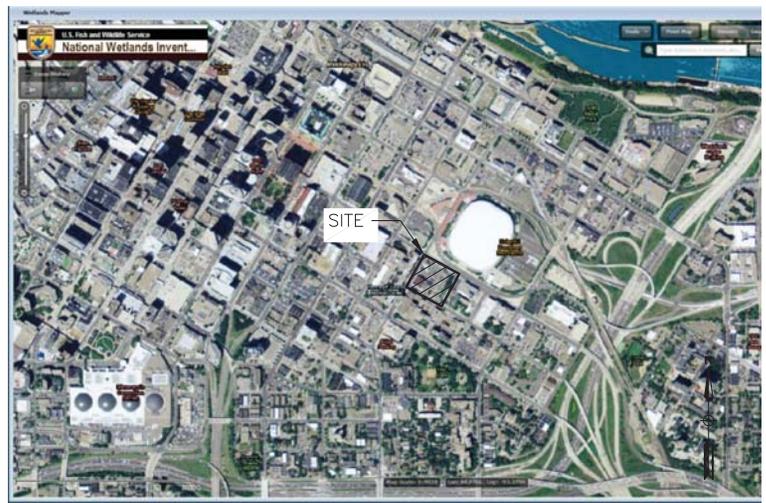
DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	2-YR STORM (0.87") RUNOFF (CFS)	10-YR STORM (2.33") RUNOFF (CFS)	100-YR STORM (7.49") RUNOFF (CFS)
1	1.29	0.02	1.31	2,100	10,000	15,000
2	0.19	0.01	0.20	800	1,222	1,988
3	0.00	0.00	0.00	0.00	0.00	0.00
TOTAL	1.48	0.03	1.51	6.16	11.22	16.99

PROPOSED DRAINAGE AREAS

DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	2-YR STORM (0.87") RUNOFF (CFS)	10-YR STORM (2.33") RUNOFF (CFS)	100-YR STORM (7.49") RUNOFF (CFS)
1	0.48	0.07	0.55	2,011	3,133	5,211
2	0.71	0.00	0.71	3,711	8,853	15,100
3	0.09	0.13	0.22	0.39	0.73	1.51
4	0.07	0.00	0.07	0.29	0.44	0.76
TOTAL	1.35	0.20	1.55	3.40	5.13	8.42

STORMWATER RUNOFF SUMMARY

	2-YR STORM (0.87") RUNOFF (CFS)	10-YR STORM (2.33") RUNOFF (CFS)	100-YR STORM (7.49") RUNOFF (CFS)
EXISTING SITE	6.16	9.44	15.38
PROPOSED SITE	3.40	5.13	8.42

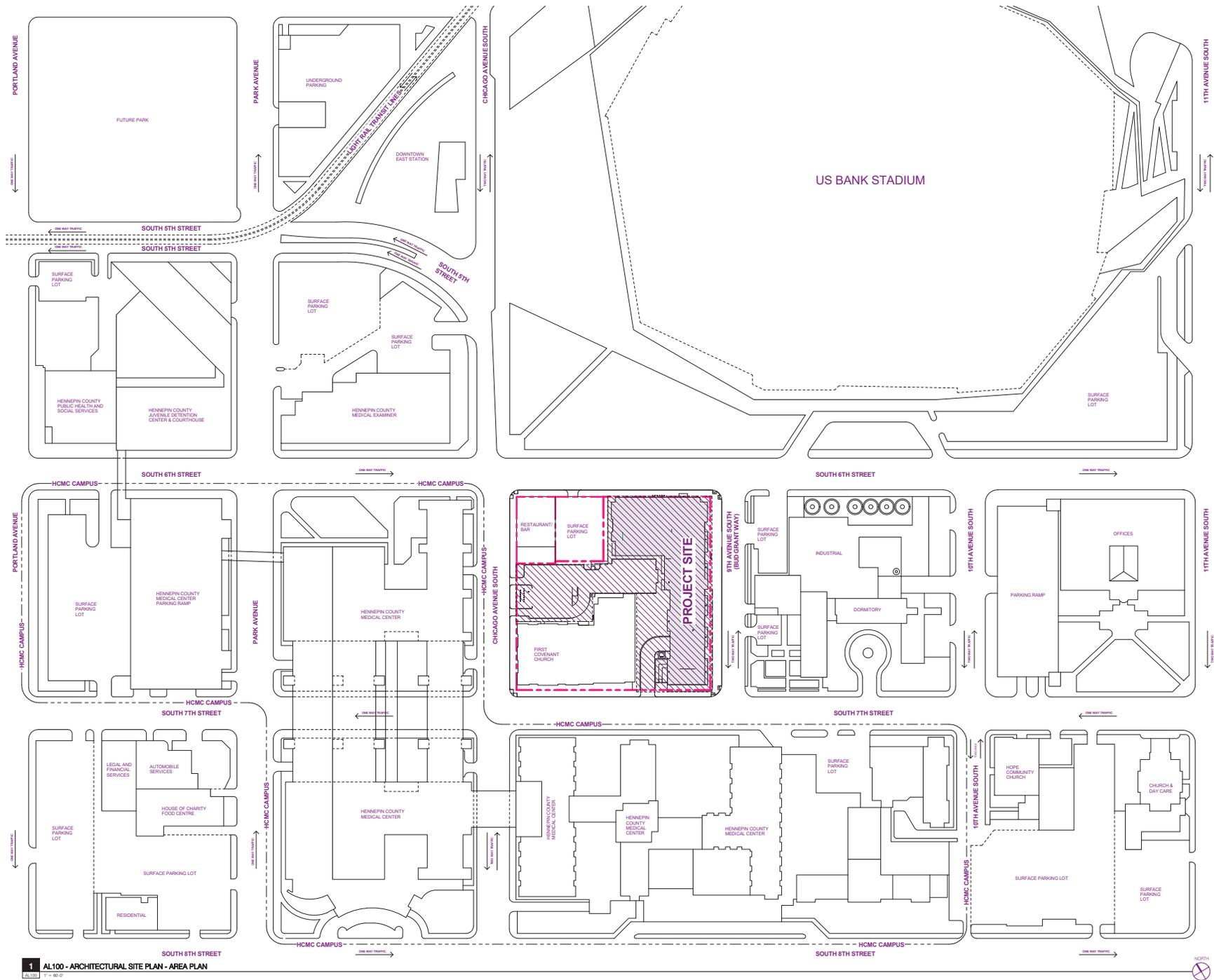


NATIONAL WETLANDS INVENTORY MAP
MINNEAPOLIS, MN

SPECIAL AND IMPAIRED WATERS

THESE SPECIAL AND IMPAIRED WATERS ARE LOCATED WITHIN ONE MILE AROUND PERIMETER OF THE PROJECT LIMITS AND RECEIVE RUNOFF FROM THESE SPECIAL AND IMPAIRED WATERS. THE BMP'S DESCRIBED IN APPENDIX A OF THE NIDES PERMIT WILL APPLY TO ALL AREAS OF THE SITE.

WATERBODY	IMPAIRMENT(S)
_____	_____



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Beckman
 SIGNED: Scott Beckman
 DATE: 6.6.2016
 LICENSE #: 47331

REVISIONS
 # Date Description

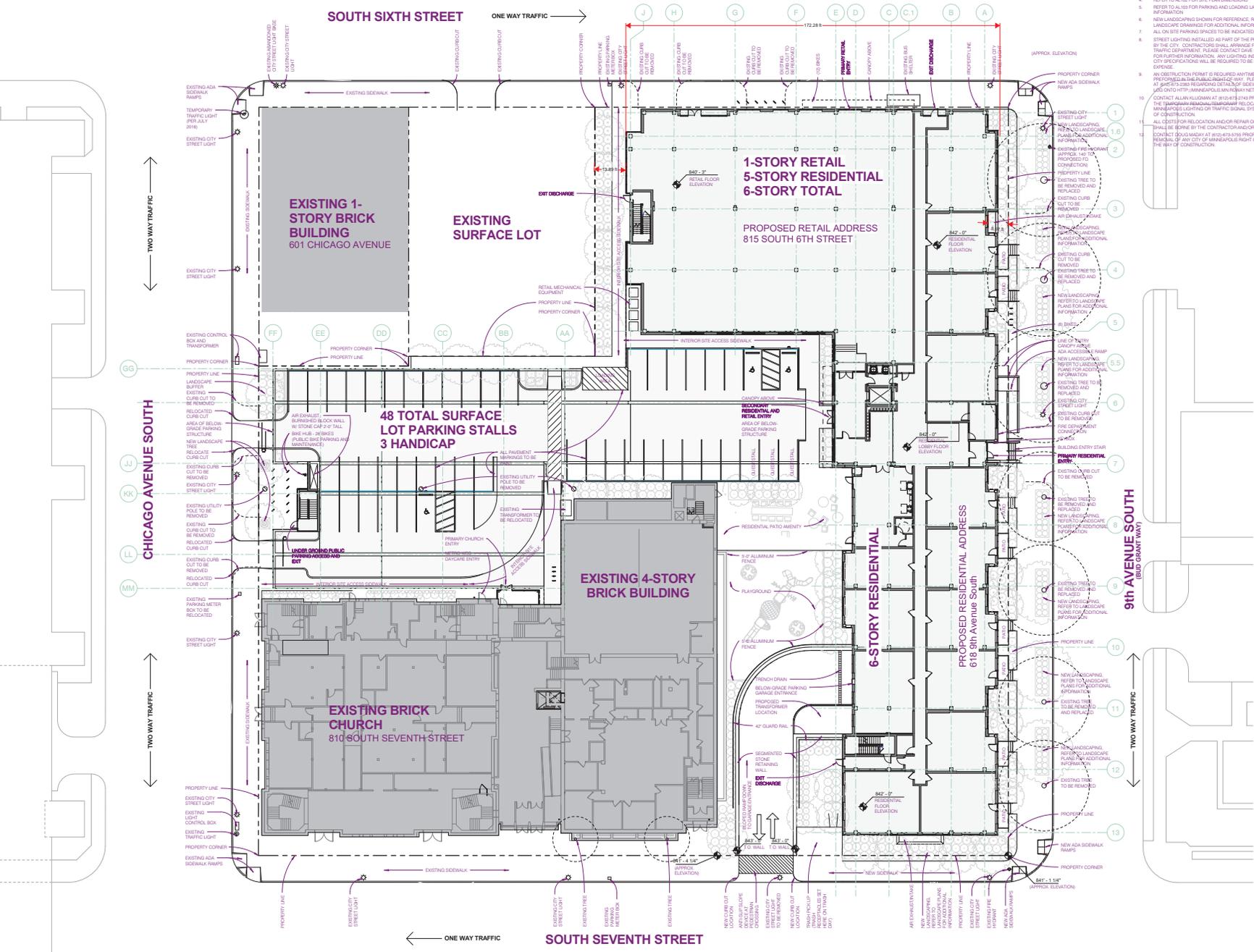
DATE: 6.6.2016
 PROJECT #: 15-0000
 PHASE: LAND USE APPLICATION

DRAWN BY: MJS
 CHECKED BY: SB

ARCHITECTURAL SITE
 PLAN - AREA
 PLAN

AL100

C:\Users\jrb\OneDrive\Documents\AL100\CHDC\WorkArea\Images\AL100_7A_6.6.2016.dwg
 6/2/2016 8:38:30 AM



- GENERAL NOTES:**
1. BAY ZONING COMMERCIAL ZONING DISTRICT. NO SETBACKS.
 2. SHOW STORAGE TO BE CONTRACTED OFFSITE BY OWNER.
 3. REFER TO AL101 FOR NOTATION OF SITE PLUM ELEMENTS.
 4. REFER TO AL102 FOR SITE PLAN DIMENSIONS.
 5. REFER TO AL103 FOR PARKING AND LOADING LANDSCAPE SCREENING INFORMATION.
 6. NEW LANDSCAPING SHOWN FOR REFERENCE. REFER TO ATTACHED LANDSCAPE DRAWINGS FOR ADDITIONAL INFORMATION.
 7. ALL ON SITE PARKING SPACES TO BE INDICATED WITH PAINTED STRIPES.
 8. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSTALLED BY THE CITY. CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT TO BE CONDUCTED PRIOR TO THE STARTING FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT OWNERS EXPENSE.
 9. AN OBSTRUCTION PERMIT IS REQUIRED. ANY CONSTRUCTION WORK IS PERFORMED ON THE PUBLIC HIGHWAY. PLEASE CONTACT SCOTT KRAMER AT 612-435-3100 FOR PERMITS. SEE SECTION 1 AND LINE CLOSURES LOGS ON TO HTTP://MINNEAPOLIS.MN.GOV/NET/FORMS.
 10. CONTACT ALAN HULOGAN AT 612-435-3100 PRIOR TO CONSTRUCTION FOR THE TRAFFIC SIGNALING DETAIL. SEE SECTION 1 AND LINE CLOSURES LOGS ON TO HTTP://MINNEAPOLIS.MN.GOV/NET/FORMS.
 11. ALL CLOSURES OR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES SHALL BE DONE BY THE CONTRACTOR AND/OR PROPERTY OWNER. CONTACT SCOTT KRAMER AT 612-435-3100 PRIOR TO CONSTRUCTION FOR THE RECORD. IF ANY CITY OF MINNEAPOLIS RIGHT OF WAY SIDE TWO MAY BE IN THE WAY OF CONSTRUCTION.

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 146
MINNEAPOLIS, MN 55401
612-435-3100
CONSULTANT

Design Development
7.19.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

SIGNED: Scott Backman
DATE: 7.19.2016
LICENSE #: 47331

REVISIONS
Date Description

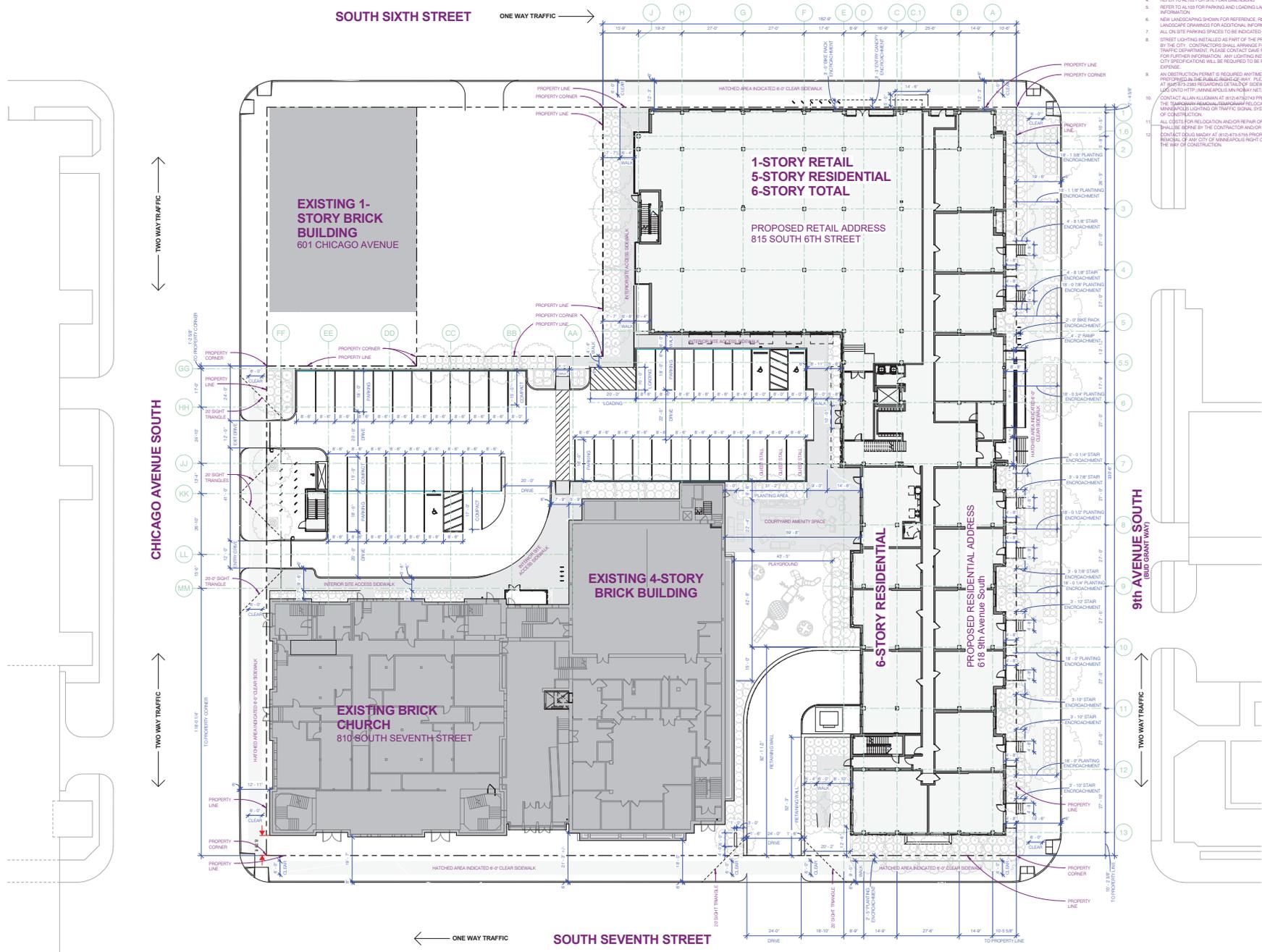
DATE: 7.19.2016
PROJECT #: 15-0000
PHASE: 03-00-00

DRAWN BY: MGS
CHECKED BY: SB

ARCHITECTURAL
SITE PLAN

AL101

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- GENERAL NOTES:**
1. SAN ZONING COMMERCIAL ZONING DISTRICT NO SETBACKS
 2. SNOW STORAGE TO BE CONTRACTED OFFSITE BY OWNER
 3. REFER TO ALL 101 FOR NOTATION OF SITE PLACEMENTS
 4. REFER TO ALL 102 FOR SITE PLAN DIMENSIONS
 5. REFER TO ALL 103 FOR PARKING AND LOADING LANDSCAPE SCREENING INFORMATION
 6. NEW LANDSCAPING SHOWN FOR REFERENCE. REFER TO ATTACHED LANDSCAPE DRAWINGS FOR ADDITIONAL INFORMATION
 7. ALL ON SITE PARKING SPACES TO BE INDICATED WITH PAINTED STRIPES
 8. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSTALLED BY THE CITY. CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT. PLEASE CONTACT SUE FRENKEL AT 612-673-8175 FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING CITY PROVISIONS WILL BE REQUIRED TO BE REINSTALLED AT OWNER'S EXPENSE.
 9. AN OBSTRUCTION PERMIT IS REQUIRED ANYTIME CONSTRUCTION WORK IS PERFORMED IN THE PUBLIC RIGHT-OF-WAY. PLEASE CONTACT SCOTT KRAMER AT 612-673-8175 REGARDING DETOURING, SIGNALS AND LANE CLOSURES. LOG ON TO [HTTP://MINNEAPOLIS.MN.GOV/NET](http://MINNEAPOLIS.MN.GOV/NET) FOR A PERMIT.
 10. CONTACT ALAN KULDAK AT 612-673-8175 PRIOR TO CONSTRUCTION FOR THE TRAFFIC SIGNALS, TRAFFIC SIGNALS AND LANE CLOSURES. LOG ON TO [HTTP://MINNEAPOLIS.MN.GOV/NET](http://MINNEAPOLIS.MN.GOV/NET) FOR A PERMIT.
 11. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER. CONTACT DEBID MUDRY AT 612-673-8175 PRIOR TO CONSTRUCTION FOR THE RECORDS OF ANY CITY OF MINNEAPOLIS REPORT OF WORK DONE THAT MAY BE IN THE WAY OF CONSTRUCTION.

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.459.3100

CONSULTANT

Land Use Application
6.30.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backlund
SIGNED: Scott Backlund
DATE: 6.30.2016
LICENSE #: 47331

REVISIONS
Date Description

DATE: 6.30.2016
PROJECT #: 15.0007
PHASE: LAND USE APPLICATION

DRAWN BY: SB
CHECKED BY: SB

ARCHITECTURAL SITE PLAN - DIMENSIONED

AL102

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901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Beckman
BIGNED Scott Beckman
DATE 6.6.2016
LICENSE # 47331

REVISIONS
Date Description

DATE 6.6.2016
PROJECT # 15-0007
PHASE LAND USE APPLICATION
DRAWN BY LM
CHECKED BY SB

SHADOW STUDY

AL104



MARCH/SEPTEMBER 21, 9:00AM



JUNE 21, 9:00AM



DECEMBER 21, 9:00AM



MARCH/SEPTEMBER 21, 12:00PM



JUNE 21, 12:00PM



DECEMBER 21, 12:00PM



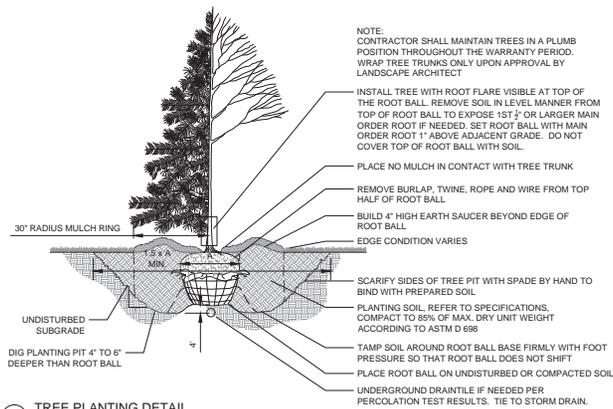
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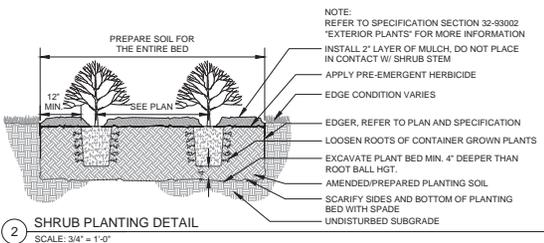
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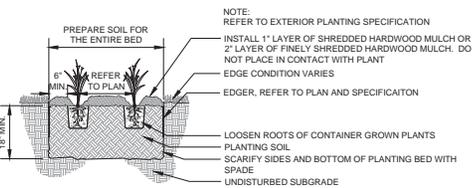
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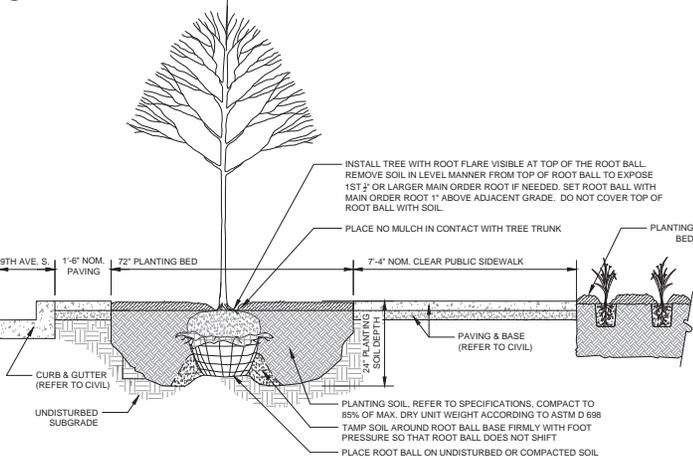
1 TREE PLANTING DETAIL
SCALE: 1/2" = 1'-0"



2 SHRUB PLANTING DETAIL
SCALE: 3/4" = 1'-0"



3 PERENNIAL PLANTING DETAIL
SCALE: 3/4" = 1'-0"



4 SECTION AT 9TH AVE S SIDEWALK - TREE PLANTING DETAIL
SCALE: 3/4" = 1'-0"

TYPE	BOTANICAL NAME	COMMON NAME	ROOT / CONT.	SIZE	COMMENTS
12	ACER X FREEMANII 'ARMSTRONG'	ARMSTRONG MAPLE	B&B	3.0" CAL	SINGLE STRAIGHT LEADER
10	TILIA AMERICANA 'BOULEVARD'	BOULEVARD LINDEN	B&B	3.0" CAL	SINGLE STRAIGHT LEADER
3	GLEDITSIA TRIACANTHOS VAR. INERMIS 'SKYLINE'	SKYLINE HONEYLOCUST	B&B	3.0" CAL	SINGLE STRAIGHT LEADER
12	BETULA POPULIFOLIA 'WHITESPIRE'	WHITESPIRE BIRCH	B&B	2.0" CAL	SINGLE STRAIGHT LEADER
8	QUERCUS X WAREI 'LONG'	REGAL PRINCE OAK	B&B	2.5" CAL	SINGLE STRAIGHT LEADER
23	CORNUS SERICEA 'BALADELIN'	FIREBRAND DOGWOOD	CONT.	#5	PLANT 3.5' OC.
20	CORNUS HESSEI 'GARDEN GLOW'	GARDEN GLOW DOGWOOD	CONT.	#5	PLANT 3.5' OC.
3	SALIX PURPUREA 'NANA'	ARCTIC BLUE LEAF WILLOW	CONT.	#5	PLANT 3.5' OC.
8	DIERVILLA LONICERA	DWARF BUSH HONEYSUCKLE	CONT.	#2	PLANT 3.0' OC.
90	PHYSCARPUS OPULIFOLIUS 'LITTLE DEVIL'	LITTLE DEVIL NINEBARK	CONT.	#5	PLANT 3.5' OC.
17	RIBES ALPINUM 'GREEN MOUND'	GREEN MOUND CURRANT	CONT.	#2	PLANT 2.5' OC.
36	SORBARIA SORBIFOLIA 'SEM'	'SEM' FALSE SPIREA	CONT.	#2	PLANT 3.0' OC.
14	HYDRANGEA ARBORESCENS 'ANNABELLE'	ANNABELLE HYDRANGEA	CONT.	#3	PLANT 3.5' OC.
74	RIBES ALPINUM	ALPINE CURRANT	CONT.	#3	PLANT 3.5' OC.
10	THUJA OCCIDENTALIS 'RUSHMORE'	RUSHMORE ARBORVITAE	CONT.	#10	PLANT 5.0' OC.
118	JUNIPERUS CHINENSIS 'SEA GREEN'	MINT JULEP JUNIPER	CONT.	#5	PLANT 4.5' OC.
8	TAXUS X MEDIA 'TAUNTON'	TAUNTON YEW	CONT.	#7	PLANT 4.0' OC.
4	TAXUS CUSPIDATA 'CAPITATA'	JAPANESE UPRIGHT YEW	CONT.	#25	PLANT 4.0' OC.
16	CALAMAGROSTIS BRACHYTRICHA	BRACHYTRICHA FEATHER REED GRASS	CONT.	#1	PLANT 3.0' OC.
52	HOSTA 'REMEMBER ME'	REMEMBER ME HOSTA	CONT.	#1	PLANT 2.0' OC.

PLANTS WITHIN PROJECT BOUNDARY

TYPE	BOTANICAL NAME	COMMON NAME	ROOT / CONT.	SIZE	COMMENTS
4	CELTIS OCCIDENTALIS	HACKBERRY	B&B	3.0" CAL	SINGLE STRAIGHT LEADER
4	GYMNOCLADUS DIOICUS	KENTUCKY COFFEETREE	B&B	2.5" CAL	SINGLE STRAIGHT LEADER
44	DIERVILLA SESSIFOLIA 'L'PDC PODARAS'	COOL SPLASH HONEYSUCKLE	CONT.	#2	PLANT 3.0' OC.
12	RIBES ALPINUM	ALPINE CURRANT	CONT.	#5	PLANT 3.5' OC.
128	CALAMAGROSTIS KARL FOERSTER	KARL FOERSTER FEATHER REED GRASS	CONT.	#1	PLANT 3.0' OC.
3	CALAMAGROSTIS BRACHYTRICHA	BRACHYTRICHA FEATHER REED GRASS	CONT.	#1	PLANT 3.0' OC.

PLANTS IN PUBLIC ROW

GENERAL NOTES

- CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF WORK.
- CONTRACTOR SHALL VERIFY PLAN LAYOUT AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN OR INTENT OF THE LAYOUT.
- CONTRACTOR SHALL ASSURE COMPLIANCE WITH APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK AND MATERIALS SUPPLIED.
- CONTRACTOR SHALL PROTECT EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING CONSTRUCTION OPERATIONS. DAMAGE TO SAME SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL VERIFY ALIGNMENT AND LOCATION OF UNDERGROUND AND ABOVE GRADE UTILITIES AND PROVIDE THE NECESSARY PROTECTION FOR SAME BEFORE CONSTRUCTION BEGINS (MINIMUM 1' CLEARANCE).
- CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.
- UNDERGROUND UTILITIES SHALL BE INSTALLED SO THAT TRENCHES DO NOT CUT THROUGH ROOT SYSTEMS OF EXISTING TREES TO REMAIN.
- EXISTING CONTOURS, TRAILS, VEGETATION, CURB/GUTTER AND OTHER ELEMENTS ARE BASED UPON INFORMATION SUPPLIED TO THE LANDSCAPE ARCHITECT BY OTHERS. CONTRACTOR SHALL VERIFY DISCREPANCIES PRIOR TO CONSTRUCTION AND NOTIFY LANDSCAPE ARCHITECT OF SAME.
- HORIZONTAL AND VERTICAL ALIGNMENT OF PROPOSED WALKS, TRAILS OR ROADWAYS ARE SUBJECT TO FIELD ADJUSTMENT REQUIRED TO CONFORM TO LOCALIZED TOPOGRAPHIC CONDITIONS AND TO MINIMIZE TREE REMOVAL AND GRADING. CHANGES IN ALIGNMENT AND GRADINGS MUST BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO IMPLEMENTATION.
- CONTRACTOR SHALL REVIEW THE SITE FOR DEFICIENCIES IN SITE CONDITIONS WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR WARRANTY. UNDESIRABLE SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK.
- CONTRACTOR IS RESPONSIBLE FOR ONGOING MAINTENANCE OF NEWLY INSTALLED MATERIALS UNTIL TIME OF SUBSTANTIAL COMPLETION. REPAIR OF ACTS OF VANDALISM OR DAMAGE WHICH MAY OCCUR PRIOR TO SUBSTANTIAL COMPLETION SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
- EXISTING TREES OR SIGNIFICANT SHRUB MASSINGS FOUND ON SITE SHALL BE PROTECTED AND SAVED UNLESS NOTED TO BE REMOVED OR ARE LOCATED IN AN AREA TO BE GRADED. QUESTIONS REGARDING EXISTING PLANT MATERIAL SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO REMOVAL.
- EXISTING TREES TO REMAIN, UPON DIRECTION OF LANDSCAPE ARCHITECT, SHALL BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD, DAMAGED AND RUBBING BRANCHES.
- CONTRACTOR SHALL PREPARE AND SUBMIT A WRITTEN REQUEST FOR THE SUBSTANTIAL COMPLETION INSPECTION OF LANDSCAPE AND SITE IMPROVEMENTS PRIOR TO SUBMITTING FINAL PAY REQUEST.
- CONTRACTOR SHALL PREPARE AND SUBMIT REPRODUCIBLE AS-BUILT DRAWING(S) OF LANDSCAPE INSTALLATION, IRRIGATION AND SITE IMPROVEMENTS UPON COMPLETION OF CONSTRUCTION INSTALLATION AND PRIOR TO SUBSTANTIAL COMPLETION.
- SYMBOLS ON PLAN DRAWING TAKE PRECEDENCE OVER SCHEDULES IF DISCREPANCIES IN QUANTITIES EXIST. SPECIFICATIONS AND DETAILS TAKE PRECEDENCE OVER NOTES.

GRADING NOTES

- CONTRACTOR SHALL CONTACT PUBLIC UTILITIES FOR LOCATION OF UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. LANDSCAPE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE IF DAMAGED DURING CONSTRUCTION AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL PROVIDE PROPER EROSION CONTROL MEASURES AS REQUIRED TO INSURE THAT EROSION IS KEPT TO AN ABSOLUTE MINIMUM - SEE CIVIL SPECIFICATIONS.
- PROVIDE TEMPORARY COVERING FOR CATCH BASINS AND MAN HOLES UNTIL FINISHED GRADING IS COMPLETE - SEE CIVIL SPECIFICATIONS.

PLANTING NOTES

- NO PLANTS WILL BE INSTALLED UNTIL FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- PROPOSED PLANT MATERIAL SHALL COMPLY WITH THE CURRENT EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, ANSI Z60.1.
- STREET AND BOULEVARD TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE PAVED SURFACE.
- PROPOSED PLANT MATERIAL SHALL BE LOCATED AND STAKED AS SHOWN ON PLAN. LANDSCAPE ARCHITECT MUST APPROVE STAKING OF PLANT MATERIAL PRIOR TO DIGGING.
- NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS GRANTED BY THE LANDSCAPE ARCHITECT TO THE CONTRACTOR PRIOR TO THE SUBMISSION OF BID.
- ADJUSTMENTS IN LOCATION OF PROPOSED PLANT MATERIALS MAY BE NEEDED IN FIELD. LANDSCAPE ARCHITECT MUST BE NOTIFIED PRIOR TO ADJUSTMENT OF PLANTS.
- PLANT MATERIALS TO BE INSTALLED PER PLANTING DETAILS.
- TREE WRAPPING MATERIAL SHALL BE TWO-WALLED PLASTIC SHEETING APPLIED FROM TRUNK FLARE TO FIRST BRANCH. WRAP SMOOTH-BARKED DECIDUOUS TREES PLANTED IN THE FALL FROM TO DECEMBER 1 AND REMOVE WRAPPING AFTER MAY 1.

IRRIGATION NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AN IRRIGATION LAYOUT PLAN AND SPECIFICATIONS AS PART OF THE SCOPE OF WORK. SUBMIT LAYOUT PLAN AND SPECIFICATIONS FOR APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO ORDER AND/OR CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT SODD/SEEDING AND PLANTED AREAS ARE IRRIGATED PROPERLY, INCLUDING THOSE AREAS DIRECTLY ADJACENT AND ABUTTING BUILDING FOUNDATION.
- CONTRACTOR SHALL VERIFY (EXISTING) IRRIGATION SYSTEM LAYOUT AND CONFIRM COMPLETE LIMITS OF IRRIGATION PRIOR TO SUPPLYING SHOP DRAWINGS.
- CONTRACTOR SHALL CONTACT LANDSCAPE ARCHITECT FOR INSPECTION AND APPROVAL OF ALL AREAS RECEIVING DRIP IRRIGATION PRIOR TO INSTALLATION OF ANY MULCH.
- CONTRACTOR SHALL PROVIDE THE OWNER WITH AN IRRIGATION SCHEDULE APPROPRIATE TO THE PROJECT SITE CONDITIONS AND TO PLANTED MATERIAL GROWTH REQUIREMENTS.
- CONTRACTOR SHALL ENSURE THAT SOIL CONDITIONS AND COMPACTION ARE ADEQUATE TO ALLOW FOR PROPER DRAINAGE AROUND THE CONSTRUCTION SITE. UNDESIRABLE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK. IT SHALL BE THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY TO ENSURE PROPER SURFACE AND SUBSURFACE DRAINAGE IN PLANTING AREAS.

LANDSCAPE NOTES



901 NORTH THIRD STREET, SUITE 148
MINNEAPOLIS, MN 55401
952-469-3100

CONSULTANT



412 2nd Avenue West, Suite 110
Minneapolis, MN 55401
p: 612-332-7527 www.dfgroup.com

PDR Application
Resubmitted
6.30.2016

I hereby certify that the plan, specifications, or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.

DESIGNED BY: Jason Sympherson
DATE: 6.30.2016
LICENSE #:

REVISIONS
Date Description

DATE: 6.30.2016
PROJECT #: 15-0027
PHASE: 02

DRAWN BY: JS

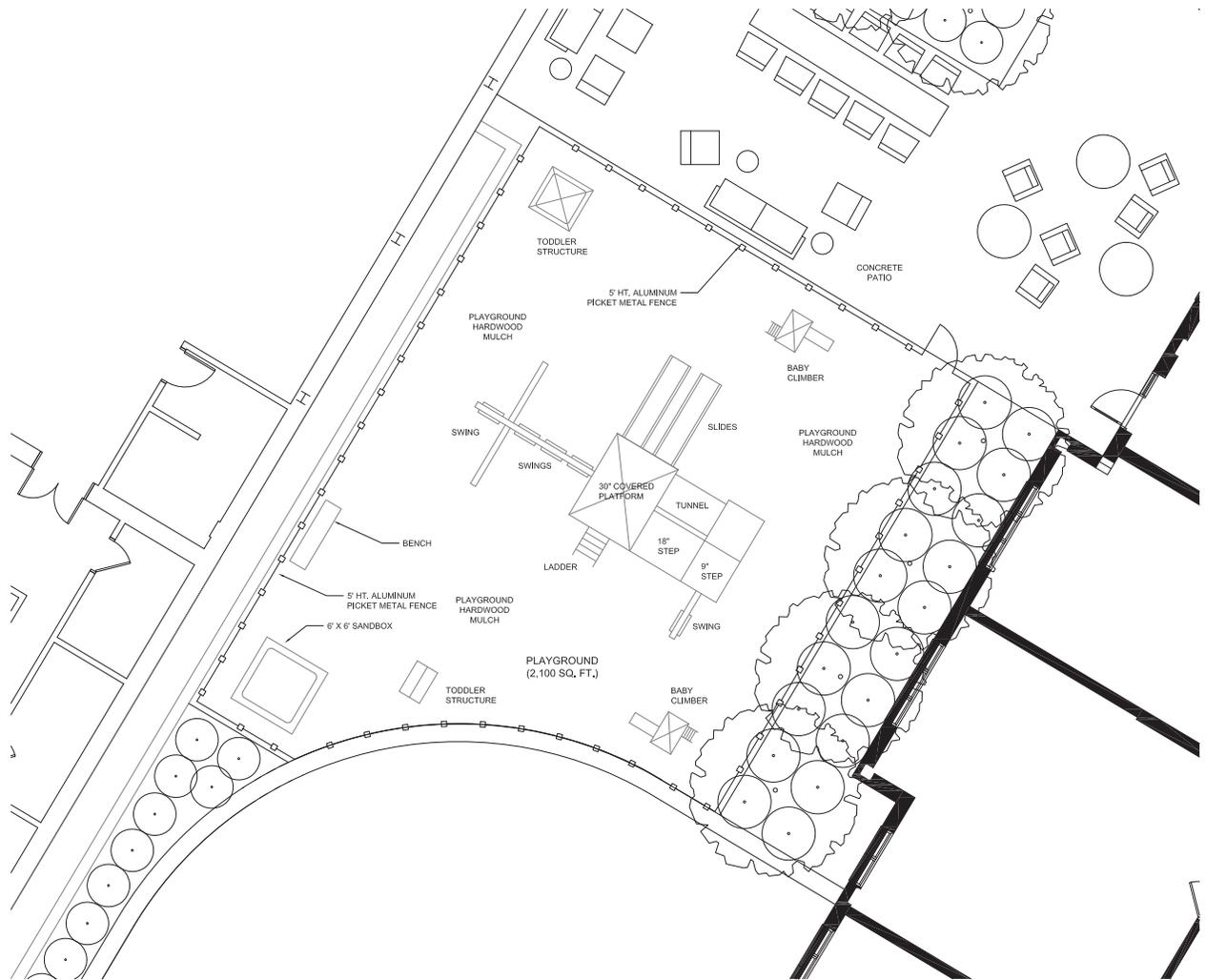
CHECKED BY: JS

LANDSCAPE
DETAILS

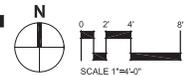
L101



RELOCATED PLAYGROUND EQUIPMENT



PLAYGROUND PLAN



East Town
Apartments
MINNEAPOLIS, MN



501 NORTH THIRD STREET, SUITE 140
MINNEAPOLIS, MN 55401
(612) 654-2100

CONSULTANT

DF/
DANON FABER
LANDSCAPE ARCHITECTS
412 DuSable North, Suite 410
Minneapolis, MN 55401
P: 612.222.1222 www.danonfaber.com

PDR Application
Revised 6/30/2016

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the state of Minnesota.

Signature
DESIGNED: Jesse Symonides
DATE: 6/30/2016
LICENSE #: 28070

REVISIONS
Date Designer

DATE: 6/30/2016
PROJECT #: 14-007
PHASE: SD

DRAWN BY: .JS

CHECKED BY: .JS

PLAYGROUND
PLAN

L102

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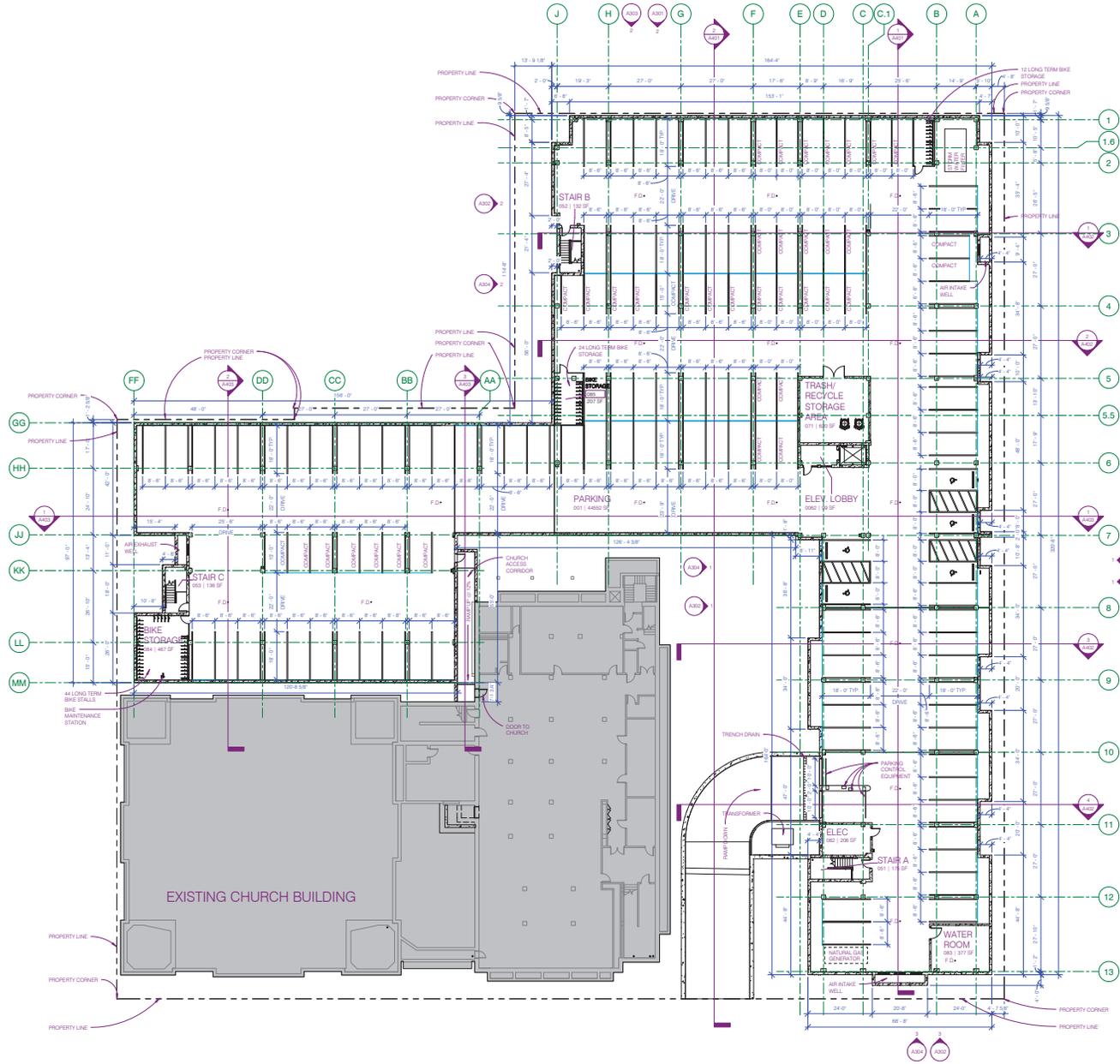
© 2016 Ryan & Associates, LLC. All Rights Reserved. 6/30/2016 1:25:57 PM
 C:\Users\jrs\AppData\Local\Temp\141007_CDDC\Workarea\Working\141007_A16_rsd\background.dwg

GENERAL NOTES:

- DIMENSIONS AT EXTERIOR WOOD STUD AND METAL STUD WALLS ARE TO OUTSIDE FACE OF SHEATHING
- DIMENSIONS AT EXTERIOR CMU WALLS ARE TO FACE OF CMU
- DIMENSIONS AT INTERIOR WALLS ARE TO FACE OF WALL SYSTEM UNLESS NOTED BY THE SYMBOL Q INDICATING CENTERLINE OF SYSTEM
- DIMENSIONS AT LIFT DIMENSIONS ARE TO CENTERLINE OF WALL SYSTEM
- ALL RESIDENTIAL DEMISING WALLS ON LEVEL 1 ARE TYPE **KA, NL, NDA** AND **KB** 1 HR FIRE RATING WITH ACUSTIC RATING
- ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 4 ARE TYPE **VA, VB, VC** AND **VB** 1 HR FIRE RATING WITH ACUSTIC RATING
- ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 1 ARE TYPE **BB** 1 HR FIRE RATING WITH ACUSTIC RATING UNLESS OTHERWISE NOTED
- ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 2 THRU LEVEL 4 ARE TYPE **VB** 1 HR FIRE RATING WITH ACUSTIC RATING UNLESS OTHERWISE NOTED
- EXTERIOR WOOD STUDS ARE FIRE TREATED CMU UNLESS OTHERWISE NOTED
- EXTEND ALL SHAFTS TO UNDERSIDE OF ROOF SHEATHING OR TOP OF ROOF PENETRATIONS WHERE APPLICABLE
- EXTEND 1 HR FIRE BARRIERS TO UNDERSIDE OF SHEATHING WHERE APPLICABLE
- NO PENETRATIONS OR OPENINGS ALLOWED IN SHEATHING UNLESS THEY EXPRESSLY DENOTE THE DRAWT
- PROVIDE NON-COMBUSTIBLE MATERIALS IN RETURN PLenums UNLESS SO NOTED
- ALL UNITS ARE TYPE B ACCESSIBLE UNLESS OTHERWISE NOTED
- MAINTAIN 8'-2" MIN. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLANS. ALL OTHER PARKING AREAS SHALL PROVIDE 7'-2" MIN. CLEAR HEIGHT

KEYNOTES:

- KA1 30" X 48" CLEAR FLOOR SPACE INDICATED BY DASHED RECTANGLE
- UA3 30" X 48" FORWARD APPROACH AT SWH AND AT WORK SPACE, CONTINUE FLOORING UNDER SWH
- UA6 DISHWASHER TO FIT UNDERNEATH 34" COUNTERTOP (32" MAX)
- U46 FRONT CONTROLS AT RANGE
- U48 PROVIDE FRONT LOADING WASHER AND DRYER
- U49 GRAB BARS
- UA2 CABINET TO HAVE DOORS WITH INTERNAL TOE KICK AND NO BARE COUNTERTOP MICROWAVE
- UA14



1 SUBLEVEL 1 PLAN - OVERALL
 1/8" = 1'-0"

East Town
 Apartments
 Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
 MINNEAPOLIS, MN 55401
 612.495.3100

CONSULTANT

Land Use Application
 6.6.2016

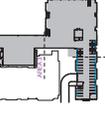
I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backstrom
 SIGNED: Scott Backstrom
 DATE: 6.6.2016
 LICENSE #: 47324

REVISIONS
 # Date Description

DATE: 6.6.2016
 PROJECT #: 15-0000
 PHASE: LAND USE APPLICATION
 DRAWN BY: MGSB
 CHECKED BY: SB

KEP PLAN



SUBLEVEL 1
 PLAN - OVERALL

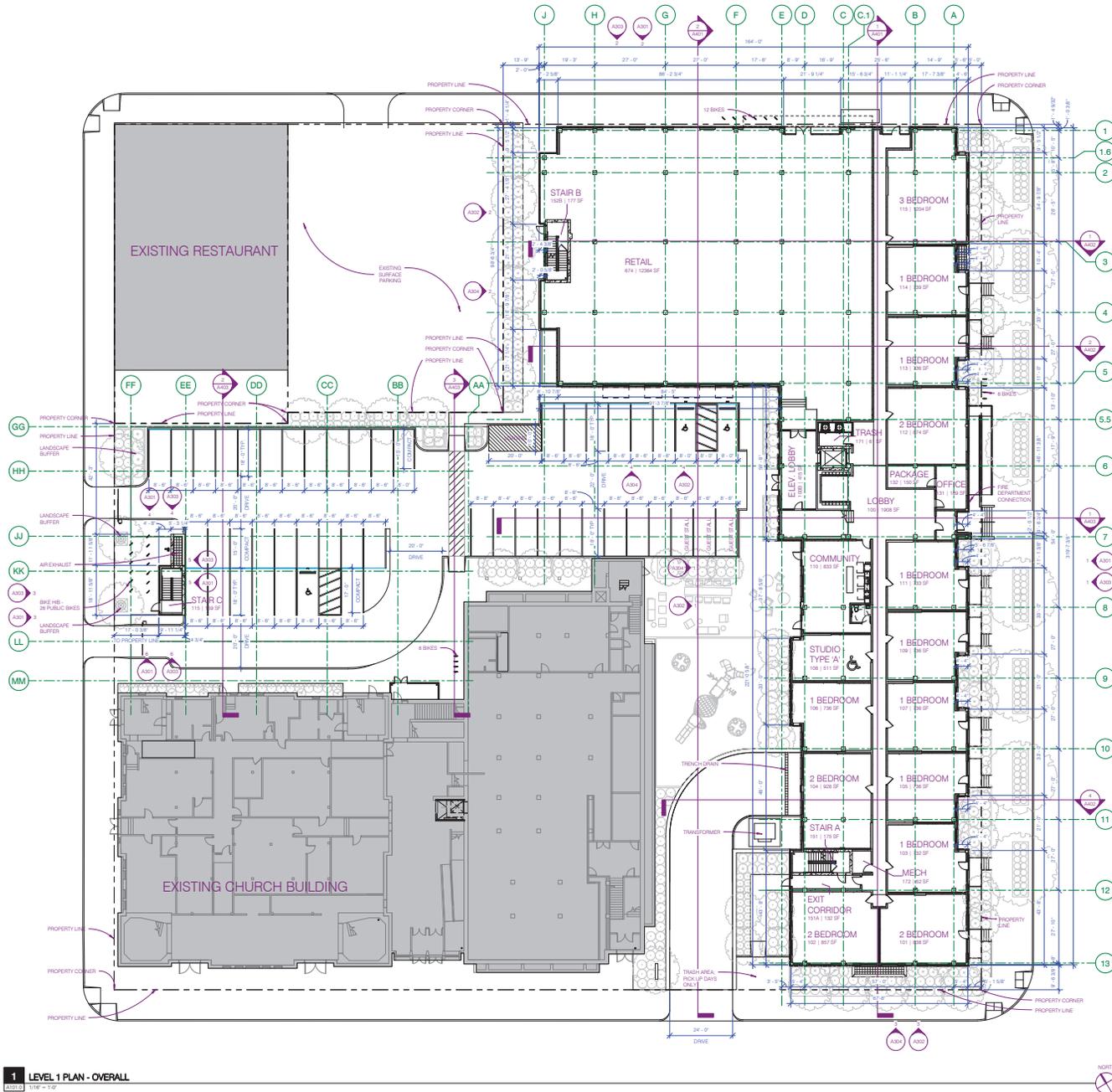
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GENERAL NOTES:

1. DIMENSIONS AT EXTERIOR WOOD STUD AND METAL STUD WALLS ARE TO OUTSIDE FACE OF SHEATHING.
2. DIMENSIONS AT EXTERIOR CMU WALLS ARE TO FACE OF CMU.
3. DIMENSIONS AT EXTERIOR WALLS ARE TO FACE OF WALL SYSTEM UNLESS NOTED BY THE SYMBOL Q INDICATING CENTERLINE OF SYSTEM.
4. DIMENSIONS AT UNIT DEMOS ARE TO CENTERLINE OF WALL SYSTEM.
5. ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 ARE TYPE **VA, VA, VA** AND **VA** FIRE RATING WITH ACoustIC RATING.
6. ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VA, VA, VA** AND **VA** FIRE RATING WITH ACoustIC RATING.
7. ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 1 ARE TYPE **BB** 1 HR FIRE RATING WITH ACoustIC RATING UNLESS OTHERWISE NOTED.
8. ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VB** 1 HR FIRE RATING WITH ACoustIC RATING UNLESS OTHERWISE NOTED.
9. EXTERIOR WOOD STUDS ARE FIRE TREATED UNLESS OTHERWISE NOTED.
10. EXTEND ALL SHAPES TO UNDERSIDE OF ROOF SHEATHING OR TOP OF ROOF PENETRATIONS WHERE APPLICABLE.
11. EXTEND 1 HR FIRE BARRIERS TO UNDERSIDE OF SHEATHING WHERE APPLICABLE.
12. NO PENETRATIONS OR OPENINGS ALLOWED IN RETURN WALLS UNLESS THEY EXPRESSLY STATE THE OPPOSITE.
13. PROVIDE NON-COMBUSTIBLE MATERIALS IN RETURN PLUMBING WALLS ON SOFFIT.
14. ALL UNITS ARE TYPE B ACCESSIBLE UNLESS OTHERWISE NOTED.
15. MAINTAIN 8'-2" MIN. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLANS. ALL OTHER PARKING AREAS SHALL PROVIDE 7'-2" MIN. CLEAR HEIGHT.

KEYNOTES:



1 LEVEL 1 PLAN - OVERALL
AS21 1/8" = 1'-0"

East Town
 Apartments
 Minneapolis, MN



901 NORTH THIRD STREET, SUITE 146
 MINNEAPOLIS, MN 55401
 612.495.3100

CONSULTANT

Land Use Application
 6.30.2016

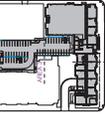
I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backlund
 SIGNED: Scott Backlund
 DATE: 6.30.2016
 LICENSE #: 47331

REVISIONS
 # Date Description

DATE: 6.30.2016
 PROJECT #: 15-0007
 PHASE: LAND USE APPLICATION
 DRAWN BY: MDSB
 CHECKED BY: SB

KEY PLAN



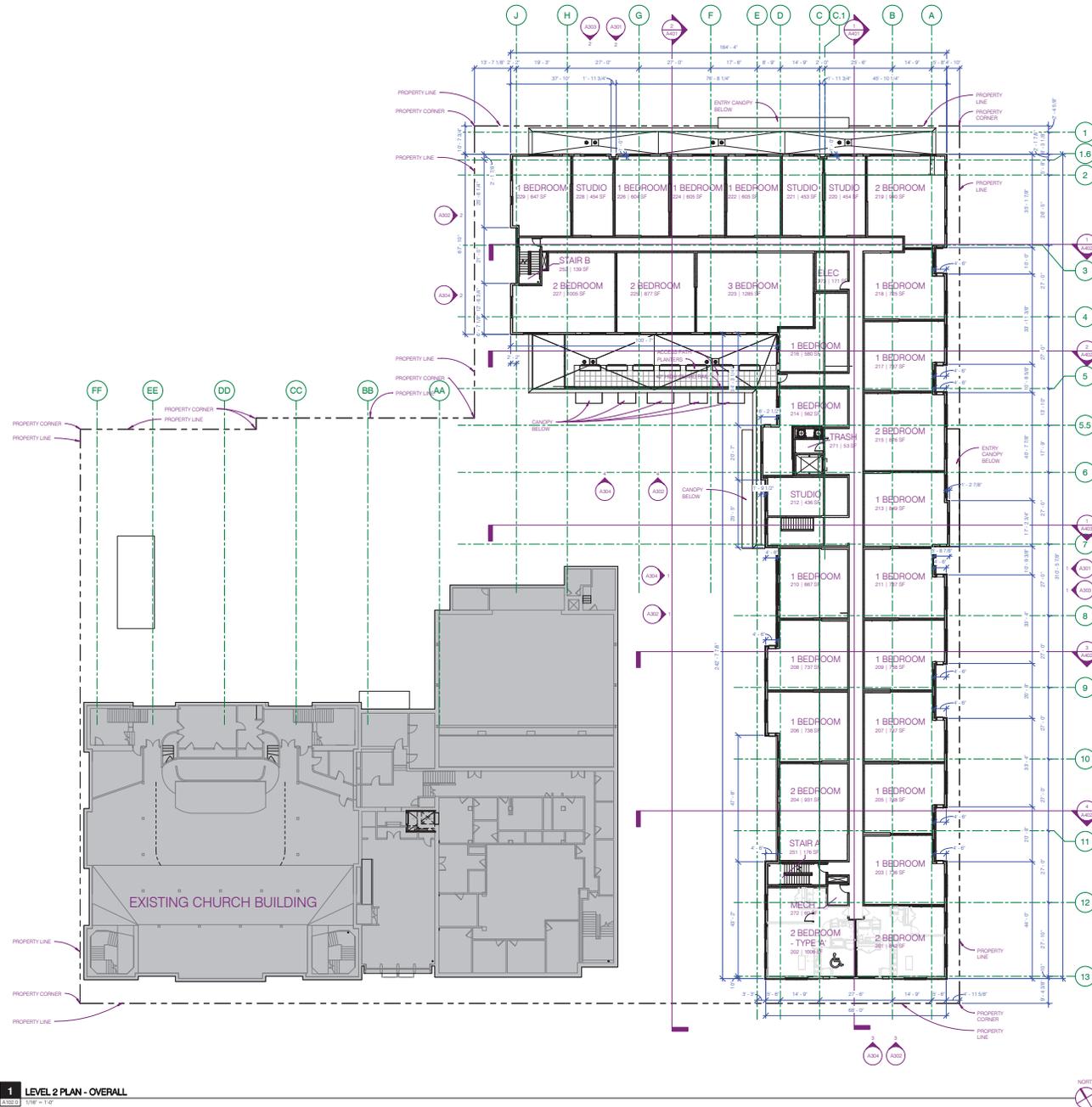
LEVEL 1 PLAN - OVERALL

A101.0

GENERAL NOTES:

1. DIMENSIONS AT EXTERIOR WOOD STUD AND METAL STUD WALLS ARE TO OUTSIDE FACE OF SHEATHING.
2. DIMENSIONS AT EXTERIOR CMU WALLS ARE TO FACE OF CMU.
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4. DIMENSIONS AT LIFT DEMISING ARE TO CENTERLINE OF WALL SYSTEM.
5. ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VA, VB, VA, VB** AND 1 HR FIRE RATING WITH ACROUSTIC RATING.
6. ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VA, VB, VA, VB** AND 1 HR FIRE RATING WITH ACROUSTIC RATING.
7. ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 1 ARE TYPE **BB** 1 HR FIRE RATING WITH ACROUSTIC RATING UNLESS OTHERWISE NOTED.
8. ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VB** 1 HR FIRE RATING WITH ACROUSTIC RATING UNLESS OTHERWISE NOTED.
9. EXTERIOR WOOD STUDS ARE FIRE TREATED UNLESS OTHERWISE NOTED.
10. EXTEND ALL SHAPES TO UNDERSIDE OF ROOF SHEATHING OR TOP OF ROOF PENETRATIONS WHERE APPLICABLE.
11. EXTEND 1 HR FIRE BARRIERS TO UNDERSIDE OF SHEATHING WHERE APPLICABLE.
12. NO PENETRATIONS OR OPENINGS ALLOWED IN SHUNT WALLS UNLESS THEY EXPRESSLY DENOTE THE DRAFT.
13. PROVIDE NON-COMBUSTIBLE MATERIALS IN RETURN PLenums (WALLS ON SOFFIT).
14. ALL UNITS ARE TYPE B ACCESSIBLE UNLESS OTHERWISE NOTED.
15. MAINTAIN 8'-2" MIN. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLANS. ALL OTHER PARKING AREAS SHALL PROVIDE 7'-2" MIN. CLEAR HEIGHT.

KEYNOTES:



1 LEVEL 2 PLAN - OVERALL
A102.0 1/8" = 1'-0"

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backman
SIGNED: Scott Backman
DATE: 6.6.2016
LICENSE #: 47331

REVISIONS

#	Date	Description

DATE: 6.6.2016
PROJECT #: 15-0000
PHASE: LAND USE APPLICATION
DRAWN BY: MGSB
CHECKED BY: SB

KEYPLAN



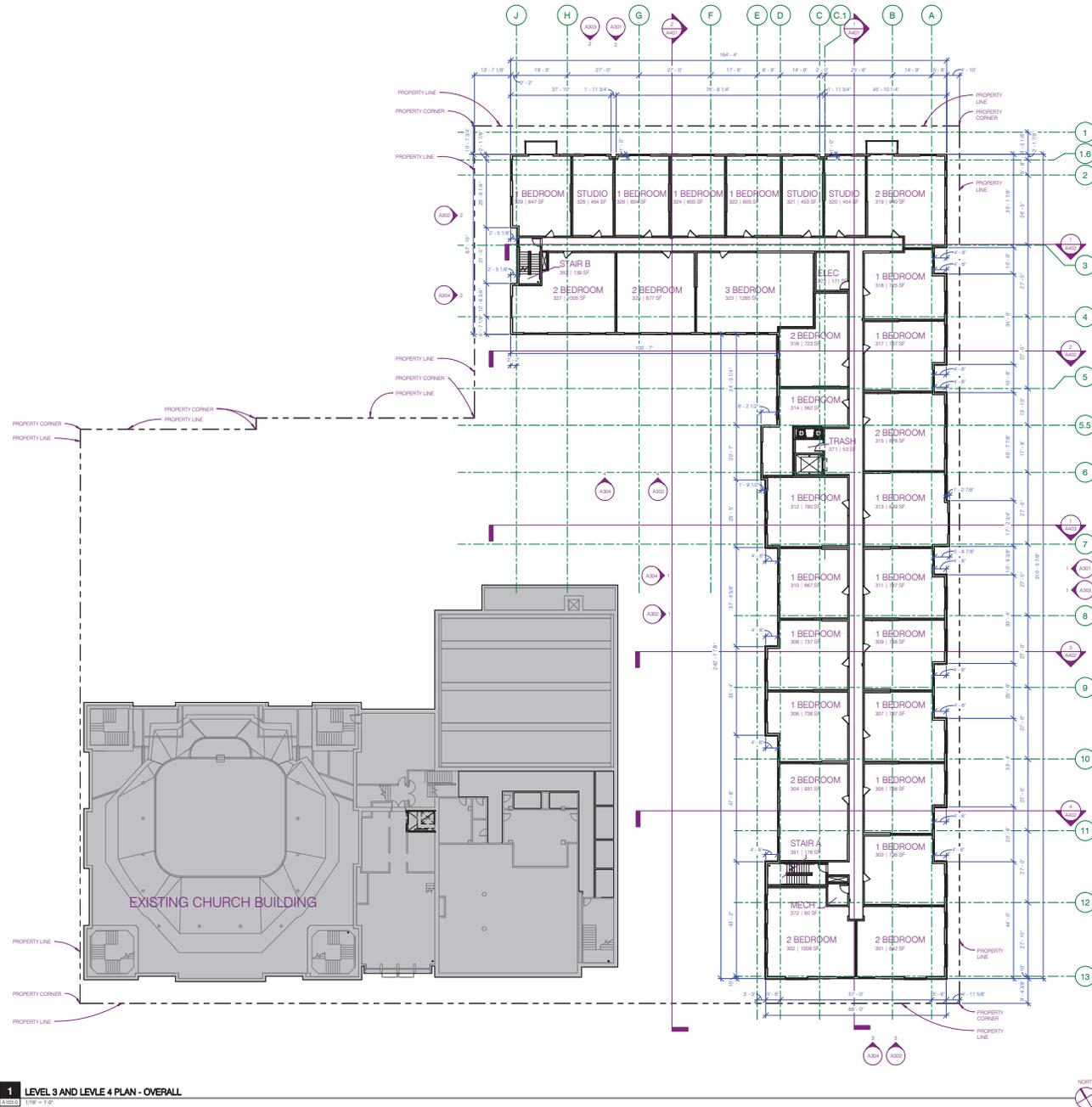
LEVEL 2 PLAN - OVERALL

A102.0

GENERAL NOTES:

1. DIMENSIONS AT EXTERIOR WOOD STUD AND METAL STUD WALLS ARE TO OUTSIDE FACE OF SHEATHING.
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4. DIMENSIONS AT LIFT DEMISING ARE TO CENTERLINE OF WALL SYSTEM.
5. ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 ARE TYPE **KA, LB, MA AND SB** 1 HR FIRE RATING WITH ACoustic RATING.
6. ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 4 ARE TYPE **VA, VB, VC AND VD** 1 HR FIRE RATING WITH ACoustic RATING.
7. ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 1 ARE TYPE **BA** 1 HR FIRE RATING WITH ACoustic RATING UNLESS OTHERWISE NOTED.
8. ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 2 THRU LEVEL 4 ARE TYPE **WB** 1 HR FIRE RATING WITH ACoustic RATING UNLESS OTHERWISE NOTED.
9. EXTERIOR WOOD STUDS ARE FIRE TREATED UNLESS OTHERWISE NOTED.
10. EXTEND ALL SHAPES TO UNDERSIDE OF ROOF SHEATHING OR TOP OF ROOF PENETRATIONS WHERE APPLICABLE.
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15. MAINTAIN 8'-2" MIN. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLANS. ALL OTHER PARKING AREAS SHALL PROVIDE 7'-2" MIN. CLEAR HEIGHT.

KEYNOTES:



1 LEVEL 3 AND LEVEL 4 PLAN - OVERALL
1/16 - 1/16

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

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Scott Backlund
 SIGNED: Scott Backlund
 DATE: 6.6.2016
 LICENSE #: 47331

REVISIONS
 # Date Description

DATE: 6.6.2016
 PROJECT #: 15-0000
 PHASE: LAND USE APPLICATION

DRAWN BY: MCGS
 CHECKED BY: SB

KEYPLAN



LEVEL 3 AND
LEVEL 4 PLAN -
OVERALL

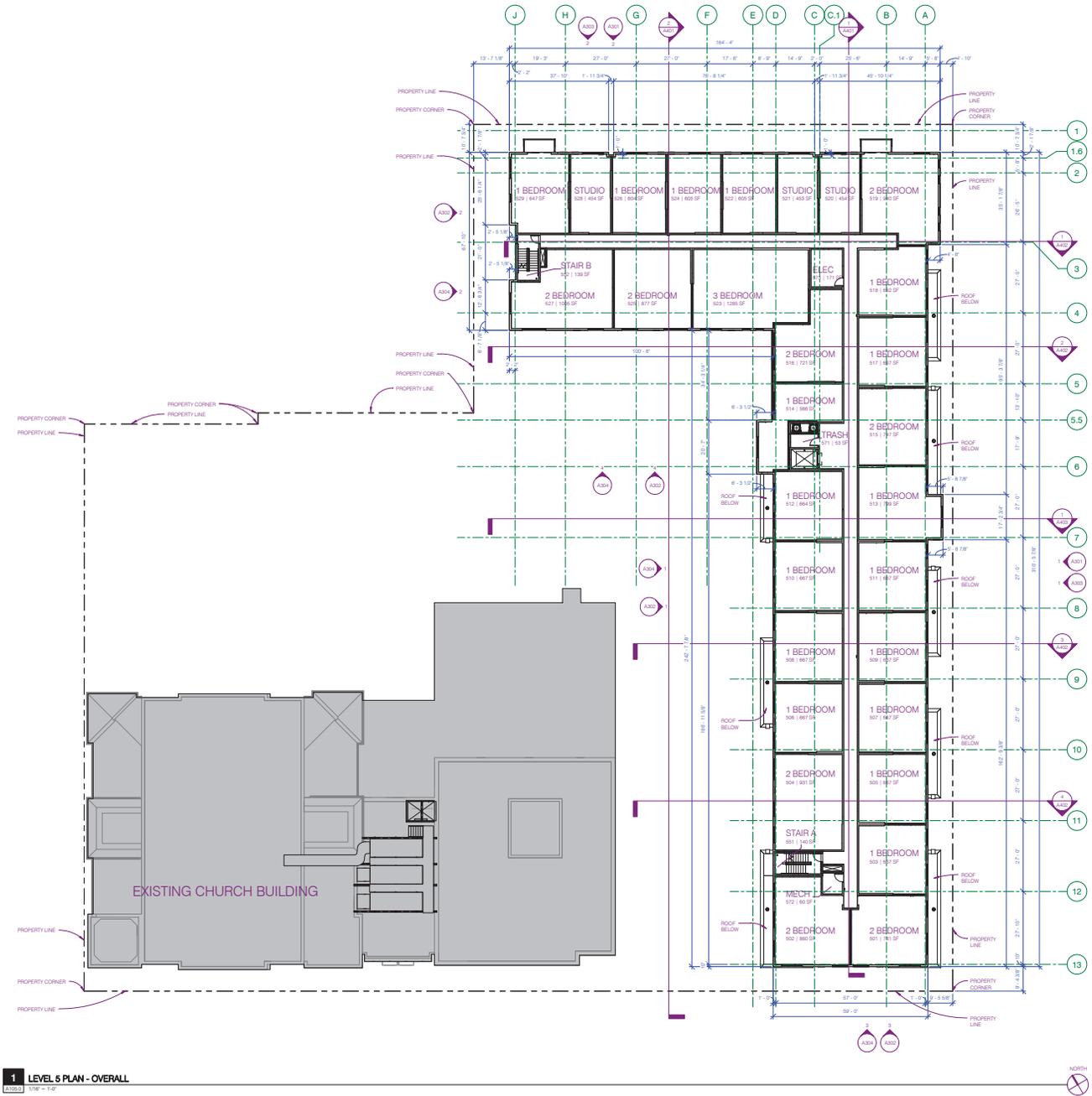
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GENERAL NOTES:

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- DIMENSIONS AT LIFT DIMENS ARE TO CENTERLINE OF WALL SYSTEM.
- ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VA, VB, VA AND VB** 1 HR FIRE RATING WITH ACROSTIC RATING.
- ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VA, VA AND VB** 1 HR FIRE RATING WITH ACROSTIC RATING.
- ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 1 ARE TYPE **BA** 1 HR FIRE RATING WITH ACROSTIC RATING UNLESS OTHERWISE NOTED.
- ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VB** 1 HR FIRE RATING WITH ACROSTIC RATING UNLESS OTHERWISE NOTED.
- EXTERIOR WOOD STUDS ARE FIRE TREATED UNLESS OTHERWISE NOTED.
- EXTEND ALL SHAFTS TO UNDERSIDE OF ROOF SHEATHING OR TOP OF ROOF PENETRATIONS WHERE APPLICABLE.
- EXTEND 1 HR FIRE BARRIERS TO UNDERSIDE OF SHEATHING WHERE APPLICABLE.
- NO PENETRATIONS OR OPENINGS ALLOWED IN SHIRT WALLS UNLESS THEY EXPRESSLY STATE THE OPPOSITE.
- PROVIDE NON-COMBUSTIBLE MATERIALS IN RETURN PLenums (WALLS ON SOFFIT).
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KEYNOTES:

East Town Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backman

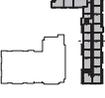
SIGNED: Scott Backman
DATE: 6.6.2016
LICENSE #: 47331

REVISIONS
Date Description

DATE: 6.6.2016
PROJECT #: 15-0000
PHASE: LAND USE APPLICATION

DRAWN BY: MCGS
CHECKED BY: SB

KEY PLAN



LEVEL 5 PLAN - OVERALL

A105.0

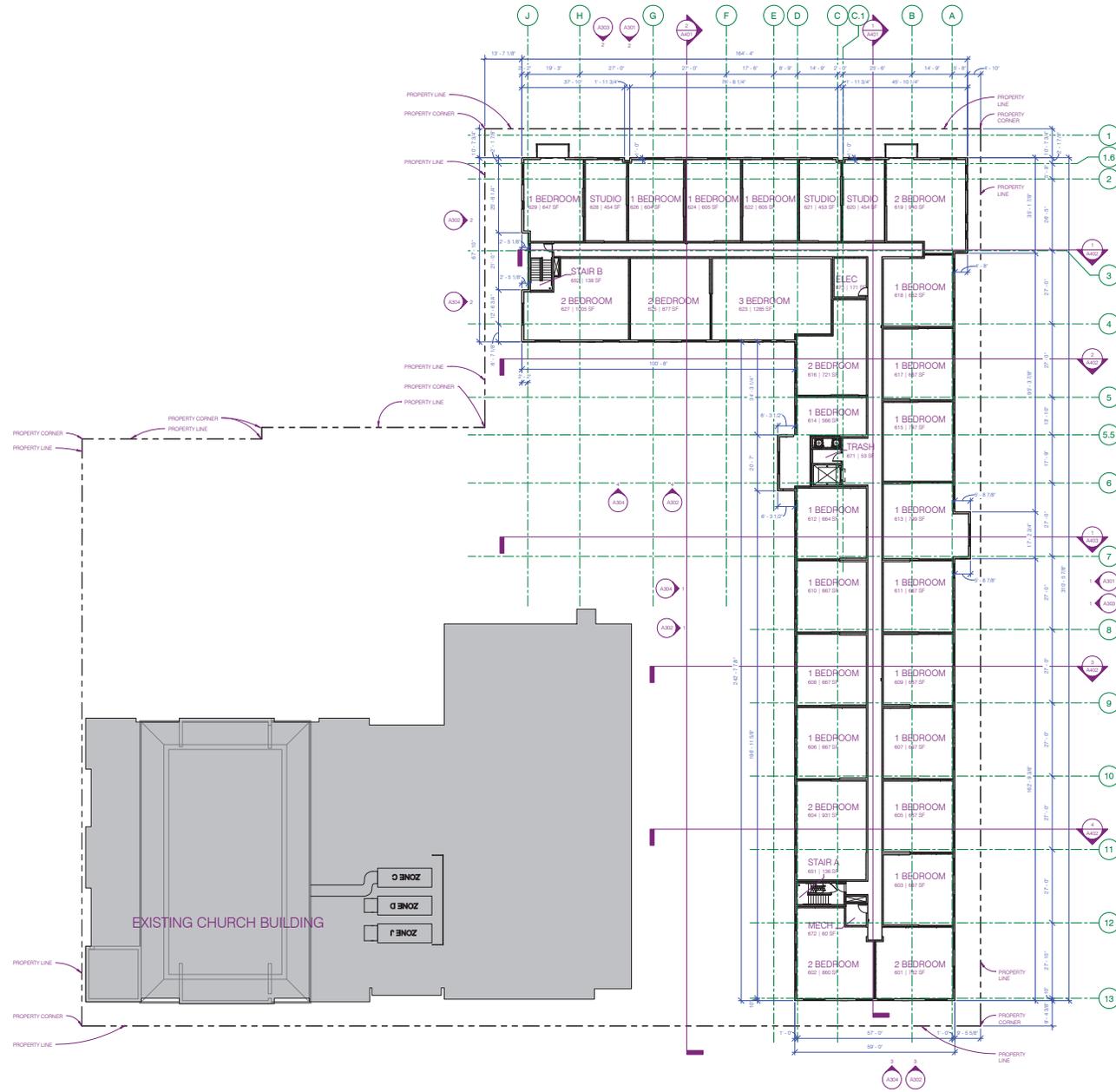
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1 LEVEL 5 PLAN - OVERALL
ASB 1/16" = 1'-0"

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GENERAL NOTES:

- DIMENSIONS AT EXTERIOR WOOD STUD AND METAL STUD WALLS ARE TO OUTSIDE FACE OF SHEATHING.
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- DIMENSIONS AT UNIT DEMISING ARE TO CENTERLINE OF WALL SYSTEM.
- ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VA, VB, VA AND VB** 1 HR FIRE RATING WITH ACoustIC RATING.
- ALL RESIDENTIAL DEMISING WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VA, VA AND VB** 1 HR FIRE RATING WITH ACoustIC RATING.
- ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 1 ARE TYPE **IB** 1 HR FIRE RATING WITH ACoustIC RATING - UNLESS OTHERWISE NOTED.
- ALL RESIDENTIAL CORRIDOR WALLS ON LEVEL 2 THRU LEVEL 8 ARE TYPE **VB** 1 HR FIRE RATING WITH ACoustIC RATING - UNLESS OTHERWISE NOTED.
- EXTERIOR WOOD STUDS ARE FIRE TREATED UNLESS OTHERWISE NOTED.
- EXTEND ALL SHAFTS TO UNDERSIDE OF ROOF SHEATHING OR TOP OF ROOF PENETRATIONS WHERE APPLICABLE.
- EXTEND 1 HR FIRE BARRIERS TO UNDERSIDE OF SHEATHING WHERE APPLICABLE.
- NO PENETRATIONS OR OPENINGS ALLOWED IN SHAFT WALLS UNLESS THEY EXPRESSLY STATE THE OPPOSITE.
- PROVIDE NON-COMBUSTIBLE MATERIALS IN RETURN PLenums (WALLS ON SOFFIT).
- ALL UNITS ARE TYPE B ACCESSIBLE UNLESS OTHERWISE NOTED.
- MAINTAIN 8'-2" MIN. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLANS. ALL OTHER PARKING AREAS SHALL PROVIDE 7'-2" MIN. CLEAR HEIGHT.



KEYNOTES:

1 LEVEL 6 PLAN - OVERALL
1/8" = 1'-0"

East Town
 Apartments
 Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
 MINNEAPOLIS, MN 55401
 612.495.3100

CONSULTANT

Land Use Application
 6.6.2016

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

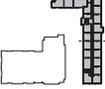
Scott Backman
 Scott Backman
 DATE: 6.8.2016
 LICENSE #: 47331

REVISIONS
 # Date Description

DATE: 6.6.2016
 PROJECT #: 15-0000
 PHASE: LAND USE APPLICATION

DRAWN BY: MCGS
 CHECKED BY: SB

KEYPLAN



LEVEL 6 PLAN - OVERALL

A106.0

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GENERAL NOTES:

1. PROVIDE WALKING PLATE FROM ROOF ACCESS POINT TO AND AROUND ALL MECHANICAL EQUIPMENT.
2. ALL MECHANICAL EQUIPMENT TO BE RETAILOUT ON CURBS OR OTHER STRUCTURE A MINIMUM OF 12" ABOVE ROOF.
3. GUARD RAILING REQUIRED AT ALL LOCATIONS WHERE ROOF TOP EQUIPMENT IS WITHIN 50' OF EDGE OF ROOF.
4. PROVIDE WINDOW WASHING ANCHORS PER THE 2013 MINNESOTA BUILDING CODE SECTION 3112.

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401

612.495.3100

CONSULTANT

Land Use Application
6.30.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backman
SIGNED: Scott Backman
DATE: 6.30.2016
LICENSE #: 47334

REVISIONS
Date Description

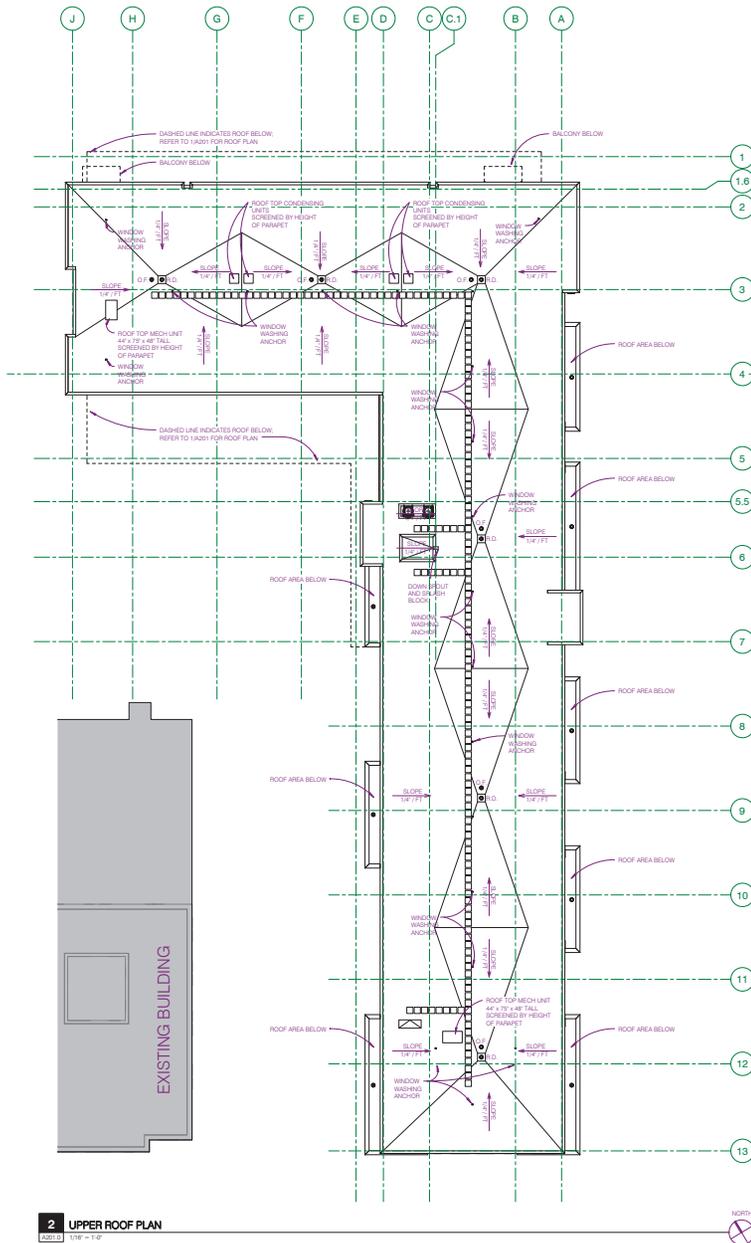
DATE: 6.30.2016
PROJECT #: 15-0007
PHASE: LAND USE APPLICATION

DRAWN BY: MS
CHECKED BY: SB

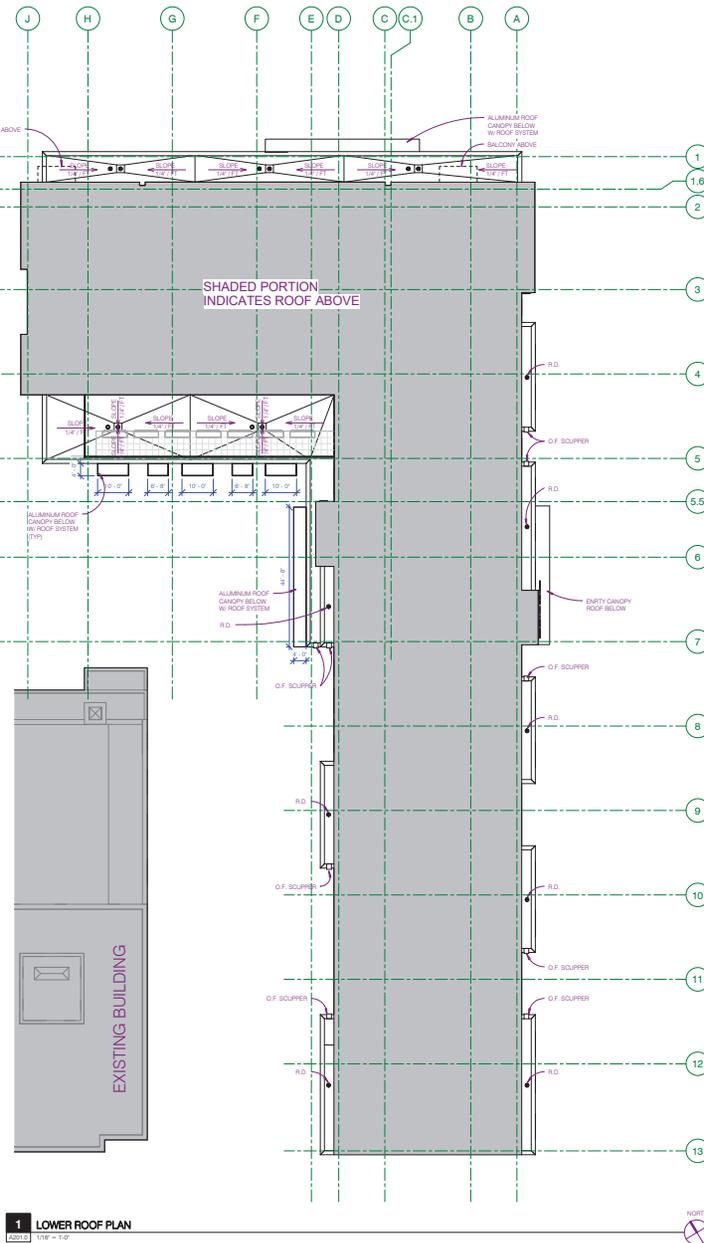
ROOF PLAN -
LOWER AND
UPPER

A201.0

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2 UPPER ROOF PLAN
A201.0 1/8" = 1'-0"



1 LOWER ROOF PLAN
A201.0 1/8" = 1'-0"



City of Minneapolis, 15000 15th Avenue Northeast, Minneapolis, MN 55412
 City of Minneapolis, 15000 15th Avenue Northeast, Minneapolis, MN 55412
 City of Minneapolis, 15000 15th Avenue Northeast, Minneapolis, MN 55412

GENERAL NOTES:

1. REFER TO SPEC EXTERIOR FINISH SCHEDULE FOR MATERIAL DETAILS

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 146
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Beck
 SIGNED: Scott Beck
 DATE: 6.6.2016
 LICENSE #: 47331

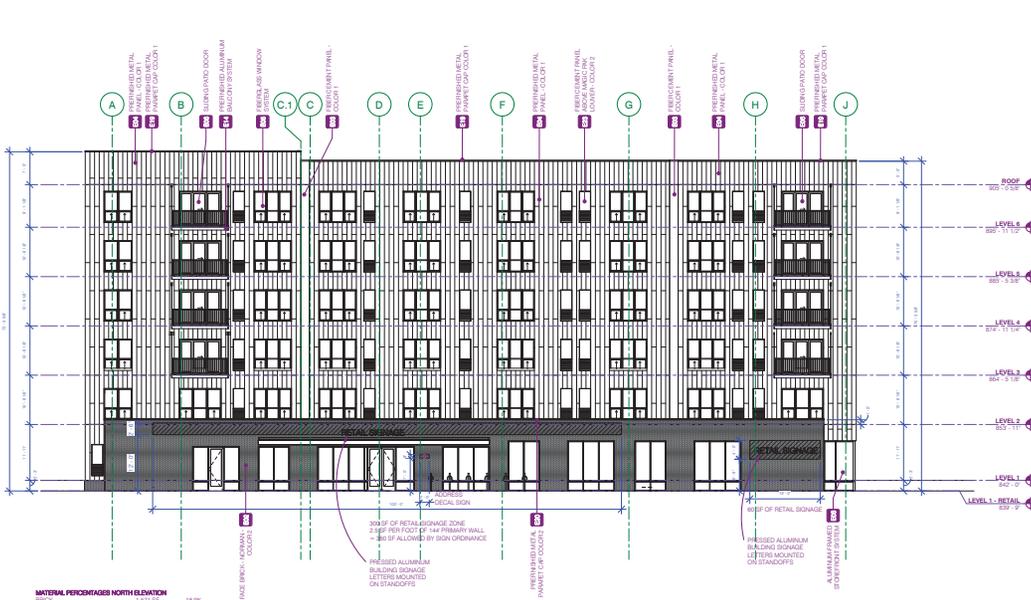
REVISIONS
 # Date Description

DATE: 6.6.2016
 PROJECT #: 15-0000
 PHASE: LAND USE APPLICATION
 DRAWN BY: MDCS
 CHECKED BY: SB

BUILDING
ELEVATIONS

A301

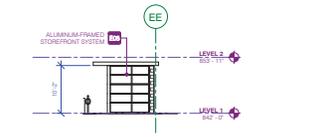
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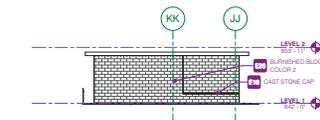
MATERIAL PERCENTAGES NORTH ELEVATION

BRICK	1,571 SF	18.3%
METAL PANEL	6,346 SF	74.2%
FIBER CEMENT PANEL SIDING	574 SF	6.3%
TOTAL MATERIAL AREA	8,291 SF	

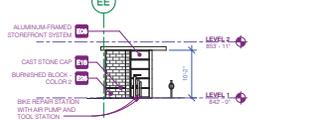
2 NORTH EXTERIOR ELEVATION - ALONG SOUTH 6TH STREET
 ASS: 3/27 - 1/7



6 SOUTH EXTERIOR STAIR ELEVATION
 ASS: 3/27 - 1/7



5 EAST EXTERIOR STAIR ELEVATION
 ASS: 3/27 - 1/7



4 NORTH EXTERIOR STAIR ELEVATION
 ASS: 3/27 - 1/7



3 WEST EXTERIOR STAIR ELEVATION - ALONG CHICAGO AVE
 ASS: 3/27 - 1/7



MATERIAL PERCENTAGES EAST ELEVATION

BRICK	1,204 SF	46.7%
METAL PANEL	1,404 SF	51.3%
FIBER CEMENT PANEL SIDING	3,299 SF	23.3%
TOTAL MATERIAL AREA	14,413 SF	

1 EAST EXTERIOR ELEVATION - ALONG BUD GRANT WAY
 ASS: 3/27 - 1/7

City of Chicago, Windows Imaging/15/0277 - 6.06.2016
 6:02PM & 10:33 AM

GENERAL NOTES:

1. REFER TO SPEC EXTERIOR FINISH SCHEDULE FOR MATERIAL DETAILS

East Town
Apartments
Minneapolis, MN



901 NORTH 7TH STREET, SUITE 146
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

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Scott Backlund
SIGNED: Scott Backlund
DATE: 6.6.2016
LICENSE #: 47334

REVISIONS
Date Description

DATE: 6.6.2016
PROJECT #: 15-0000
PHASE: LAND USE APPLICATION
DRAWN BY: MCGS
CHECKED BY: SB

BUILDING ELEVATIONS

A302

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MATERIAL PERCENTAGES SOUTH ELEVATION 1

BRICK	840 SF	17.1%
METAL PANEL	3,849 SF	79.8%
FIBER CEMENT PANEL SOUNDING	154 SF	3.1%
TOTAL MATERIAL AREA	4,843 SF	

4 SOUTH EXTERIOR ELEVATION - ALONG SOUTH 7TH STREET
AS302 3/32" = 1/4"

MATERIAL PERCENTAGES SOUTH ELEVATION 2

BRICK	1,467 SF	44.3%
METAL PANEL	1,146 SF	34.5%
FIBER CEMENT PANEL SOUNDING	701 SF	21.2%
TOTAL MATERIAL AREA	3,315 SF	

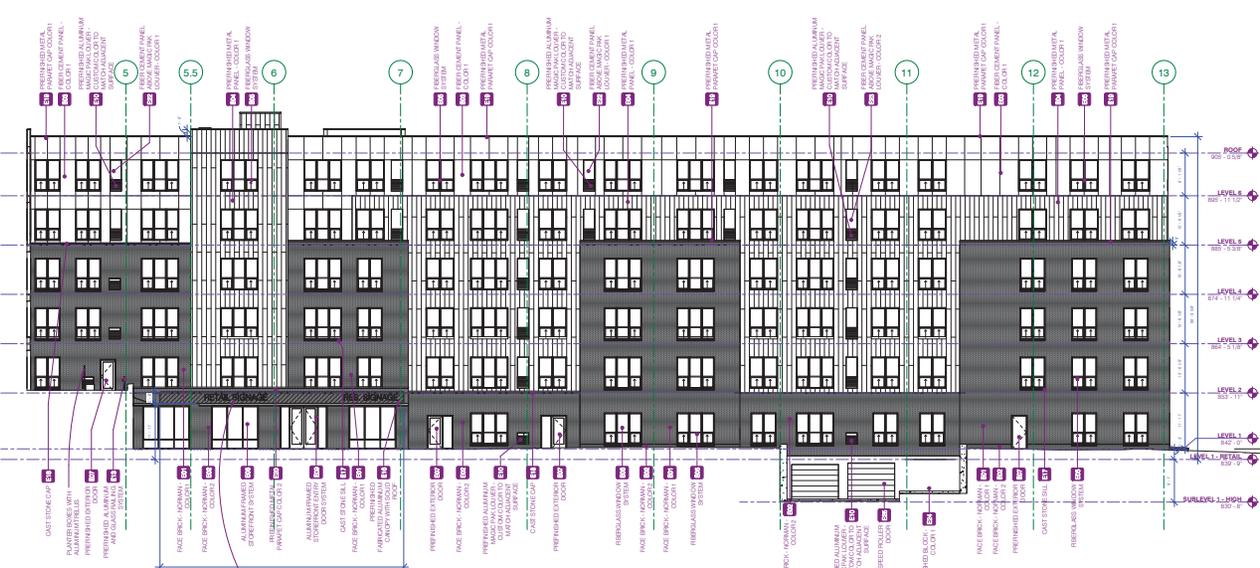
3 SOUTH EXTERIOR ELEVATION - ALONG SOUTH 7TH STREET
AS302 3/32" = 1/4"



MATERIAL PERCENTAGES WEST ELEVATION 1

BRICK	1,173 SF	19.2%
METAL PANEL	4,064 SF	64.4%
FIBER CEMENT PANEL SOUNDING	1,664 SF	26.4%
TOTAL MATERIAL AREA	6,901 SF	

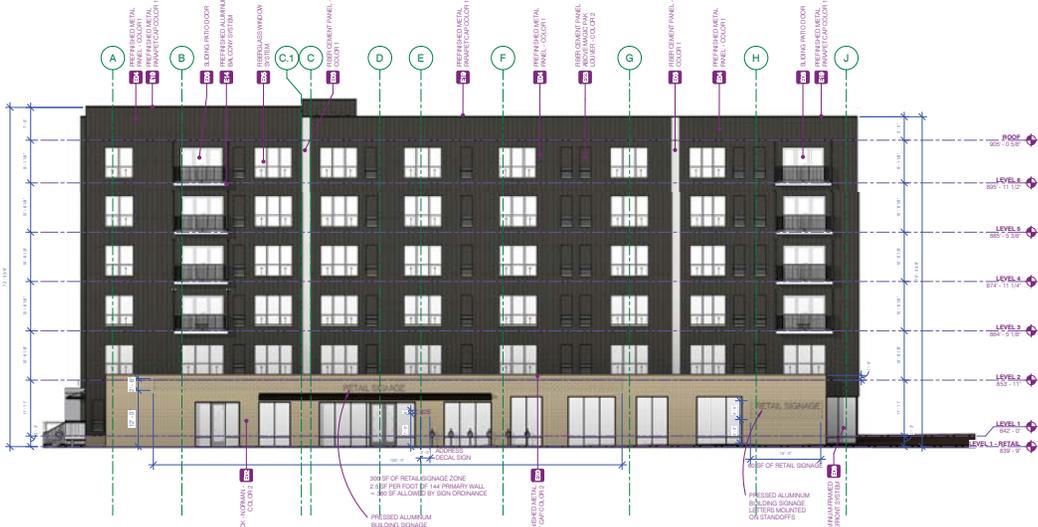
2 WEST EXTERIOR ELEVATION
AS302 3/32" = 1/4"



MATERIAL PERCENTAGES WEST ELEVATION 2

BRICK	450 SF	45.0%
METAL PANEL	3,712 SF	36.9%
FIBER CEMENT PANEL SOUNDING	747 SF	7.3%
TOTAL MATERIAL AREA	4,909 SF	

1 WEST EXTERIOR ELEVATION
AS302 3/32" = 1/4"



MATERIAL PERCENTAGES NORTH ELEVATION

BRICK PANEL	4,112 SF	73.2%
FIBER CEMENT PANEL SIDING	1,374 SF	24.3%
TOTAL MATERIAL AREA	5,486 SF	

2 NORTH EXTERIOR ELEVATION - ALONG SOUTH 6TH STREET
A303 3/32" = 1'-0"



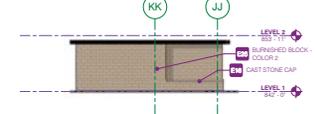
MATERIAL PERCENTAGES EAST ELEVATION

BRICK PANEL	2,750 SF	46.7%
FIBER CEMENT PANEL SIDING	3,299 SF	57.3%
TOTAL MATERIAL AREA	6,049 SF	

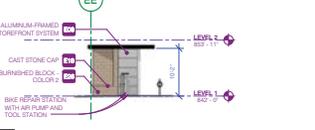
1 EAST EXTERIOR ELEVATION - ALONG BUD GRANT WAY
A303 3/32" = 1'-0"



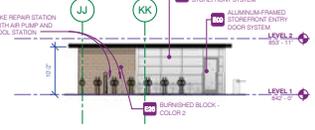
6 SOUTH EXTERIOR STAIR ELEVATION
A303 3/32" = 1'-0"



5 EAST EXTERIOR STAIR ELEVATION
A303 3/32" = 1'-0"



4 NORTH EXTERIOR STAIR ELEVATION
A303 3/32" = 1'-0"



3 WEST EXTERIOR STAIR ELEVATION - ALONG CHICAGO AVE
A303 3/32" = 1'-0"

GENERAL NOTES:

1. REFER TO SPEC EXTERIOR FINISH SCHEDULE FOR MATERIAL DETAILS

KEYNOTES:

- E01 FACE BRICK - NORMAN - COLOR 1
- E02 FACE BRICK - NORMAN - COLOR 2
- E03 FIBER CEMENT PANEL - COLOR 1
- E04 PRE-FINISHED METAL PANEL - COLOR 1
- E05 FIBERGLASS WINDOW SYSTEM
- E06 SLIDING PATIO DOOR
- E07 PRE-FINISHED EXTERIOR DOOR
- E08 ALUMINUM FRAMED STOREFRONT SYSTEM
- E09 ALUMINUM FRAMED STOREFRONT ENTRY DOOR SYSTEM
- E10 PRE-FINISHED ALUMINUM MAGIC PAK LOUVER - CUSTOM COLOR TO MATCH ADJACENT SURFACE
- E12 PRE-FINISHED ALUMINUM PATIO RAILING
- E13 PRE-FINISHED ALUMINUM HAND SLASH RAILING SYSTEM
- E14 PRE-FINISHED ALUMINUM BALCONY SYSTEM
- E15 ALUMINUM STAR & UNIT PATIO ON PER FOOTINGS & ALUMINUM SCREENING BELOW
- E16 PRE-FINISHED FABRICATED ALUMINUM CANOPY WITH SOLID ROOF
- E17 CAST STONE SILL
- E18 CAST STONE CAP
- E19 PRE-FINISHED METAL PARAPET CAP COLOR 1
- E20 PRE-FINISHED METAL PARAPET CAP COLOR 2
- E21 ENTRY CANOPY
- E22 FIBER CEMENT PANEL ABOVE MAGIC PAK LOUVER - COLOR 1
- E23 FIBER CEMENT PANEL ABOVE MAGIC PAK LOUVER - COLOR 2
- E24 BURNISHED BLOCK - COLOR 1
- E25 HIGH SPEED ROLLER DOOR
- E26 BURNISHED BLOCK - COLOR 2

East Town
Apartments
Minneapolis, MN



901 NORTH THIRD STREET, SUITE 146
MINNEAPOLIS, MN 55401
612.495.3100

CONSULTANT

Land Use Application
6.6.2016

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the state of Minnesota.

Scott Backstrom
 SIGNED: Scott Backstrom
 DATE: 6.6.2016
 LICENSE #: 47334

REVISIONS
 # Date Description

DATE: 6.6.2016
 PROJECT #: 15-0000
 PHASE: LAND USE APPLICATION
 DRAWN BY: MCGS
 CHECKED BY: SB

BUILDING ELEVATIONS - COLOR



GENERAL NOTES:

1. REFER TO SPEC EXTERIOR FINISH SCHEDULE FOR MATERIAL DETAILS

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Scott Beckman
 SIGNED: Scott Beckman
 DATE: 6.6.2016
 LICENSE #: 47324

REVISIONS
 # Date Description

DATE: 6.6.2016
 PROJECT #: 15-00007
 PHASE: LAND USE APPLICATION
 DRAWN BY: MGSB
 CHECKED BY: SB

BUILDING
ELEVATIONS -
COLOR

A304

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MATERIAL PERCENTAGES SOUTH ELEVATION 1

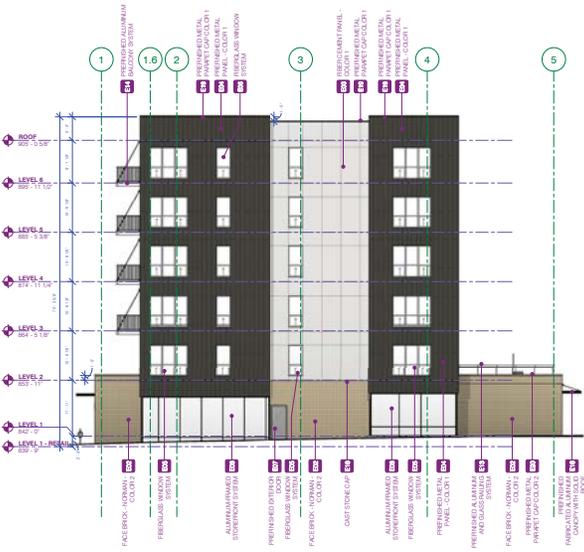
BRICK	17.1%
METAL PANEL	3.32%
FIBER CEMENT PANEL BODING	79.8%
TOTAL MATERIAL AREA	4,900 SF

4 SOUTH EXTERIOR ELEVATION - ALONG SOUTH 7TH STREET
 A304 3/32" = 1'-0"

MATERIAL PERCENTAGES SOUTH ELEVATION 2

BRICK	44.3%
METAL PANEL	1.14%
FIBER CEMENT PANEL BODING	34.5%
TOTAL MATERIAL AREA	3,313 SF

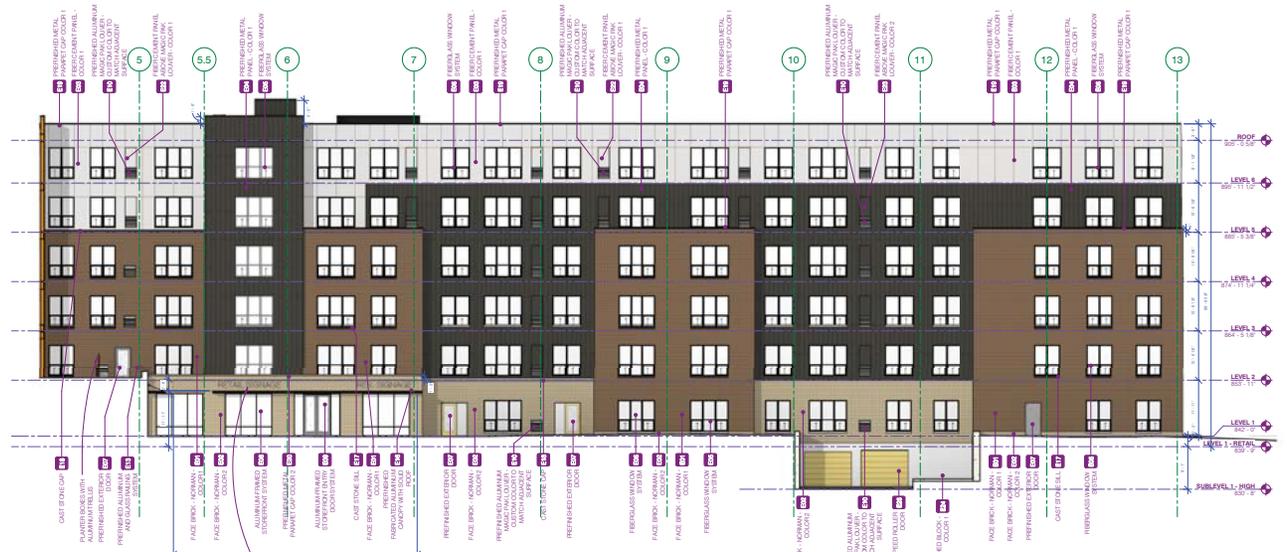
3 SOUTH EXTERIOR ELEVATION - ALONG SOUTH 7TH STREET
 A304 3/32" = 1'-0"



MATERIAL PERCENTAGES WEST ELEVATION 1

BRICK	19.2%
METAL PANEL	2.17%
FIBER CEMENT PANEL BODING	78.6%
TOTAL MATERIAL AREA	3,989 SF

2 WEST EXTERIOR ELEVATION
 A304 3/32" = 1'-0"

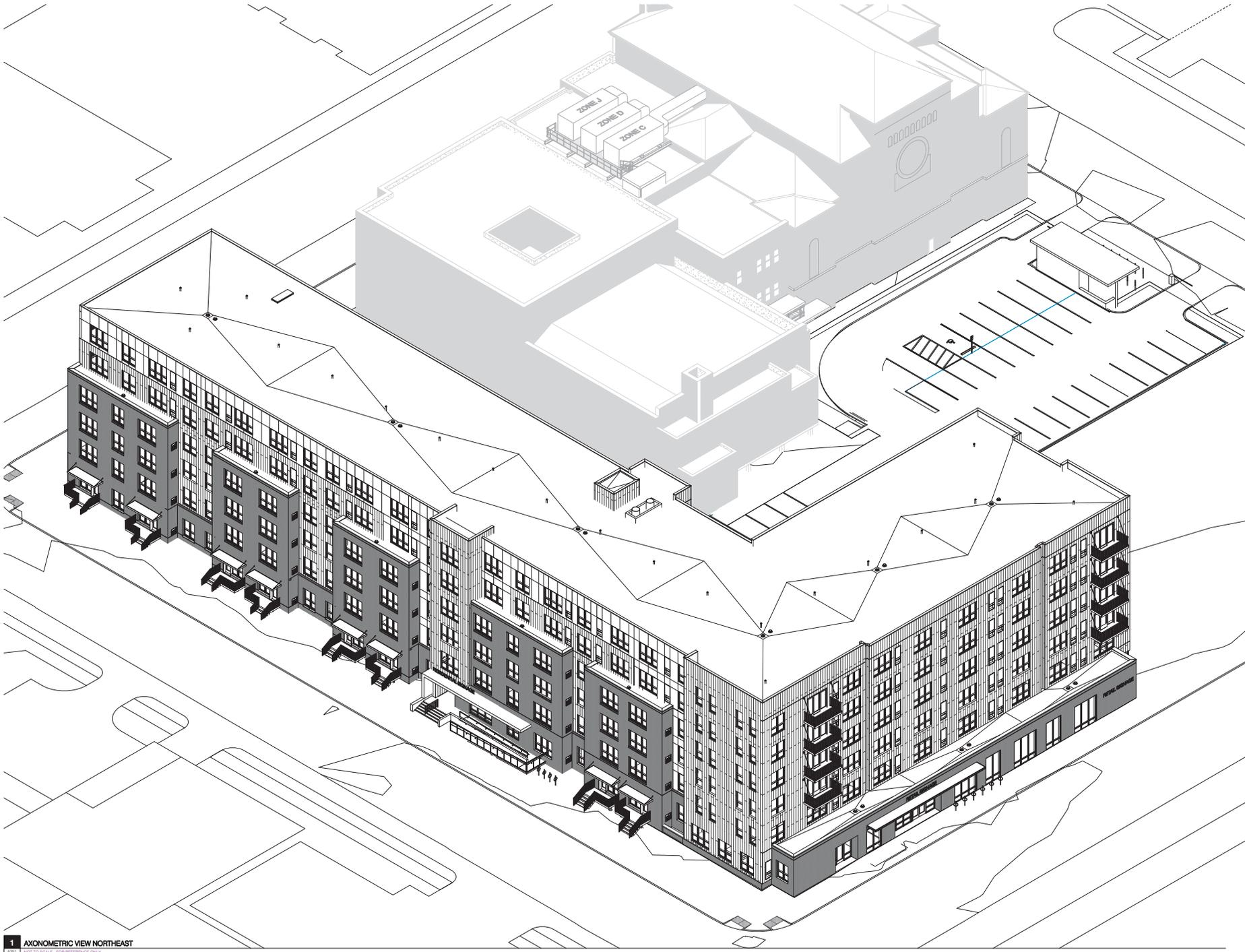


MATERIAL PERCENTAGES WEST ELEVATION 2

BRICK	48.0%
METAL PANEL	4.71%
FIBER CEMENT PANEL BODING	47.3%
TOTAL MATERIAL AREA	11,248 SF

1 WEST EXTERIOR ELEVATION
 A304 3/32" = 1'-0"

C:\Users\jacob\OneDrive\Documents\A304.dwg, Wednesday, June 6, 2016 10:30 AM
 6/2/2016 8:30:30 AM



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Scott Backman
SIGNED: Scott Backman
DATE: 6.6.2016
LICENSE #: 47331

REVISIONS
Date Description

DATE: 6.6.2016
PROJECT #: 15-0001
PHASE: LAND USE APPLICATION
DRAWN BY: SB
CHECKED BY: SB

BUILDING
AXONOMETRIC
VIEW

A351

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1 AXONOMETRIC VIEW NORTHEAST
A351 NOT TO SCALE - FOR REFERENCE ONLY

City of Minneapolis, 15007771010, WorkArea: Housing/3540274, 6/6/2016 8:18:38 AM



1 AXONOMETRIC VIEW NORTHEAST
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East Town
 Apartments
 Minneapolis, MN



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Scott Backman
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 DATE: 6.6.2016
 LICENSE #: 47391

REVISIONS
 # Date Description

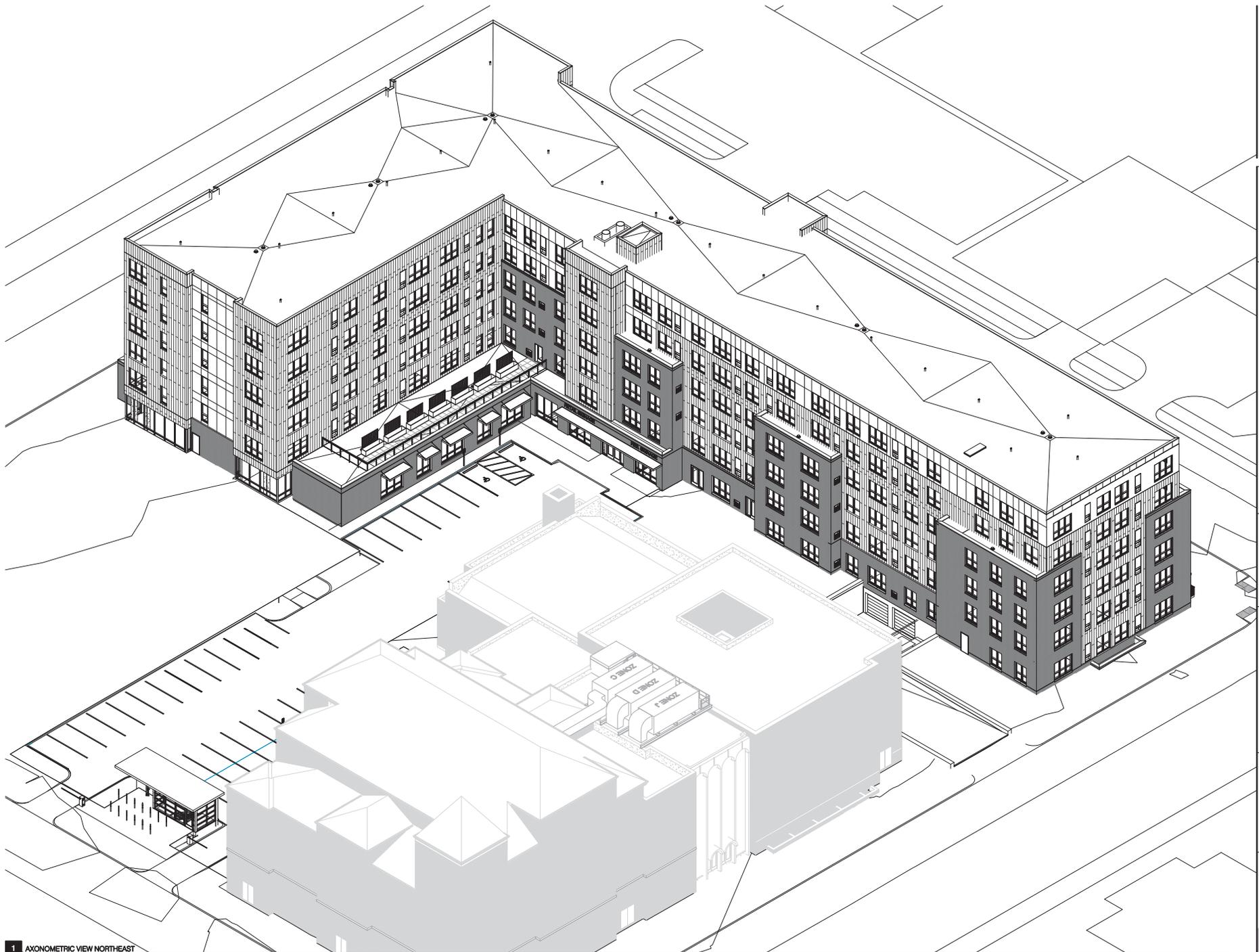
DATE: 6.6.2016
 PROJECT #: 15-0001
 PHASE: LAND USE APPLICATION

DRAWN BY: SB
 CHECKED BY: SB

BUILDING
 AXONOMETRIC
 VIEW

A352

City of Minneapolis 15007710102, WorkArea: Housing/3540277, 6/6/2016 8:18:22 AM



East Town
Apartments
Minneapolis, MN



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6.6.2016

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Scott Backman

SIGNED: Scott Backman
DATE: 6.6.2016
LICENSE #: 47331

REVISIONS
Date Description

DATE: 6.6.2016
PROJECT #: 15-0007
PHASE: LAND USE APPLICATION

DRAWN BY: SB
CHECKED BY: SB

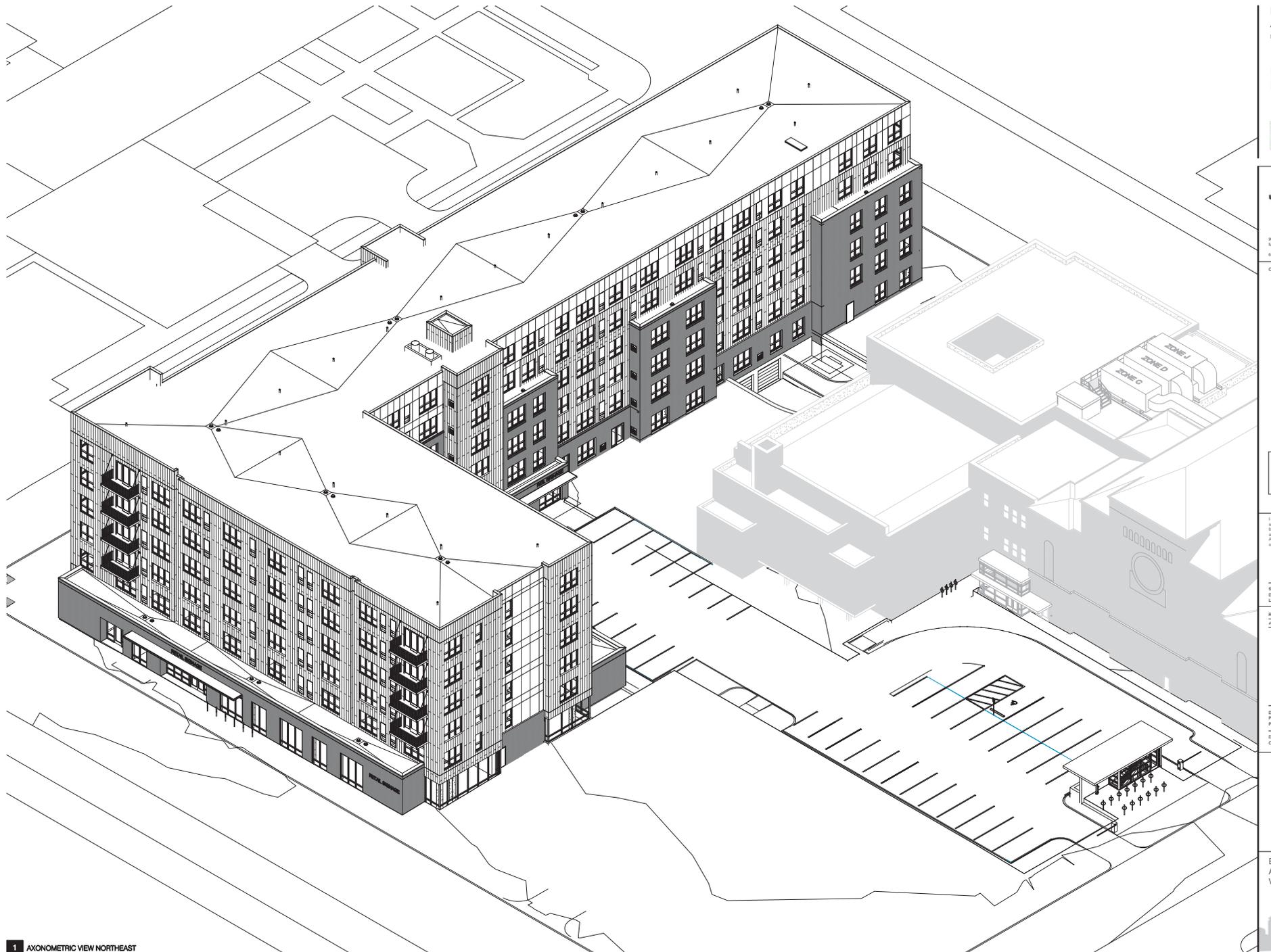
BUILDING
AXONOMETRIC
VIEW

A353

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1 AXONOMETRIC VIEW NORTHEAST
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C:\Users\Public\Desktop\A353\DWG\A353.dwg, Wednesday, 6/6/2016 11:38:28 AM
 6/20/2016 11:38:28 AM



East Town
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Scott Backstrom

SIGNED: Scott Backstrom
DATE: 6.6.2016
LICENSE #: 47331

REVISIONS
Date Description

DATE: 6.6.2016
PROJECT #: 15-0000
PHASE: LAND USE APPLICATION

DRAWN BY: SB
CHECKED BY: SB

BUILDING
AXONOMETRIC
VIEW

A354

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City of Minneapolis 15067710162, WorkArea: Housing/15067710162, 6/2/2016 8:18:35 AM

1 AXONOMETRIC VIEW NORTHEAST
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Scott Backman

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DATE: 6.6.2016
LICENSE #: 47334

REVISIONS
Date Description

DATE: 6.6.2016
PROJECT #: 15-0029
PHASE: LAND USE APPLICATION

DRAWN BY: MJC/SB
CHECKED BY: SB

BUILDING SECTIONS

A401



2 BUILDING SECTION - NORTH/SOUTH
A401 3/32" = 1'-0"



1 BUILDING SECTION - NORTH/SOUTH
A401 3/32" = 1'-0"



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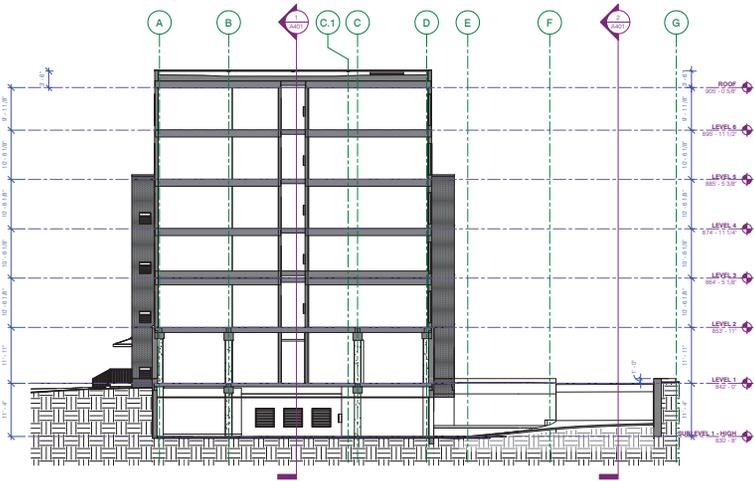
Scott Backman
SIGNED: Scott Backman
DATE: 6.8.2016
LICENSE #: 47331

REVISIONS
Date Description

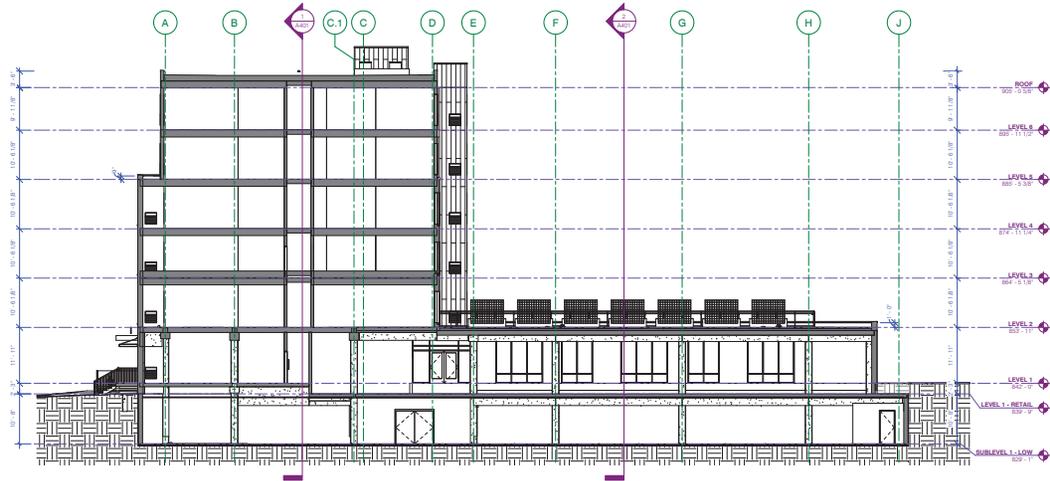
DATE: 6.6.2016
PROJECT #: 15-0000
PHASE: LAND USE APPLICATION
DRAWN BY: MCGS
CHECKED BY: SB

BUILDING SECTIONS

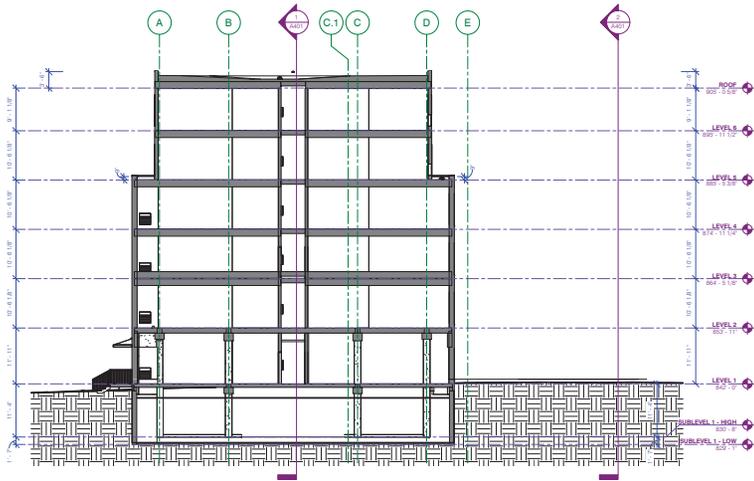
A402



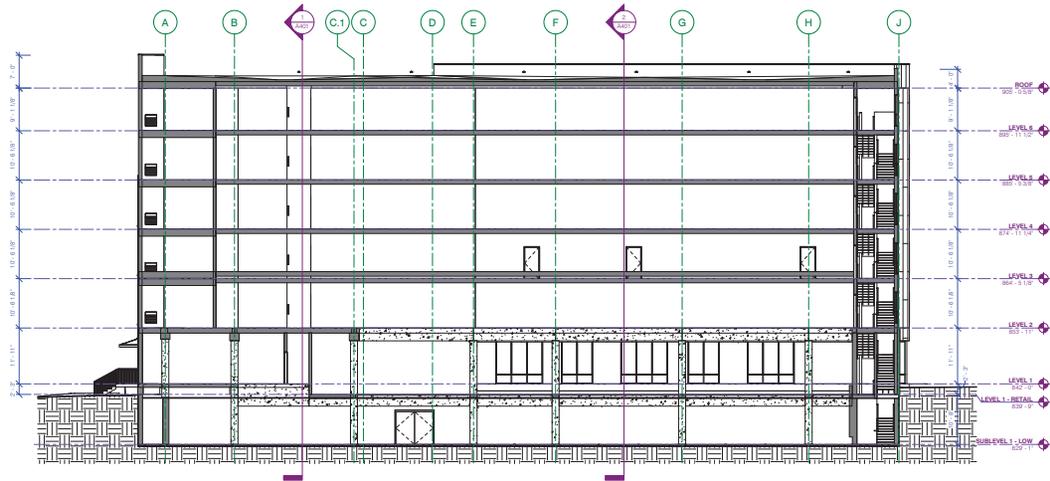
4 BUILDING SECTION - EAST/WEST
A402 3/32" = 1'-0"



2 BUILDING SECTION - EAST/WEST
A402 3/32" = 1'-0"



3 BUILDING SECTION - EAST/WEST
A402 3/32" = 1'-0"



1 BUILDING SECTION - EAST/WEST
A402 3/32" = 1'-0"



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CONSULTANT

Land Use Application
6.6.2016

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Scott Backman

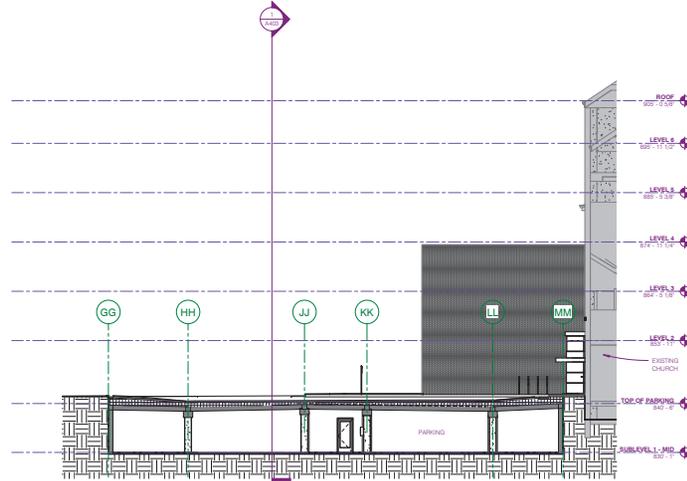
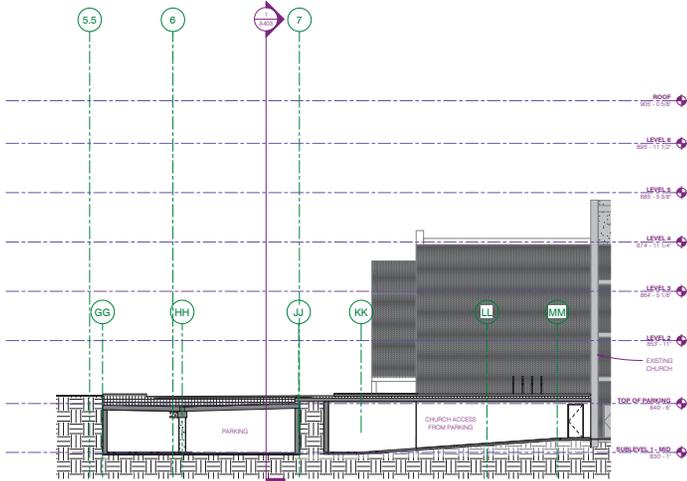
SIGNED: Scott Backman
DATE: 6.6.2016
LICENSE #: 47334

REVISIONS
Date Description

DATE: 6.6.2016
PROJECT #: 15.0007
PHASE: LAND USE APPLICATION
DRAWN BY: MCGS
CHECKED BY: SB

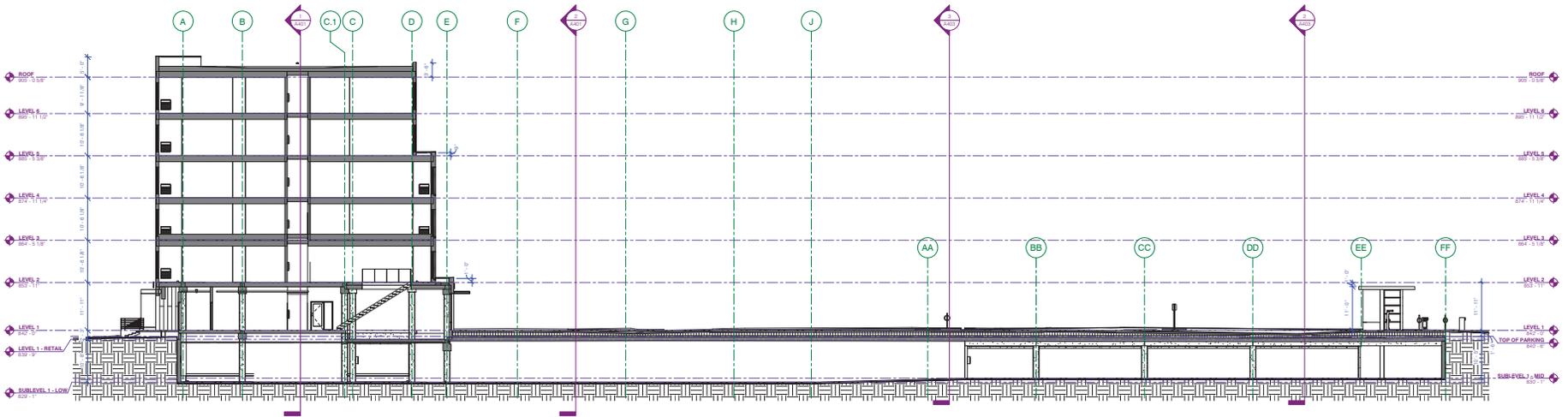
BUILDING SECTIONS

A403



3 BUILDING SECTION - NORTH/SOUTH - SUBLEVEL PARKING @ CHURCH ENTRY
A403 3/32" = 1'-0"

2 BUILDING SECTION - NORTH/SOUTH - SUBLEVEL PARKING ONLY
A403 3/32" = 1'-0"



1 BUILDING SECTION - EASTWEST
A403 3/32" = 1'-0"

Luminaire Schedule							
Symbol	Qty	Label	Arrangement	LLF	Description	Arr. Watts	Lum. Lumens
	9	AA	SINGLE	0.900	LUMARK XTOR1A MOUNT AT 7FT	7	722
	3	BB	SINGLE	0.900	LUMARK PRV-A40-D-UNV-T4-BZ MOUNT ON 20FT POLE WITH 3FT BASE	143	15157
	2	BB2	BACK-BACK	0.900	LUMARK PRV-A40-D-UNV-T4-BZ MOUNT ON 20FT POLE WITH 3FT BASE	286	15157
	6	CC	SINGLE	0.900	WAC WS-W24509 A121814 MOUNT AT 8FT	9.6	365
	1	DD	SINGLE	0.900	LUMARK XTOR3A MOUNT AT 12FT	27	2804

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
SITE GROUND	Illuminance	Fc	1.89	6.5	0.0	N.A.	N.A.
PARKING	Illuminance	Fc	2.79	5.8	0.0	N.A.	N.A.

Luminaire Location Summary						
LumNo	Label	X	Y	Z	Orient	Tilt
24	BB2	-126.7	139	23	56.964	0
25	BB2	-184.8	173.7	23	56.964	0
26	BB	-58.7	100	23	59.216	0
27	BB	21.6	107.3	23	149.824	0
28	AA	-149	100.2	7	56.182	0
29	AA	24.1	89.5	7	147.242	0
30	AA	-3.9	217.5	7	148.912	0
31	AA	41.8	238.6	7	57.907	0
32	AA	76.2	39.1	7	331.79	0
33	AA	-50.7	-43.6	7	146.602	0
34	AA	-36.1	85.4	7	328.263	0
35	CC	68.487	17.729	8	329.18	0
36	CC	58.187	0.029	8	329.18	0
37	CC	41.087	-28.771	8	329.18	0
38	CC	30.687	-46.171	8	329.18	0
39	CC	13.687	-74.671	8	329.18	0
40	CC	0.187	-97.371	8	329.18	0
41	AA	3.9	38.2	7	148.042	0
42	AA	17.4	60.7	7	152.904	0
43	DD	-28.8	-15	12	150.66	0
44	BB	-87.7	-9.6	23	330.859	0



Plan View
Scale: 1 inch= 30 Ft.

GENERAL NOTES:

A. PULSE PRODUCTS DOES NOT ASSUME RESPONSIBILITY FOR THE INTERPRETATION OF THIS CALCULATION OR COMPLIANCE TO THE LOCAL, STATE, OR FEDERAL LIGHTING CODES OR ORDINANCES.

B. LIGHTING LAYOUT IS NOT INTENDED FOR CONSTRUCTION DOCUMENTS BUT ONLY TO ILLUSTRATE THE PERFORMANCE OF THE PRODUCT.

C. ALL READINGS/CALCULATIONS SHOWN ARE SHOWN ON OBJECTS/SURFACES.



#	Date	Comments

Revisions

Drawn By: SANDY
Checked By: ROSS
Date: 6/2/2016
Scale: AS NOTED

CHDC WORKFORCE

DESCRIPTION

The Prevail LED area, site luminaire combines optical performance, energy efficiency and long term reliability in an advanced, patent pending modern design. Utilizing the latest LED technology, the Prevail luminaire delivers unparalleled uniformity resulting in greater pole spacing. A versatile mount standard arm facilitates ease of installation for both retrofit and new installations. With energy savings greater than 62%, the Prevail fixture replaces 150-400W metal halide fixtures in general area lighting applications such as parking lots, walkways, roadways and building areas.

SPECIFICATION FEATURES

Construction

Construction is comprised of a heavy-duty, single-piece die-cast aluminum housing. The LED drivers are mounted in direct contact with the casting to promote low operating temperature and long life. The die-cast aluminum door is tethered to provide easy access to the driver if replacement is required. A one-piece silicone gasket seals the door to the fixture housing. The optics is mounted on a versatile, aluminum plate that dissipates heat from the LEDs resulting in longer life of the fixture. The fixture is IP66 and 3G vibration rated (ANSI C136.31) to insure strength of construction and longevity in the selected application.

Optics

Precision molded, high efficiency optics are precisely designed to shape the distribution, maximizing efficiency and application spacing. Available in Type II, III, IV and V distributions with lumen packages ranging from 6,100 to 15,100 nominal lumens. Light engine configurations consist of 1 or 2 high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L92/60,000 hours at 25°C) per IESNA TM-21. For the ultimate level of spill light control, an optional house side shield accessory can be field or factory installed.

Electrical

LED drivers are mounted to the fixture for optimal heat sinking and ease of maintenance. Thermal management incorporates both conduction and convection to transfer heat rapidly away from the LED source for optimal efficiency and light output. Class 1 electronic drivers have a power factor >90%, THD <20%, and an expected life of 100,000 hours with <1% failure rate. Available in 120-277V 50/60Hz, 347V 60Hz or 480V 60Hz operation. 480V is compatible for use with 480V Wye systems only. 10kV/10 kA surge protection standard. 0-10V dimming driver is standard with leads external to the fixture to accommodate controls capability such as dimming and occupancy. Suitable for ambient temperatures from -40°C to 40°C. Optional 50°C HA (high ambient) available. Standard NEMA 3-PIN twistlock photocontrol receptacle and NEMA 7-PIN twistlock photocontrol receptacles are available as options.

Controls

The Prevail LED luminaire control options are designed to be simple and cost-effective ASHRAE and California Title 24 compliant solutions. The ANSI C136.41 compliant NEMA 7-PIN receptacle enables wireless dimming when used with compatible photocontrol. An integrated dimming and occupancy sensor is a standalone control option available in on/off (MSP) and bi-level dimming

Catalog #		Type	
Project		Date	
Comments		Prepared by	

(MSP/DIM) operation. The optional LumaWatt system is best described as a peer-to-peer wireless network of luminaire-integral sensors that operate in accordance with programmable profiles. Each sensor is capable of motion and photo sensing, metering power consumption and wireless communication.

Mounting

Standard pole mount arm is bolted directly to the pole and the fixture slides onto the arm and locks in place with a bolt facilitating quick and easy installation. The versatile, patent pending, standard mount arm accommodates multiple drill patterns ranging from 1-1/2" to 4-7/8". Removal of the door on the standard mounting arm enables wiring of the fixture without having to access the driver compartment. A knock-out on the standard mounting arm enables round pole mounting. Wall mount and mast arm mounting options are available. Mast arm adapter fits 2-3/8" O.D. tenon.

Finish

Housing and cast parts finished in five-stage super TGIC polyester powder coat paint, 2.5 mil nominal thickness for superior protection against fade and wear. Standard color is bronze. Additional colors available in white, grey, black, dark platinum and graphite metallic.

Warranty

Five-year warranty.



PRV PREVAIL

LED

AREA / SITE / ROADWAY LUMINAIRE



CERTIFICATION DATA

UL and cUL Wet Location Listed
IP66-Rated
3G Vibration Rated
ISO 9001
DesignLights Consortium™ Qualified*

ENERGY DATA

Electronic LED Driver
0.9 Power Factor
<20% Total Harmonic Distortion
120-277V/50 and 60Hz,
347V/60Hz, 480V/60Hz
-40°C Minimum Temperature Rating
+40°C Ambient Temperature Rating

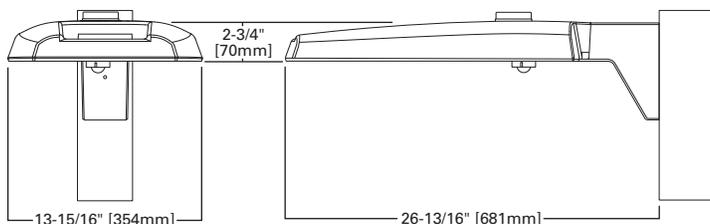
EPA

Effective Projected Area (Sq. Ft.): 0.75

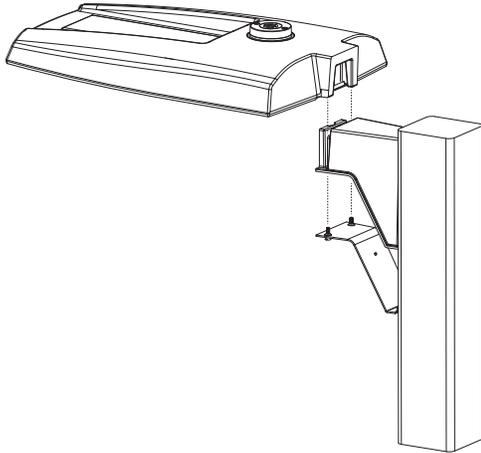
SHIPPING DATA

Approximate Net Weight:
20 lbs. (9.09 kgs.)

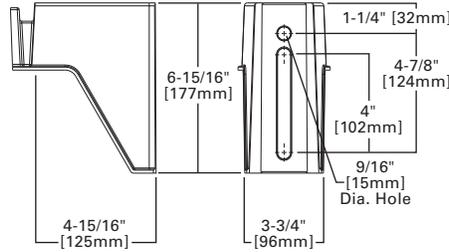
DIMENSIONS



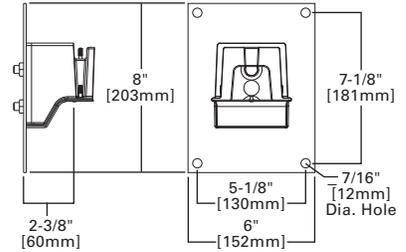
VERSATILE MOUNT SYSTEM



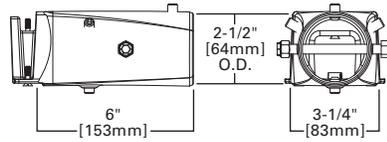
POLE MOUNT ARM



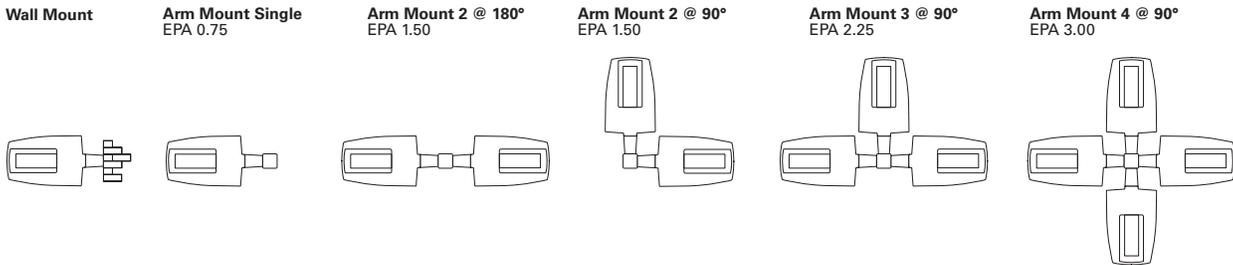
WALL MOUNT



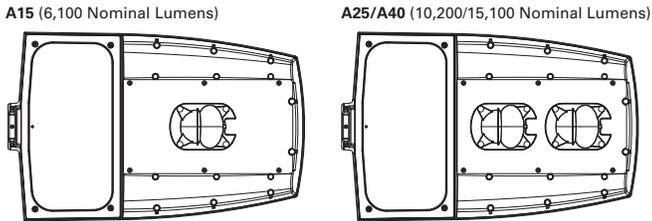
MAST ARM MOUNT



MOUNTING CONFIGURATIONS AND EPAS



OPTICAL CONFIGURATIONS



POWER AND LUMENS

Light Engine		A15	A25	A40
Nominal Power (Watts)		57W	87W	143W
Input Current @ 120V (A)		0.49	0.76	1.23
Input Current @ 277V (A)		0.22	0.35	0.54
Input Current @ 347V (A)		0.18	0.28	0.45
Input Current @ 480V (A)		0.13	0.21	0.33
Type II	Lumens	6,139	10,204	15,073
	BUG Rating	B1-U0-G1	B2-U0-G2	B3-U0-G3
Type III	Lumens	6,192	10,292	15,203
	BUG Rating	B1-U0-G2	B2-U0-G3	B2-U0-G3
Type IV	Lumens	6,173	10,261	15,157
	BUG Rating	B1-U0-G3	B2-U0-G3	B2-U0-G4
Type V	Lumens	6,393	10,627	15,697
	BUG Rating	B3-U0-G3	B4-U0-G3	B4-U0-G4

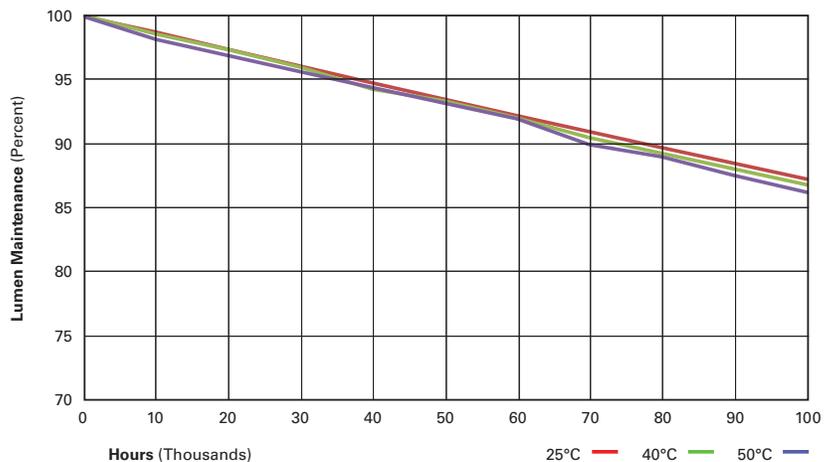
NOTE: Lumen output for standard bronze fixture color. Different housing colors impact lumen output. IES files for the non-standard colors are available upon request.

LUMEN MAINTENANCE

Ambient Temperature	25,000 Hours*	50,000 Hours*	60,000 Hours*	Theoretical 100,000 Hours	Theoretical L70 (Hours)*
25°C	> 96%	> 93%	> 92%	> 87%	> 260,000
40°C	> 96%	> 93%	> 92%	> 87%	> 255,000
50°C	> 95%	> 92%	> 91%	> 86%	> 250,000

LUMEN MULTIPLIER

Ambient Temperature	Lumen Multiplier
10°C	1.02
15°C	1.01
25°C	1.00
40°C	0.99



ORDERING INFORMATION

Sample Number: PRV-A25-D-UNV-T3-SA-BZ

Product Family ^{1,2}	Light Engine ³	Driver ⁴	Voltage	Distribution	Mounting	Color ⁶
PRV=Prevail	A15=(1 LED) 6,100 Nominal Lumens A25=(2 LEDs) 10,200 Nominal Lumens A40=(2 LEDs) 15,100 Nominal Lumens	D=Dimming (0-10V)	UNV=Universal (120-277V) 347=347V 480=480V ⁵	T2=Type II T3=Type III T4=Type IV T5=Type V	SA=Standard Versatile Arm MA=Mast Arm WM=Wall Mount Arm	AP=Grey BZ=Bronze (Standard) BK=Black DP=Dark Platinum GM=Graphite Metallic WH=White
Options (Add as Suffix)				Accessories (Order Separately) ¹¹		
7030=70 CRI / 3000K CCT ⁷ 7050=70 CRI / 5000K CCT ⁷ 10K=10kV/10kA UL 1449 Fused Surge Protective Device DIMRF-LW=LumaWatt Wireless Sensor, Wide Lens for 8' - 16' Mounting Height ^{8,9} DIMRF-LN=LumaWatt Wireless Sensor, Narrow Lens for 16' - 40' Mounting Height ^{8,9} MSP/DIM-L12=Integrated Sensor for Dimming Operation, 8' - 12' Mounting Height MSP/DIM-L30=Integrated Sensor for Dimming Operation, 12' - 30' Mounting Height MSP-L12=Integrated Sensor for ON/OFF Operation, 8' - 12' Mounting Height MSP-L30=Integrated Sensor for ON/OFF Operation, 12' - 30' Mounting Height PER=NEMA 3-PIN Twistlock Photocontrol Receptacle ¹⁰ PER7=NEMA 7-PIN Twistlock Photocontrol Receptacle ¹⁰ HSS=House Side Shield HA=50°C High Ambient Temperature				PRVWM-XX=Wall Mount Kit PRVMA-XX=Mast Arm Mounting Kit PRVSA-XX=Standard Arm Mounting Kit HS/VERD=House Side Shield MA1010-XX=Single Tenon Adapter for 3-1/2" O.D. Tenon MA1011-XX=2@180° Tenon Adapter for 3-1/2" O.D. Tenon MA1012-XX=3@120° Tenon Adapter for 3-1/2" O.D. Tenon MA1013-XX=4@90° Tenon Adapter for 3-1/2" O.D. Tenon MA1014-XX=2@90° Tenon Adapter for 3-1/2" O.D. Tenon MA1015-XX=2@120° Tenon Adapter for 3-1/2" O.D. Tenon MA1016-XX=3@90° Tenon Adapter for 3-1/2" O.D. Tenon MA1017-XX=Single Tenon Adapter for 2-3/8" O.D. Tenon MA1018-XX=2@180° Tenon Adapter for 2-3/8" O.D. Tenon MA1019-XX=3@120° Tenon Adapter for 2-3/8" O.D. Tenon MA1045-XX=4@90° Tenon Adapter for 2-3/8" O.D. Tenon MA1048-XX=2@90° Tenon Adapter for 2-3/8" O.D. Tenon MA1049-XX=3@90° Tenon Adapter for 2-3/8" O.D. Tenon MA1191-XX=2@120° Tenon Adapter for 2-3/8" O.D. Tenon OA/RA1013=Photocontrol Shorting Cap OA/RA1014=NEMA Photocontrol - 120V OA/RA1016=NEMA Photocontrol - Multi-Tap 105-285V OA/RA1027=NEMA Photocontrol - 480V OA/RA1201=NEMA Photocontrol - 347V ISHH-01=Integrated Sensor Programming Remote		

- NOTES:**
- Customer is responsible for engineering analysis to confirm pole and fixture compatibility for all applications. Refer to installation instructions IB500002EN and pole white paper WP513001EN for additional support information.
 - DesignLights Consortium™ Qualified and classified for both DLC Standard and DLC Premium, refer to www.designlights.org for details.
 - Standard 4000K CCT and 70 CRI.
 - Consult factory for driver surge protection values.
 - Only for use with 480V Wye systems. Per NEC, not for use with ungrounded systems, impedance grounded systems or corner grounded systems (commonly known as Three Phase Three Wire Delta, Three Phase High Leg Delta and Three Phase Corner Grounded Delta systems).
 - Different housing colors impact lumen output. IES files for the non-standard colors are available upon request.
 - Extended lead times apply. Use dedicated IES files for 3000K and 5000K when performing layouts. These files are published on the Prevail luminaire product page on the website.
 - LumaWatt wireless sensors are factory installed and require network components RF-EM-1, RF-GW-1 and RF-ROUT-1 in appropriate quantities. See website for LumaWatt application information.
 - LumaWatt wireless system is not available with photocontrol receptacle (Not needed).
 - Not available with MSP or DIMRF options.
 - Replace XX with paint color.

STOCK ORDERING INFORMATION

Stock Sample Number: PRVS-A25-UNV-T3

Product Family	Light Engine	Voltage	Distribution	Options (Add as Suffix)
PRVS=Prevail	A15=(1 LED) 6,100 Nominal Lumens A25=(2 LEDs) 10,200 Nominal Lumens A40=(2 LEDs) 15,100 Nominal Lumens	UNV=Universal (120-277V) 347=347V	T3=Type III T4=Type IV	MSP/DIM-L30=Integrated Sensor for Dimming Operation, Maximum 30' Mounting Height

NOTE: Bronze only, 4000K CCT, 120-277V, 347V, standard mounting arm, standard non-fused 10kV MOV and 0-10V dimming.

STEAMPUNK – model: WS-W245

dwelLED™ Outdoor

WAC LIGHTING

Responsible Lighting®



Fixture Type:

Catalog Number:

Project: _____

Location: _____

PRODUCT DESCRIPTION

Designed with an updated version of a vintage industrial design, this Steampunk lantern pairs clear hammered seeded mouth blown glass with a stunning metal design. Perfect for indoor and outdoor applications. Two sizes and two finishes available.

FEATURES

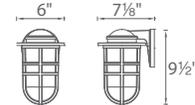
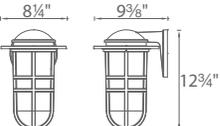
- ETL & cETL wet location listed, IP65 rated
- Clear hammered seeded mouth blown glass
- Die-cast Aluminum Construction
- Smooth and continuous ELV dimming
- 80,000 hour rated life
- 5 year warranty
- Color Temp: 3000K
- 90 CRI

SPECIFICATIONS

Construction: Aluminum construction with seeded glass

Light Source: LED

Standards: ETL & cETL wet location listed, IP 65

					Watt	LED Lumens	Delivered Lumens	Finish
		9"	WS-W24509	10W	624	365	BZ Bronze GH Graphite	
		13"	WS-W24513	10W	657	440		

WS - -

Example: **WS-W24513-BZ**

waclighting.com
Phone (800) 526.2588
Fax (800) 526.2585

Headquarters/Eastern Distribution Center
44 Harbor Park Drive
Port Washington, NY 11050

Central Distribution Center
1600 Distribution Ct
Lithia Springs, GA 30122

Western Distribution Center
1750 Archibald Avenue
Ontario, CA 91760

DESCRIPTION

The patented Lumark Crosstour™ LED Wall Pack Series of luminaires provides an architectural style with super bright, energy efficient LEDs. The low-profile, rugged die-cast aluminum construction, universal back box, stainless steel hardware along with a sealed and gasketed optical compartment make the Crosstour impervious to contaminants. The Crosstour wall luminaire is ideal for wall/surface, inverted mount for façade/canopy illumination, post/bollard, site lighting, floodlight and low level pathway illumination including stairs. Typical applications include building entrances, multi-use facilities, apartment buildings, institutions, schools, stairways and loading docks test.

SPECIFICATION FEATURES

Construction

Slim, low-profile LED design with rugged one-piece, die-cast aluminum hinged removable door and back box. Matching housing styles incorporate both a small and large design. The small housing is available in 7W and 18W. The large housing is available in the 26W model. Patent pending secure lock hinge feature allows for safe and easy tool-less electrical connections with the supplied push-in connectors. Back box includes three (3) half-inch, NPT threaded conduit entry points. The universal back box supports both the small and large forms and mounts to standard 3-1/2" to 4" round and octagonal, 4" square, single gang and masonry junction boxes. Key hole gasket allows for adaptation to junction box or wall. External fin design extracts heat from the fixture surface. One-piece silicone gasket seals door and back box. Minimum 5" wide pole for site lighting application. Not recommended for car wash applications.

Optical

Silicone sealed optical LED chamber incorporates a custom engineered mirrored anodized reflector providing high-efficiency illumination. Optical assembly includes impact-resistant tempered glass and meets IESNA requirements for full cutoff compliance. Solid state LED Crosstour luminaires are thermally optimized with five (5) lumen packages in cool 5000K or neutral warm 3500K LED color temperature (CCT).

Electrical

LED driver is mounted to the die-cast housing for optimal heat sinking. LED thermal management system incorporates both conduction and natural convection to transfer heat rapidly away from the LED source. 7W models operate in -40°C to 40°C [-40°F to 104°F]. 18W and 26W models operate in -40°C to 40°C [-40°F to 104°F]. High ambient 50°C models available. Crosstour luminaires maintain greater than 90% of initial

Catalog #		Type
Project		
Comments		Date
Prepared by		

light output after 72,000 hours of operation. Three (3) half-inch NPT threaded conduit entry points allow for thru-branch wiring. Back box is an authorized electrical wiring compartment. Integral LED electronic driver incorporates surge protection. 120-277V 50/60Hz or 347V 60Hz models.

Finish

Crosstour is protected with a Super durable TGIC carbon bronze or summit white polyester powder coat paint. Super durable TGIC powder coat paint finishes withstand extreme climate conditions while providing optimal color and gloss retention of the installed life.

Warranty

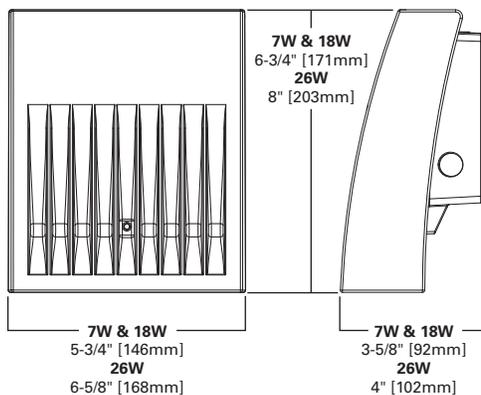
Five-year warranty.



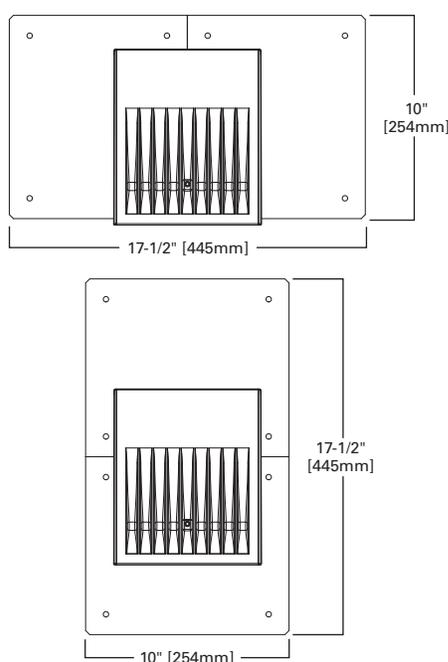
XTOR CROSSTOUR LED

APPLICATIONS:
WALL / SURFACE
POST / BOLLARD
LOW LEVEL
FLOODLIGHT
INVERTED
SITE LIGHTING

DIMENSIONS



ESCUTCHEON PLATES



CERTIFICATION DATA

UL/cUL Wet Location Listed
LM79 / LM80 Compliant
ROHS Compliant
ADA Compliant
NOM Compliant Models
IP66 Ingressed Protection Rated
Title 24 Compliant
DesignLights Consortium® Qualified*

TECHNICAL DATA

40°C Maximum Ambient Temperature
External Supply Wiring 90°C Minimum

EPA

Effective Projected Area (Sq. Ft.):
XTOR1A/XTOR2A=0.34
XTOR3A=0.45

SHIPPING DATA:

Approximate Net Weight:
3.7 – 5.25 lbs. [1.7 – 2.4 kgs.]

LUMEN MAINTENANCE

Ambient Temperature	TM-21 Lumen Maintenance (72,000 Hours)	Theoretical L70 (Hours)
XTOR1A Model		
25°C	> 92%	> 290,000
40°C	> 92%	> 290,000
50°C	> 91%	> 270,000
XTOR2A Model		
25°C	> 91%	> 270,000
40°C	> 90%	> 260,000
50°C	> 88%	> 225,000
XTOR3A Model		
25°C	> 91%	> 280,000
40°C	> 91%	> 270,000
50°C	> 89%	> 240,000

LUMENS - CRI/CCT TABLE

LED Information	XTOR1A	XTOR2A	XTOR2A-N	XTOR3A	XTOR3A-N
Delivered Lumens (Wall Mount)	722	1,633	1,523	2,804	2,284
Delivered Lumens (With Flood Accessory Kit) ¹	468	1,060	978	2,168	1,738
B.U.G. Rating ²	B0-U0-G0	B1-U0-G0	B1-U0-G0	B1-U0-G0	B1-U0-G0
CCT (Kelvin)	5,000	5,000	3,500	5,000	3,500
CRI (Color Rendering Index)	65	65	70	65	70
Power Consumption (Watts)	7W	18W	18W	26W	26W

NOTES: 1 Includes shield and visor. 2 B.U.G. Rating does not apply to floodlighting.

CURRENT DRAW

Voltage	Model Series		
	XTOR1A	XTOR2A	XTOR3A
120V	0.05A	0.15A	0.22A
208V	0.03A	0.08A	0.13A
240V	0.03A	0.07A	0.11A
277V	0.03A	0.06A	0.10A
347V	0.025A	0.058A	0.082A

ORDERING INFORMATION

Sample Number: XTOR2A-N-WT-PC1

Series ¹	LED Kelvin Color	Housing Color	Options (Add as Suffix)	Accessories (Order Separately)
XTOR1A=Small Door, 7W XTOR2A=Small Door, 18W XTOR3A=Small Door, 26W	[Blank]=Bright White (Standard) 5000K N=Neutral Warm White, 3500K ²	[Blank]=Carbon Bronze (Standard) WT=Summit White	PC1=Photocontrol 120V ³ PC2=Photocontrol 208-277V ^{3,4} 347V=347V ⁵ HA=50°C High Ambient ⁵	WG/XTOR=Wire Guard ⁶ XTORFLD-KNC=Knuckle Floodlight Kit ⁷ XTORFLD-TRN=Trunnion Floodlight Kit ⁷ XTORFLD-KNC-WT=Knuckle Floodlight Kit, Summit White ⁷ XTORFLD-TRN-WT=Trunnion Floodlight Kit, Summit White ⁷ EWP/XTOR=Escutcheon Wall Plate, Carbon Bronze EWP/XTOR-WT=Escutcheon Wall Plate, Summit White

NOTES: 1 DesignLights Consortium® Qualified. Refer to www.designlights.org Qualified Products List under Family Models for details. 2 XTOR1A not available in 3500K. 3 Photocontrols are factory installed. 4 Order PC2 for 347V models. 5 Thru-branch wiring not available with HA option or with 347V. 6 Wire guard for wall/surface mount. Not for use with floodlight kit accessory. 7 Floodlight kit accessory supplied with knuckle (KNC) or trunnion (TRN) base, small and large top visors and small and large impact shields.

STOCK ORDERING INFORMATION

7W Series	18W Series	26W Series
XTOR1A=7W, 5000K, Carbon Bronze	XTOR2A=18W, 5000K, Carbon Bronze	XTOR3A=26W, 5000K, Carbon Bronze
XTOR1A-WT=7W, 5000K, Summit White	XTOR2A-N=18W, 3500K, Carbon Bronze	XTOR3A-N=26W, 3500K, Carbon Bronze
XTOR1A-PC1=7W, 5000K, 120V PC, Carbon Bronze	XTOR2A-WT=18W, Summit White	XTOR3A-WT=26W, Summit White
	XTOR2A-PC1=18W, 120V PC, Carbon Bronze	XTOR3A-PC1=26W, 120V PC, Carbon Bronze

5-DAY QUICK SHIP ORDERING INFORMATION

7W Series	18W Series	26W Series
XTOR1A-WT-PC1=7W, 5000K, Summit White, 120V PC	XTOR2A-PC2=18W, 5000K, 208-277V PC, Carbon Bronze	XTOR3A-PC2=26W, 5000K, 208-277V PC, Carbon Bronze
	XTOR2A-WT-PC1=18W, 5000K, Summit White, 120V PC	XTOR3A-WT-PC1=26W, 5000K, Summit White, 120V PC
	XTOR2A-WT-PC2=18W, 5000K, Summit White, 208-277V PC	XTOR3A-WT-PC2=26W, 5000K, Summit White, 208-277V PC
	XTOR2A-N-WT=18W, 3500K, Summit White	XTOR3A-N-WT=26W, 3500K, Summit White
	XTOR2A-N-PC1=18W, 3500K, 120V PC, Carbon Bronze	XTOR3A-N-PC1=26W, 3500K, 120V PC, Carbon Bronze
	XTOR2A-N-PC2=18W, 3500K, 208-277V PC, Carbon Bronze	XTOR3A-N-PC2=26W, 3500K, 208-277V PC, Carbon Bronze
	XTOR2A-N-WHT-PC1=18W, 3500K, Summit White, 120V PC	XTOR3A-N-WHT-PC1=26W, 3500K, Summit White, 120V PC
	XTOR2A-N-WT-PC2=18W, 3500K, Summit White, 208-277V PC	XTOR3A-N-WT-PC2=26W, 3500K, Summit White, 208-277V PC





1.
VIEW FROM NORTHEAST



2
VIEW FROM NORTHWEST



3
VIEW FROM WEST



4
VIEW FROM CHURCH PARKING



5
VIEW EAST ON 7TH ST.



6
VIEW NORTHEAST ON 7TH ST.



7
VIEW NORTHWEST ON 7TH ST.



8
VIEW NORTH ON 7TH ST.



9

VIEW FROM BUD GRANT WAY & 7TH ST



10

VIEW FROM BUD GRANT WAY



East Town Apartments - Land Use Application

MINNEAPOLIS, MN | 06.06.2016 | 15-0027













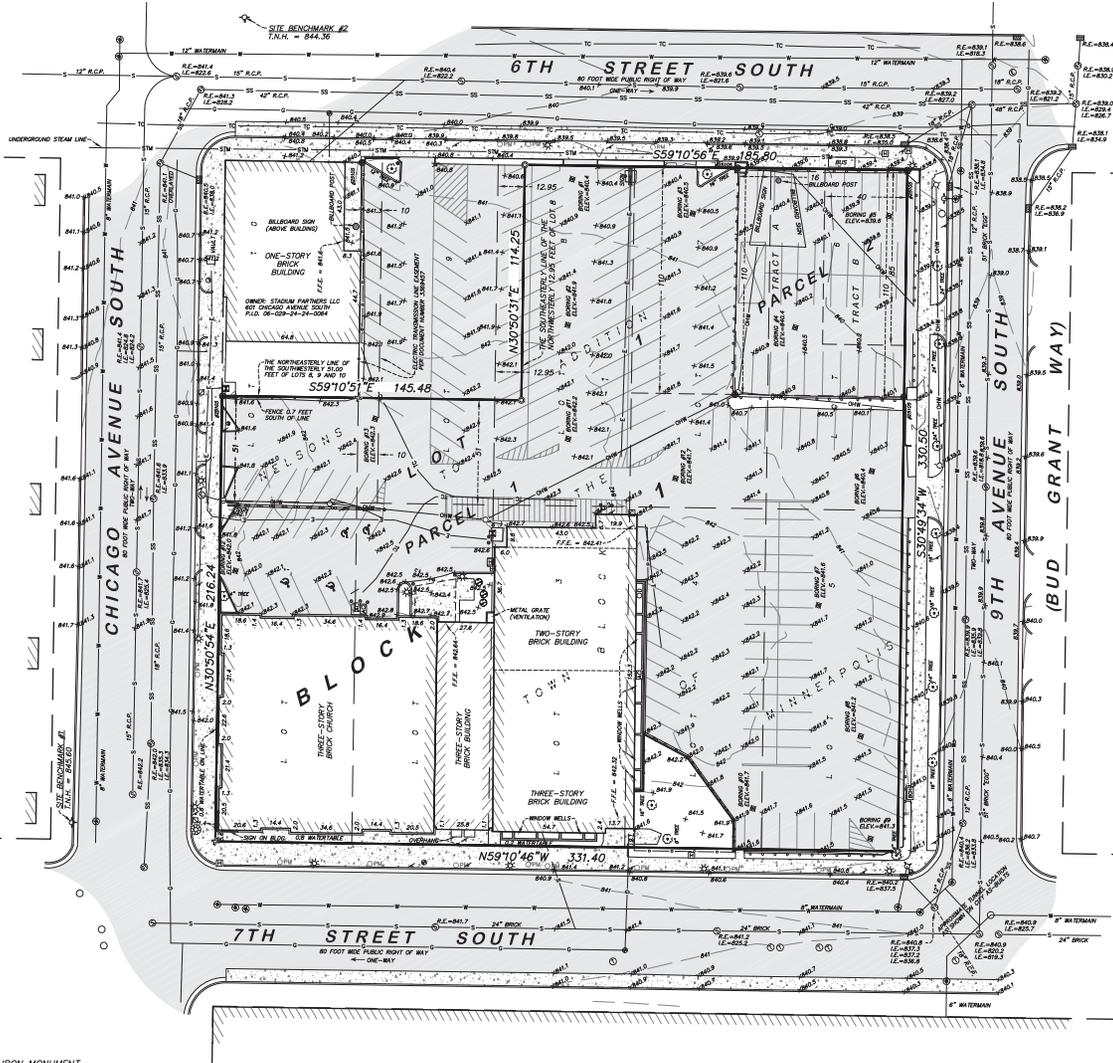


VICINITY MAP
NOT TO SCALE

OWNER/SUBDIVIDER
The First Covenant Church of Minneapolis, Minnesota
810 South 7th Street
Minneapolis, Minnesota 55415
Phone: 612-332-9093

ARCHITECT / DESIGNER
UrbanWorks Architecture, LLC
901 North 3rd Street - Suite 145
Minneapolis, Minnesota 55401
Phone: 651-455-3100

LAND SURVEYOR
Egan, Field & Nowak, Inc.
1229 Tyler Street Northeast - Suite 100
Minneapolis, Minnesota 55413
Phone: 612-466-3300



**PRELIMINARY PLAT OF:
FIRST COVENANT REDEVELOPMENT**

LEGAL DESCRIPTION:

Parcel 1:
Lots 1, 2, 3, 4 and 5, and that part of Lot 6 and 7 lying southeasterly of the northeasterly 110.00 feet thereof; that part of northeasterly 110.00 feet of Lot 7 lying northeasterly of the southeasterly 16.00 feet thereof; the southeasterly 51.00 feet of Lots 8, 9, and 10, and that part of Lot 8 lying southeasterly of the northeasterly 12.95 feet and lying northeasterly of the southeasterly 31.00 feet thereof all in Block 113, Nelsons Addition to Minneapolis, according to the recorded plat thereof, Hennepin County, Minnesota.

Parcel 2:
Tract A: The Northeasterly 110 feet of Lot 6, except the Southeasterly 40 feet of the Northeasterly 85 feet thereof; The Southeasterly 16 feet of the Northeasterly 110 feet of Lot 7, Block 113, Nelsons Addition to the Town of Minneapolis, Hennepin County, Minnesota.
Tract B: Southeasterly 40 feet of Lot 6, Block 113, Nelsons Addition to the Town of Minneapolis, Hennepin County, Minnesota.

PROPOSED LEGAL DESCRIPTION:
Lot 1, Block 1, FIRST COVENANT REDEVELOPMENT

PROPOSED LOT AREA:
Lot 1, Block 1, FIRST COVENANT REDEVELOPMENT is 92,884 square feet or 2.1323 acres.

- NOTES:**
- The orientation of this bearing system is based on the Hennepin County coordinate grid (NAD 83-96 Ad).
 - The total area of the property described herein is 92,884 square feet or 2.1323 acres.
 - Existing utilities, services and underground structures shown herein were located either physically, from existing records more reliable to us, by resident testimony, or by locations provided by Greater State One Call, per Ticket No. 169820717. However, lacking excavation, the exact location of underground features cannot be accurately ascertained and reliability is limited. Where additional or more detailed information is required, the client is advised that excavation may be necessary. Other utilities and services may be present and verification and location of all utilities and services should be obtained from the owners of the respective utilities prior to any design, planning or excavation.
 - The property described herein lies within Flood Zone X (Area determined to be outside of the 0.2% annual chance floodplain) per Federal Insurance Rate Map No. 27052 C 0357 E, dated September 2, 2004.
 - BENCHMARK:** Minnesota Department of Transportation Geodetic Monument "TEATER" GSD Station #72258 Elevation = 830.56 feet (N.A.V.D. 88)
- SITE BENCHMARK #1:** Top nut of hydrant in the north quadrant of Chicago Avenue South & 7th Street Elevation = 845.60 feet (N.A.V.D. 88)
- SITE BENCHMARK #2:** Top nut of hydrant in the east quadrant of Chicago Avenue South & 6th Street Elevation = 844.36 feet (N.A.V.D. 88)

CERTIFICATION:

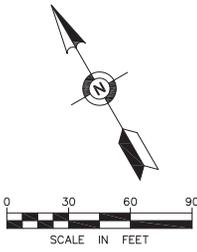
I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 5th day of July, 2016

[Signature]
Lee J Nord
Minnesota License No. 22033

LEGEND:

○ SANITARY MANHOLE	○ GAS VALVE	— CHAIN LINK FENCE
○ STORM MANHOLE	○ MANHOLE	— GUARDRAIL
○ CATCH BASIN	○ BENCH	— SANITARY SEWER
○ DRAIN (INLET)	— SIGN	— STORM SEWER
○ WATER MANHOLE	— BOLLARD	— WATERMAIN
○ HYDRANT	— HANDICAPPED PARKING SPACE	— UNDERGROUND ELECTRIC OVERHEAD WIRE
○ ELECTRIC MANHOLE	— HANDICAPPED PARKING SIGN	— UNDERGROUND GAS
○ UTILITY POLE	— POWER POLE WITH LIGHT	— TELECOMMUNICATION LINE
○ GUY WIRE	— LIGHT	— SPOT ELEVATION
○ ELECTRIC BOX	— HANDHOLE	— EXISTING CONTOUR LINE
○ TRAFFIC CONTROL BOX	○ ELECTRIC METER	— BITUMINOUS SURFACE
○ TELECOMMUNICATIONS MANHOLE	○ COMMUNICATION BOX	— CONCRETE SURFACE
		— GRAVEL SURFACE



- FOUND IRON MONUMENT
- SET IRON MONUMENT MARKED WITH LICENSE NUMBER 22033
- ✱ FOUND "X" IN CONCRETE

FIELD BOOK	PAGE	FIELDWORK CHIEF:	REVISIONS	
2876	24	PMD	NO.	DATE
			01	7/11/16
				REVISE STREET NAME (9TH AVE. S.)
DRAWING NAME:		CHECKED BY:		
36814 PRELIMINARY.dwg		LJN		
JOB NO. 36614				
FILE NO. 45				

PRELIMINARY PLAT OF:
FIRST COVENANT REDEVELOPMENT

SURVEY FOR:
Community Housing Development Corp.

PROPERTY ADDRESS:
**601 Chicago Avenue South
815 & 827 6th Street South
810 7th Street South
Minneapolis, Minnesota 55415**



1229 Tyler Street NE, Suite 100
Minneapolis, Minnesota 55413
PHONE: (612) 466-3300
WWW.EFNSURVEY.COM
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FIRST COVENANT REDEVELOPMENT

R.T. DOC. NO. _____

KNOW ALL PERSONS BY THESE PRESENTS: That The First Covenant Church of Minneapolis, Minnesota, a religious corporation, fee owner of the following described property situate in the County of Hennepin, State of Minnesota to wit:

Lots 1, 2, 3, 4 and 5; and that part of Lots 6 and 7 lying southeasterly of the northeasterly 110.00 feet thereof; that part of northeasterly 110.00 feet of Lot 7 lying northeasterly of the southeasterly 16.00 feet thereof; the Southeasterly 51.00 feet of Lots 8, 9, and 10; and that part of Lot 8 lying southeasterly of the northeasterly 12.95 feet and lying northeasterly of the southeasterly 51.00 feet thereof all in Block 113, Nelsons Addition to Minneapolis, according to the recorded plat thereof, Hennepin County, Minnesota.

AND

The Northeasterly 110 feet of Lot 6, except the Southeasterly 40 feet of the Northeasterly 85 feet thereof; The Southeasterly 16 feet of the Northeasterly 110 feet of Lot 7, Block 113, Nelsons Addition to the Town of Minneapolis, Hennepin County, Minnesota.

AND

Southeasterly 40 feet of Northeasterly 85 feet of Lot 6, Block 113, Nelsons Addition to the Town of Minneapolis, Hennepin County, Minnesota.

Has caused the same to be surveyed and plotted as FIRST COVENANT REDEVELOPMENT.

In witness whereof said The First Covenant Church of Minneapolis, Minnesota, a religious corporation, has caused these presents to be signed by its proper officer this _____ day of _____, 201____.

The First Covenant Church of Minneapolis, Minnesota

by _____, its _____

STATE OF _____

COUNTY OF _____

This instrument was acknowledged before me on this _____ day of _____, 201____ by _____ of The First Covenant Church of Minneapolis, Minnesota, a religious corporation, on behalf of the corporation.

Notary Public: _____ County, _____
My commission expires _____

I, Lee J. Nord do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on the plat; that all monuments depicted on the plat have been or will be correctly set within one year as indicated on the plat; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on the plat; that all public ways are shown and labeled on the plat.

Dated this _____ day of _____, 201____.

Lee J. Nord, Licensed Land Surveyor
Minnesota License No. 22033

STATE OF MINNESOTA

COUNTY OF HENNEPIN

This instrument was acknowledged before me this _____ day of _____, 201____, by Lee J. Nord.

Notary Public, _____ County, _____
My Commission Expires January 31, 202____

MINNEAPOLIS, MINNESOTA

I, the Secretary of the Planning Commission of the City of Minneapolis, Minnesota, do hereby certify that on this _____ day of _____, 20____ the City of Minneapolis acting by and through its City Planning Commission duly approved the plat of FIRST COVENANT REDEVELOPMENT, and duly authorized such action of the Commission by its Secretary. The prescribed ten-day period for appeal has elapsed without receipt of an appeal, as provided by Title 22, Section 596.320 of the Minneapolis Code of Ordinances.

Secretary of Planning Commission

MINNEAPOLIS, MINNESOTA

I hereby certify that this plat of FIRST COVENANT REDEVELOPMENT was approved by the City Planning Commission of the City of Minneapolis at a meeting thereof held this _____ day of _____, 20____. If applicable, the written comments and recommendations of the Commissioner of Transportation and County Highway Engineer have been received by the City or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minnesota Statutes, Section 505.03, Subdivision 2.

CITY CLERK OF MINNEAPOLIS, MINNESOTA

by _____ City Clerk

RESIDENT AND REAL ESTATE SERVICES
Hennepin County, Minnesota

I hereby certify that taxes payable in 201____ and prior years have been paid for the land described on this plat.

Dated this _____ day of _____, 201____.

Mark V. Chapin, Hennepin County Auditor By _____, Deputy

SURVEY DIVISION
Hennepin County, Minnesota

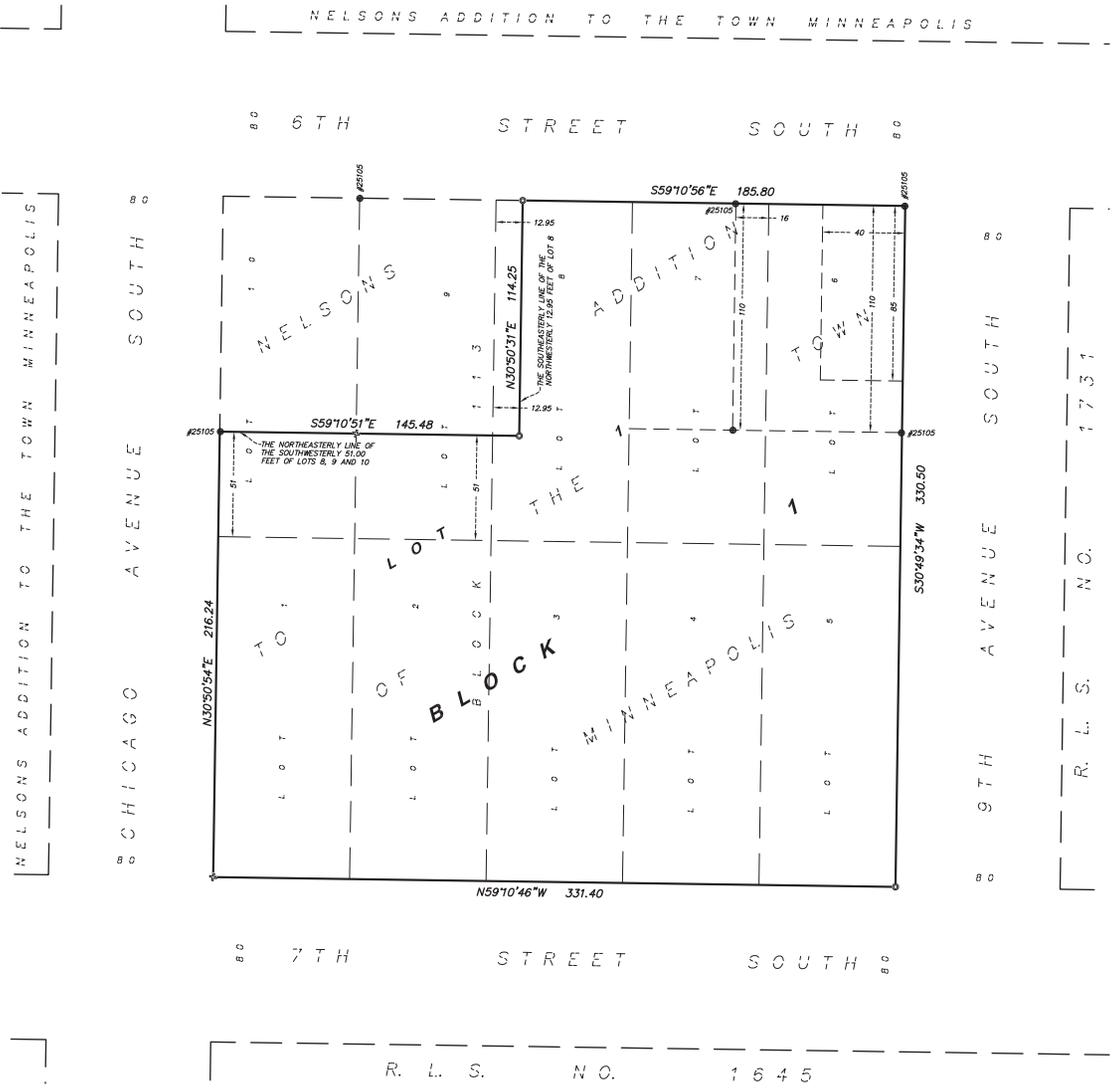
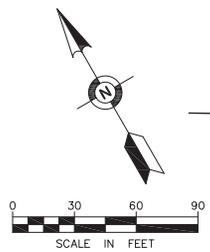
Pursuant to MINN. STAT. Sec. 383B.565 (1989), this plat has been approved this _____ day of _____, 201____.

Chris F. Mavis, Hennepin County Surveyor By _____

REGISTRAR OF TITLES
Hennepin County, Minnesota

I hereby certify that the within plat of FIRST COVENANT REDEVELOPMENT was filed in this office this _____ day of _____, 201____ at _____ o'clock _____ M.

Martin McCormick, Registrar of Titles By _____, Deputy



- ⊕ DENOTES FOUND CROSS IN CONCRETE
- DENOTES FOUND 1/2 INCH IRON PIPE
- DENOTES 5/8 INCH BY 14 INCH REBAR SET AND MARKED BY LICENSE NO. 22033

The orientation of this bearing system is based on the northeasterly line of Block 113, Nelsons Addition to the Town of Minneapolis, which is assumed to have a bearing of South 59 degrees 10 minutes 56 seconds East

